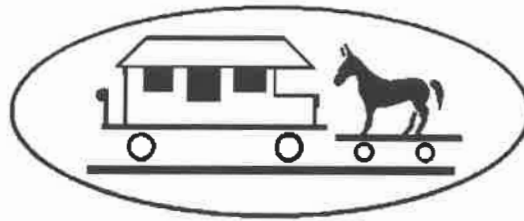


Historic Ontario



The "Model Colony"

**CITY OF ONTARIO HISTORIC PRESERVATION
COMMISSION
HISTORIC PRESERVATION SUBCOMMITTEE**

AGENDA

January 13, 2022

All documents for public review are on file in the Planning Department located in City Hall at 303 East "B" Street, Ontario, CA 91764.

MEETINGS WILL BE HELD AT 5:30 PM VIA ZOOM

PUBLIC COMMENTS

Citizens wishing to address the Historic Preservation Subcommittee on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Historic Preservation Subcommittee values your comments, the members cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

AGENDA ITEMS

For each of the items listed below the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. The Historic Preservation Subcommittee may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

CONSENT CALENDAR ITEMS

A. MINUTES APPROVAL

Historic Preservation Subcommittee Minutes of December 9, 2021, approved as written.

Motion to Approve/Deny

PUBLIC HEARING ITEMS

- B. REQUEST TO REASSESS POTENTIAL HISTORIC RESOURCES TO THE ONTARIO REGISTER FOR FILE NO. PHP17-031:** A request to reassess the following potential historic resources on the Ontario Register: 1) The Lockheed Aircraft Services Historic District, located on the northwest portion of the Ontario International Airport property, south of East Airport Drive and west of Vineyard Avenue (APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03); 2) The Terminal One Historic District, located at 1820-1800 East Moore Way (APN: 113-261-18); 3) The General Electric Aircraft Engines Historic District, generally located on East Avion Street west of Vineyard Avenue (APN: 113-261-06); 4) The Aerojet-General Hangar, located at 301 S. Hellman Avenue west of the Cucamonga Creek Channel (APN: 113-261-03); 5) The Air National Guard Hangar, located at 2475 East Avion Street (APN: 113-271-02); The request is not a “Project” pursuant to Section 21065 of the CEQA Guidelines. **City initiated.**
- C. REQUEST FOR A REASSESSMENT OF THE TIER DETERMINATION FOR THE PROPOSED LOCKHEED AIRCRAFT SERVICES HISTORIC DISTRICT FOR FILE NO. PHP17-026:** A request for a reassessment of the Tier Determination for the Proposed Lockheed Aircraft Services Historic District located on the northwest portion of the Ontario International Airport property, south of East Airport Drive and west of Vineyard Avenue. The request is not a “Project” pursuant to Section 21065 of the CEQA Guidelines. (APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03); **City initiated.**
- D. REQUEST FOR A REASSESSMENT OF THE TIER DETERMINATION FOR THE PROPOSED TERMINAL ONE HISTORIC DISTRICT FOR FILE NO. PHP17-027:** A request for a reassessment of the Tier Determination for the Proposed Terminal One Historic District located at 1820-1800 East Moore Way within the Ontario International Airport property boundary. The request is not a “Project” pursuant to Section 21065 of the CEQA Guidelines. (APN: 113-261-18); **City initiated.**
- E. REQUEST FOR A REASSESSMENT OF THE TIER DETERMINATION FOR THE AIR NATIONAL GUARD HANGAR FOR FILE NO. PHP17-028:** A request for a reassessment of the Tier Determination for the Air National Guard Hangar located at 2475 East Avion Street within the Ontario International Airport boundary. The request is not a “Project” pursuant to Section 21065 of the CEQA Guidelines. (APN: 113-271-02); **City initiated.**
- F. REQUEST FOR A REASSESSMENT OF THE TIER DETERMINATION FOR THE PROPOSED GENERAL ELECTRIC AIRCRAFT ENGINES HISTORIC DISTRICT FOR FILE NO. PHP17-029:** A request for a reassessment of the Tier Determination for the Proposed General Electric Aircraft Engines Historic District generally located on East Avion Street west of Vineyard Avenue within the Ontario International Airport property boundary. The request is not a “Project” pursuant to Section

21065 of the CEQA Guidelines. (APN: 113-261-06); **City initiated.**

- G. REQUEST FOR A REASSESSMENT OF THE TIER DETERMINATION FOR THE AEROJET-GENERAL HANGAR FOR FILE NO. PHP17-030:** A request for a reassessment of the Tier Determination for the Aerojet-General Hangar located at 301 S. Hellman Avenue west of the Cucamonga Creek Channel within the Ontario International Airport property boundary. The request is not a “Project” pursuant to Section 21065 of the CEQA Guidelines. (APN: 113-261-03); **City initiated.**

1. CEQA Determination

No action necessary – Not a project pursuant to CEQA Guidelines Section § 21065

- 2. File Nos. PHP17-031, PHP17-026, PHP17-027, PHP17-028, PHP17-029, and PHP17-030** (Reassessment of Historical Resources)

Motion to Approve/Deny

If you wish to appeal a decision of the **Historic Preservation Subcommittee**, you must do so within ten (10) days of the **Historic Preservation Subcommittee** action. Please contact the **Planning Department** for information regarding the appeal process.

If you challenge any action of the **Historic Preservation Subcommittee** in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the **Historic Preservation Subcommittee** at, or prior to, the public hearing.

The next **Historic Preservation Subcommittee** meets on February 10, 2022.

I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on or before **January 10, 2022**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East “B” Street, Ontario.


Administrative Assistant

**CITY OF ONTARIO HISTORIC PRESERVATION COMMISSION
(Presented to public via ZOOM)**

Historic Preservation Subcommittee Meeting

Minutes

December 9, 2021

HPSC MEETING: City Hall, 303 East B Street, Ontario, CA 91764 Called to order via ZOOM, by Jim Willoughby at 5:37pm

BOARD MEMBERS PRESENT

Jim Willoughby, Chairman
Nancy DeDiemar, Commissioner
Rick Gage, Planning Commissioner

BOARD MEMBERS ABSENT

None

STAFF MEMBERS PRESENT

Kimberly Ruddins, Sustainability Program Manager
Diane Ayala, Senior Planner
Elly Antuna, Associate Planner
Robert Morales, Assistant Planner

PUBLIC COMMENTS

No one responded from the public.

MINUTES

- A. **APPROVAL OF MINUTES:** Motion to approve the minutes of the November 10, 2021 meeting of the Historic Preservation Subcommittee as written was approved by Commissioner Gage and Commissioner DeDiemar, Commissioner Willoughby abstained (2-0).

PUBLIC HEARING ITEMS

- B. ENVIRONMENTAL ASSESSMENT AND CERTIFICATE OF APPROPRIATENESS REVIEW FOR FILE NO. PHP21-015:** A request for a Certificate of Appropriateness to demolish a Tier III historic resource (a 1,008 square-foot Craftsman single-family residence) to allow the construction of one industrial warehouse and office building totaling 174,603 square feet on 7.47 acres of land located at 1948 South Bon View Avenue, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan (File No. PGPA06-001), for which an Environmental Impact Report (SCH# 2008101140) was certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN: 1050-441-05, 1050-441-09, 1050-441-11 through -62, and 1050-441-73) submitted by Bon View Land 10, LLC & BV Investments 10, LLC as tenants-in-common.

Robert Morales, Assistant Planner, presented the staff report for File No. PHP21-015.

Project Applicant, Luke Rutherford spoke on the application.

Commissioner Gage asked if the cost to relocate the house would be paid by the developer or would a prospective owner have to pay the cost to relocate the house.

Diane Ayala, Senior Planner explained that the house is offered for free, but the cost to move it and any associated permitting fees is the responsibility of the prospective owner.

Commissioner Gage asked what the mitigation fees would be and how the City would use those funds.

Ms. Ayala explained the mitigation fee calculation and how those funds may be used.

Motion to approve **File No. PHP21-015** was approved unanimously by those present (3-0).

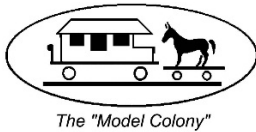
DISCUSSION ITEMS

1. Granada Theater – Diane Ayala, Senior Planner updated the Historic Preservation Subcommittee (HPSC) on the status of the Granada Theater.
2. Fallis House – Ms. Ayala updated the HPSC on the Fallis House condition after a fire occurred in November 2021.

Respectfully submitted,



Elly Antuna
Associate Planner



Historic Preservation Subcommittee

January 13, 2022

DECISION NO.:

FILE NO.: PHP22-001

RELATED FILE NOS.: PHP17-026, PHP17-027, PHP17-028, PHP17-029, and PHP17-030

DESCRIPTION: A request to rescind determinations of Tier and eligibility for inclusion on the Ontario Register of Historic Resources for the following “Eligible” historic resources and Proposed Historic Districts: 1) Lockheed Aircraft Services Historic District, located on the northwest portion of the Ontario International Airport property, south of East Airport Drive and west of Vineyard Avenue (APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03); 2) Terminal One Historic District, located at 1820-1800 East Moore Way (APN: 113-261-18); 3) General Electric Aircraft Engines Historic District, generally located on East Avion Street west of Vineyard Avenue (APN: 113-261-06); 4) Aerojet-General Hangar, located at 301 S. Hellman Avenue west of the Cucamonga Creek Channel (APN: 113-261-03); and 5) Air National Guard Hangar, located at 2475 East Avion Street (APN: 113-271-02); **submitted by City of Ontario.**

PART I: BACKGROUND & ANALYSIS

CITY OF ONTARIO, (herein after referred to as “Applicant”) has made a request to remove certain historic resources determined to be “Eligible” or within a “Proposed Historic district” for inclusion on the Ontario Register of Historic Places (File No. PHP17-031) and Tier Determinations (File Nos. PHP17-026, PHP17-027, PHP17-028, PHP17-029, and PHP17-030) as described in the subject of this Decision (herein after referred to as “Application” or “Project”).

(1) Project Setting: The historic resources for consideration are located within the bounds of the Ontario International Airport (ONT) properties within the ONT (Ontario International Airport) zoning district and is depicted in Exhibit A- Historic Resource Map, attached.

(2) Background: The Certified Local Government (CLG) program, which the City of Ontario is a participant, requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. In August 2016, the City of Ontario received a CLG grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties. As

a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for historic significance under the context theme of Aviation in Ontario.

On September 14, 2017, the Historic Preservation Subcommittee (HPSC) reviewed potential historic resources identified, documented, and evaluated in the Ontario International Airport Historic (ONT) Context Statement and Survey project. The HPSC made an eligibility determination for local listing and determined that 3 historic districts, containing 18 buildings/structures, and 2 buildings appeared to meet local landmark and historic district designation criteria consistent with historic context and survey findings. As a result, the “potential” historic resources were included in the Ontario Register of Historic Resources as “Proposed Historic Districts” or “Eligible.” At the same meeting, the HPSC determined that the proposed/eligible historic resources met the Tier I and II criteria.

On May 1, 2018, the City Council adopted Ordinance No. 3098 (File No. PDCA18-002) amending the Development Code to designate the Ontario International Airport Authority (OIAA) as the lead agency for environmental review and limit development plan review and permit issuance to ministerial (administrative) approval for projects located within the ONT (Ontario International Airport) zoning district.

(3) Discussion: The transfer of the Ontario International Airport’s (ONT) ownership from Los Angeles Worlds Airports (LAWA) to the OIAA, a joint powers authority who provides overall direction for the management, operation, development and marketing of the airport, occurred on November 1, 2016. As the OIAA is a responsible agency for all activity at ONT that is defined as a “project,” they must undergo environmental evaluation to support and comply with the National Environmental Protection Act (NEPA) and California Environmental Quality Act (CEQA). As such, OIAA is operated by a board of directors who adopt and certify environmental actions, which include determination of eligibility for listing on historic resource registers and identifying and mitigating for any potential impacts to the resources. If a lead agency determines that there are historic resources within the project area, those resources are classified “discretionary” historic resources for CEQA purposes. Other CEQA classifications of historic resources are “mandatory,” which are listed or formally determined eligible on the State or National Register by the State Historical Resources Commission, and “presumptive” historic resources, which are identified in an intensive level survey as eligible for listing in state or national register or is listed or determined eligible for listing in local register.

The intensive level survey, completed by a qualified preservation professional in 2017, found that the properties were eligible for local, state and national registers and therefore are considered under CEQA’s definition to be “presumptive” historic resources. The subsequent HPSC determination of eligibility for inclusion on the local inventory of historic resources and associated Tier determinations did not change their classification as “presumptive” historic resources.

In October 2021, the OIAA notified the City of Ontario of their position as the environmental lead agency for a future development project and preparation of an environmental impact report (NOP). The City was invited to comment on the content of the project and draft environmental document. City staff responded by providing the historic context statement, intensive level survey, and HPSC Decisions (eligibility and Tier determinations) which are related to the project. Through consultation it was determined that the HPSC Decisions, which are discretionary actions, may no longer be applicable.

(4) Evaluation: Section 4.02.045, Historic Preservation- Rescind or Amend the Status of a Historic Resources, of the Development Code establishes a process by which a historic resource may rescind or amend its assigned status, including a Local Landmark or Local District Designation, a Tier Determination, an Eligibility Determination, or an Architectural Conservation Area. The original Approving Authority can reevaluate eligibility for listing and tier determinations and amend or rescind actions at the request of the City or property owner when there is new information presented. The subsequent Development Code clarifies that the OIAA is the lead agency in approving land use plans, projects, and associated environmental decisions to further their objective of benefiting the Southern California economy and the residents of the airport's four-county catchment area. The City is requesting that the HPSC rescind eligibility and tier determinations Decisions.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee ("HPSC") the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, the Community Design element of The Ontario Plan ("TOP") sets forth Goals and Policies to conserve and preserve Ontario's historic buildings and sites; and

WHEREAS, on January 13, 2022, the HPSC of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The transfer of the Ontario International Airport's (ONT) ownership from Los Angeles World Airports (LAWA) to the OIAA, a joint powers authority who provide overall direction for the management, operation, development and marketing of the airport, occurred on November 1, 2016;

(2) The OIAA is the responsible agency for all activity at ONT that is defined as a "project," they must undergo environmental evaluation to support and comply with the National Environmental Protection Act (NEPA) and California Environmental Quality Act (CEQA);

(3) The bounds of the Ontario International Airport (ONT) properties are located within the ONT (Ontario International Airport) zoning district;

(4) The Development Code limits development plan review and permit issuance to ministerial (administrative) approval for projects located within the ONT (Ontario International Airport) zoning district; and

(5) Previous eligibility and tier determinations made by the Historic Preservation Subcommittee on September 14, 2017, which are discretionary decisions, are no longer applicable.

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby rescinds its eligibility and tier determinations the properties considered and removes them from the Ontario Register of Historic Resources.

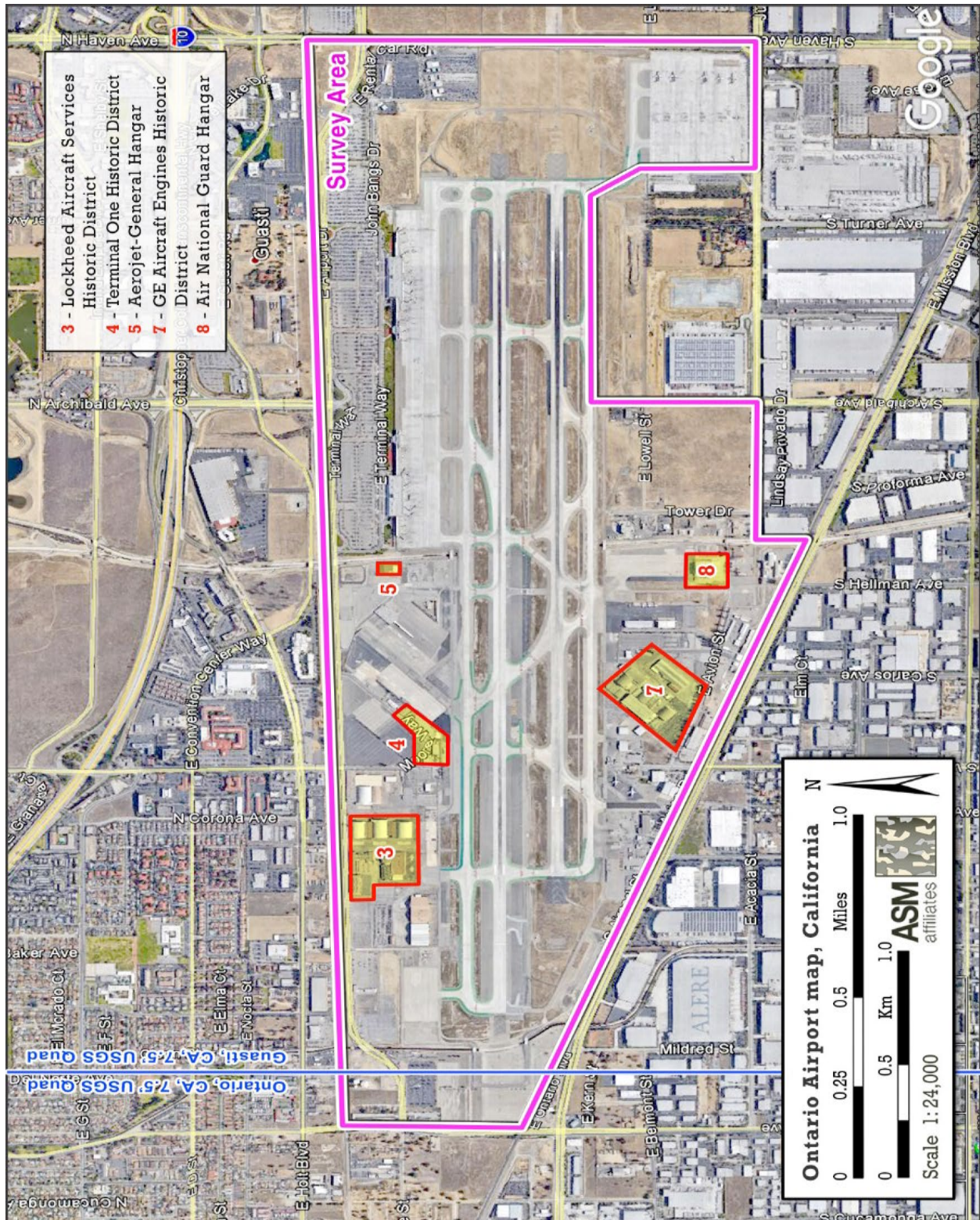
SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

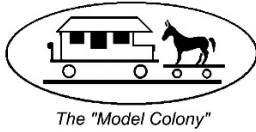
APPROVED AND ADOPTED this 13th day of January 2022.

Historic Preservation Subcommittee

Exhibit A: Historic Resource Map



***Attachment A:
HPSC Decisions (File Nos. PHP17-026, PHP17-027,
PHP17-028, PHP17-029, PHP17-030 and PHP17-031)***



Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-017

FILE NO: PHP17-026

DESCRIPTION: A City initiated request for a Tier Determination for the Lockheed Aircraft Services (LAS) Historic District located on the northwest portion of the Ontario International Airport property, south of East Airport Drive and west of Vineyard Avenue. (APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as "Applicant") has made a request for a Tier Determination of the Lockheed Aircraft Services (LAS) Historic District, File No.PHP17-026, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: The proposed historic district is comprised of 6 parcels containing 10 contributing buildings (historic resources) located on the northwest portion of the Ontario International Airport (ONT) property, south of East Airport Drive and west of Vineyard Avenue, on the former Lockheed Aircraft Services (LAS) complex which include:

- a. Hangars 2*, 4*, and 6*,
- b. Industrial Buildings No. 5 and 3,
- c. Executive Office Building No. 10*,
- d. Cafeteria Building No. 11*,
- e. Mail Room Building No. 12,
- f. Administration Office Building No. 15, and
- g. Warehouse Building No. 14.

**Historic resources that are contributors to the historic district and eligible for local landmark designation.*

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (GLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive

reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as "Eligible" for local, state, and national registers based on local historic significance. Seven of the individually "Eligible" buildings were also found to be contributors to their perspective historic district.

The Ontario Development Code includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition. The Development Code establishes criteria for Tier I, Tier II or Tier III historic resources, with Tier I and II being of the highest value. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. Major modification or demolition should not occur for Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

(3) Evaluation: A set of criteria, which is based on architecture and history, is used to determine the Tier recommendation. Tier I historic resources must meet at least one of the criterion within the Architecture/Form category and 3 criteria within the History category. Tier II historic resources may be determine eligible for listing in the National Register or the California Register of Historic Places or be listed in the Ontario Register and meet at least 2 criteria within the Architecture/Form or History categories. Tier III historic resources are those that are Designated Local Historic Landmarks, are contributing properties within Designated Local Historic Districts, or are eligible historic resources, including proposed historic districts.

A Tier Determination record was completed for the Lockheed Aircraft Services (LAS) Historic District and is attached to this report in *Exhibit A: Tier Determination*.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee (“HPSC”) the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The Lockheed Aircraft Services (LAS) Historic District meets the Tier I criteria as identified in the attached Tier Determination record; and

(2) Historic resources which contribute to the significance of the Lockheed Aircraft Services (LAS) Historic District will have the same Tier determination as the historic district.

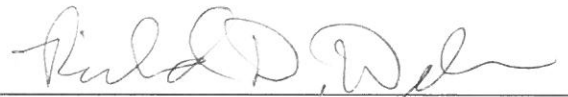
SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or

proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

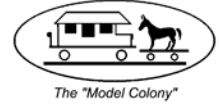
SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.



Historic Preservation Subcommittee
Chairman

Exhibit A: Tier Determination Form



HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION

TIER DETERMINATION

Date: September 14, 2017

Location: Northwestern portion of the ONT property, south of East Airport Dr. and west of Vineyard Ave.

Historic Name: Lockheed Aircraft Services (Proposed Historic District)

APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03

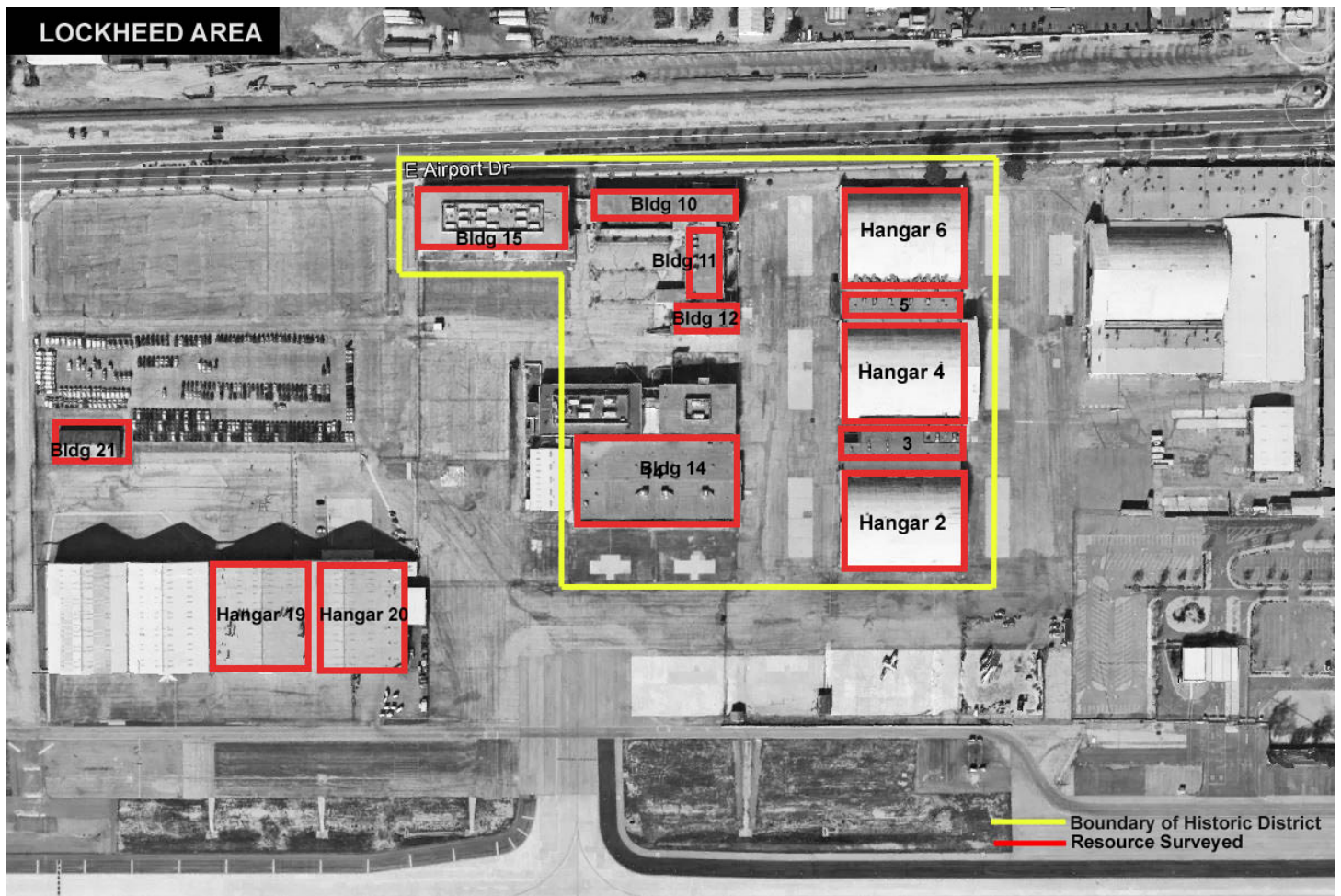
Decision Date: 9/14/2017

File No.: PHP17-026

Decision Making Body: HPSC

Tier Determination: I

Current Historic Status: Proposed Historic District



Map showing buildings within the area and boundary of Lockheed Aircraft Services Historic District.
 Source: ASM Affiliates, Inc., June 2017.

Description: The Lockheed Aircraft Services Historic District is comprised of 10 contributing buildings which include Hangars *2, *4, and *6, Industrial Buildings No. 5 and 3, *Executive Office Building No. 10, *Cafeteria Building No. 11, Mail Room Building No. 12, Administration Office Building No. 15, and Warehouse Building No. 14. **Historic resources that are also individually eligible for local, state and national registers.*

INDIVIDUAL PROPERTY

HISTORIC DISTRICT

TIER DETERMINATION

- Tier I** – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:
- A property listed on the Ontario Register and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 - A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.
- Tier II** – Properties where demolition should be avoided. These properties must meet any of the following:
- Any property listed or determined eligible for listing in the National Register of Historic Places; or
 - Any property listed or determined eligible for listing in the California Register of Historic Resources; or
 - A property listed on the Ontario Register and meets at least 2 of the criteria in either the architecture or history categories; or
 - A contributing structure in an Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.
- Tier III** – Properties where demolition should be avoided where possible, but may be appropriate under certain circumstances. These properties must be one of the following:
- Designated Historic Landmarks, or
 - Contributing structures in a Designated Historic District, or
 - Eligible Historical Resources as defined in Section 7.01.010.

TIER CRITERIA

Architecture (Check all that apply)

- The structure is (or the district contains resources which are) a prototype of, or one of the finest examples of a period, style, architectural movement, or construction in the City or a particular style of architecture or building type.
- The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer or major importance to the City, state or nation.

Explanation: The historic district contains resources that represent an architectural movement, period, and style design by notable architect George Vernon Russell and built by Pozzo Construction Company. Russell's design of the Executive Office and attached Cafeteria buildings were said to have embodied "new concepts of structural design, sun protection devices, and use of colors not common in the industrial field." Additionally, the hangars are a fine representation of construction technology and display character defining features of typical hangars during this period in aviation history.

History (Check all that apply)

- It is the location of an historic event(s) that have had a significant contribution to the history of the City, state or nation.
- It is associated with a business, company, or individual that has made a significant, cultural, social, or scientific contribution to the City, state, or nation.
- It is identified with a person(s) who has exerted a major influence on the heritage or history of the City, state, or nation.
- It embodies the ideals or principles of the "Model Colony" or furthers the ideals or principals established by the Chaffey Brothers.
- It has a direct relationship to one of the principle historic contexts in the City's history.
- It is related with a business, company or individual significant in the agricultural history of the City.

Explanation: The district is a large complex within the former property of Lockheed Aircraft Services (LAS), a division of Lockheed Aircraft Corporation, which operated at Ontario International Airport from 1952 to 1998. LAS activities were primarily within a 70-acre parcel in the northwest area of the airport. During its 46 years of operation at Ontario, Lockheed built more than 25 structures, including hangars, office buildings, machine shops, and auxiliary buildings (Douglas and Livingstone 2006). Primary LAS activities at Ontario consisted of modifying and refurbishing commercial and military aircraft. The Ontario facilities served as headquarters for LAS's domestic and international operations. LAS also developed and produced a complete line of flight data recording devices, data playback stations, and training and simulation devices. Lockheed's manufacture of flight recorders began in 1958 with the introduction of the Model 109 (LADOA 1983).

After World War II, with its expertise in maintenance, modification, and overhaul of aircraft, LAS saw an opportunity to expand its support services. In the U.S., the division constructed facilities in California, New York, Louisiana, South Carolina, and Hawaii. In the 1960s, LAS in Ontario became the maintenance and modification center for the highly classified U.S. Air Force fleet of four-engine turbo-prop C-130 aircraft under the program known as "Big Safari" (Lockheed 2017). Big Safari was an Air Force program responsible for maintenance and modification of specialized mission aircraft. It was not a technology development project, it was a management program to support multiple projects simultaneously. Big Safari Detachment 4 was located at LAS in 1964 specifically to oversee modification of aircraft for special missions to Southeast Asia. LAS ONT also modified six C-123Bs, which were first-generation deep-penetration jamming aircraft fitted with special receivers and transmitters, Doppler navigation systems, and camouflage paint (Jenkins 2001:121). In 1998, LAS ended 46 years at ONT and permanently closed the facility (Sable 1998).

Lockheed's commercial aviation support services for primarily military aircraft played an important role in the growth and development of ONT and was a major employment center. Through development of the Ontario International Airport Historic Context Statement and intensive level survey of the property, the Lockheed Aircraft Services Historic District is eligible for listing on the local, state and national registers under Criterion A/1 and local designation criterion 1.



Left: Lockheed Executive Office, Building No. 10

Below: Lockheed Cafeteria, Building No. 11





Above: Lockheed Mail Room, Building No. 12



Above: Lockheed Warehouse, Building No. 14



Above: (Left to right) Lockheed Hangar 6, Building No. 5, Lockheed Hangar 4 Warehouse, Building No.3, and Lockheed Hangar 2

Below: Lockheed Administration Office Building No. 15





Image 9. View looking north at the southwest and southeast façades of the Commissary Building.



Image 10. Interior view of the Commissary Building.



Image 11. View looking east at the northwest and southwest façades of Building 21.



Image 12. View looking south at the northwest and northeast façades of the Wash Building.



Image 13. View looking north at the southwest and southeast façades of the Wash Building.



Image 14. Interior view of the Wash Building.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 4 *Resource Name or #: GE Hangar 4 and Ancillary Buildings
 P1. Other Identifier: GE Aircraft Engines District, Ontario International Airport
 *P2. Location: Not for Publication Unrestricted
 *a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
 *b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.
 c. Address 1923 East Avion Street City Ontario Zip 91761
 d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444001.69 mE/ 3768173.52 mN;
 e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)
 GE Hangar 4 is located northeast of Hangar 3 in the GE Aircraft Engine area at Ontario International Airport. It is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. The hangar is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. The hangar has been modified by an addition on the southeast façade that effectively blocks the hangar doors. The northwest façade has been replaced with a corrugated metal wall with a personnel door and three vehicle bay doors, two with corrugated metal roll-up doors and one with a flat door. In the interior, the hangar is open to the adjoining warehouse at the southeast façade. The hangar has a large open space with a concrete floor and exposed steel trusses. Partitions for offices and restroom facilities have been added along the sides. Lighting consists of rows of pendant industrial fixtures toward the northeast and southwest walls.

(continued on page 2)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure
 *P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
 P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)
View looking east at the northwest and southwest façades.
 *P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
Pre-1948
Historic aerials
 *P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761
 *P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 1, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive
 *P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
 *Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking northeast at the southwest façade.



Image 2. View looking south at the northwest and northeast façades of Hangar 4 and Building M.



Image 3. View looking southwest at the northeast façades of Building M and Hangar 4.



Image 4. Interior view of Hangar 4 looking southeast.



Image 5. Detail interior view looking east.



Image 6. Interior view looking southeast.



Image 7. View looking northeast at the southwest façade of Building J.



Image 8. View looking north at the southwest and southeast façades of Building J.

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: GE Hangar 7

P1. Other Identifier: Building 34, GE Aircraft Engine District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1923 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443849.23 mE/ 3768020.20 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

GE Hangar 7 is the southernmost of three barrel-roofed hangars in the GE Aircraft Engines area at Ontario International Airport. A 1992 report labels the hangar as a Machine Shop and Heat Treat facility.¹ It is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. At the northwest end is a set of telescoping metal hangar doors that run on steel tracks, allowing them to slide fully into a housing apparatus that extends beyond the mass of the building. A horizontal row of three-by-three windows is set into the hangar doors. It appears that an additional set of hangar doors at the southeast façade has been replaced with a corrugated metal wall and two large vehicle bays with roll-up corrugated doors and two flat metal personnel doors. The interior of the hangar is a single open space with a concrete floor and exposed steel trusses. Lighting consists of regularly spaced rows of pendant industrial fixtures.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

*P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking northwest at the southeast façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

ca 1955

Ontario International Airport Master Plan, 1963

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 1, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ Contingency and Post-Closure Plan, Hazardous Waste Management Unit, GE Aircraft Engine Maintenance Center, Ontario International Airport. Prepared for General Electric Aircraft Engines by Dames & Moore, February 14, 1992.



Image 1. View looking north at the southeast and northeast façades.



Image 2. View looking east at the northwest and southwest façades.



Image 3. Detail view looking at the northwest façade.



Image 4. Interior view looking southeast.



Image 5. View of the interior looking northwest.



Image 6. Detail view of the interior looking west.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: GE Storage Hangars

P1. Other Identifier: GE Aircraft Engine District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 2043 E. Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444029.46 mE/ 3767987.90 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The GE Storage Hangers are located at 2043 E. Avion St. at Ontario International Airport. They consist of two adjoining single-story front-gabled hangar-type buildings with long, rectangular plans set on poured-concrete foundations. The moderately pitched roofs are covered in sheet asphalt and have narrow eaves. Vents and other utilities are visible on the roof. The buildings are clad in corrugated metal. Fenestration includes corrugated metal sliding barn-style doors and personnel doors on the southwest and northeast facades. At the southeast façade is a series of personnel doors and windows, as well as a row of freestanding exterior lighting fixtures on steel posts. A scale mechanism is located near the northeast façade. Three smaller buildings constructed at a later date adjoin the hangars on the northwest, obscuring the northwest façade of the hangars. The buildings were used for parts storage. The interiors were not accessible at the time of survey.

(continued on page 3)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View northeast at the southwest façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

ca 1955

Ontario International Airport Authority records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking north at the southwest and southeast façades.



Image 2. View looking west at the northeast and southeast façades.



Image 3. View looking south at the northwest and northeast façades.



Image 4. Detail view looking west at the southeast façade.



Image 5. View looking west at the northeast façade.



Image 6. Detail view of scale mechanism to the northeast of the storage hangars.

Page 1 of 7

***Resource Name or #:** Terminal One Historic District

D1. Historic Name: Terminal One, Baggage Claim B, Control Tower, FAA office building

D2. Common Name: Terminal One, Baggage Claim B, Control Tower, FAA office building

***D3. Detailed Description:** (Describe overall coherence of the district, its setting, visual characteristics, and minor features. List all elements of district.)

The Terminal One Historic District consists of a group of related buildings north of the primary runway at Ontario International Airport (ONT). Terminal One replaced an earlier terminal and was constructed in 1956-1960. The terminal building contained a full complement of passenger services, including a lobby, ticket counters, and a restaurant. Designed for expansion, the Terminal One building was enlarged extensively in two phases in the 1960s, and again in the 1970s. In 1983 and 1993, the terminal received two more additions. The complex includes the 1953 control tower, built adjacent to the prior terminal. In 1965, a freestanding single-story Federal Aviation Authority (FAA) office building was added to the complex. Terminal One was vacated in 1998, when the current ONT terminals two and four were opened to the east.

(Continued on page 2)

***D4. Boundary Description:** (Describe limits of district and attach map showing boundary and district elements.)

The Terminal One Historic District is located at 1820-1822 East Moore Way and 525 South Vineyard Avenue, and south of East Airport Drive. The boundary includes Terminal One, the control tower, and FAA building.

***D5. Boundary Justification:**

The boundary of the Terminal One Historic District encompasses the core of the extant buildings that served passenger travel during the period of significance.

D6. Significance: Theme Civil Aviation

Area Early Passenger Travel

Period of Significance 1950-1967, 1955-1970

Applicable Criteria NRHP Criterion A, CRHR Criterion 1, and Local District Criteria 1-3;
NRHP Criterion C, CRHR Criterion 3, and Local District Criterion 1

(Discuss district's importance in terms of its historical context as defined by theme, period of significance, and geographic scope. Also address the integrity of the district as a whole.)

The Terminal One Historic District was evaluated under the context of Aviation in Ontario; theme Civil Aviation, 1946-1967; and sub-themes Early Passenger Travel, 1950-1967; and Aviation and Modernism, 1950-1970, according to the guidelines established in the *Ontario International Airport Historic Context Statement*, prepared by ASM Affiliates, Inc., for the City of Ontario. Eligible properties under this sub-theme include historic districts that retain the buildings and structures, and their spatial relationships, from the period of significance. Eligible districts include buildings that serve specialized functions, including a prominent terminal with a control tower that overlooks facilities and runways; vehicle access for picking up and dropping off passengers; associated baggage claim and handling facilities including physical association with passenger, ticketing, and aircraft loading; buildings and structures located adjacent to aircraft aprons and runways; paved surfaces surrounding buildings and structures; parking closely associated with terminals; and landscaping associated with terminals and administrative and office buildings.

As noted in the Historic Context Statement registration requirements, the Terminal One Historic District represents important patterns and trends in early passenger travel at ONT, contains a grouping of buildings and structures typical of a passenger aviation support facility, retains a majority of the buildings/structures present during the period of significance, and retains most of its character-defining features and essential aspects of integrity. ASM recommends the Terminal One Historic District eligible under national and state Criteria A/1 and local District Criteria 1 through 3 for its association with civil aviation and early passenger travel at ONT during the period of significance.

(Continued on page 2)

***D8. Evaluator:** Shannon Davis and Marilyn Novell **Date:** June 2017

Affiliation and Address: ASM Affiliates, Inc., 20 N. Raymond Avenue, Pasadena, CA 91103

D3. Detailed Description: (Continued from page 1)

Prior to construction of Terminal One, Bonanza Air Lines began services out of the prior terminal in 1955, and continued to operation in the new Terminal One. In the 1950s, nonstop flights by Western and Bonanza airlines did not travel farther than Las Vegas. In 1962, Western began nonstop flights to San Francisco, and Bonanza began nonstop F27 flights to Phoenix in 1967 (USACE 1998:3-4). By 1967, Bonanza and Western were joined by Los Angeles Airways (a helicopter airmail service to downtown Los Angeles and LAX).

On October 18, 1967, a contract was signed by the City of Los Angeles and the City of Ontario agreeing to jointly contribute to the further expansion and development of ONT. The City of Ontario would benefit economically from a larger airport but lacked the necessary funds to expand, which the City of Los Angeles was able to provide. Los Angeles also agreed to promote and manage the airport (Agreement 1967).

On November 1, 1967, ONT was officially added to the Los Angeles Department of Airports (LADOA) regional network of satellite airports, which included Van Nuys and Palmdale as well (Figure 27). At that time, development at ONT was already fully under way, with the 1960 terminal already being doubled to accommodate increased traffic, 350 acres acquired at the east end for runway expansion, and plans for additional extensions of runways. As the only airport in the eastern Los Angeles metro area capable of serving large commercial jetliners, and with existing facilities including a fully equipped passenger terminal and six airlines with daily scheduled service, ONT was ideally situated for inclusion in a regional airport system based at LAX. The Civil Aeronautics Board approved service that would allow all domestic airlines serving LAX to provide similar service out of ONT (LADOA 1967).

Since the new terminals were opened east of Terminal One, the complex has been a popular location for filming. Classic Mid-Century-Modern in style, Terminal One has stood in for a number of airports, both fictional and real, in movies and television shows. ONT represented Miami International Airport, Tehran Airport, Las Vegas Airport, and LAX in the 1960s, among other airports.

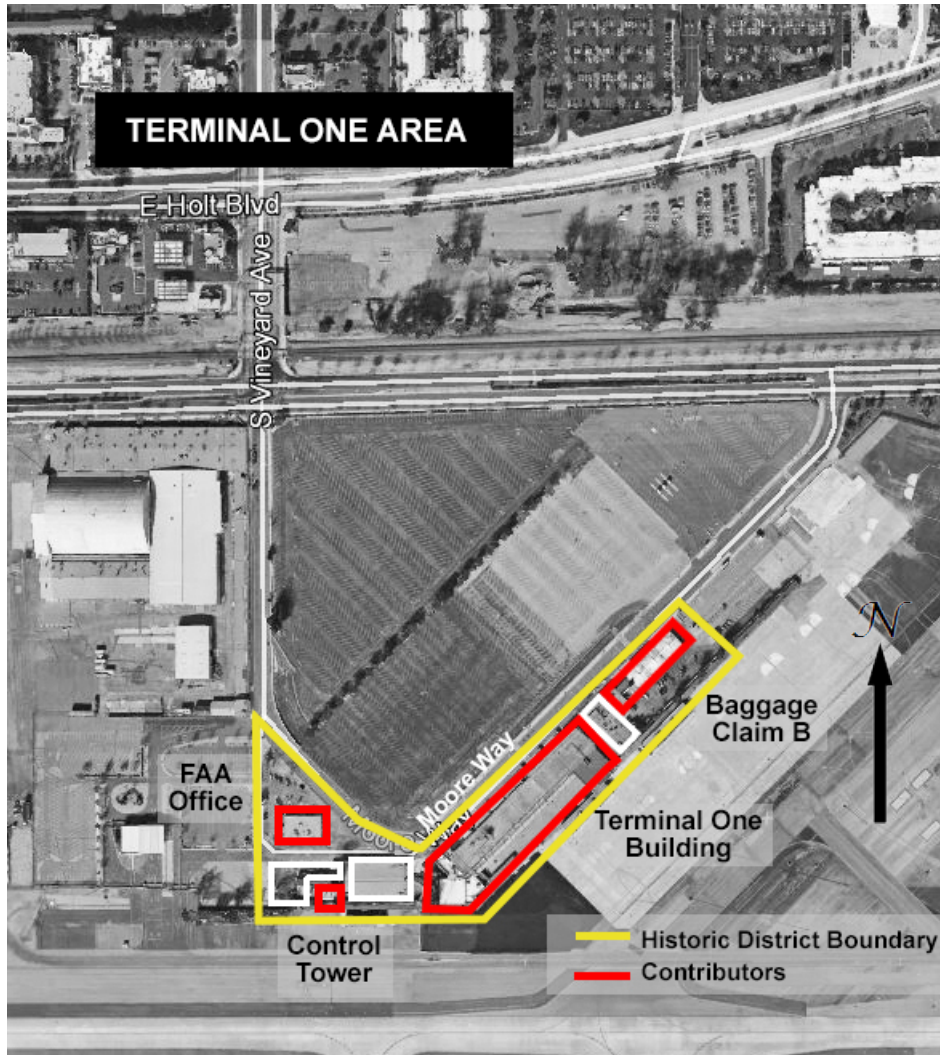
D6. Significance (Continued from page 1):

Recommended contributors to the Terminal One Historic District are the portions of the terminal building built during the period of significance, the baggage claim building to the northeast of the terminal, the control tower, and the FAA services building, as representing an important association with early passenger travel during the period of significance. The terminal building and the control tower were also found individually eligible under this sub-theme under national and state Criteria A/1 and Local Criteria 3 a and b.

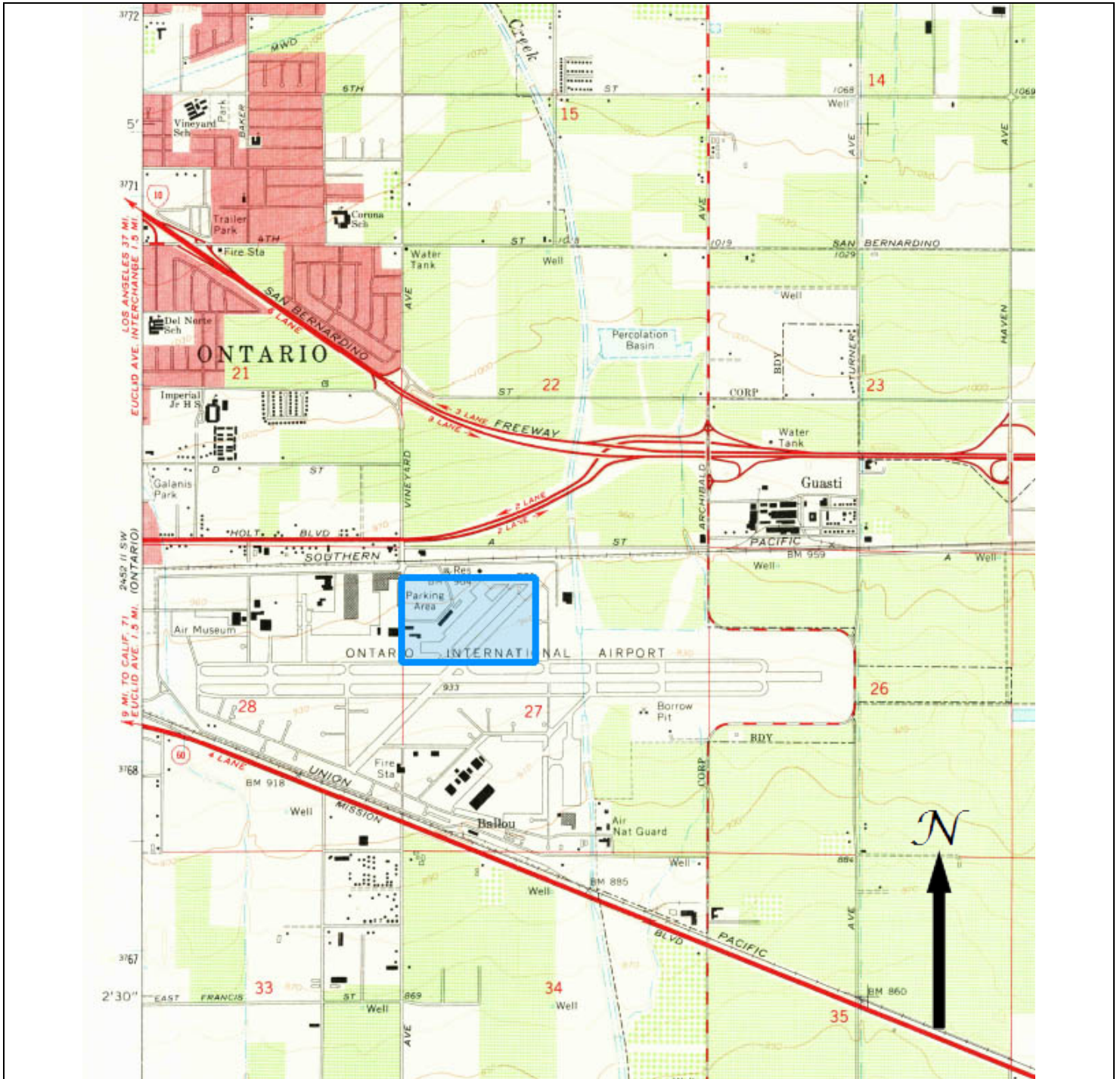
The terminal building and the control tower were also evaluated as individually eligible under the context of Aviation in Ontario; theme of Aviation and Architecture; and sub-theme of Modernism and Aviation, 1955-1970. The two buildings are good representations of Mid-Century-Modern architecture designed by a known local architect and represent the use of the style for prominent buildings visible to and used by the public. They were constructed during the period of significance and retain most of the character-defining features of the style to convey their historical association. Although Terminal One has been altered over time by additions to expand the space as passenger travel increased, the core of the original building retains integrity and remains clearly identifiable as the central portion of the terminal. The Control Tower retains all aspects of integrity. Therefore, the Terminal One building and the Control Tower are found individually eligible under national and state Criteria C/3 and Local Criteria 3 c-d, f-h.

D7. References (Give full citations including the names and addresses of any informants, where possible.):

- Los Angeles Department of Airports (LADOA). 1983. *Ontario International Airport Information: Service, Economics, Improvements and Growth Potential* (Quarterly Report). Ontario, CA: Los Angeles Department of Airports.
- National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.
- National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.
- Ontario International Airport Historic Context Statement*. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
- "Ontario Airport Adds New Aviation Facilities." *Los Angeles Times*, June 21, 1950.



Map of the Terminal One Historic District showing contributors, other resources surveyed, and boundary.
Source: ASM Affiliates, Inc., June 20, 2017.



Location map of the Terminal One area at ONT. (USGS Guasti, 1966).



Image 1. View of Terminal One and ancillary buildings looking east from the Control Tower cab.



Image 2. View of Terminal One primary facade looking east. The historic core of the Terminal One building is recommended as a contributor to the historic district.



Image 3. View of Control Tower and ancillary buildings looking east. Only the Control Tower is recommended as a contributor to the historic district.



Image 4. Historic view of Terminal One in original configuration. Source: HCM brochure, Ontario City Library Robert E. Ellingwood Model Colony Room. Undated.

Page 6 of 7 ***Resource Name or # (Assigned by recorder)** Terminal One Historic District
Recorded by: Shannon Davis and Marilyn Novell **Date:** June 2017



Image 5. View of Control Tower looking west from runway. As a later addition to the terminal, this portion of the building does not contribute to the historical significance of the building.



Image 6. Detail view looking north at the southwest façade of Baggage Claim B. This building is recommended as a contributor to the historic district.



Image 6. View looking south at the north façade of the FAA building. This building is recommended as a contributor to the historic district.

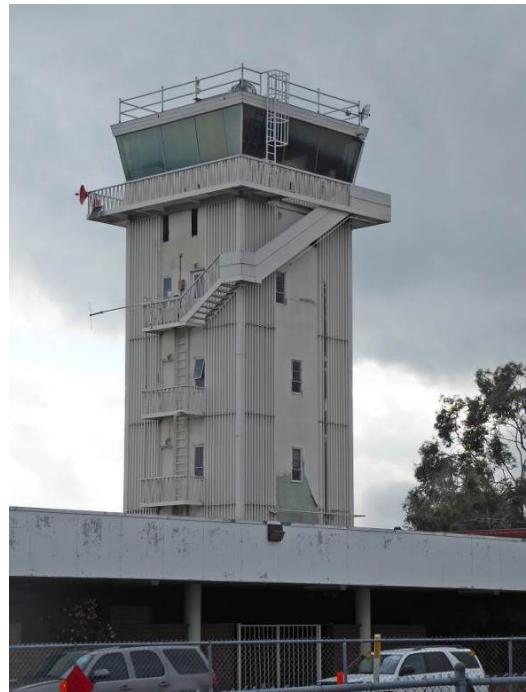


Image 7. View looking southwest at the east and north façades of the Control Tower. The Control Tower is recommended as a contributor to the historic district.

D3. Detailed Description: (Continued from page 1)

Prior to construction of Terminal One, Bonanza Air Lines began services out of the prior terminal in 1955, and continued to operation in the new Terminal One. In the 1950s, nonstop flights by Western and Bonanza airlines did not travel farther than Las Vegas. In 1962, Western began nonstop flights to San Francisco, and Bonanza began nonstop F27 flights to Phoenix in 1967 (USACE 1998:3-4). By 1967, Bonanza and Western were joined by Los Angeles Airways (a helicopter airmail service to downtown Los Angeles and LAX).

On October 18, 1967, a contract was signed by the City of Los Angeles and the City of Ontario agreeing to jointly contribute to the further expansion and development of ONT. The City of Ontario would benefit economically from a larger airport but lacked the necessary funds to expand, which the City of Los Angeles was able to provide. Los Angeles also agreed to promote and manage the airport (Agreement 1967).

On November 1, 1967, ONT was officially added to the Los Angeles Department of Airports (LADOA) regional network of satellite airports, which included Van Nuys and Palmdale as well (Figure 27). At that time, development at ONT was already fully under way, with the 1960 terminal already being doubled to accommodate increased traffic, 350 acres acquired at the east end for runway expansion, and plans for additional extensions of runways. As the only airport in the eastern Los Angeles metro area capable of serving large commercial jetliners, and with existing facilities including a fully equipped passenger terminal and six airlines with daily scheduled service, ONT was ideally situated for inclusion in a regional airport system based at LAX. The Civil Aeronautics Board approved service that would allow all domestic airlines serving LAX to provide similar service out of ONT (LADOA 1967).

Since the new terminals were opened east of Terminal One, the complex has been a popular location for filming. Classic Mid-Century-Modern in style, Terminal One has stood in for a number of airports, both fictional and real, in movies and television shows. ONT represented Miami International Airport, Tehran Airport, Las Vegas Airport, and LAX in the 1960s, among other airports.

D6. Significance (Continued from page 1):

Recommended contributors to the Terminal One Historic District include a prominent terminal, a control tower, and an FAA services building, as representing an important association with early passenger travel during the period of significance. The terminal building and the control tower were also found individually eligible under this sub-theme under national and state Criteria A/1 and local Individual Criteria 3 a and b.

The terminal building and the control tower were also evaluated as individually eligible under the context of Aviation in Ontario; theme of Aviation and Architecture; and sub-theme of Modernism and Aviation, 1955-1970. The two buildings are good representations of Mid-Century-Modern architecture designed by a known local architect and represent the use of the style for prominent buildings visible to and used by the public. They were constructed during the period of significance and retain most of the character-defining features of the style to convey their historical association. Although Terminal One has been altered over time by additions to expand the space as passenger travel increased, the core of the original building retains integrity and remains clearly identifiable as the central portion of the terminal. The Control Tower retains all aspects of integrity. Therefore, the Terminal One building and the Control Tower are found individually eligible under national and state Criteria C/3 and local Individual Criteria 3 c-h.

D7. References (Give full citations including the names and addresses of any informants, where possible.):

- Los Angeles Department of Airports (LADOA). 1983. *Ontario International Airport Information: Service, Economics, Improvements and Growth Potential* (Quarterly Report). Ontario, CA: Los Angeles Department of Airports.
- National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.
- National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.
- Ontario International Airport Historic Context Statement*. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
- "Ontario Airport Adds New Aviation Facilities." *Los Angeles Times*, June 21, 1950.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 4 *Resource Name or #: FAA Flight Services Building
 P1. Other Identifier: Flight Services Building; Terminal One Historic District, Ontario International Airport
 *P2. Location: Not for Publication Unrestricted
 *a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
 *b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____
 c. Address 525 South Vineyard Avenue City Ontario Zip 91761
 d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443649.93 mE/ 3768906.91 mN; _____
 e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____
 *P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The FAA Building is a freestanding Mid-Century Modern office building located in the Terminal One area of Ontario International Airport (ONT) and designed by Harnish Morgan and Causey Architects, who were also responsible for Terminal One. The 4,050-square-foot building contained offices for general aviation and the systems maintenance sector, as well as the chief of the combined station and tower.¹ It is located at 525 South Vineyard Avenue across from the old control tower. It is a flat-roofed horizontally oriented building with a rectangular plan set on a poured-concrete foundation. The north and south façades are characterized by a deep horizontal overhanging fascia spanning the façade. The west and east façades are composed of prominent red-brick wing walls that extend

(continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP6, 1-3 story commercial building
 *P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
 P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)
View looking northeast at the west and south façades.

*P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
1965
Architectural drawings, Los Angeles World Airports (LAWA) archives

*P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ "Ontario Airport Study to Be Presented Today." *Los Angeles Times*. May 28, 1964.



Image 1. View looking south at the north façade.

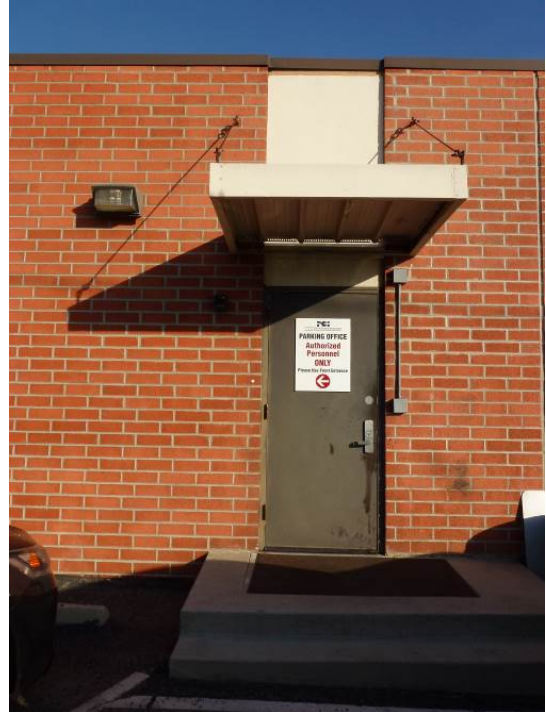


Image 2. Detail view of secondary entrance on west façade.



Image 3. View looking southwest at the east and north façades.



Image 4. Detail view looking at the north façade.

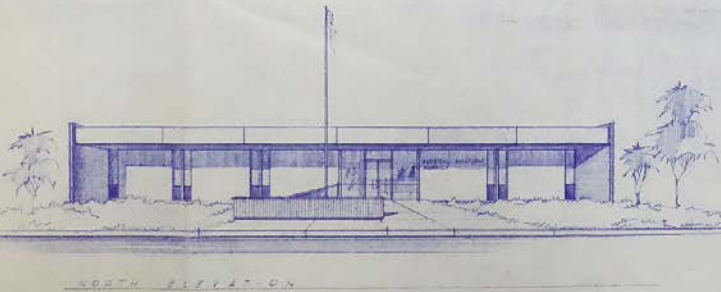
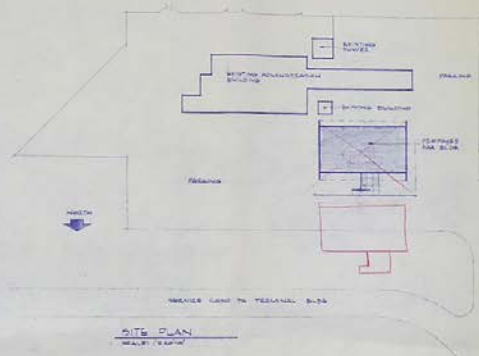
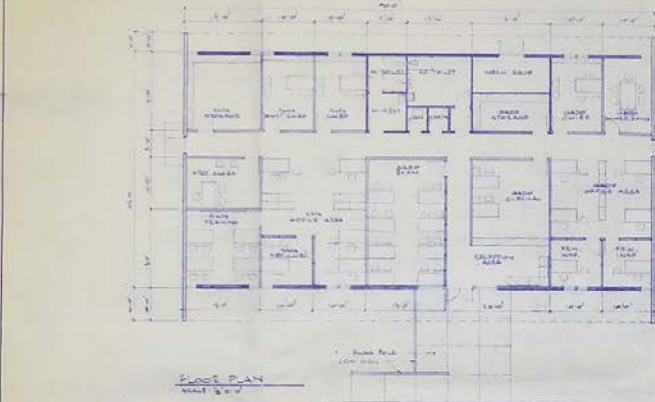
Page 3 of 4 *Resource Name or # (Assigned by recorder) FAA Flight Services Building
 Recorded by: Shannon Davis and Marilyn Novell Date: December 2016



Image 5. Interior view looking west of office.



Image 6. Interior view.



APPROVED	APPROVED	OWNER	ENGINEER	PROPOSED OFFICE BLDG. FOR F14 ONTARIO INTERNATIONAL AIRPORT ONTARIO, CALIF.	HARRIS-MORGAN AND CASSEY ARCHITECTS 222 E. 9th STREET, ONTARIO, CALIFORNIA 91764-4304 PHONE: (951) 835-1111 FAX: (951) 835-1112	DATE: 12/16/16	SCALE: 1/8" = 1'-0"
						BY: [Signature]	OF: 1 SHEET

Image 7. Architectural drawing showing floor plan and façade drawing.
 [architectural drawing dated 1965, Ontario International Airport Authority records]

Page 4 of 4 ***Resource Name or # (Assigned by recorder)** FAA Flight Services Building
Recorded by: Shannon Davis and Marilyn Novell **Date:** December 2016
 Continuation Update

***P3a. Description:** (continued from page 1)

slightly above the roof line. Pairs of vertically oriented fixed-pane windows with lower opaque panels are set into stucco-clad walls recessed below the, fascia on the north and south façades. A similar single window is located at the end of each stucco wall flush with the brick wing walls, creating a sense that the wing walls are supporting the entire building. At the north (primary) façade, a glass door in a metal frame is set between panels of floor-to-ceiling plate glass with a fixed-pane glass transom above. At the east and west facades are flat metal doors set below a vertical stucco panel. The door on the west façade is sheltered by a narrow, flat canopy suspended from the exterior wall by steel cables. The interior of the building retains the original drop ceiling of acoustic tile and flush fluorescent fixtures. Partition walls are composed of wallboard, and walls flush with the exterior are painted masonry. Floors are carpet and vinyl tile. The building currently houses the parking management offices for the airport.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: Baggage Claim B

P1. Other Identifier: Baggage Claim Building; Terminal One Historic District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 525 South Vineyard Avenue City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443649.93 mE/ 3768906.91 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) _____

Baggage Claim B is a Mid-Century Modern building in the Terminal One area of Ontario International Airport (ONT). Although it was likely constructed concurrent with Terminal One, which was designed by Harnish Morgan and Causey Architects, no original architectural plans or construction history of the baggage claim were found to confirm the architect or year built. The building was originally freestanding and located to the northeast of Terminal One, at the time of survey it was connected via a corridor to Terminal One and ancillary buildings to the southwest. The roof of the building displays a unique technology in which three cast-concrete segments are each supported at the center by a large, square concrete column. The four-part roofs slope down toward the walls of the building, creating a tent-like effect. The roof is supported atop a red-brick wall at the primary façade by a concrete horizontal beam connected to the wall by thin posts, leaving a gap between the wall and roof that creates a floating effect. On the exterior, the roofs

(Continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking east at the northwest and southwest façades

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both
ca 1959

Architectural drawings, Los Angeles World Airports (LAWA) archives

*P7. Owner and Address:

Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking north at the southwest façade.



Image 2. Detail view looking north at the southwest façade.



Image 3. Detail view looking north at the southwest façade.



Image 4. Interior view looking west.

Page 3 of 3

*Resource Name or # (Assigned by recorder) Baggage Claim B

Recorded by: Shannon Davis and Marilyn Novell

Date: December 2016



Image 5. Interior view looking west.



Image 6. Detail view of interior of northwest façade.

***P3a. Description:** (continued from page 1)

have four prominent standing seams. At the runway side of the building, a half-height concrete block screen sits below the slightly undulating edges of the roofline. At the primary façade, a flat canopy supported by square concrete posts shelters the entrances. The interior floor is composed of poured concrete. Four elongated oval-shaped conveyor systems clad in stainless steel pierce the back wall of the building for loading and circulate to the interior for passenger access to baggage.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3S

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 4

*Resource Name or #: Aerojet-General Hangar

P1. Other Identifier: United States Postal Service facilities, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 301 South Hellman Avenue City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444500.89 mE/ 3769053.34 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.)

Located within Ontario International Airport

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Aerojet-General Hangar is located at 301 S. Hellman Av. adjacent to the Cucamonga Creek channel on the north side of the Ontario International Airport. Constructed in 1958, the building has an irregular plan and is set on a poured-concrete foundation. The nearly 19,000-square-foot hangar has a moderately pitched front-gabled roof clad in corrugated metal with regularly spaced inset corrugated fiberglass skylights. The exterior walls are formed of corrugated metal panels. At the west façade is a set of telescoping corrugated metal doors that run on steel tracks set into the concrete pavement, allowing them to retract fully from the mass of the building. The doors are hung on a steel structure that projects beyond the sides of the building. At the center below the gable is an opening for the aircraft tail with a roll-up metal door. At the east façade is an array of utilities in a chain-link-fenced area and vents attached to the exterior walls. A single-story flat-roofed concrete masonry unit building, originally built to accommodate offices, a lobby, radio testing facility, engine storage, and repair shop, is attached to the north façade.¹

(continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking east at the west façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both
1958

Los Angeles Times; historicaerials 1959

*P7. Owner and Address:

Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):

¹ "\$95,000 facility rising at airport." *Los Angeles Times*, September 21, 1958.



Image 1. View looking northeast at the west and south façades.



Image 2. View looking southwest at the east and north façades.



Image 3. Detail view looking north at the rails on the west façade.



Image 4. Interior view looking southeast.



Image 5. Interior view looking southwest.



Image 6. Interior view of south wing.

***P3a. Description:** (continued from page 1)

A flat-roofed warehouse area with a bay door and a metal personnel door adjoins the hangar on the south façade. The interior of the hangar is a single open space with exposed steel frame. Hanging fluorescent tubing fixtures augment the lighting on the interior. The building was used for aircraft maintenance and modification.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 4 of 4

*NRHP Status Code 3S

*Resource Name or # (Assigned by recorder) Aerojet-General Hangar

B1. Historic Name: Aerojet-General Hangar

B2. Common Name: United States Postal Service Hangar

B3. Original Use: Hangar and offices for aircraft maintenance operations

B4. Present Use: Post office hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1958

The Aerojet-General Hangar and adjoining single-story concrete masonry block offices were constructed in 1958. A wing housing a storage area was added to the south sometime before 1980 (historicaerials.com 1966, 1980).

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron

B9a. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture Area: Developments in Construction Technology

Period of Significance: 1958 Property Type: Aircraft hangar and offices Applicable Criteria: C/3/3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Aerojet-General Hangar is an example of construction technology eligible under the context of Aviation in Ontario, Theme of Aviation and Architecture, 1942–1975, and the Sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features of the style, including a front-gabled roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction and embodies the distinctive characteristics of the type of hangar during the period of significance. Although the use of the hangar and offices has changed, it is the only historic hangar of the simple gable-roof type at ONT that retains all seven aspects of integrity. After careful consideration, ASM recommends the Aerojet-General Hangar eligible for listing at the federal, state, and local level under Criterion C/3/3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3S

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 9 *Resource Name or #: Air National Guard Hangar

P1. Other Identifier: Air National Guard Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 2475 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444499.47 mE/ 3767880.54 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Air National Guard Hangar, located at 2475 E. Avion St. at Ontario International Airport, is a complex comprising an aircraft hangar with a two-story office/workshop complex known as a "lean-to" adjoining the hangar on three sides. In addition, there are two wings on the south façade, housing a boiler room and a diesel tank facility. The hangar is a multi-story building of steel frame construction enclosing a single open space to accommodate aircraft. It is rectangular in form and sits on a poured-concrete foundation that opens on the north to the aircraft apron. The hangar is composed of corrugated metal and is capped by a low-pitched front-gabled roof. A continuous row of steel-frame six-over-three fixed windows spans three sides of the hangar. At the north façade is a double set of telescoping hangar-type doors that retract into enclosed housing at the sides. A continuous row of multi-light windows spans the width of the doors. At the interior of the hangar the steel truss construction is visible on the ceiling and walls. The floor is smooth poured concrete, and lighting is provided by regularly spaced industrial pendant fixtures, as well as the rows of windows on all sides.

(continued on page 7)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP34. Military property

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking south at the north façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1955

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking southeast at the north and west façades, with Fire Station in the foreground.



Image 2. View looking east at the west façade.



Image 3. Detail view looking southeast at the north and west façades.



Image 4. View looking southwest at the east and north façades.

Page 3 of 9 *Resource Name or # (Assigned by recorder) Air National Guard Hangar
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016



Image 5. View looking north at the south façade.



Image 6. View looking northwest at the south and east façades.



Image 7. View looking northeast at the west and south façades.



Image 8. View looking west at the east façade.



Image 5. Looking southwest at the north and east façades.



Image 6. View looking northwest at the south and east facades of the Diesel Tanks room.



Image 7. Detail view looking east at the west façade of the Diesel Tanks room.



Image 8. Detail view of the entrance on the east façade of the Diesel Tanks room.



Image 9. Detail view looking northwest at the south façade of the Hangar.



Image 10. Interior view of the hangar looking northeast.



Image 11. Interior view of the hangar looking northwest.



Image 13. Interior view of the hangar looking southeast.



Image 14. Detail view of the interior of the hangar ceiling.



Image 15. Detail interior view of the hangar doors looking east.



Image 16. Detail view of the hangar door track mechanism.

Page 7 of 9

*Resource Name or # (Assigned by recorder)

Air National Guard Hangar

Recorded by: Shannon Davis and Marilyn Novell

Date: December 2016

Continuation Update

***P3a. Description:** (continued from page 1)

The flat-roofed two-story wings, or "lean-tos," as described in the technical specifications for the hangar,¹ are clad in corrugated metal with a red brick water line. A continuous row of steel-frame windows resembling those on the hangar have both fixed and awning-style operable portions. All of the windows on the south façade and approximately half of the windows on the other façades have been painted over. The west wing functioned as offices for operations, and the east wing housed maintenance and shops. The interiors of the wings were not accessible at the time of the survey.

Two wings extend from the south façade of the hangar complex. A large flat-roofed corrugated steel industrial-style building set on a concrete foundation is located toward the east side of the south façade. A second, much smaller, flat-roofed wing extends from approximately the center of the south façade. The industrial building is clad in red brick and sits on a poured concrete foundation. Fenestration includes a set of double doors with a vent in a transom above and a single door, with three horizontal lights each, located on the east façade; a single door with similar lights is located on the west façade. Windows are two-by-five in a combination of fixed and operable portions.

¹ "Technical Specifications, Part IV, for Hangar Building—with two-story lean-to with exterior Utilities and Facilities. California Air National Guard, Ontario, California, June 6, 1952. [from Model Colony Room archives]

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 8 of 9

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Air National Guard Hangar

B1. Historic Name: Air National Guard Hangar

B2. Common Name: _____

B3. Original Use: Hangar and offices/workshops to support Air National Guard operations

B4. Present Use: Hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1955

The Air National Guard Hangar was constructed in 1955. Technical specifications were prepared by the California Air National Guard, Ontario, in 1952. The hangar and lean-to buildings are minimally altered

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture

Area: Developments in Construction Technology

Period of Significance: 1955-1975

Property Type: Aircraft hangar and offices

Applicable Criteria: NRHP/CRHR Criteria C/3; Local Individual Criteria 3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Air National Guard Hangar is an example of construction technology considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, 1942–1975, and the sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features one style typical of Air National Guard facilities during the period of significance, including a front-gabled roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction. The multi-leaved telescoping hangar doors with extensive fenestration and the mass of the building formed by the pop-up center section with clerestory windows and the lower “lean-to” sections all original features (Aaron 2011). Although the National Guard no longer occupies the site, suggesting some change in use, both the interior and exterior of the building reflect all seven aspects of integrity. After careful consideration, ASM recommends the Air National Guard Hangar individually eligible for listing at the federal, state or local level under Criteria C/c or Local Individual Criteria 3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

Aaron, Jayne. 2011. *Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War*. Prepared for the Department of Defense Legacy Resource Management Program.

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks: _____

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

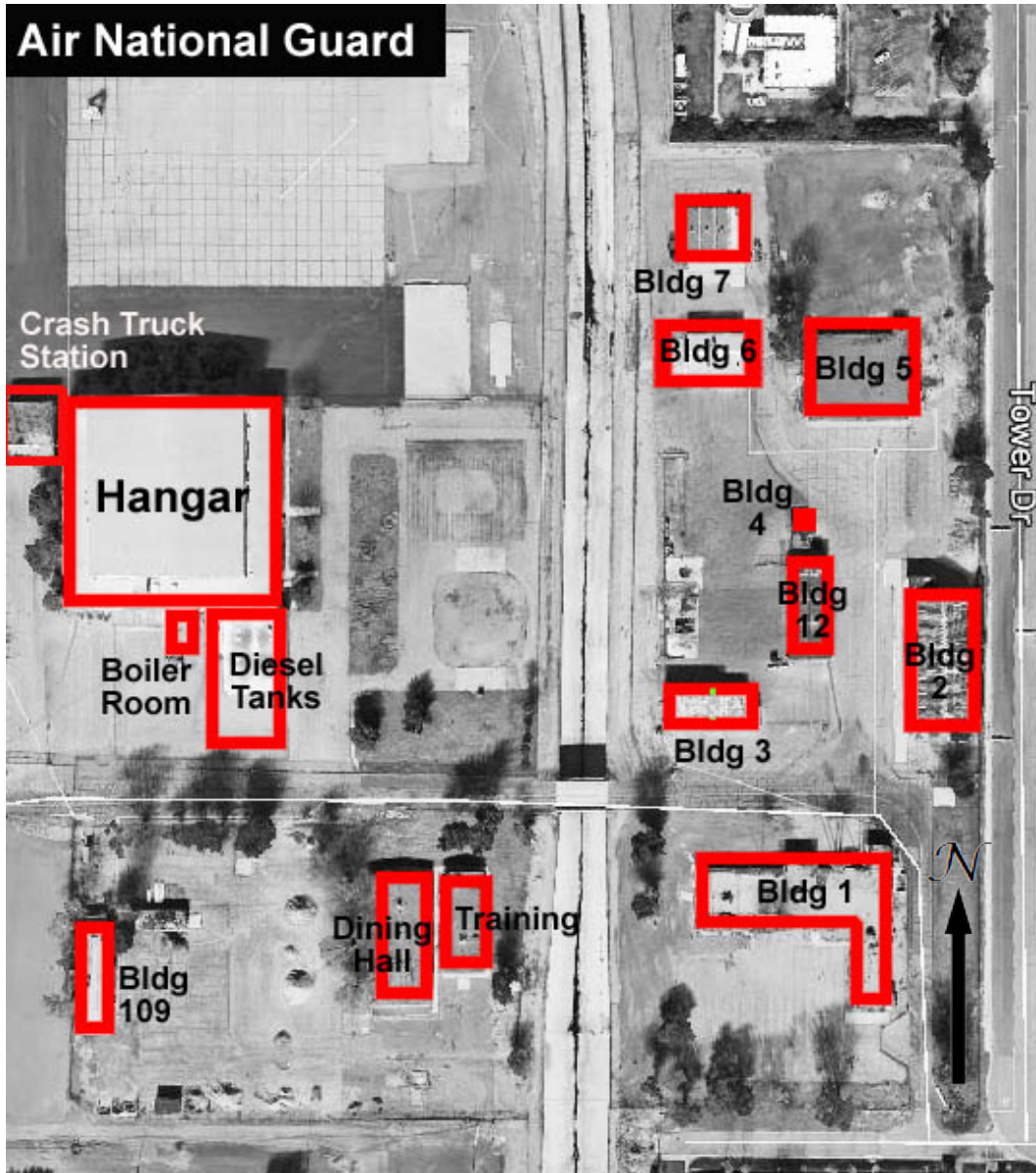
Sketch Map with north arrow required.



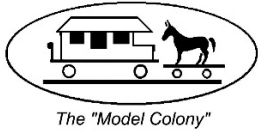
(This space is reserved for official comments)

Page 9 of 9 *Resource Name or # (Assigned by recorder) Air National Guard Hangar
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

Continuation Update



Map of the Air National Guard area surveyed, showing building locations.
Source: ASM Affiliates, June 20, 2017.



Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-018

FILE NO: PHP17-027

DESCRIPTION: A City initiated request for a Tier Determination for the Terminal One Historic District located at 1800-1820 East Moore Way within the bounds of the Ontario International Airport (ONT) property. (APN: 113-261-18).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as "Applicant") has made a request for a Tier Determination of the Terminal One Historic District, File No.PHP17-027, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: The proposed historic district is single parcel containing 4 contributing buildings (historic resources), generally located east of Vineyard Avenue and north of the ONT runways, which include:

- a. Terminal One*
- b. Air Control Tower*
- c. Baggage Clam B, and
- d. FAA Flight Services Building

**Historic resources that are contributors to the historic district and eligible for local landmark designation.*

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (GLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as “Eligible” for local, state, and national registers based on local historic significance. Seven of the individually “Eligible” buildings were also found to be contributors to their perspective historic district.

The Ontario Development Code includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition. The Development Code establishes criteria for Tier I, Tier II or Tier III historic resources, with Tier I and II being of the highest value. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. Major modification or demolition should not occur for Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

(3) Evaluation: A set of criteria, which is based on architecture and history, is used to determine the Tier recommendation. Tier I historic resources must meet at least one of the criterion within the Architecture/Form category and 3 criteria within the History category. Tier II historic resources may be determine eligible for listing in the National Register or the California Register of Historic Places or be listed in the Ontario Register and meet at least 2 criteria within the Architecture/Form or History categories. Tier III historic resources are those that are Designated Local Historic Landmarks, are contributing properties within Designated Local Historic Districts, or are eligible historic resources, including proposed historic districts.

A Tier Determination record was completed for the Terminal One Historic District and is attached to this report in *Exhibit A: Tier Determination*.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee (“HPSC”) the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The Terminal One Historic District meets the Tier I criteria as identified in the attached Tier Determination record; and

(2) Historic resources which contribute to the significance of Terminal One Historic District will have the same Tier determination as the historic district.

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.



Historic Preservation Subcommittee
Chairman

Exhibit A: Tier Determination Form

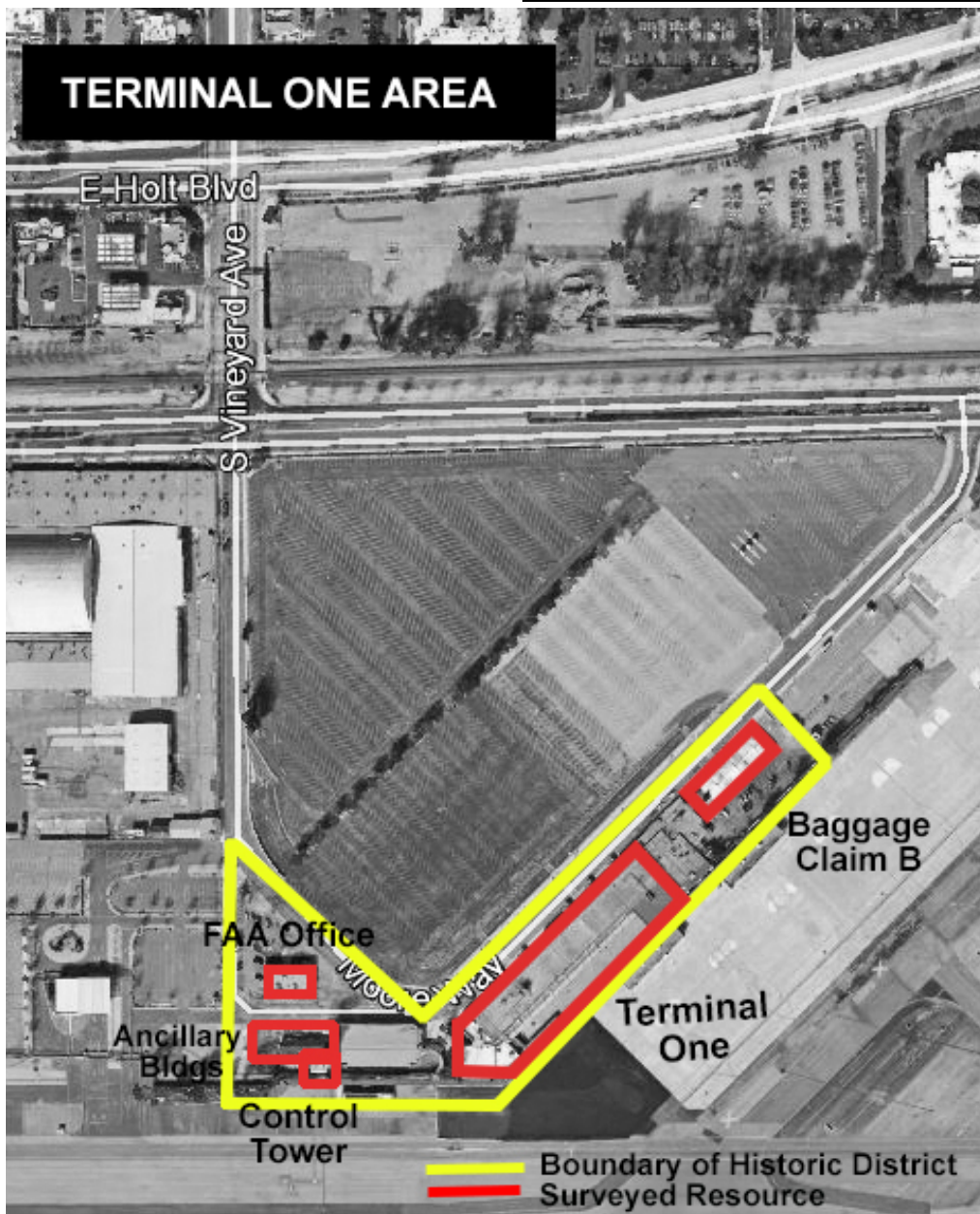
HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION



TIER DETERMINATION

Date: September 14, 2017
Location: 1800-1820 East Moore Way
Historic Name: Terminal One
(Proposed Historic District)
APN: 113-261-18
Description:

Decision Date: 9/14/2017
File No.: PHP17-027
Decision Making Body: HPSC
Tier Determination: I
Current Historic Status: Proposed Historic District



Map showing location of buildings within the Terminal One area.
Source: ASM Affiliates, Inc., June 2017.

The Proposed Terminal One Historic District is comprised of 4 contributing buildings located north of the west side of the primary runways at Ontario International Airport, which include *Terminal One, Baggage Claim B (east), *Control Tower, and the FAA Office. *Historic Resources that are also individually eligible for local, state, and national registers for their local significance.

INDIVIDUAL PROPERTY HISTORIC DISTRICT

TIER DETERMINATION

- Tier I** – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:
- A property listed on the Ontario Register and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 - A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.
- Tier II** – Properties where demolition should be avoided. These properties must meet any of the following:
- Any property listed or determined eligible for listing in the National Register of Historic Places; or
 - Any property listed or determined eligible for listing in the California Register of Historic Resources; or
 - A property listed on the Ontario Register and meets at least 2 of the criteria in either the architecture or history categories; or
 - A contributing structure in a Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.
- Tier III** – Properties where demolition should be avoided where possible, but may be appropriate under certain circumstances. These properties must be one of the following:
- Designated Historic Landmarks, or
 - Contributing structures in a Designated Historic District, or
 - Eligible Historical Resources as defined in Section 7.01.010.

TIER CRITERIA

Architecture (Check all that apply)

- The structure is (or the district contains resources which are) a prototype of, or one of the finest examples of a period, style, architectural movement, or construction in the City or a particular style of architecture or building type.
- The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer or major importance to the City, state or nation.

Explanation: The Proposed Terminal One Historic District contains resources that are the only examples of early passenger airport during the period of significance within the City. HMC architects, founded and based in Ontario, are well known in the region for their Mid-Century Modern architectural style designs which are evident in many of office, school, civic, and hospital buildings throughout the City.

History (Check all that apply)

- It is the location of an historic event(s) that have had a significant contribution to the history of the City, state or nation.
- It is associated with a business, company, or individual that has made a significant, cultural, social, or scientific contribution to the City, state, or nation.
- It is identified with a person(s) who has exerted a major influence on the heritage or history of the City, state, or nation.
- It embodies the ideals or principles of the "Model Colony" or furthers the ideals or principals established by the Chaffey Brothers.
- It has a direct relationship to one of the principle historic contexts in the City's history.
- It is related with a business, company or individual significant in the agricultural history of the City.

Explanation: Evaluated under the context of Aviation in Ontario, theme Civil Aviation (1946-1967) and subtheme Early Passenger Travel (1952-1967). The Proposed Terminal One historic District character defining features include buildings used for dropping off and picking up passengers, ticketing, aircraft loading, runways, and control tower to direct air traffic, administration and close parking lots. Terminal One HD represents important patterns and trends in early passenger travel at ONT.



Above: Terminal One



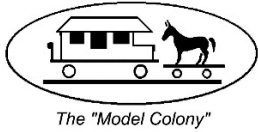
Above: Baggage Claim B



Above: FAA Office



Above: Control Tower



Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-019

FILE NO: PHP17-028

DESCRIPTION: A City initiated request for a Tier Determination for the Air National Guard (ANG) Hangar located at 2475 East Avion Street, south of the ONT runways on the former Air National Guard complex, within the bounds of the Ontario International Airport (ONT). (APN: 113-271-02).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as “Applicant”) has made a request for a Tier Determination of the Air National Guard Hangar, File No.PHP17-028, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: The Air National Guard Hangar is situated on a single parcel located at 2475 East Avion Street within the bounds of the Ontario International Airport (ONT) and is currently vacant.

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (CLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as “Eligible” for local, state, and national registers based on local historic significance. Seven

of the individually “Eligible” buildings were also found to be contributors to their perspective historic district.

The Ontario Development Code includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition. The Development Code establishes criteria for Tier I, Tier II or Tier III historic resources, with Tier I and II being of the highest value. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. Major modification or demolition should not occur for Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

(3) Evaluation: A set of criteria, which is based on architecture and history, is used to determine the Tier recommendation. Tier I historic resources must meet at least one of the criterion within the Architecture/Form category and 3 criteria within the History category. Tier II historic resources may be determine eligible for listing in the National Register or the California Register of Historic Places or be listed in the Ontario Register and meet at least 2 criteria within the Architecture/Form or History categories. Tier III historic resources are those that are Designated Local Historic Landmarks, are contributing properties within Designated Local Historic Districts, or are eligible historic resources, including proposed historic districts.

A Tier Determination record was completed for the Air National Guard Hangar and is attached to this report in *Exhibit A: Tier Determination*.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act (“CEQA”) Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee (“HPSC”) the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The Air National Guard Hangar meets the Tier II criteria as identified in the attached Tier Determination record; and

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

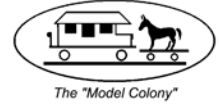
SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.



Historic Preservation Subcommittee
Chairman

Exhibit A: Tier Determination Form



HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION

TIER DETERMINATION

Date: September 14, 2017

Location: 2475 East Avion Street,
south of ONT runways

Historic Name: Air National Guard Hangar

APN: 113-261-03

Description:

Decision Date: 9/14/2017

File No.: PHP17-028

Decision Making Body: HPSC

Tier Determination: II

Current Historic Status: Eligible



Constructed in 1955, the two-story, steel-framed hangar has a low pitched front-gabled roof, clad in corrugated metal, a continuous row of steel frame six-over-three fixed window spaces which spans over three sides of the building. The north façade features a double set of telescoping hangar-type doors that retract into enclosed housing at the sides.

INDIVIDUAL PROPERTY

HISTORIC DISTRICT

TIER DETERMINATION

- Tier I** – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:
- A property listed on the Ontario Register and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 - A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.
- Tier II** – Properties where demolition should be avoided. These properties must meet any of the following:
- Any property listed or determined eligible for listing in the National Register of Historic Places; or
 - Any property listed or determined eligible for listing in the California Register of Historic Resources; or
 - A property listed on the Ontario Register and meets at least 2 of the criteria in either the architecture or history categories; or
 - A contributing structure in an Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.

- Tier III** – Properties where demolition should be avoided where possible, but may be appropriate under certain circumstances. These properties must be one of the following:
- Designated Historic Landmarks, or
 - Contributing structures in a Designated Historic District, or
 - Eligible Historical Resources as defined in Section 7.01.010.

TIER CRITERIA

Architecture (Check all that apply)

- The structure is (or the district contains resources which are) a prototype of, or one of the finest examples of a period, style, architectural movement, or construction in the City or a particular style of architecture or building type.
- The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer of major importance to the City, state or nation.

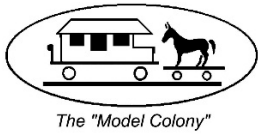
Explanation: Evaluated under the context of Aviation in Ontario, theme Aviation and Architecture (1942-1975) and subtheme Developments in Construction Technology (1942-1975), the hangar displays character-defining features of one style typical of Air National Guard facilities during the period of significance, including a front-gabled roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction. The multi-leaved telescoping hangar doors with extensive fenestration and the mass of the building formed by the pop-up center section with clerestory windows and the lower “lean-to” sections all original features. Although the National Guard no longer occupies the site, suggesting some change in use, both the interior and exterior of the building reflect all seven aspects of integrity. The building embodies distinctive features of the type of hangar constructed during the period of significance.

History (Check all that apply)

- It is the location of an historic event(s) that have had a significant contribution to the history of the City, state or nation.
- It is associated with a business, company, or individual that has made a significant, cultural, social, or scientific contribution to the City, state, or nation.
- It is identified with a person(s) who has exerted a major influence on the heritage or history of the City, state, or nation.
- It embodies the ideals or principles of the “Model Colony” or furthers the ideals or principals established by the Chaffey Brothers.
- It has a direct relationship to one of the principle historic contexts in the City’s history.
- It is related with a business, company or individual significant in the agricultural history of the City.

Explanation: Evaluated under the context of Aviation in Ontario, theme Aviation and Architecture (1942-1975) and subtheme Developments in Construction Technology (1942-1975), the hangar was determined to be eligible for the local, state, and national registers. Although is hangar was

constructed during the period of significance for the ANG, it was determined through a previous report by Jayne Aaron titled Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War (2011) that the California Air National Guard did not contribute to important patterns and trends in its role in military operations from WWII through the Cold War.



Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-20

FILE NO: PHP17-029

DESCRIPTION: A City initiated request for a Tier Determination for the General Electric Aircraft Engines Historic District generally located on East Avion Street, west of Vineyard Avenue within the Ontario Internal Airport property boundary. (APN: 113-261-06).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as “Applicant”) has made a request for a Tier Determination of the Proposed General Electric Aircraft Engines Historic District, File No.PHP17-029, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: The proposed historic district is a single parcel containing 4 contributing buildings (historic resources) which include Hangars 3, 4, and 7 and Storage Hangars generally located on East Avion Street, west of Vineyard Avenue, on the former General Electric Aircraft Engines complex.

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (CLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as “Eligible” for local, state, and national registers based on local historic significance.

Seven of the individually “Eligible” buildings were also found to be contributors to their perspective historic district.

The Ontario Development Code includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition. The Development Code establishes criteria for Tier I, Tier II or Tier III historic resources, with Tier I and II being of the highest value. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. Major modification or demolition should not occur for Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

(3) Evaluation: A set of criteria, which is based on architecture and history, is used to determine the Tier recommendation. Tier I historic resources must meet at least one of the criterion within the Architecture/Form category and 3 criteria within the History category. Tier II historic resources may be determine eligible for listing in the National Register or the California Register of Historic Places or be listed in the Ontario Register and meet at least 2 criteria within the Architecture/Form or History categories. Tier III historic resources are those that are Designated Local Historic Landmarks, are contributing properties within Designated Local Historic Districts, or are eligible historic resources, including proposed historic district.

A Tier Determination record was completed for the Historic District and is attached to this report in *Exhibit A: Tier Determination*.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee (“HPSC”) the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The General Electric Aircraft Engines Historic District meets the Tier I criteria as identified in the attached Tier Determination record; and

(2) Historic resources which contribute to the significance of the General Electric Aircraft Engines Historic District will have the same Tier Determination as the historic district.

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

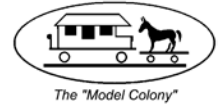
SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.



Historic Preservation Subcommittee
Chairman

Exhibit A: Tier Determination Form



HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION

TIER DETERMINATION

Date: September 14, 2017

Location: Southwestern portion of the ONT property on East Avion Street, west of Vineyard Avenue

Historic Name: General Electric Aircraft Engines (Proposed Historic District)

APN: 113-261-06

Decision Date: 9/14/2017

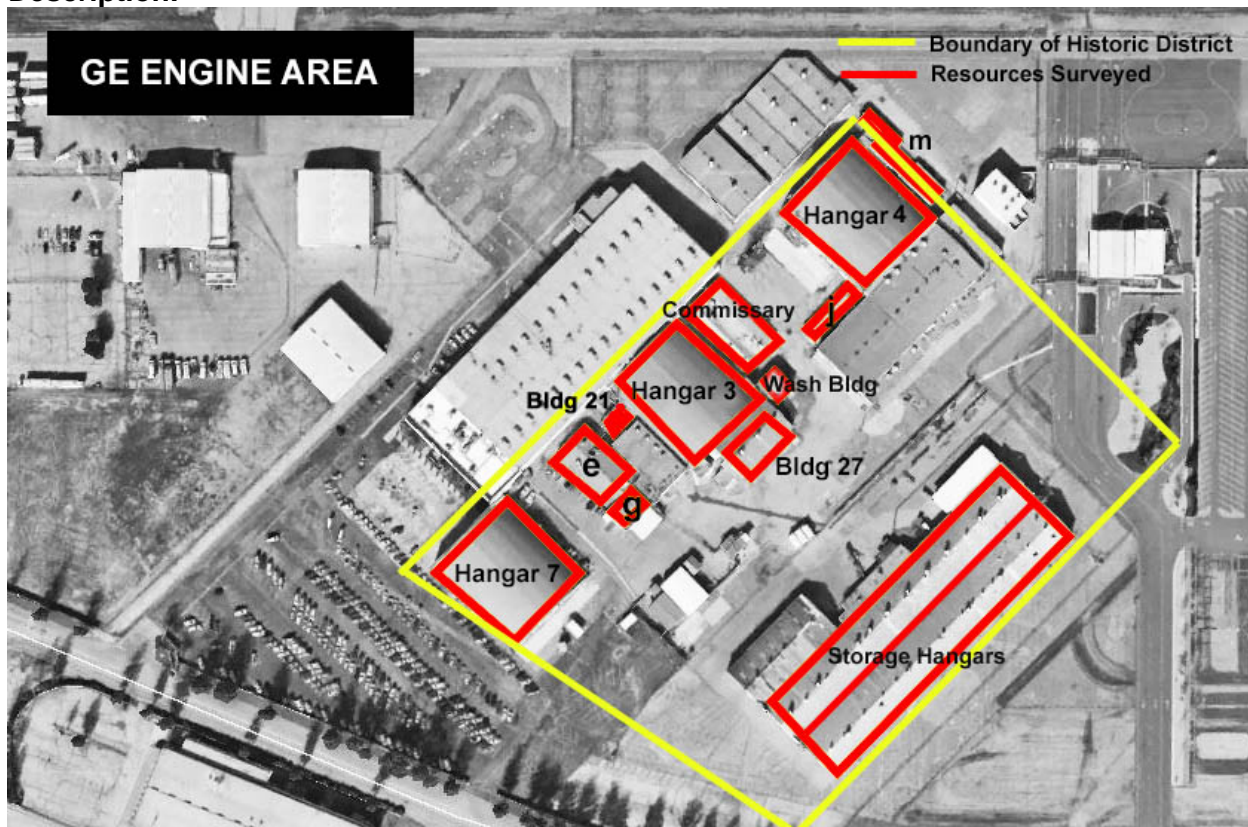
File No.: PHP17-029

Decision Making Body: HPSC

Tier Determination: I

Current Historic Status: Proposed Historic District

Description:



Map showing resources surveyed and recommended historic district boundary.

The General Electric Historic District is comprised of 4 contributing buildings which include Hangars 3, 4, and 7, and the Storage Hangars.

- INDIVIDUAL PROPERTY HISTORIC DISTRICT

TIER DETERMINATION

- Tier I** – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:
 - A property listed on the Ontario Register and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 - A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.



- Tier II** – Properties where demolition should be avoided. These properties must meet any of the following:
 - Any property listed or determined eligible for listing in the National Register of Historic Places; or
 - Any property listed or determined eligible for listing in the California Register of Historic Resources; or
 - A property listed on the Ontario Register and meets at least 2 of the criteria in either the architecture or history categories; or
 - A contributing structure in a Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.

- Tier III** – Properties where demolition should be avoided where possible, but may be appropriate under certain circumstances. These properties must be one of the following:
 - Designated Historic Landmarks, or
 - Contributing structures in a Designated Historic District, or
 - Eligible Historical Resources as defined in Section 7.01.010.

TIER CRITERIA

Architecture (Check all that apply)

- The structure is (or the district contains resources which are) a prototype of, or one of the finest examples of a period, style, architectural movement, or construction in the City or a particular style of architecture or building type.

- The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer or major importance to the City, state or nation.

Explanation: Hangars 3 (built 1940) and 4 (built pre-1948) appear to be World War II era hangars first built and used by the Army Air Corps. GE Aircraft Engines Maintenance Center moved into the facility in 1956 after Northrop and Double Aircraft vacated.

History (Check all that apply)

- It is the location of an historic event(s) that have had a significant contribution to the history of the City, state or nation.

- It is associated with a business, company, or individual that has made a significant, cultural, social, or scientific contribution to the City, state, or nation.

- It is identified with a person(s) who has exerted a major influence on the heritage or history of the City, state, or nation.

- It embodies the ideals or principles of the “Model Colony” or furthers the ideals or principals established by the Chaffey Brothers.

- It has a direct relationship to one of the principle historic contexts in the City’s history.

- It is related with a business, company or individual significant in the agricultural history of the City.

Explanation: The Proposed GE Historic District represents important patterns and trends in commercial aviation development from this period (1942-1967), contains a grouping of buildings and structures typical of a commercial aviation support facility, retains a majority of the buildings/structures present during the period of significance, and retains most of its character-defining features and essential aspects of integrity. Only the hangars are recommended as contributors to the historic district, as known locations where aircraft modifications, repair, and/or testing was performed; insufficient information exists function of the ancillary buildings to recommend them as contributors. The majority of the hangar doors have been altered or replaced, and construction adjacent to or near the hangars obscure their original function. While the hangars retain sufficient integrity as a historic district, they do not retain sufficient integrity of design and materials to be recommended as individually eligible, and no other individually eligible properties were identified within the survey area.

Through development of the Ontario International Airport Historic Context Statement and intensive level survey of the property, the Proposed GE Historic District is eligible for listing on the local, state and national registers under Criterion A/1 and local district designation criterion 1-3 for its association with aviation support services at ONT during the period significance (1942-1967).



Above: GE Hangar 3

Below: GE Hangar 4

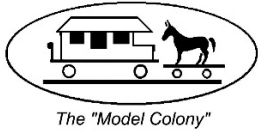




Above: GE Storage Hangars

Below: GE Hangar 7





Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-021

FILE NO: PHP17-030

DESCRIPTION: A City initiated request for a Tier Determination for the Aerojet-General Hangar located at 301 South Hellman Avenue, west of the Cucamonga Creek Channel, within the bounds of the Ontario International Airport (ONT). (APN: 113-261-03).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as “Applicant”) has made a request for a Tier Determination of the Aerojet-General Hangar, File No.PHP17-030, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: The Aerojet-General Hangar is situated on a single parcel located at 301 South Hellman Avenue, west of the Cucamonga Creek Channel, within the bounds of the Ontario International Airport (ONT).

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (CLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as “Eligible” for local, state, and national registers based on local historic significance. Seven

of the individually “Eligible” buildings were also found to be contributors to their perspective historic district.

The Ontario Development Code includes a tier system with standard criteria and procedures for evaluating the significance of historic or potentially historic resources threatened by major modifications or demolition. The Development Code establishes criteria for Tier I, Tier II or Tier III historic resources, with Tier I and II being of the highest value. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and method to evaluate the significance of their loss in the case of major modification or demolition. Major modification or demolition should not occur for Tier I or Tier II historic resources and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

(3) Evaluation: A set of criteria, which is based on architecture and history, is used to determine the Tier recommendation. Tier I historic resources must meet at least one of the criterion within the Architecture/Form category and 3 criteria within the History category. Tier II historic resources may be determine eligible for listing in the National Register or the California Register of Historic Places or be listed in the Ontario Register and meet at least 2 criteria within the Architecture/Form or History categories. Tier III historic resources are those that are Designated Local Historic Landmarks, are contributing properties within Designated Local Historic Districts, or are eligible historic resources, including proposed historic districts.

A Tier Determination record was completed for the Aerojet-General Hangar and is attached to this report in *Exhibit A: Tier Determination*.

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee (“HPSC”) the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The Aerojet-General Hangar meets the Tier II criteria as identified in the attached Tier Determination record; and

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

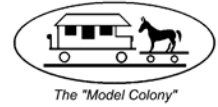
SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.



Historic Preservation Subcommittee
Chairman

Exhibit A: Tier Determination Form



HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION

TIER DETERMINATION

Date: September 14, 2017

Location: 301 S. Hellman Avenue, west of the Cucamonga Creek Channel and north of Terminal One

Historic Name: Aerojet-General Hangar

APN: 113-261-03

Decision Date: 9/14/2017

File No.: PHP17-030

Decision Making Body: HPSC

Tier Determination: II

Current Historic Status: Eligible

Description:



Constructed in 1958 and nearly 19,000-square-foot, the hangar has a moderately pitched front-gabled roof clad in corrugated metal with regularly spaced inset corrugated fiberglass skylights. At the west façade is a set of telescoping corrugated metal doors that run on steel tracks set into the concrete pavement, allowing them to retract fully from the mass of the building. The doors are hung on a steel structure that projects beyond the sides of the building. At the center below the gable is an opening for the aircraft tail with a roll-up metal door.

INDIVIDUAL PROPERTY

HISTORIC DISTRICT

TIER DETERMINATION

- Tier I** – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:
- A property listed on the Ontario Register and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 - A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.
- Tier II** – Properties where demolition should be avoided. These properties must meet any of the following:
- Any property listed or determined eligible for listing in the National Register of Historic Places; or
 - Any property listed or determined eligible for listing in the California Register of Historic Resources; or
 - A property listed on the Ontario Register and meets at least 2 of the criteria in either the architecture or history categories; or

- A contributing structure in an Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.
- Tier III** – Properties where demolition should be avoided where possible, but may be appropriate under certain circumstances. These properties must be one of the following:
 - Designated Historic Landmarks, or
 - Contributing structures in a Designated Historic District, or
 - Eligible Historical Resources as defined in Section 7.01.010.

TIER CRITERIA

Architecture (Check all that apply)

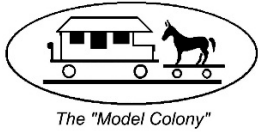
- The structure is (or the district contains resources which are) a prototype of, or one of the finest examples of a period, style, architectural movement, or construction in the City or a particular style of architecture or building type.
- The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer of major importance to the City, state or nation.

Explanation: Evaluated under the context of Aviation in Ontario, theme Aviation and Architecture (1942-1975) and subtheme Developments in Construction Technology (1942-1975), the building's character defining features include large open space to accommodate aircraft with an exposed steel frame, front-gabled roof, telescoping doors that run on steel tracks, and an opening for aircraft tail with retracting doors. The building embodies distinctive features of the type of hangar constructed during the period of significance.

History (Check all that apply)

- It is the location of an historic event(s) that have had a significant contribution to the history of the City, state or nation.
- It is associated with a business, company, or individual that has made a significant, cultural, social, or scientific contribution to the City, state, or nation.
- It is identified with a person(s) who has exerted a major influence on the heritage or history of the City, state, or nation.
- It embodies the ideals or principles of the "Model Colony" or furthers the ideals or principals established by the Chaffey Brothers.
- It has a direct relationship to one of the principle historic contexts in the City's history.
- It is related with a business, company or individual significant in the agricultural history of the City.

Explanation: Evaluated under the context of Aviation in Ontario, theme Aviation and Architecture (1942-1975) and subtheme Developments in Construction Technology (1942-1975), the hangar was determined to be eligible for the local, state, and national registers.



Historic Preservation Subcommittee

September 14, 2017

DECISION NO: HPSC17-016

FILE NO: PHP17-031

DESCRIPTION: A City initiated request to add the following potential historic resources to the Ontario Register: 1) The Lockheed Aircraft Services Historic District, located on the northwest portion of the Ontario International Airport property, south of East Airport Drive and west of Vineyard Avenue (APNs: 113-231-05, 113-231-06, 113-241-06, 113-241-07, 113-231-04, and 113-231-03); 2) The Terminal One Historic District, located at 1820-1800 East Moore Way (APN: 113-261-18); 3) The General Electric Aircraft Engines Historic District, generally located on East Avion Street west of Vineyard Avenue (APN: 113-261-06); 4) The Aerojet-General Hangar, located at 301 S. Hellman Avenue west of the Cucamonga Creek Channel (APN: 113-261-03); 5) The Air National Guard Hangar, located at 2475 East Avion Street (APN: 113-271-02);. (APN: 113-261-06).

PART I: BACKGROUND & ANALYSIS

THE CITY OF ONTARIO, (herein after referred to as "Applicant") has made a request to add certain potential historic resources to the Ontario Register as "Eligible" for individually listed properties or "Proposed" as part of a historic district, File No.PHP17-031, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

(1) Project Setting: Potential historic resources for consideration are located within the bounds of the Ontario International Airport (ONT) properties. A map to illustrate the general locations of each resource is attached to this report in *Exhibit A: Potential Historic Resources Location Map*.

(2) Project Background: In 2016, the City of Ontario received a Certified Local Government (GLG) grant to develop a historic context statement for the ONT and conduct an intensive level survey of aviation related properties that are older than 45 years located within the bounds of ONT properties.

The historic context and survey project provides the foundation for preservation planning by documenting, understanding, identifying, and evaluating potential historic resources. It is the first step in guiding land use decisions which includes consideration of adaptive reuse alternatives for identified historic properties. Additionally, the project deliverables have the potential to streamline and strengthen the environmental review process for future redevelopment and growth of the project area and benefit the community by developing and promoting airport history as an opportunity for heritage tourism.

The CLG program requires local governments to conduct surveys based on historic contexts on a regular basis to identify and document potential historic resources for the purpose of evaluation of placement on the local, state, and national historic registers. As a result of the project, a total of 55 properties were surveyed within the project boundary and evaluated for significance under the context theme of Aviation in Ontario.

(3) Historic Context: The Ontario International Airport (ONT) Historic Context Statement (Aviation in Ontario) identifies 4 themes and 5 subthemes in which the potential historic resources were evaluated under and include:

1. Theme: Commercial Aviation, 1946–1967
 - a. Sub-Theme: Aviation Support Services, 1952–1967
 - b. Sub-Theme: International Cargo and Freight Operations, 1946–1967
2. Theme: Civil Aviation, 1950–1967
 - a. Sub-Theme: Early Passenger Travel, 1950–1967
3. Theme: Military Aviation, 1942–1991
4. Theme: Aviation and Architecture, 1952–1975
 - a. Sub-Theme: Developments in Construction Technology, 1952–1975
 - b. Sub-Theme: Modernism and Aviation, 1955–1970

(4) Evaluation: The Ontario Development Code allows for potential historic resources surveyed at an intensive level pursuant to standards established by the California Office of Historic Preservation to be considered for inclusion on the Ontario Register. Three historic districts (containing 18 contributors) and 9 individual buildings were identified as “Eligible” for local, state, and national registers based on local historic significance. Seven of the individually “Eligible” buildings were also found to be contributors to their perspective historic district. Reconnaissance and intensive level survey record for each potential historic resource is attached to this report in *Exhibit B: DPR523 Records*. Eligibility to the local register is based on designation criteria as contained in Section 4.02.040 of the Ontario Development Code and is listed below.

A historic resource may be designated an “historic landmark” by the City if it meets the criteria for listing in the National Register of Historic Places or the California Register of Historic Resources, or it meets one or more of the following criteria:

- a) The historic resource exemplifies or reflects special elements of the City’s history;
- b) The historic resource is identified with persons or events significant in local, state, or national history;
- c) The historic resource is representative of the work of a notable builder, designer, architect, or artist;

- d) The historic resource embodies distinguishing architectural characteristics of a style, type, period, or method of construction;
- e) The historic resource is a noteworthy example of the use of indigenous materials or craftsmanship;
- f) The historic resource embodies elements that represent a significant structural, engineering, or architectural achievement or innovation;
- g) The historic resource has a unique location, a singular physical characteristic, or is an established and familiar visual feature of a neighborhood, community, or the City;
- h) The historic resource is one of the few remaining examples in the City, region, state or nation, possessing distinguishing characteristics of an architectural or historical type or specimen: or
- i) The historic resource has yielded, or is likely to yield, information important to the City's history or prehistory.

A neighborhood or area listed as a historic resource may be designated a "Local Historic District" by the City if the neighborhood meets the criteria for listing in the National Register of Historic Places or the California Register of Historic Resources, or it meets one or more of the following criteria:

1. The historic resource is a geographically definable area possessing a concentration of historic resources or a thematically related grouping of structures that contribute to each other and are unified by plan, style, or physical development, and embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work of a master or possesses high artistic values;
2. The historic resource reflects significant geographical patterns, including those associated with different eras of settlement and growth, particular transportation modes, or distinctive examples of a park landscape, site design, or community planning;
3. The historic resource is associated with, or the contributing resources are unified by, events that have made a significant contribution to the broad patterns of local or regional history, or the cultural heritage of California or the United States; or

4. The historic resource is, or the contributing resources are, associated with the lives of persons important to the City, State or National history.

Discussion of the historic significance and evaluation using the criteria for each potential historic resource being considered for inclusion on the Ontario Register is summarized in *Table 1* below with the full evaluation attached to this report in *Exhibit B: DPR523 Records*.

Table1. List of Eligible Historical Resources Surveyed

Name of Property	Theme/Sub-theme	NRHP Criteria	CRHR Criteria	Local Criteria
Lockheed Aircraft Services Historic District (10 Contributors)	Commercial Aviation, 1946-1967/ Aviation Support Services, 1952-1967	A,C	1	District 1-3
Terminal One Historic District (4 Contributors)	Civil Aviation, 1950-1967/Early Passenger Travel, 1950-1967/ Modernism and Aviation, 1955-1970	A,C	1	District 1-3
GE Aircraft Engines Historic District (4 Contributors)	Commercial Aviation, 1946-1967/ Aviation Support Services, 1952-1967/ Modernism and Aviation, 1955-1970	A	1	District 1-3
Lockheed Executive Office Building (Bldg. 10)	Aviation and Architecture, 1942-1975/ Modernism and Aviation, 1955-1970	C	3	Individual c-d, f-h
Lockheed Cafeteria (Bldg. 11)	Aviation and Architecture, 1942-1975/ Modernism and Aviation, 1955-1970	C	3	Individual c-d, f-h
Terminal One Building	Civil Aviation, 1950-1967/ Early Passenger Travel, 1950-1967/ Modernism and Aviation, 1955-1970	A, C	1, 3	Individual c-d, f-h
Control Tower	Civil Aviation, 1950-1967/ Early Passenger Travel, 1950-1967	A	1, 3	Individual d, f-h
Air National Guard Hangar	Aviation and Architecture, 1942-1975/ Developments in Construction Technology, 1942-1975	C	3	Individual d, f-h
Lockheed Hangar 2	Aviation and Architecture, 1942-1975/ Developments in Construction Technology, 1942-1975	C	3	Individual d, f-h
Lockheed Hangar 4	Aviation and Architecture, 1942-1975/ Developments in Construction Technology, 1942-1975	C	3	Individual d, f-h
Lockheed Hangar 6	Aviation and Architecture, 1942-1975/ Developments in Construction Technology, 1942-1975	C	3	Individual d, f-h
Aerojet-General Hangar	Aviation and Architecture, 1942-1975/ Developments in Construction Technology, 1942-1975	C	3	Individual d, f-h

PART II: RECITALS

WHEREAS, the Application is not a project pursuant to the California Environmental Quality Act ("CEQA") Guidelines Section 21065; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Subcommittee ("HPSC") the responsibility and authority to review and act, or make recommendation to the Planning/Historic Preservation Commission, on the subject Application; and

WHEREAS, all members of the HPSC of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the Project; and

WHEREAS, on September 14, 2017, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

PART III: THE DECISION

NOW, THEREFORE, IT IS HEREBY FOUND AND DETERMINED by the Historic Preservation Subcommittee of the City of Ontario, as follows:

SECTION 1: As the decision-making body for the Project, the HPSC has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the HPSC, the HPSC finds as follows:

(1) The Application is not a project pursuant to Section 21065 of the CEQA Guidelines; and

SECTION 2: Based upon the substantial evidence presented to the HPSC during the above-referenced hearing and upon the specific findings set forth in Section 1, above, the HPSC hereby concludes as follows:

(1) The potential historic resources identified, documented, and evaluated in the Ontario international Airport Historic (ONT) Context Statement and Survey project meet the designation criteria as stated the attached *Exhibit B: DPR523 Records*.

SECTION 3: Based upon the findings and conclusions set forth in Sections 1 and 2 above, the HPSC hereby approves the Application.

SECTION 4: The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

APPROVED AND ADOPTED this 14th day of September 2017.


Historic Preservation Subcommittee
Chairman

Exhibit B: DPR523 Records

Page 1 of 8

*Resource Name or #: Lockheed Aircraft Services Historic District

D1. Historic Name: Lockheed Aircraft Services Historic District

D2. Common Name: Lockheed Aircraft Services Historic District

*D3. Detailed Description: (Describe overall coherence of the district, its setting, visual characteristics, and minor features. List all elements of district.)

The district is a large complex within the former property of Lockheed Aircraft Services (LAS), a division of Lockheed Aircraft Corporation, which operated at Ontario International Airport from 1952 to 1998. LAS activities were primarily within a 70-acre parcel in the northwest area of the airport. During its 46 years of operation at Ontario, Lockheed built more than 25 structures, including hangars, office buildings, machine shops, and auxiliary buildings (Douglas and Livingstone 2006). Primary LAS activities at Ontario consisted of modifying and refurbishing commercial and military aircraft. The Ontario facilities served as headquarters for LAS's domestic and international operations. LAS also produced a complete line of flight data recording devices, data playback stations, and training and simulation devices. Lockheed's manufacture of flight recorders began in 1958 with the introduction of the Model 109 (LADOA 1983).

After World War II, with its expertise in maintenance, modification, and overhaul of aircraft, LAS saw an opportunity to expand its support services. In the U.S., the division constructed facilities in California, New York, Louisiana, South Carolina, and Hawaii. In the 1960s, LAS in Ontario became the maintenance and modification center for the highly classified U.S. Air Force fleet of four-engine

(Continued on page 2)

*D4. Boundary Description: (Describe limits of district and attach map showing boundary and district elements.)

The historic district is bounded on the north by East Airport Drive; on the east by the east facades of Hangars 2, 4, and 6; on the south by a south facades of Hangar 6 and Building 14; and on the west to the western facades of Buildings 14 and 15. (see Location Map)

*D5. Boundary Justification:

The boundary of the Lockheed Aircraft Services Historic District encompasses the concentration of resources that reflect the historic significance of the LAS facility, which is a subset of the area surveyed. The district comprises LAS properties extant during the period of significance.

D6. Significance: Theme Commercial Aviation

Area Aviation Support Services

Period of Significance 1952-1968, 1955-1970

Applicable Criteria NRHP Criterion A, CRHR Criterion 1, and
Local District Criteria 1-3;
NRHP Criterion C, CRHR Criterion 3, and
Local District Criterion 1

(Discuss district's importance in terms of its historical context as defined by theme, period of significance, and geographic scope. Also address the integrity of the district as a whole.)

The Lockheed Aircraft Services Historic District was evaluated under the context of Aviation in Ontario; theme Commercial Aviation, 1946-1967; and sub-theme Aviation Support Services, 1952-1967, according to the guidelines established in the *Ontario International Airport Historic Context Statement*, prepared by ASM Affiliates, Inc., for City of Ontario, June 2017. Lockheed's commercial aviation support services for primarily military aircraft played an important role in the growth and development of ONT. Eligible properties under this sub-theme include historic districts that retain the buildings and structures, and their spatial relationships, associated with an aircraft service facility that performed aircraft modifications, repair, and/or testing. Individually eligible properties are limited to hangars and office or administrative buildings that reflect architectural styles that were popular during the period of significance.

The historic district comprises an executive office building, along with associated cafeteria, a mail room, a warehouse, three barrel-roofed hangars and associated ancillary buildings and structures. An additional office building constructed in 1968 in the vicinity of the core of the historic district is included because it was integral to the operations of the facility. As such, the period of significance for the district extends to 1968, to encompass this associated building. Per NRHP guidelines, the majority of buildings in the district are more than 50 years old and the majority of the years of the period of significance are more than 50 years old as well.

(Continued on page 2)

*D8. Evaluator: Shannon Davis and Marilyn Novell

Date: _____

Affiliation and Address: ASM Affiliates, Inc., 20 N. Raymond Avenue, Pasadena, CA 91103

Page 2 of 8 *Resource Name or # (Assigned by recorder) Lockheed Aircraft Services Historic District
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

Continuation Update

***D3. Detailed Description:** (Continued from page 1)

turbo-prop C-130 aircraft under the program known as "Big Safari" (Lockheed 2017). Big Safari was an Air Force program responsible for maintenance and modification of specialized mission aircraft. It was not a technology development project, but a management program to support multiple projects simultaneously. Big Safari Detachment 4 was located at LAS in 1964 specifically to oversee modification of aircraft for special missions to Southeast Asia. LAS ONT also modified six C-123Bs, which were first-generation deep-penetration jamming aircraft fitted with special receivers and transmitters, Doppler navigation systems, and camouflage paint (Jenkins 2001:121). In 1998, LAS ended 46 years at ONT and permanently closed the facility (Sable 1998).

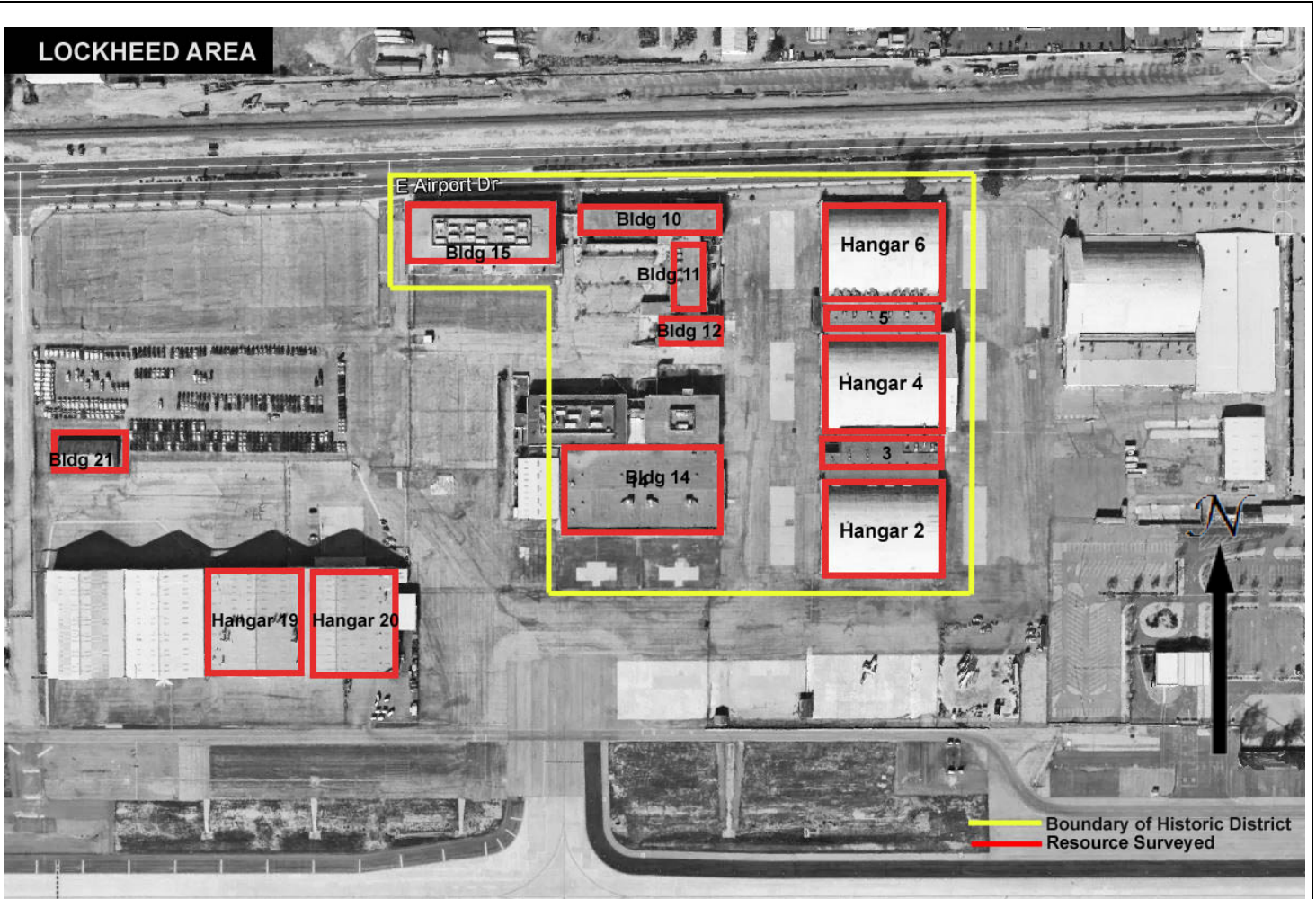
D6. Significance: (Continued from page 1)

As noted in the Historic Context Statement registration requirements, the Lockheed Historic District represents important patterns and trends in commercial aviation development from this period, contains a grouping of buildings and structures typical of a commercial aviation support facility, retains a majority of the buildings/structures present during the period of significance, and retains most of its character-defining features and essential aspects of integrity. Three hangars, two office buildings, a warehouse, and a mail room, as well as ancillary buildings serving the hangars, are recommended contributors to the district, as representing functions related to the operations of the facility. Hangars constructed in 1968 do not retain sufficient integrity to be recommended as contributors. Insufficient information exists on the function of Building 21 to recommend it as a contributor. ASM recommends the Lockheed Aircraft Services Historic District as eligible under Criteria A/1 and local District Criteria 1-3 for its association with aviation support services at ONT during the period of significance.

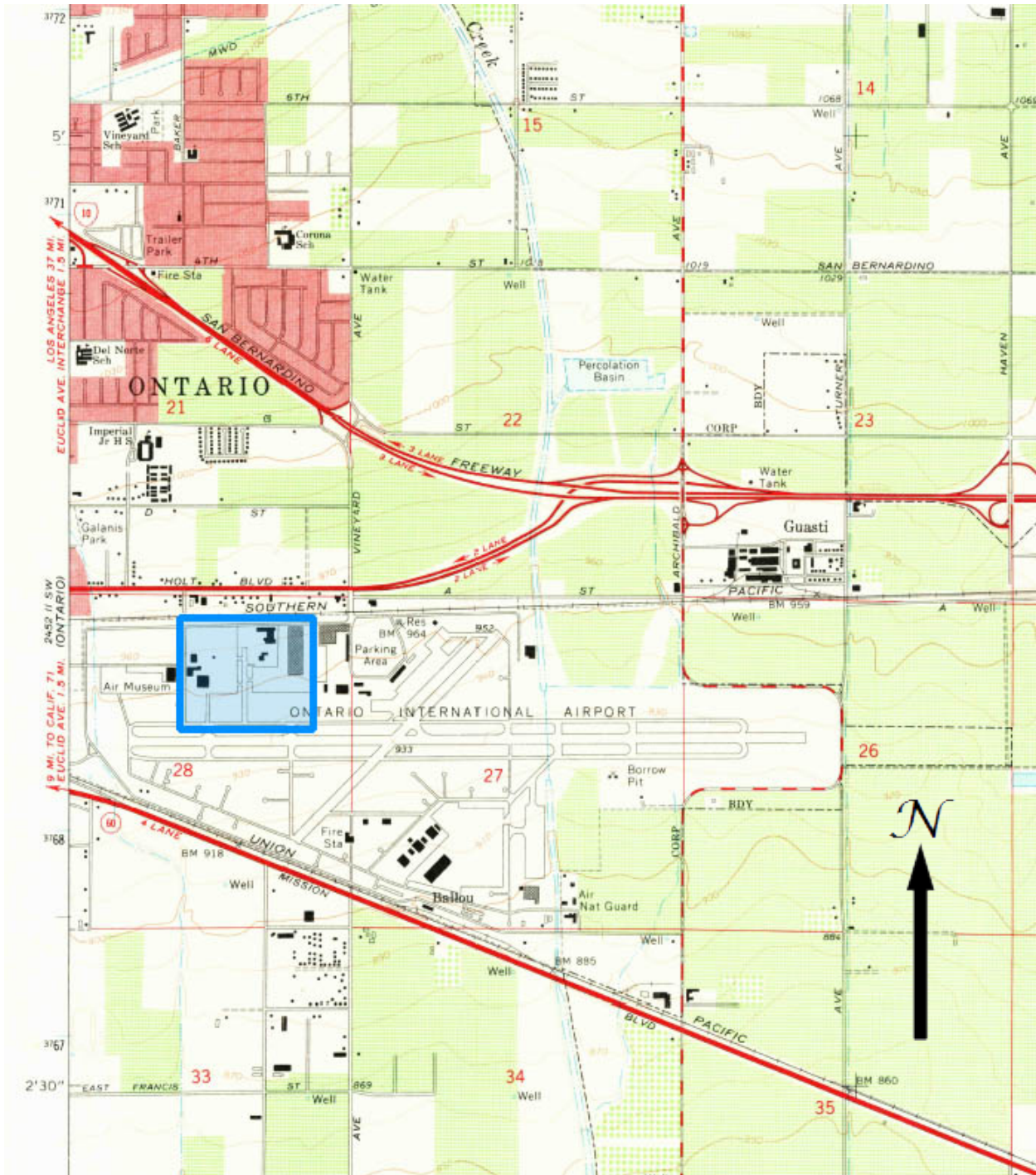
The Lockheed Historic District also contains several buildings that are significant for architecture. The Executive Office Building (Building 10) and the Lockheed Cafeteria Building (Building 11) were evaluated as individually eligible under the context of Aviation in Ontario; theme of Aviation and Architecture; and sub-themes of Modernism and Aviation, 1955-1970. The two buildings are good representations of Mid-Century-Modern architecture designed by a known local architect and represent the use of the style for prominent buildings visible to and used by the public. They were constructed during the period of significance and retain most of the character-defining features of the style to convey their historical association. Although there is some loss of integrity of materials to the buildings, they retain the aspects of integrity of location, design, setting, workmanship, feeling and association. The district also contains three aircraft hangars that are recommended individually eligible under the subtheme of Developments in Construction Technology, 1942-1975. Because these buildings are contributors to the historic district, the district is recommended eligible under Criteria C/3 and local District Criterion 1 for its association with Aviation and Architecture.

D7. References:

- Douglas, Diane L., and David Livingstone. (2006). *Historical Architectural and Cultural Resources Inventory Report: Pacific Gateway Cargo Center, Ontario International Airport*. Prepared by URS for Aero Ontario.
- Jenkins, Dennis R. 2001. *Lockheed Secret Projects: Inside the Skunk Works*. St. Paul, MN: MBI Publishing.
- Lockheed Aircraft Service Company. 2017. "The Beginning of a Global Lockheed Martin." Available at Lockheed Martin website.
- Los Angeles Department of Airports (LADOA). 1983. *Ontario International Airport Information: Service, Economics, Improvements and Growth Potential (Quarterly Report)*. Ontario, CA: Los Angeles Department of Airports.
- National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.
- National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.
- Ontario International Airport Historic Context Statement*. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
- Sable, Julie. 1998. "Lockheed Closes Doors at Ontario Airport Site," *Ontario Daily Bulletin*, April 11.



Map showing buildings within the area and boundary of Lockheed Aircraft Services Historic District.
Source: ASM Affiliates, Inc., June 2017.



Map showing location of Lockheed Aircraft Services area relative to the airport (USGS Guasti, 1966, 1:24,000 scale).



Image 1. View looking north at the south façade of Building 15.



Image 2. View looking northwest at the east façades of the Mail Room, the Cafeteria, and the Executive Office Building.



Image 3. View looking southeast at the west and north façades of hangars 6, 4, and 2.



Image 4. View looking southwest and the east and north façades of hangars 20 and 19.



Image 5. View looking northwest at the LAS facilities.



Image 6. View looking southwest at the east and north façades of Building 21.



Image 7. Aerial view of LAS area (the hangar in the foreground right has been demolished), post-1953. Photographer: Gordon Ayers.
Source: Ontario City Library Robert E. Ellingwood Model Colony Room. Accession No. 3677.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3B

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 4

*Resource Name or #: Lockheed Hangar 2

P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443342.05 mE/ 3768985.81 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Hangar 2, part of the Lockheed Aircraft Services Area at Ontario International Airport, is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in corrugated metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. At the east and west ends are sets of telescoping metal doors that run on steel tracks, allowing them to slide fully into a housing apparatus that extends beyond the mass of the building. Above and at the center of each set of doors is a retractable corrugated metal tail door. Additional fenestration consists of vents arranged in horizontal banks on the hangar doors, metal personnel doors, and shed dormers with louvered vents arranged in a horizontal row on the south side. Hangar 2 is connected to Building 3 on the north. The interior of the hangar is a single open space. The building was used for aircraft maintenance and modification.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

*P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking northeast at the west and south façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1952

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking north at the south façade.



Image 2. View looking southeast at the north and west façades.



Image 3. View looking southwest at the east and north façades.



Image 4. Detail view looking northwest at the south façade.



Image 5. View of the interior looking west.



Image 6. Detail view looking west at the south façade.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 4

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Lockheed Hangar 2

B1. Historic Name: Lockheed Hangar 2

B2. Common Name: _____

B3. Original Use: Hangar

B4. Present Use: Hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1952

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron, workshops

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture

Area: Developments in Construction Technology

Period of Significance: 1955-1975

Property Type: Aircraft hangar

Applicable Criteria: NRHP Criterion C, CRHR Criterion 3, and Local Individual Criteria 3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Hangar 2 in the Lockheed Aircraft Services area at ONT is an example of construction technology considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, 1942–1975, and the sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features typical of aircraft hangars during the period of significance, including a barrel roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction. The hangar was used for aircraft maintenance and modification. Although Lockheed Aircraft Services no longer occupies the site, suggesting change in use, both the interior and exterior of the building retain all seven aspects of integrity. After careful consideration, ASM recommends Lockheed Hangar 2 eligible for listing at the federal, state, and local level under Criteria C/3 and Local Individual Criteria 3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

Aaron, Jayne. 2011. *Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War*. Prepared for the Department of Defense Legacy Resource Management Program.

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 2 *Resource Name or #: Lockheed Building 3
 P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport
 *P2. Location: Not for Publication Unrestricted
 *a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
 *b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____
 c. Address 1800 East Airport Drive City Ontario Zip 91761
 d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443342.80 mE/ 3769113.01 mN; _____
 e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____
 *P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 3 is a single-story industrial building part of the Lockheed Aircraft Services area at Ontario International Airport. It has an irregular plan set on a poured-concrete foundation. The flat roof is formed of corrugated metal covered with sheet asphalt and supported by steel truss framing. Walls are composed of concrete masonry units. The building adjoins Hangar 4 to the north and Hangar 2 to the south. Fenestration consists of two personnel doors and two metal freight doors on the east façade, and two personnel doors and a sliding metal freight door on the west façade. A shallow shed-roof canopy supported by metal pipe columns extends from the east façade. Each of the exposed façades has a fixed steel ladder for roof access. Building 3 functioned as a subassembly, sheet metal assembly, and paint shop (LAWA: ca. 1952 Dwg No. F001CFile0001). Alterations consist of building upgrades such as mechanical/electrical utility modifications, possible room partitions, and installation of sprinkler system, 1987-1990.¹

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building
 *P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
 P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)
View looking west at the east façade.

*P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
1952
Ontario International Airport Authority records

*P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive
 *P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
 *Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ Douglas, Diane L., and Livingston, David. 2006. *Historical Architectural and Cultural Resources Inventory Report: Pacific Gateway Cargo Center, Ontario International Airport*. Prepared by USR for Aero Ontario RFP, LLC. January 2006.



Image 1. View looking northwest at the east façade, with the south façade of Hangar 2 to the right.



Image 2. View looking east at the west façade.

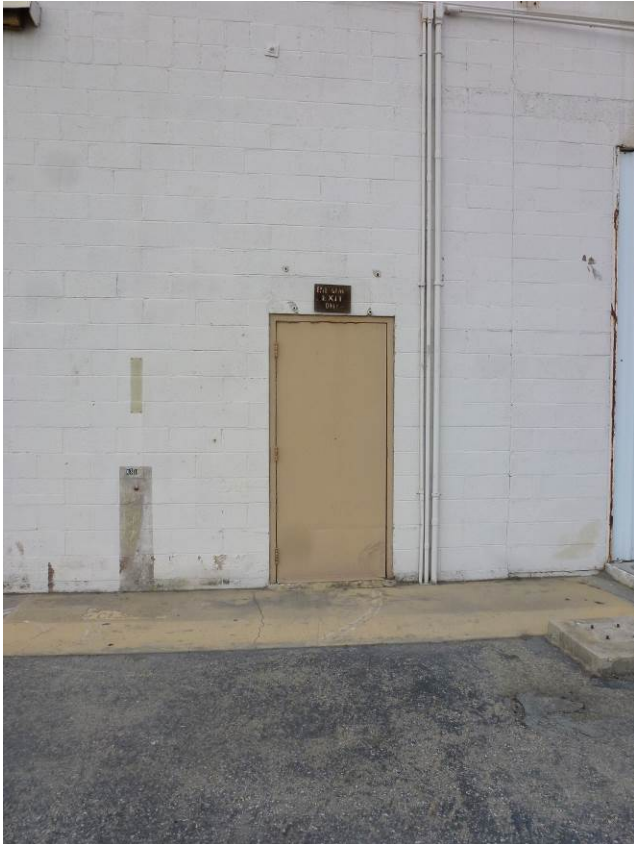


Image 3. Detail view looking west at the east façade.



Image 4. View of the interior looking southwest.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # 36-012630
 HRI # _____
 Trinomial _____
 NRHP Status Code 3B

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 13 *Resource Name or #: Terminal One

P1. Other Identifier: Terminal One, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W 1/4 of 1/4 of Sec S.B. B.M.

c. Address 1820-1822 E. Moore Way City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443937.59 mE/ 3769041.92 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.)

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Terminal One consists of a group of related buildings north of the primary runway of the Ontario International Airport (ONT). The original terminal building was designed by architects Jay Dewey Harnish and Eugene Weldon Fickes.¹ The terminal was built by Hoefer Construction Co. of Fontana.² Terminal One replaced the original passenger terminal located west of the control tower. Designed to be built in phases, the original configuration was constructed in 1959-1960. The terminal was expanded in 1964 to include a restaurant/coffee shop and cocktail lounge on the main floor, and additional ticketing space and restrooms.³ In 1969 the building was expanded toward the southwest, with porcelain enamel panels and glass to match the existing primary façade and the addition of a

(continued on page 9)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking east at the northwest façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1959-1960, 1964, 1969, 1975, 1977

Ontario International Airport Authority records (architectural drawings)

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Photograph Record Other (List):

¹ Architectural drawings. Ontario International Airport Authority records.

² "Ontario Airport's New Million-Dollar Bay." HCM archives. April 3, 1960. Ontario International Airport Authority records; architectural drawings dated July 1, 1958. A 1962 *Architectural Forum* article attributes the construction to Service Construction Co.

³ "Ontario Will Expand Terminal at Airport." *Los Angeles Times*, May 21, 1964.



Image 1. Detail view looking east at the northwest façade.



Image 2. Detail view looking northeast at the northwest façade.



Image 3. View looking south at the northeast and northwest façades.



Image 4. Interior view looking west.

Page 3 of 13
Recorded by: Shannon Davis and Marilyn Novell

*Resource Name or # (Assigned by recorder)

Terminal One

Date: December 2016



Image 5. Interior view of ticketing area looking northeast.

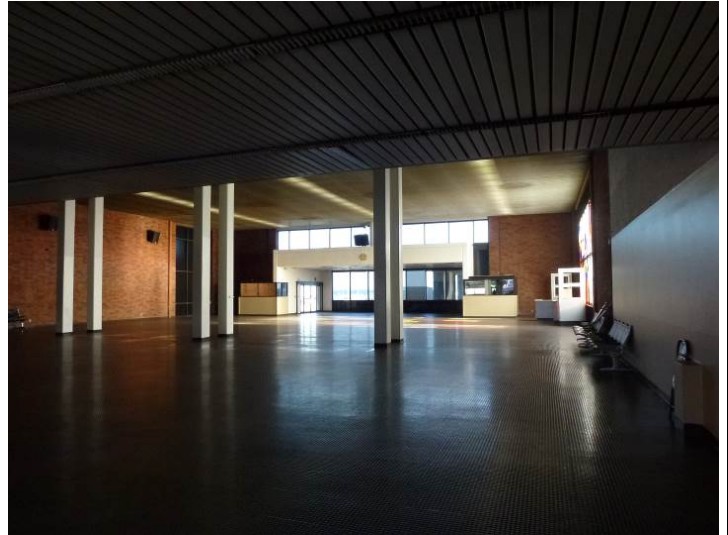


Image 6. Interior view of waiting room looking southeast.

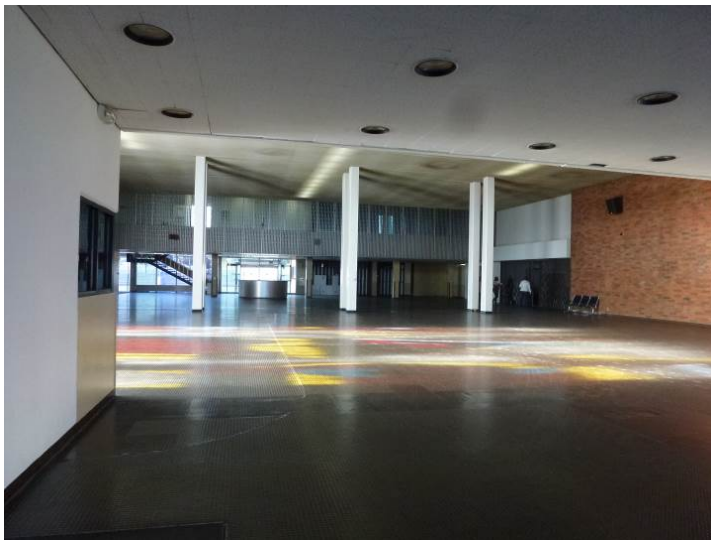


Image 7. Interior view of waiting room looking northwest.



Image 8. Interior view looking southwest.



Image 9. Interior view of second-floor offices.



Image 10. Interior view of second-floor conference room.



Image 11. View looking south of the passenger promenade southwest of the terminal.



Image 12. View looking east at the northwest and southwest façades of Baggage Claim Building B.



Image 13. View looking north at Baggage Claim Building B.



Image 14. Interior view of Baggage Claim Building B looking southwest.



Image 15. View looking northeast at the passenger arcade behind Terminal One. As a later addition to the terminal, this portion of the building does not contribute to the historical significance.



Image 16. View looking east at the northwest façade of one of the gate buildings. As a later addition to the terminal, this portion of the building does not contribute to the historical significance.



Image 17. View looking west from the runway area at the southeast and northeast façades of Terminal One.



Image 18. View looking northeast at the west façades of the Control Tower and ancillary buildings.



Image 19. View looking north from the runway area at the southwest and southeast façades of one of the holdroom (gate) buildings. As later additions to the terminal, these portions of the terminal do not contribute to the historical significance.



Image 20. View looking northwest at the south façade of the USO building (formerly the West Lobby).

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Recorded by: Shannon Davis and Marilyn Novell

*Resource Name or # (Assigned by recorder)

Terminal One Complex

Date: December 2016



Image 21. View of Terminal One in original configuration. Source: HCM brochure, undated; Ontario City Library Robert E. Ellingwood Model Colony Room.

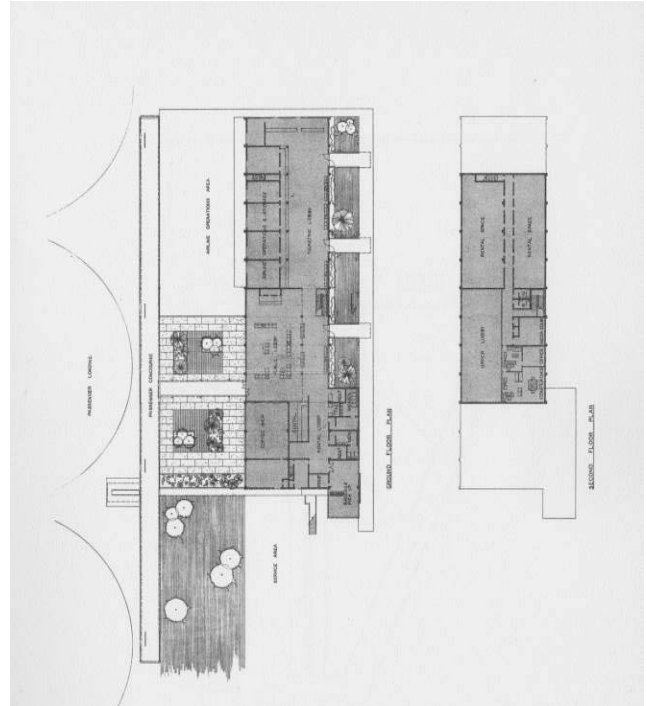


Image 22. Floor plan of Terminal One in original configuration. Source: HCM brochure, undated; Ontario City Library Robert E. Ellingwood Model Colony Room.



Image 23. Historic view of the primary façade. Source: HCM brochure, undated; Ontario City Library Robert E. Ellingwood Model Colony Room.



Image 24. View from the original lobby looking toward the entrance. Source: Los Angeles Public Library Herald-Examiner Collection, April 1, 1967.

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Recorded by: Shannon Davis and Marilyn Novell

*Resource Name or # (Assigned by recorder)

Terminal One

Date: December 2016



Image 25. Historic view of the ticketing hall. January 1968.



Image 26. Interior view of second-story offices. Source: HCM brochure, undated [in Model Colony Room clipping files].

***P3a. Description:** (continued from page 1)

projecting red-brick clad portion.⁴ Further additions included a gift shop in 1974. In 1973-1977, a major expansion was undertaken consisting of extension of the existing lobby toward the runways (the "East Lobby"), another wing to the southwest housing another lobby (the "west lobby," currently housing a USO facility), and a concourse at the runway side of the terminal.⁵ In 1983, the concourse was extended,⁶ and in 1993 new "holdrooms" (free-standing buildings housing gates) were added.⁷ In its current configuration, the 25,000-square-foot two-story terminal building includes a double-height waiting area and a hall for ticketing counters.⁸ The second level of the building housed airport offices.

Terminal One is a horizontally oriented Mid-Century Modern style building with a rectangular plan on a concrete foundation. The building consists of several discrete sections. A two-story section at the north end of the complex presents arriving passengers with a stark elevation faced with a broad expanse of decorative metal grille. A flat metal canopy connects the glass doors of the building to the traffic lanes. The building was included in a 1962 *Architectural Forum* article on Modern designs for international terminals and described as a fairly modest contribution to the genre:

"Ontario, Calif., Airport, by architects Harnish, Morgan & Causey, is a small, efficient flying facility which also pays some attention to architecture. The tall, two-story waiting room with ticket offices has the usual glass fronts facing the field and the approach road, but in this case they were handsome, glare-shielding grilles. California's climate permits open-air walkways to the loading stations. Pleasant planting has been started around them. General contractor for the terminal is Service Construction Co."⁹

Within the glass-walled single-story interior of the entrance is a suspended stairway leading to offices on the second level. Through a series of double columns, the entry opens up toward the back, forming an expansive two-story waiting room lit by a full-height window framing a graphic design composed of colored plastic panels and a regular arrangement of can lights in the ceiling. A single-story cafeteria/dining area is located to the east as passengers enter the waiting room. On the second level is a series of airport offices and conferences rooms with screened windows overlooking the waiting room or toward the front of the building.

In the southwest section of the building, a single-story ticketing hall is set back from the drive, where a series of metal arcades shelter arriving passengers. The primary/northwest façade is composed of glass and opaque turquoise panels set in floor-to-ceiling metal frames. The interior is carpeted, and the ceiling composed of metal panels interspersed with textured "popcorn"-style panels. A continuous row of counters and ticketing stations runs along the far wall. Behind the ticketing counters are doors providing access to offices and employee services.

From the terminal lobby, flat steel pedestrian arcades connect to a series of freestanding gate buildings (or holdrooms). Each gate building houses two holdrooms, with a single entrance on the terminal side and two separate entrances on the runway side. The gate buildings are constructed of concrete with deep stucco bulkheads and flat pilasters between rows of fixed-pane windows.

Baggage Claim Building A

Baggage Claim Building A is a horizontally oriented freestanding building on a concrete foundation located to the west of Terminal One. Modern in style, it is a red-brick-clad flat-roofed pavilion with a deep overhanging stucco-clad fascia supported by a series of smooth, round concrete columns. The primary façade is composed of partial walls and planters of varying sizes and heights. The sides of the building consist of red brick wing walls. Heavy metal beams span the interior ceiling, where curved steel baggage-handling conveyor systems continue through the walls to a loading area at the back of the building. Designed by architects Wolff Lang Christopher in 1984,¹⁰ Baggage Claim Building A does not meet the age criterion for consideration as a historic resource.

⁴ Ontario International Airport Authority records; architectural drawings by Harnish-Morgan and Causey, architects, dated April 2, 1969.

⁵ Ontario International Airport Authority records; architectural drawings by Cashion-Horie, architects, dated June 27, 1973.

⁶ Ontario International Airport Authority records; architectural drawings by Rivers & Christian, architects, dated March 16, 1993.

⁷ Ontario International Airport Authority records; architectural drawings by Barkmakian, Wolff, Lang, and Christopher, architects, dated June 27, 1973.

⁸ "Director of Ontario Airport Sees Bright Future for Facility." *Los Angeles Times*, December 18, 1960.

⁹ "New Airport Design, in the U.S. and Abroad." *Architectural Forum*, July 1962.

¹⁰ Ontario International Airport Authority records; architectural drawings by Wolff, Lang, and Christopher, architects, dated March 9, 1984.

Primary # _____
HRI # _____
Trinomial _____

Page 10 of 13 *Resource Name or # (Assigned by recorder) Terminal One
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

Continuation Update

Baggage Claim Building B

Baggage Claim Building B is a freestanding horizontally oriented Mid-Century Modern style pavilion set on a concrete foundation located northeast of Terminal One. The walls of the primary façade are clad in red brick, and at the back of the building is a concrete block screen. The ceiling is composed of three poured-concrete tent-like roofs, each supported by a square column within the pavilion. As the walls at the primary façade fall short of meeting the ceiling, the ceiling gives the impression of floating over the structure. Curved steel baggage-handling systems within the building continue through the back wall to a loading area. Although no original architectural plans were identified for this baggage claim building, it appears on historic aerial photos from 1959, indicating it was built concurrently with the original terminal building.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 11 of 13

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Terminal One

B1. Historic Name: _____

B2. Common Name: Terminal One, Ontario International Airport

B3. Original Use: Airport terminal

B4. Present Use: N/A

*B5. Architectural Style: Mid-Century Modern

*B6. Construction History: (Construction date, alterations, and date of alterations) 1959-1977

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: _____

B9a. Architect: Jay Dewey Harnish and Eugene Weldon Fickes

b. Builder: Hoefler Construction Co. of Fontana

*B10. Significance: Theme Civil Aviation; Aviation and Architecture

Area: Early Passenger Travel; Modernism and Aviation

Period of Significance: 1959-1967 and 1959-1977

Property Type: Control tower Applicable Criteria: NRHP Criteria A/1, CRHR Criteria C/3, and Local Individual Criteria 3 a-d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Terminal One was evaluated under the context of Aviation in Ontario; theme Civil Aviation; sub-theme Early Passenger Travel, 1950-1967 according to the guidelines established in the *Ontario International Airport Historic Context Statement*, prepared by ASM Affiliates, Inc., for the City of Ontario. Eligible properties under this sub-theme include buildings associated with early passenger travel that were present during the period of significance. Terminal One displays character-defining features of the property type including 2 stories in height, horizontal massing, ticketing and baggage services, double-height lobby, and its location near loading zones, runways, and taxiways. For these reasons, ASM recommends Terminal One and early additions eligible for association with Early Passenger Travel at ONT under Criteria A/1 and local Individual Criteria 3 a-b.

(Continued on page 12)

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: December 2016

(This space is reserved for official comments)

Sketch Map with north arrow required.



CONTINUATION SHEET

Primary # _____

HRI # _____

Trinomial _____

Page 12 of 13

*Resource Name or # (Assigned by recorder)

Terminal One

Recorded by: Shannon Davis and Marilyn Novell

Date: December 2016

Continuation Update

B10. Significance: (Continued from page 11)

Terminal One was also evaluated under the theme of Architecture and Aviation; sub-theme Modernism and Aviation, 1955-1970. The original section of Terminal One was constructed during the period of significance and retains the essential aspects of integrity. It is a property type associated with this sub-theme and displays character-defining features of Mid-Century Modernism popular at the time of initial construction, including its horizontal orientation and minimal ornamentation, flat roof, wide expanses of glazing, simple geometric forms, and a strong connection between the interior and exterior. Of the multiple additions to the terminal that occurred after original construction of the core terminal building, the extension of the façade and ticketing hall to the southwest took place in 1969, within the period of significance for Modernism and Aviation, which ends in 1970. This addition emulates the existing façade. However, two major additions were planned beginning in 1973 and completed in 1977: a large extension of the original lobby toward the runway (the East Lobby) and the addition of a lobby to the southwest (the West Lobby). A gift shop was also added in 1975, which was incorporated into the existing wing to the northeast by continuing an existing brick-clad bump-out along the primary façade to the right of the entrances. Although designed slightly later than the period of significance for this theme as defined in the Historic Context Statement, these important additions are included as eligible elements of Terminal One because (a) the Terminal One building core was originally designed for future expansion, (b) the additions echo the materials and design of the original terminal, and while clearly differentiated, are fully integrated with the original building, and (c) these elements of the building reflect the character-defining features of Modernism even though designed/constructed as this style was waning in popularity. Several more utilitarian additions occurred in the 1980s and 1990s that do not respond to the Mid-Century Modern style of the earlier parts of the terminal. Terminal One is a good representation of the Modern style on the local level and is among the most recognizable buildings designed by an important local architect. It retains all seven aspects of integrity. After careful consideration, ASM recommends Terminal One and early additions individually eligible under the theme of Modernism and Aviation at ONT for Criteria C/3 and local Individual Criteria 3 c-d, f-h.



Map showing location of buildings within the Terminal One area, including contributors to the recommended historic district, resources surveyed, and contributors.
Source: ASM Affiliates, Inc., June 2017.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3B

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 4

*Resource Name or #: Lockheed Hangar 4

P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443341.43 mE/ 3769075.90 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Hangar 4, part of the Lockheed Aircraft Services Area at Ontario International Airport, is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in corrugated metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. At the east and west ends are sets of telescoping metal doors that run on steel tracks, allowing them to slide fully into a housing apparatus that extends beyond the mass of the building. Above and at the center of each set of doors is a retractable corrugated metal tail door. A gabled extension at the east façade appears to be a later addition. Additional fenestration consists of louvered openings arranged in horizontal and vertical banks on the hangar doors, and metal personnel doors. The interior of the hangar is a single open space with office areas constructed of plywood along the sides. Hangar 4 is connected to Building 3 on the south and Building 5 on the north. The building was used for aircraft maintenance and modification, with office space on a second level.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

*P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking west at the east façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1953

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by
ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking southeast at the west façade.



Image 2. View looking southeast at the north and west façades



Image 3. View looking northwest at the east façade.



Image 4. Interior view looking northeast.



Image 5. View from the interior looking east.



Image 6. Detail view looking east at west façade.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 4

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Lockheed Hangar 4

B1. Historic Name: Lockheed Hangar 4

B2. Common Name: _____

B3. Original Use: Hangar

B4. Present Use: Hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1953

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron, workshops

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture

Area: Developments in Construction Technology

Period of Significance: 1955-1975

Property Type: Aircraft hangar

Applicable Criteria: NRHP Criterion C, CRHR Criterion 3, and Local Individual Criteria 3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Hangar 4 in the Lockheed Aircraft Services area at ONT is an example of construction technology considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, 1942–1975, and the sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features typical of aircraft hangars during the period of significance, including a barrel roof, a multi-leaved hangar door and tail cut at each end, and a large open space to accommodate aircraft enabled by steel truss construction. At the east façade, the doors retract into a gabled-roof structure; at the west façade, the barrel roof is visible and the doors travel on rails outside the main mass of the building to open. The hangar was used for aircraft maintenance and modification. Although Lockheed Aircraft Services no longer occupies the site, suggesting change in use, both the interior and exterior of the building retain all seven aspects of integrity. After careful consideration, ASM recommends Lockheed Hangar 4 eligible for listing at the federal, state, and local level under Criteria C/3 and Local Individual Criteria 3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

Aaron, Jayne. 2011. *Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War*. Prepared for the Department of Defense Legacy Resource Management Program.

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

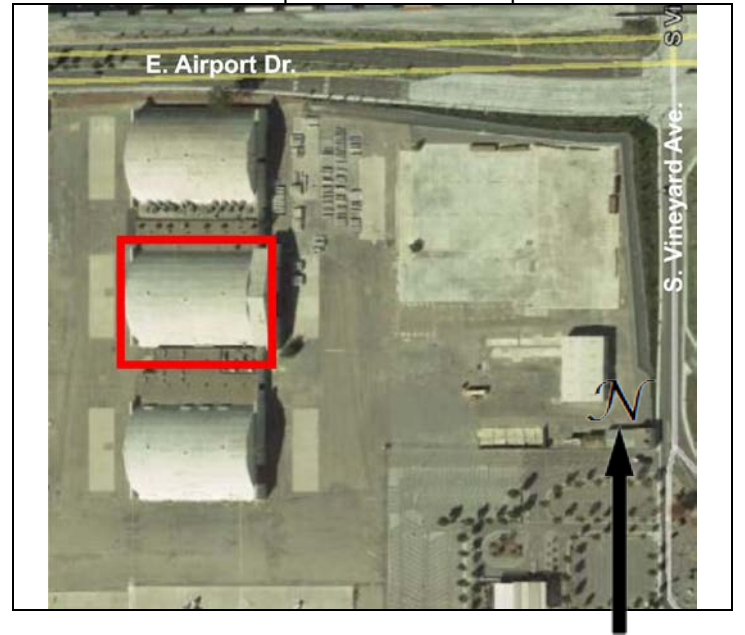
B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2

*Resource Name or #: Lockheed Building 5

P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443340.13 mE/ 3769028.61 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 5, part of the Lockheed Aircraft Services area at Ontario International Airport, provided electrical support for adjoining hangars 4 and 6.¹ It is an industrial building with a rectangular plan set on a poured-concrete foundation. The flat roof is covered with sheet asphalt. Walls are composed of concrete masonry units. Fenestration consists of a set of double doors and a sliding metal freight door on the east façade, and a single door and a set of double doors on the west façade. A flat-roofed cantilevered canopy supported by steel L beams extends across the west façade, sheltering a set of double metal doors and a single personnel door. The interior was not accessible at the time of survey.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking west at the east façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1955

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ Douglas, Diane L., and Livingston, David. 2006. *Historical Architectural and Cultural Resources Inventory Report: Pacific Gateway Cargo Center, Ontario International Airport*. Prepared by URS for Aero Ontario RFP, LLC. January 2006.



Image 1. View looking northeast at the west façade.

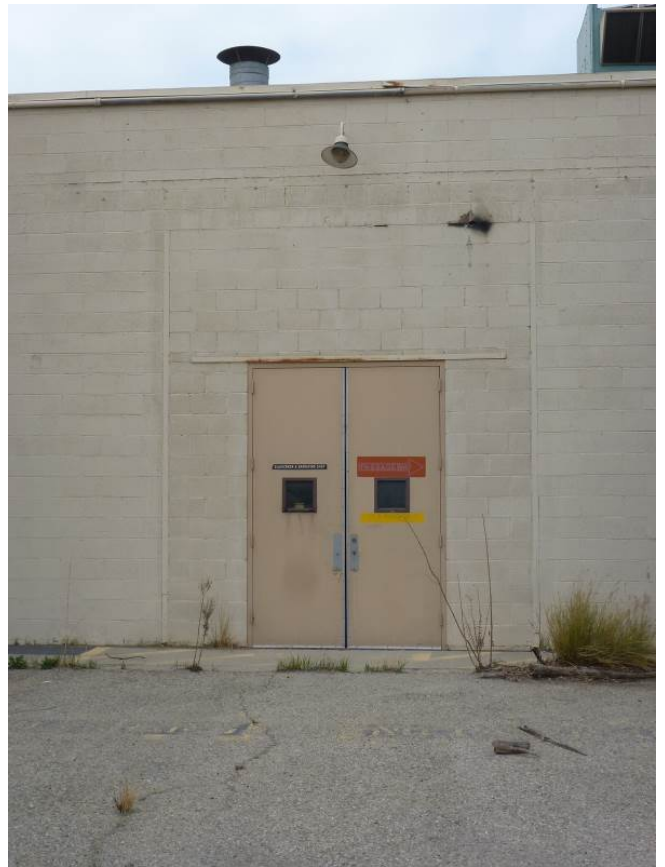


Image 2. Detail view looking west at the east façade.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3B

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 4

*Resource Name or #: Lockheed Hangar 6

P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443411.95 mE/ 3769033.71 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Hangar 6, part of the Lockheed Aircraft Services (LAS) area at Ontario International Airport, is located in the northeastern portion of the former LAS facility north of Hangar 4. Hangar 6 is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in corrugated metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. At the east and west ends are sets of telescoping metal doors that run on steel tracks, allowing them to slide fully into a housing apparatus that extends beyond the mass of the building. Above and at the center of each set of doors is a retractable corrugated metal tail door. Additional fenestration consists of metal personnel doors. Hangar 6 is connected to Building 5 on the south. The interior of the hangar was converted to a multi-story office space in 1988. The building was used for aircraft maintenance and modification.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking east at the west façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1955

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking southeast at the north and west façades



Image 2. View looking northeast at the south and west façades.



Image 3. Detail view looking southwest at the east façade.



Image 4. View of the interior looking west.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 3 of 4

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Lockheed Hangar 6

B1. Historic Name: Lockheed Hangar 6

B2. Common Name: _____

B3. Original Use: Hangar

B4. Present Use: Hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1955

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron, workshops

B9a. Architect: Unknown

b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture

Area: Developments in Construction Technology

Period of Significance: 1955-1975

Property Type: Aircraft hangar

Applicable Criteria: NRHP Criterion C, CRHR Criterion 3, and Local Individual Criteria 3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

Hangar 6 in the Lockheed Aircraft Services area at ONT is an example of construction technology considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, 1942–1975, and the sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features typical of aircraft hangars during the period of significance, including a barrel roof, a multi-leaved hangar door and tail cut at each end, and a large open space to accommodate aircraft enabled by steel truss construction. The east and west façades each have entrances with telescoping doors. The hangar was used for aircraft maintenance and modification. Although Lockheed Aircraft Services no longer occupies the site, suggesting change in use, the exterior displays all seven aspects of integrity. A two-story office building has been constructed inside the building, apparently without disturbing the materials or structure of the hangar. After careful consideration, ASM recommends Lockheed Hangar 6 eligible for listing at the federal, state, and local level under Criteria C/3 and Local Individual Criteria 3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

***B12. References:**

Aaron, Jayne. 2011. *Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War*. Prepared for the DoD Legacy Resource Management Program.

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3B

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 7

*Resource Name or #: Lockheed Executive Office Building (Building 10)

P1. Other Identifier: Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443197.77 mE/ 3769174.38 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 10, part of the Lockheed Aircraft Services area at Ontario International Airport, is a Mid-Century Modern style administrative/executive office building. It was designed by architect George Vernon Russell and built by Pozzo Construction Co. It was said to have embodied "New concepts of structural design, sun protection devices, and use of colors not common in the industrial field."¹ Exterior enameled metal panels were colored in "the vivid red, white, and blue of the corporation's trademark, with contrasts of textured gray walls and the bluish-green tint of glare-reducing glass."²

(continued on page 5)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking northeast at the west south façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1956

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ "Lockheed Dedicates New Ontario Airport Facilities." *Los Angeles Times*, July 1, 1956.

² *Ibid.*



Image 1. View looking southeast at the north and west façades.



Image 2. View looking east at the west façade.



Image 3. Detail view looking east at the west façade.



Image 4. View looking northwest at the east façade of Building 11 and the south façade of Building 10.

Page 3 of 7 *Resource Name or # (Assigned by recorder) Lockheed Executive Office Building (Building 10)
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016



Image 5. Detail view looking northeast at the south façade.



Image 6. Detail view of the south façade.



Image 7. Detail view looking east at the primary entrance on the west façade.



Image 8. View of the first-floor interior looking southeast.

Page 4 of 7 *Resource Name or # (Assigned by recorder) Lockheed Executive Office Building (Building 10)
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016



Image 9. View looking northeast at the south façade, ca. 1956.
Source: Collection of Colin Russell.



Image 10. View looking northeast at the west and south façades, ca. 1956. Source: Collection of Colin Russell.



Image 11. View looking southeast at the north and west façades, ca. 1956. Source: Collection of Colin Russell.



Image 12. View looking northeast at the west and south façades, ca. 1956. Source: Collection of Colin Russell.



Image 13. An architect's rendering of Building 10 (in back), Cafeteria (Building 11), and Mail Room (Building 12), n.d. Source: Model Colony Room photos.



Image 14. Lockheed executive office building primary entrance, ca 1967. Source: Ontario Chamber of Commerce brochure, in Model Colony Room clipping files.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 6 of 7

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Lockheed Executive Office Building (Building 10)

B1. Historic Name: Executive Office Building

B2. Common Name: _____

B3. Original Use: Office and Production Headquarters

B4. Present Use: _____

*B5. Architectural Style: Mid-Century Modern

*B6. Construction History: (Construction date, alterations, and date of alterations) 1956

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: _____

B9a. Architect: George Vernon Russell b. Builder: Pozzo Construction Co.

*B10. Significance: Theme Aviation and Architecture Area: Modernism and Aviation

Period of Significance: 1955-1970

Property Type: Corporate offices
Applicable Criteria: NRHP Criterion C, CRHR Criterion 3, and Local Individual Criteria 3 c-d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Executive Office Building in the Lockheed Aircraft Services (LAS) area at ONT served as corporate headquarters for the LAS division of Lockheed. The building is a good example of Mid-Century Modernism considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, and the sub-theme of Modernism in Architecture, 1942–1970. It exhibits character-defining features of the style including horizontal orientation, minimal ornamentation, a flat roof with wide overhanging eaves, and simple, orthogonal massing. In the prominent vertical exterior supports and connected brise-soleils that suggest an exoskeleton, it also displays direct expression of the structural system and function. Architect George Vernon Russell studied at the estimable Ecole des Beaux-Arts in

(Continued on page 7)

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

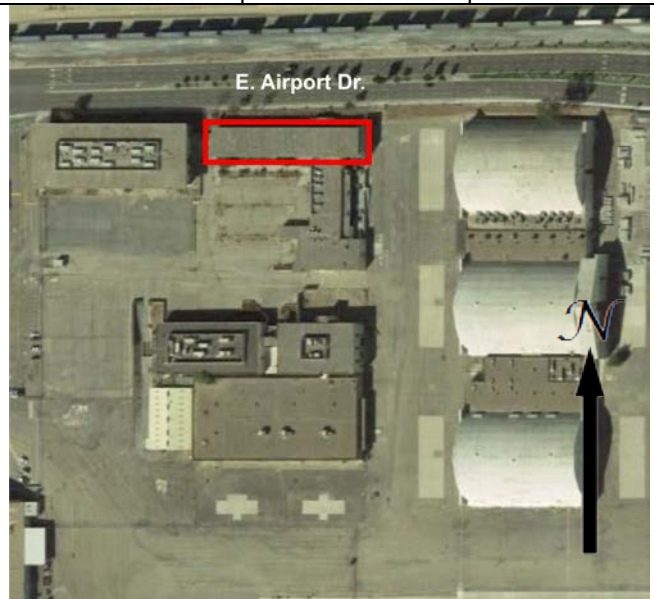
B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3B

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 5

*Resource Name or #: Lockheed Cafeteria Building (Building 11)

P1. Other Identifier: Cafeteria, Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443197.77 mE/ 3769174.38 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 11, part of the Lockheed Aircraft Services area at Ontario International Airport, is a single-story cafeteria constructed in 1956 in the Mid-Century Modern style. Designed by architect George Vernon Russell and built by Pozzo Construction Co., the cafeteria was part of a complex including adjacent buildings 10 and 12. Building 11 is a steel-frame cafeteria building with a generally rectangular plan that adjoins Building 10 at the north and Building 12 at the south. The horizontally oriented flat-roofed building is set on a poured-concrete foundation. Fenestration at the west façade consists of a high row of horizontal vents running the length of the façade, with no windows or doors. At the primary (east) façade, a wide concrete dining terrace extends across the space created by the setback between buildings 12 and 10. The entrance is recessed beneath a deep canopy. The façade consists of continuous rows of floor-to-ceiling windows set in projecting vertical dividers and interspersed with metal-framed glass doors. Ornamentation includes three sets of vertical wood screens that continue at a right angle across a cutout in the canopy. Square planters with attached benches are dispersed across the patio, which is paved in square concrete tiles and originally accommodated tables and seating. The interior includes a large open space with an open steel-truss beam ceiling and lower soffits, below which are arrays of metal-clad cafeteria counters and series of can lights. The walls are clad in drywall and floors are concrete.

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking southwest at the east façade of Building 11 and the north façade of Building 12.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1956

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking northwest at the west and east façades of buildings 12, 11, and 10.



Image 2. Detail view looking northwest at the east façade of Building 11 and the south façade of Building 10.



Image 3. Detail view looking southeast from Building 11.



Image 4. Interior view looking southwest.

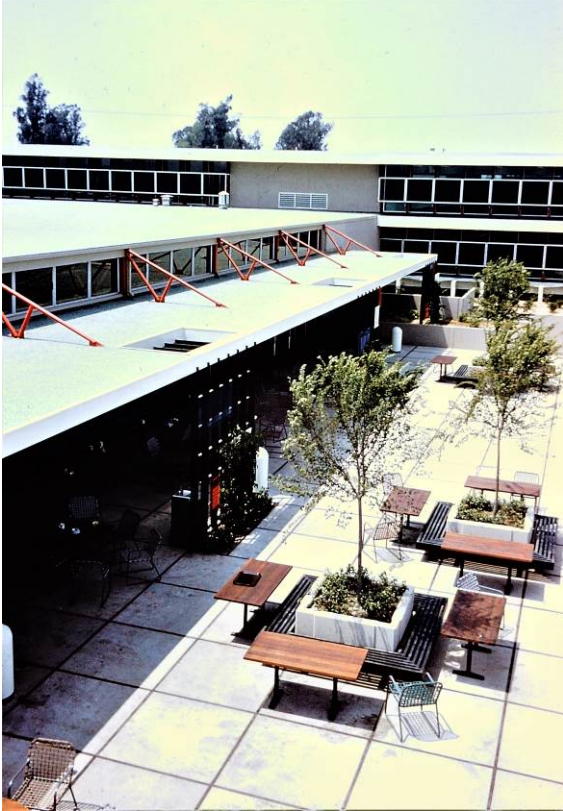


Image 5. View looking northwest at Building 11 (Building 10 in background), ca. 1956. Source: Collection of Colin Russell.



Image 6. Interior view looking northeast, ca. 1956. Source: Collection of Colin Russell.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 4 of 5

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Lockheed Cafeteria Building (Building 11)

B1. Historic Name: _____

B2. Common Name: _____

B3. Original Use: Cafeteria

B4. Present Use: _____

*B5. Architectural Style: Mid-Century Modern

*B6. Construction History: (Construction date, alterations, and date of alterations) 1956

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: _____

B9a. Architect: George Vernon Russell b. Builder: Pozzo Construction Co.

*B10. Significance: Theme Aviation and Architecture Area: Modernism and Aviation

Period of Significance: 1955-1970 Property Type: Corporate offices Applicable Criteria: NRHP Criterion C, CRHR Criterion 3, and Local Individual Criteria 3 c-d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Cafeteria building in the Lockheed Aircraft Services (LAS) area at ONT served the employees and personnel at the LAS facilities. The building is a good example of Mid-Century Modernism considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, and the sub-theme of Modernism in Architecture, 1942–1970. It exhibits character-defining features of the style including horizontal orientation, minimal ornamentation, a flat roof with wide overhanging eaves, and simple, orthogonal massing. The large outdoor dining patio with planters surrounded by fixed tables and benches is connected visually and functionally by the floor-to-ceiling glazing of the cafeteria interior and the wide cantilevered canopy. The distinctive feature of Mid-Century Modern architecture of connection

(Continued on page 5)

B11. Additional Resource Attributes: (List attributes and codes) _____

*B12. References:

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

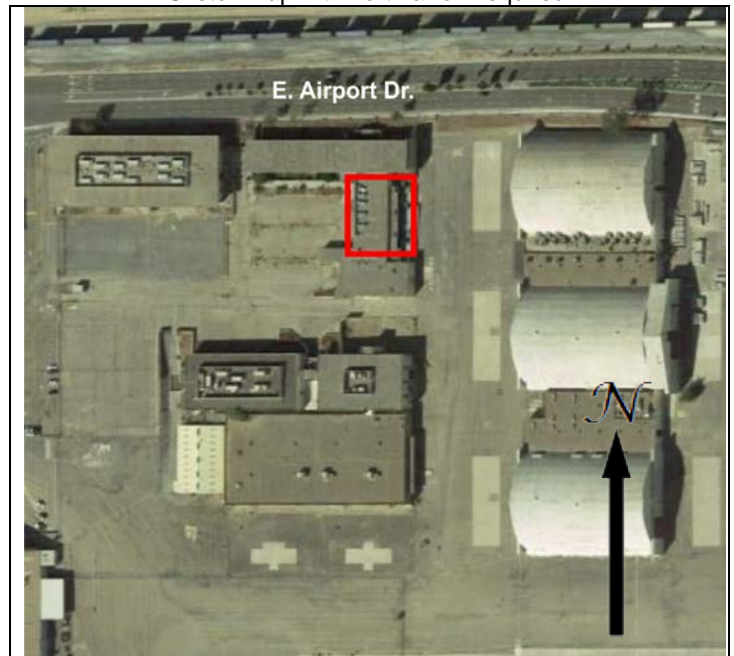
B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

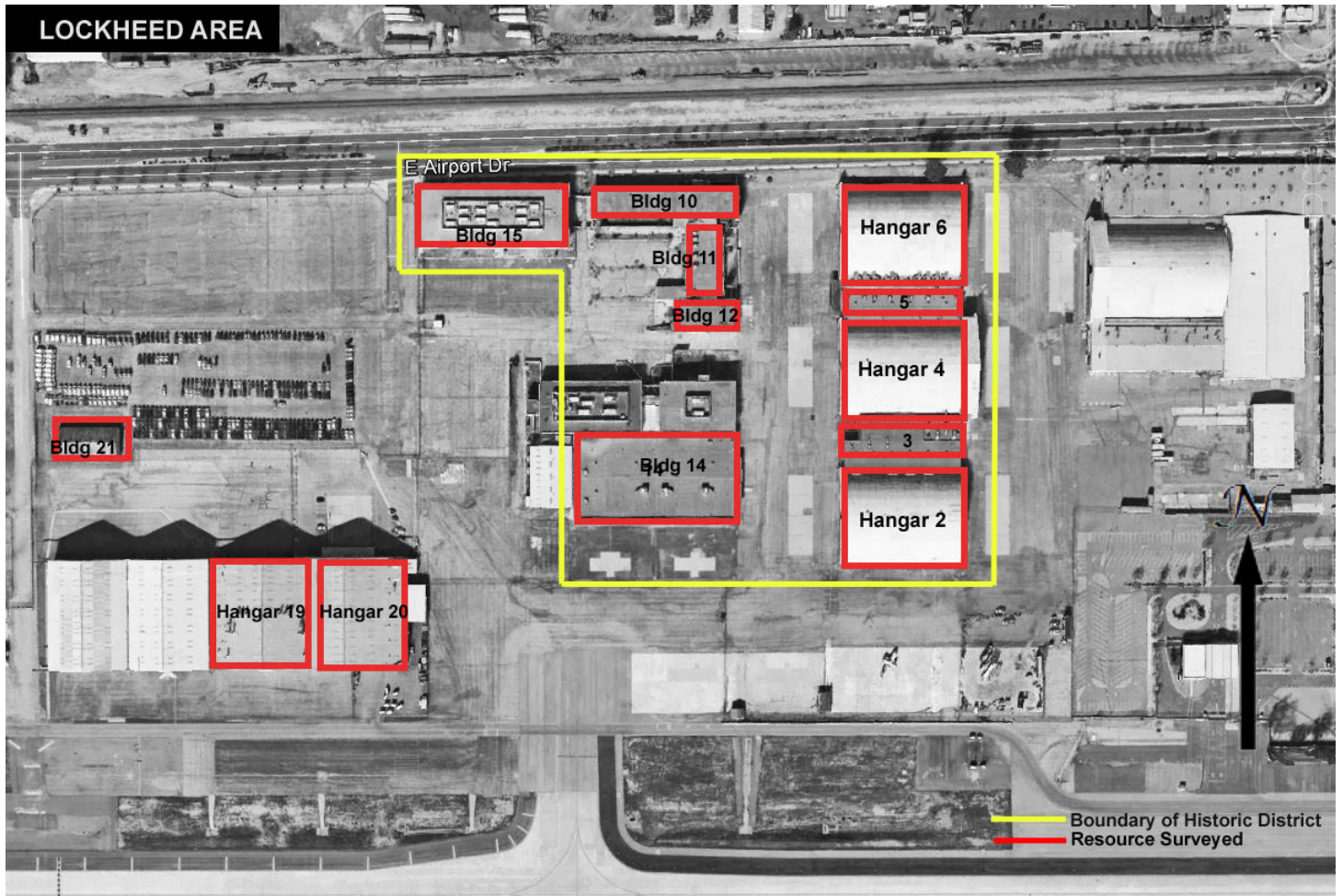
Sketch Map with north arrow required.



Page 5 of 5 *Resource Name or # (Assigned by recorder) Lockheed Cafeteria Building (Building 11)
 Recorded by: Shannon Davis and Marilyn Novell Date: December 2016
 Continuation Update

B10. Significance: (Continued from page 4)

between the indoors is apparent in this design. Architect George Vernon Russell studied at the estimable Ecole des Beaux-Arts in France and experienced a long and prolific career. Among his well-known works are the Flamingo Hotel in Las Vegas, Sunset Plaza in West Hollywood, Ciro's Restaurant, and the iconic Deco-style Hollywood Reporter building. He became a fellow of the American Institute of Architects and served as president of its Southern California chapter. Although the building has deteriorated and lost some of its original materials, it retains integrity of location, design, setting, workmanship, feeling, and association. The building meets several of the requirements for significance under Criterion C: it embodies the distinctive characteristics of a type and period, it possesses high artistic value, and it can be considered the work of a master architect. After careful consideration, ASM recommends the Lockheed Cafeteria building eligible for listing at the federal, state, and local level under Criteria C/3 and Local Individual Criteria 3 c-d, f-h.



Map showing location of buildings within the Lockheed Aircraft Services area.
 Source: ASM Affiliates, Inc., June 2017.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2 *Resource Name or #: Lockheed Mail Room (Building 12)
P1. Other Identifier: Shipping and Receiving, Mail Room, Lockheed Aircraft Services Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____

c. Address 1800 East Airport Drive City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443197.77 mE/ 3769174.38 mN; _____

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 12, part of the Lockheed Aircraft Services area at Ontario International Airport, adjoins the Cafeteria (Building 11) to the north. It served as the shipping and receiving area, the mailroom, and a warehouse. It is a single-story building with a recessed loading dock and ramp that are partially below grade and accessed from the west. The shipping and receiving facility is a flat-roof building constructed of concrete tilt-up panels. It has an irregular plan with a wing extending to the west. Fenestration consists of large freight bays at the loading docks and at the south and east facades and several personnel doors. A control room with fixed-pane windows is located to the south of the loading dock.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking northeast at the west and south façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1956

Los Angeles World Airports records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking northwest at the south and east façades.



Image 2. View looking east at the west façade.



Image 3. From the interior looking northwest.



Image 4. View looking northeast at the west and south façades, ca. 1956. Source: Collection of Colin Russell.

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 2 *Resource Name or #: Lockheed Warehouse (Building 14)
P1. Other Identifier: Spare parts warehouse, Lockheed Aircraft Services Area, Ontario International Airport
*P2. Location: Not for Publication Unrestricted
*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____
c. Address 1800 East Airport Drive City Ontario Zip 91761
d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443189.91 mE/ 3769011.26 mN; _____
e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 14, part of the Lockheed Aircraft Services (LAS) area at Ontario International Airport, is a single-story industrial building that served LAS as a spare parts warehouse. It is a flat-roof building with a rectangular plan set on a poured-concrete foundation. The primary (east) facade is constructed of tilt-up panels faced with aggregate stone interspersed with smooth vertical concrete dividers, echoing the surfaces on buildings 10 and 11 to the north. At the primary façade, a flat canopy shelters two vehicle bay doors and two personnel doors. The other façades are smooth tilt-up concrete. Other than the doors at the primary façade, the building lacks fenestration. The interior appeared to consist of one open warehouse space, although because of lack of lighting it was not photographable.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building
*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)
View looking northwest at the south and east façades.

*P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
1967
Los Angeles World Airports records
*P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761
*P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking west at the east façade of Building 14 (left).



Image 2. Detail view of aggregate concrete panel.

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 5 *Resource Name or #: Lockheed Office Building (Building 15)
 P1. Other Identifier: Office Building, Lockheed Aircraft Services Area, Ontario International Airport
 *P2. Location: Not for Publication Unrestricted
 *a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
 *b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____
 c. Address 1800 East Airport Drive City Ontario Zip 91761
 d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443091.81 mE/ 3769169.40 mN;
 e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____
 *P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

Building 15, part of the Lockheed Aircraft Services Area at Ontario International Airport, is a two-story office building designed by architect J. Dewey Harnish in the Mid-Century Modern style. A company brochure describes it as "crisp, straightforward design, with precast concrete walls and columns providing texture and shadow [that] give this low-profile building an easy grace."¹ It is a steel-frame building with a rectangular plan set on a poured-concrete foundation. It has a flat roof and a wide overhang with a deep painted concrete fascia with vertical scoring that encircles the building. The building is clad in smooth concrete with a series of regularly spaced projecting vertical members marking the locations of windows and doors. At the primary (south) façade, a flat canopy with rectangular concrete supports extends from the entrance, accessed by a short flight of brick-lined steps. Wide planters span the east façade, retained by a row of aggregate concrete panels. The landscaping plan on the south façade formerly included of a row of evenly spaced trees, which have been removed. The entrance is a set of metal and glass doors set in a wall of glass. In addition to the entrance, fenestration consists of regularly spaced vertical columns of fixed-pane glass on all facades, a secondary entrance of glass and metal

(continued on page 2)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building
 *P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
 P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)
View looking north at the south façade.

*P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
1968
Los Angeles World Airports records

*P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive
 *P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
 *Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ HCM company promotional brochure. HCM archives.

Page 2 of 5 *Resource Name or # (Assigned by recorder) Lockheed Office Building (Building 15)
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

***P3a. Description:** (continued from page 1)

doors set in a wall of glass at the west façade, and flat metal doors on the east façade. At the interior is a foyer paved in ceramic tiles that extend to the exterior and wood-paneled walls. At each level, offices open off of a central hall. At the second level is a large unpartitioned office space. Flooring in the offices and halls is carpet, and walls are plaster. The ceiling is composed drywall and acoustic tile.



Image 1. View looking southeast at the north and west façades.



Image 2. View looking northeast at the west and south façades.



Image 3. Detail view looking northwest at the south and east façades.



Image 4. Detail view looking northeast at the south façade.



Image 5. Detail view of concrete fascia on south façade.



Image 6. Detail view looking northeast at entrance steps.



Image 7. View of interior foyer looking north.



Image 8. Interior view of second floor looking southwest.

Page 4 of 5 *Resource Name or # (Assigned by recorder) Lockheed Office Building (Building 15)
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

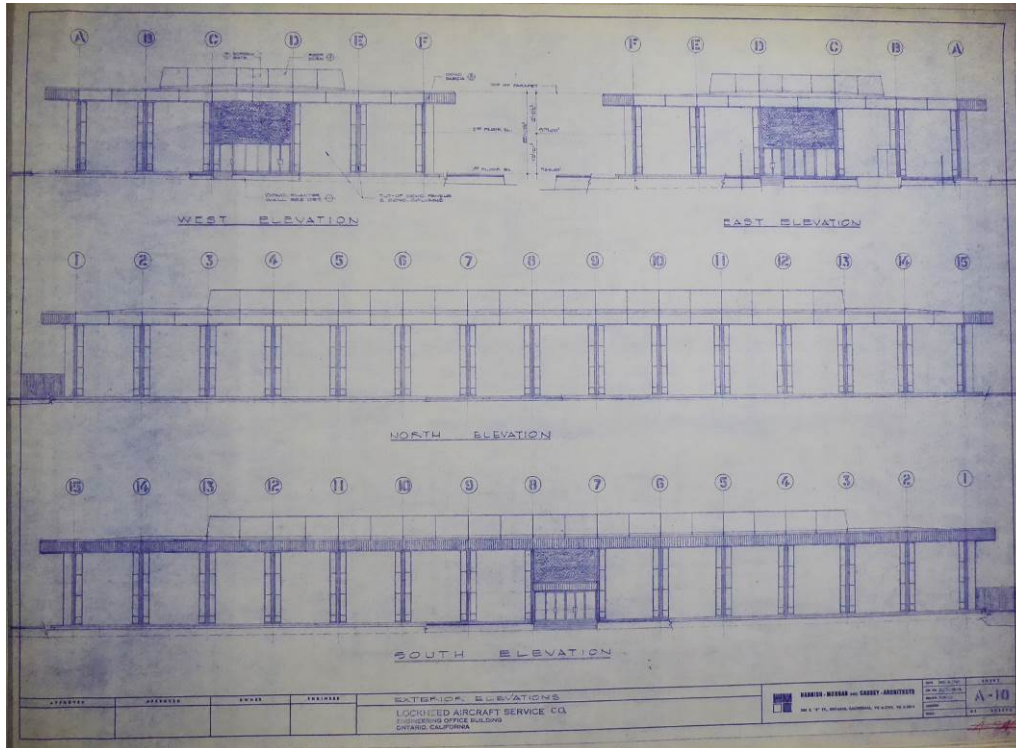


Image 9. Elevations. Architect's drawing, 1967. Source: LAWA records.

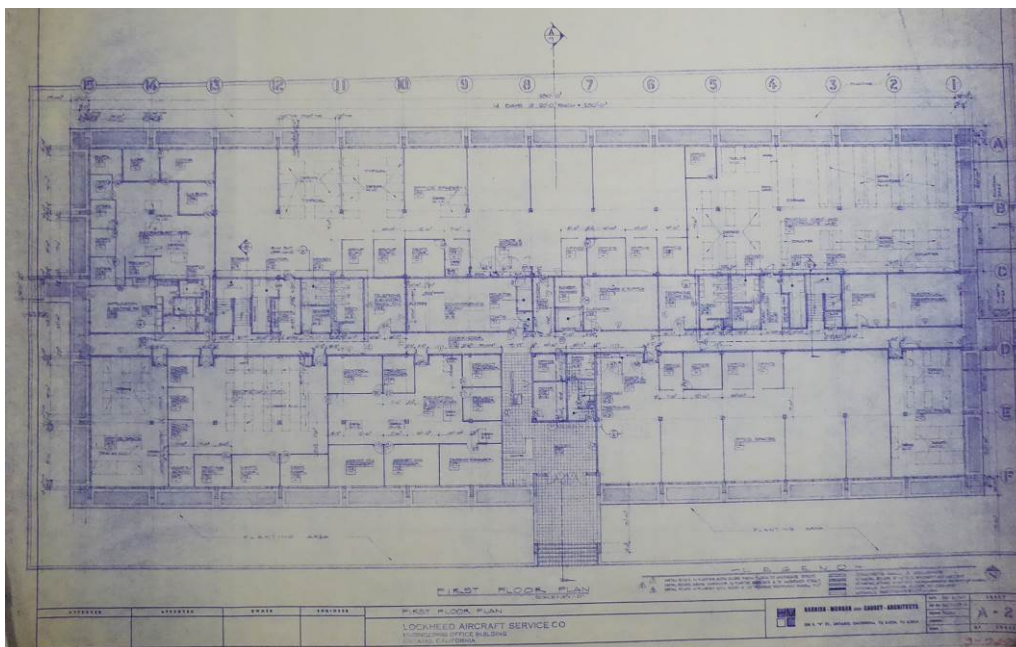


Image 10. First floor plan. Architect's drawing, 1967. Source: LAWA records.

Page 5 of 5 *Resource Name or # (Assigned by recorder) Lockheed Office Building (Building 15)
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

Continuation Update

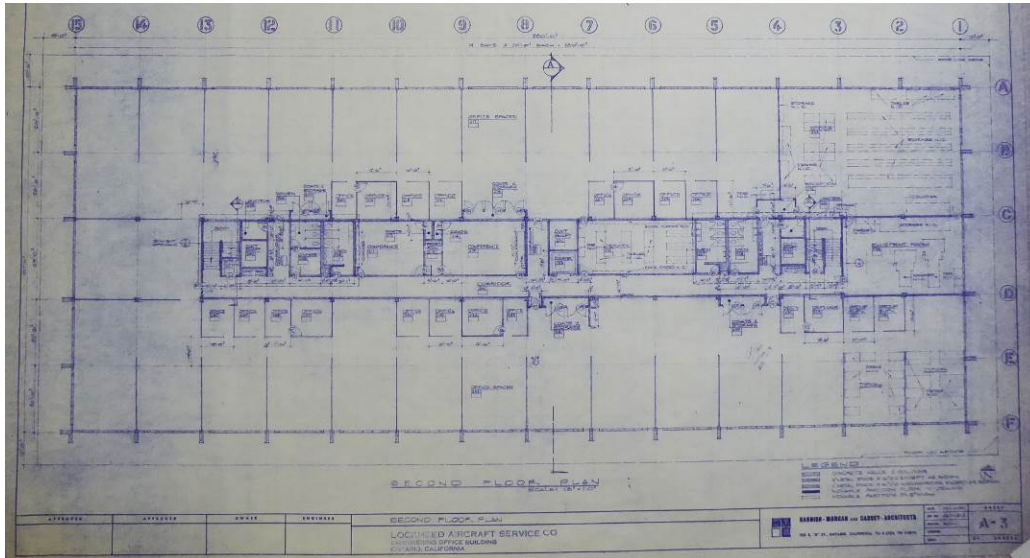


Image 11. Second floor plan. Architect's drawing, 1967. Source: LAWA records.

Page 1 of 7

*Resource Name or #: General Electric Aircraft Engines Historic District

D1. Historic Name: General Electric Aircraft Engines

D2. Common Name: General Electric Aircraft Engines

*D3. Detailed Description: (Describe overall coherence of the district, its setting, visual characteristics, and minor features. List all elements of district.)

The district is a large complex within the former property of General Electric Aircraft Engines, which operated at Ontario International Airport from 1956 to 2010, providing aircraft maintenance facilities, as well as jet engine testing at a nearby site. In addition to three imposing barrel-roofed hangars and three metal gable-roofed hangars, the complex includes multiple utilitarian single-story buildings associated with the hangars. These buildings housed offices, commissary services, and all of the activities required for a self-contained industrial facility. The main facility is adjacent to airport runways to the north and a railroad to the south. Only the hangars are recommended contributors to the historic district, as those are the buildings where aircraft modifications, repair, and/or testing was performed.

*D4. Boundary Description: (Describe limits of district and attach map showing boundary and district elements.)

The historic district is within the boundaries of the Ontario International Airport in Ontario, California, on the south side of the airport property on East Avion Street just north of East Mission Boulevard. A secondary non-contiguous area, the GE Jet Engine Test facility, is located southeast of the main plant.

*D5. Boundary Justification:

The boundary of the General Electric Aircraft Engines Historic District encompasses the historic boundary of the facility.

D6. Significance: Theme Commercial Aviation

Area Aviation Support Services

Period of Significance 1952-1967

Applicable Criteria NRHP Criterion A, CRHR Criterion 1, and
Local District Criteria 1-3

(Discuss district's importance in terms of its context as defined by theme, period of significance, and geographic scope. Also address the integrity of the district as a whole.)

The General Electric (GE) Aircraft Engines Historic District was evaluated under the context of Aviation in Ontario; theme Commercial Aviation, 1946-1967; and sub-theme Aviation Support Services, 1952-1967, according to the guidelines established in the *Ontario International Airport Historic Context Statement*, prepared by ASM Affiliates, Inc., for City of Ontario, June 2017. Commercial aviation support services for both general and military aircraft played an important role in the growth and development of ONT. Property types with the ability to individually represent this sub-theme are limited to office/administration buildings and hangars, as these property types represent the strongest association with the sub-theme. Eligible districts under this sub-theme retain the buildings and structures associated with an aircraft service facility that performed aircraft modifications, repair, and/or testing.

As noted in the Historic Context Statement registration requirements, the GE historic district represents important patterns and trends in commercial aviation development from this period, contains a grouping of buildings and structures typical of a commercial aviation support facility, retains a majority of the buildings/structures present during the period of significance, and retains most of its character-defining features and essential aspects of integrity. Only the hangars are recommended as contributors to the historic district, as known locations where aircraft modifications, repair, and/or testing was performed; insufficient information exists function of the ancillary buildings to recommend them as contributors. The majority of the hangar doors have been altered or replaced, and construction adjacent to or near the hangars obscure their original function. While the hangars retain sufficient integrity as a historic district, they do not retain sufficient integrity of design and materials to be recommended as individually eligible, and no other individually eligible properties were identified within the survey area. ASM recommends the General Electric Aircraft Engines Historic District as significant under Criteria A/1 and local District Criteria 1-3 for its association with aviation support services at ONT during the period of significance.

D7. References (Give full citations including the names and addresses of any informants, where possible.):

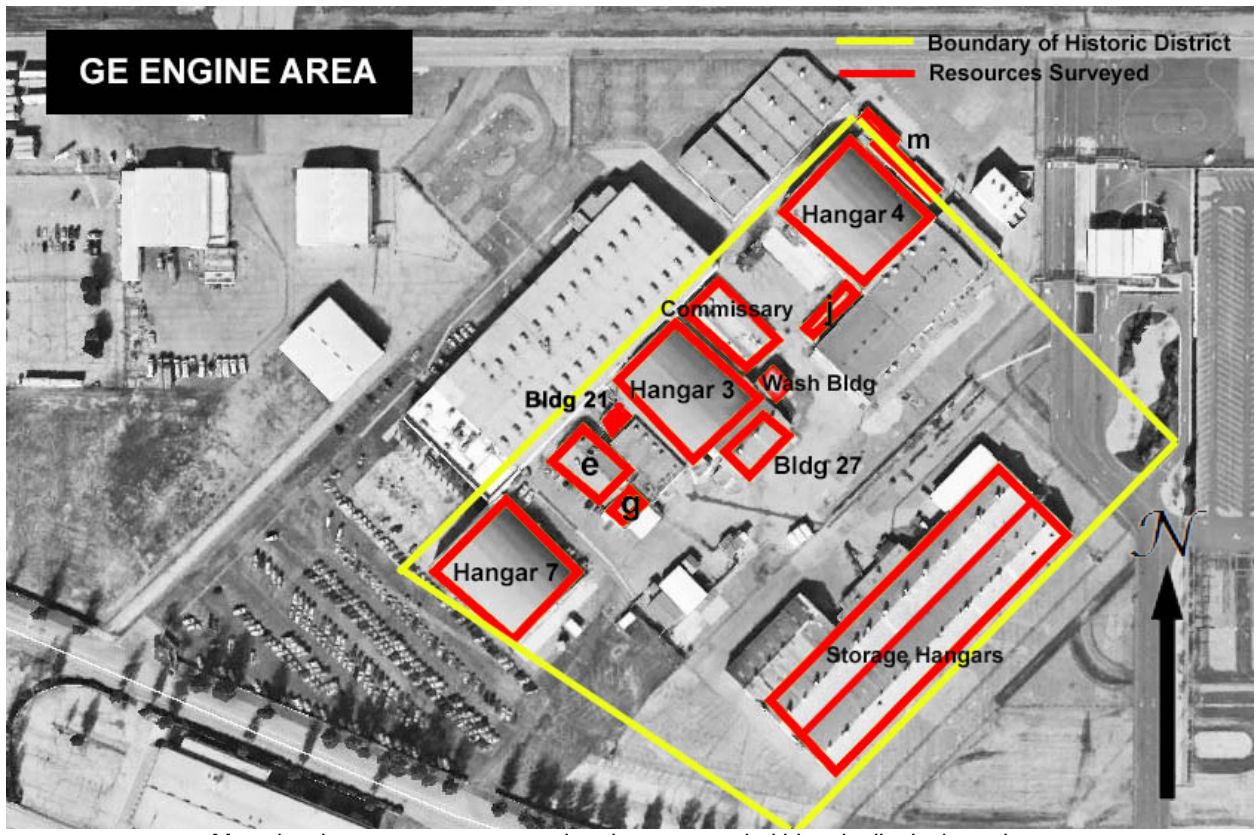
National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*D8. Evaluator: Shannon Davis and Marilyn Novell Date: December 2016

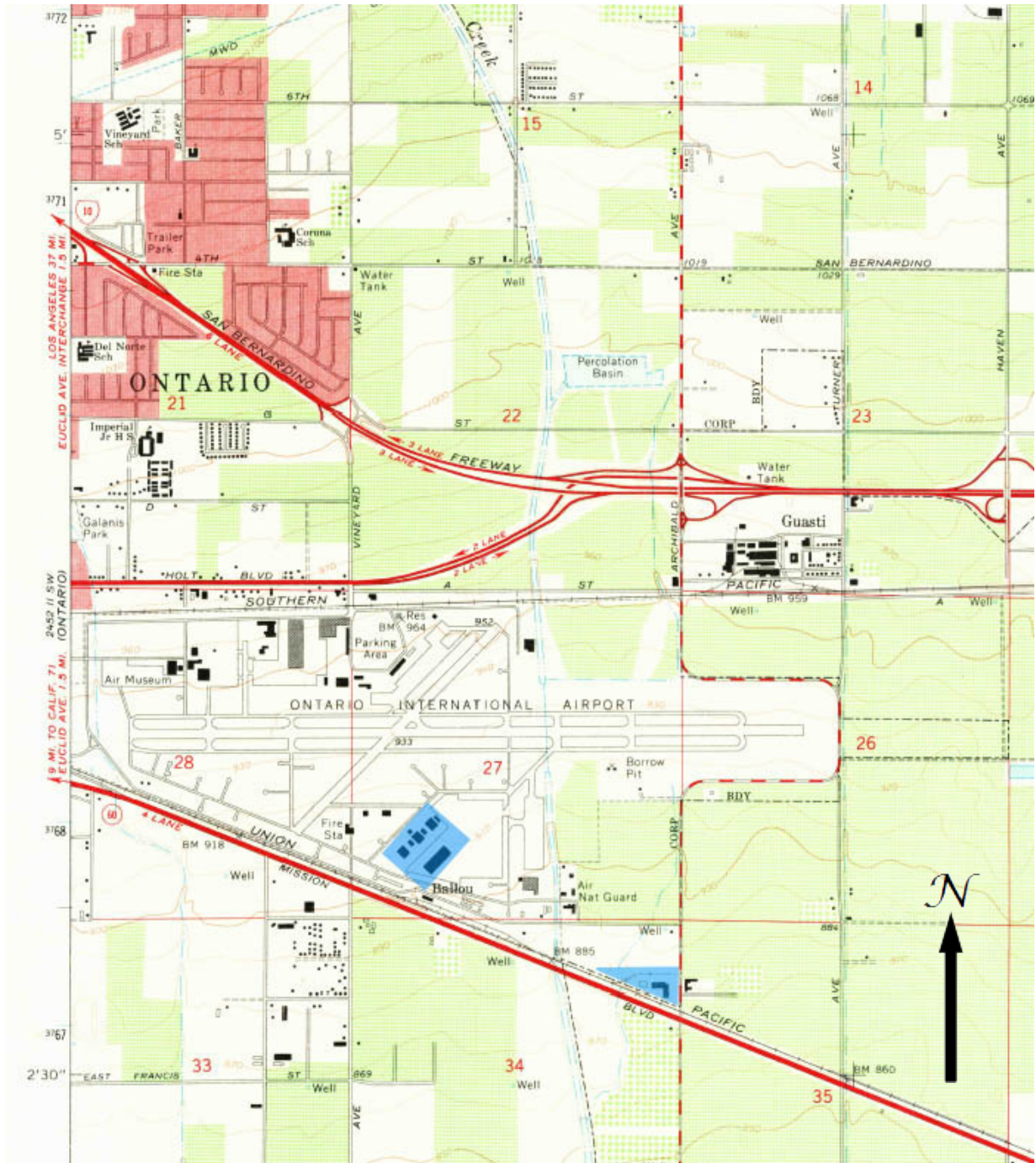
Affiliation and Address: ASM Affiliates, Inc., 20 N. Raymond Ave., Pasadena, CA



Map showing resources surveyed and recommended historic district boundary.



Map showing resources surveyed in the GE Jet Engine Cell Test area.



Map showing location of GE Aircraft Engine area and GE Test Cell area relative to the airport (USGS Guasti, 1966, 1:24,000 scale).

Page 4 of 7
Recorded by: Shannon Davis and Marilyn Novell

*Resource Name or # (Assigned by recorder)

General Electric Aircraft Engines Historic District

Date: December 2016

Continuation Update



Image 1. View looking west at the southeast and northeast facades of Hangar 7 with the administration building to the right. ASM, December 1, 2016.



Image 2. View looking south at the northwest and northeast facades of the Commissary Building, with Hangar 3 in the background. ASM, December 1, 2016.



Image 3. View looking southwest at the northeast façade of ancillary buildings M, with Hangar 4 in the background. ASM, December 1, 2016.



Image 4. View looking north at the southwest and southeast façades of the Storage Hangars. ASM, December 1, 2016.



Image 5. View looking east at the Jet Engine Test Cell area.
ASM, December 1, 2016.



Image 6. Detail view looking south at the north façade of
Test Cell 1. ASM, December 1, 2016.



Image 7. Historic aerial view of GE Engine area, post-1953.
Source: Ontario City Library Robert E. Ellingwood Model Colony Room. Accession No. 6018.

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 6 *Resource Name or #: GE Hangar 3 and Ancillary Buildings

P1. Other Identifier: GE Aircraft Engine District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 1923 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443923.97 mE/ 3768094.66 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

GE Hangar 3 is located between hangars 7 and 4 in the GE Aircraft Engine area at Ontario International Airport. It is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. The hangar has been modified by additions on three sides and no longer functions as a hangar. In the interior, a set of telescoping metal hangar doors that run on steel tracks with a horizontal row of three-by-three windows is visible at the southeast façade. The interior of the hangar is a single open space with a concrete floor and exposed steel trusses. Lighting consists of regularly spaced rows of pendant industrial fixtures. At the northwest façade the hangar doors have been retracted and a large warehouse addition is accessible from the interior of the hangar. The Shipping and Receiving building (Building 27) and a steam clean shed are connected to the hangar at the southeast façade.

(continued on page 2)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking north at the southwest and southeast façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

Pre-1948

Historic aerials

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 1, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

Page 2 of 6 *Resource Name or # (Assigned by recorder) GE Hangar 3 and Ancillary Buildings
Recorded by: Shannon Davis and Marilyn Novell Date: December 2016

Continuation Update

***P3a. Description:** (continued from page 1)

Steam Clean Shed

The steam clean shed is an open shelter adjoining Hangar 3 at the southeast façade. It is constructed of a metal roof and supported by steel I-beams and sits on a sloped concrete foundation. Lighting is provided by fluorescent tubing.

Shipping and Receiving (Building 27)

Building 27 is a single-story utilitarian building constructed of corrugated metal connected to the southeast façade of Hangar 3. It has an irregular plan and sits on a poured-concrete foundation. Along the ridgeline of the moderately pitched gabled roof are turban-style vents and other ventilation utilities. At the southwest façade are three vehicle bays with metal roll-up doors, and at the northwest façade is a flat-roofed addition with bay doors covered in woven metal slats and an additional corrugated metal door under a flat canopy. The interior of the building was not accessible at the time of survey.

Commissary Building

The single-story commissary building is connected to Hangar 3 at the northeast façade. It is a horizontally oriented utilitarian building with a rectangular plan sitting on a poured-concrete foundation. It is a double side-gabled building with very narrow eaves and ventilation and other utilities atop the roof. The roof is covered in asphalt shingles, and the exterior walls are clad in textured stucco. The interior includes a large room with adjacent food-preparation facilities and a series of offices located off of a central hall. Ceilings are acoustical tile interspersed with recessed fluorescent light panels. Walls are plaster, and the flooring is vinyl. Fenestration consists of fixed-pane metal replacement windows of various sizes on the northwest and southwest façades, and several flat-metal personnel doors, some with single lights, on the three exposed sides of the building.

Wash Building

The Wash Building is a utilitarian structure located east of Hangar 3 and southeast of the Commissary Building. It is a flat-roofed shed-like building with a rectangular plan constructed of corrugated metal. At the southeast and northwest façades are sliding barn-type doors made of corrugated metal. The interior has an exposed wood framework and hanging fluorescent tube lighting fixtures.

Building F

This small side-gabled building adjoins Hangar 3 on the northeast façade and a warehouse building to the northwest. It has a rectangular plan and sits on a concrete foundation. A 1992 report labels the hangar as a Safety and Dispensary facility.¹ The roof has slightly overhanging eaves with exposed rafter beams at the primary (northwest) façade. The exterior walls are clad in textured stucco. Fenestration consists of a centrally located door under a small canopy and two symmetrical windows at the primary façade. The interior of the building was not accessible at the time of survey.

¹ *Contingency and Post-Closure Plan, Hazardous Waste Management Unit, GE Aircraft Engine Maintenance Center, Ontario International Airport.* Prepared for General Electric Aircraft Engines by Dames & Moore, February 14, 1992.



Image 1. View looking southwest at the northeast façades of Hangar 3 and the Commissary Building.



Image 2. Interior view looking northwest.

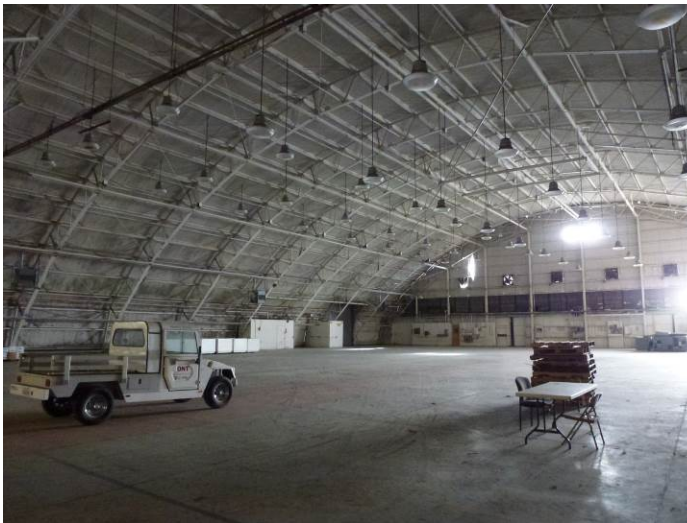


Image 3. Interior view looking east.



Image 4. View looking north at the southwest and southeast façades of Hangar 3 and the steam clean shed.



Image 5. View looking northeast at the southwest façade of Building 27.



Image 6. View looking north at the southwest and southeast façades of Building 27.



Image 7. View looking west at the southeast and northeast façades of Building 27.



Image 8. View looking south at the northwest and northeast façades of the Commissary Building.



Image 9. View looking north at the southwest and southeast façades of the Commissary Building.



Image 10. Interior view of the Commissary Building.



Image 11. View looking east at the northwest and southwest façades of Building 21.



Image 12. View looking south at the northwest and northeast façades of the Wash Building.



Image 13. View looking north at the southwest and southeast façades of the Wash Building.



Image 14. Interior view of the Wash Building.

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 4 *Resource Name or #: GE Hangar 4 and Ancillary Buildings

P1. Other Identifier: GE Aircraft Engines District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W 1/4 of 1/4 of Sec S.B. B.M.

c. Address 1923 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444001.69 mE/ 3768173.52 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

GE Hangar 4 is located northeast of Hangar 3 in the GE Aircraft Engine area at Ontario International Airport. It is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. The hangar is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. The hangar has been modified by an addition on the southeast façade that effectively blocks the hangar doors. The northwest façade has been replaced with a corrugated metal wall with a personnel door and three vehicle bay doors, two with corrugated metal roll-up doors and one with a flat door. In the interior, the hangar is open to the adjoining warehouse at the southeast façade. The hangar has a large open space with a concrete floor and exposed steel trusses. Partitions for offices and restroom facilities have been added along the sides. Lighting consists of rows of pendant industrial fixtures toward the northeast and southwest walls.

(continued on page 2)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking east at the northwest and southwest façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

Pre-1948

Historic aerials

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 1, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

Primary # _____

HRI # _____

Trinomial _____

Page 2 of 4

*Resource Name or # (Assigned by recorder)

GE Hangar 4 and Ancillary Buildings

Recorded by: Shannon Davis and Marilyn Novell

Date: December 2016

Continuation Update

*P3a. Description: (continued from page 1)

Building J

Building J, located to the west of Hangar 4, is a single-story utilitarian building with a rectangular plan on a poured-concrete foundation. It is capped with a moderately pitched side-gabled roof with narrow eaves and is constructed of concrete masonry units. At the southeast façade is a partial porch housing a variety of mechanical equipment. The interior was not accessible at the time of survey.

Building M Area

Building M is an attached group of single-story side-gabled buildings adjoining the northeast façade of Hangar 4. The buildings are clad in smooth stucco, and the roofs are covered in asphalt roll material. Fenestration includes vehicle bay doors, and a variety of windows and personnel doors facing the runway area.



Image 1. View looking northeast at the southwest façade.



Image 2. View looking south at the northwest and northeast façades of Hangar 4 and Building M.



Image 3. View looking southwest at the northeast façades of Building M and Hangar 4.



Image 4. Interior view of Hangar 4 looking southeast.



Image 5. Detail interior view looking east.



Image 6. Interior view looking southeast.



Image 7. View looking northeast at the southwest façade of Building J.



Image 8. View looking north at the southwest and southeast façades of Building J.

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: GE Hangar 7

P1. Other Identifier: Building 34, GE Aircraft Engine District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W 1/4 of 1/4 of Sec S.B. B.M.

c. Address 1923 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443849.23 mE/ 3768020.20 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

GE Hangar 7 is the southernmost of three barrel-roofed hangars in the GE Aircraft Engines area at Ontario International Airport. A 1992 report labels the hangar as a Machine Shop and Heat Treat facility.¹ It is a barrel-roofed aircraft hangar with a rectangular plan set on a poured-concrete foundation. It is constructed of a series of arched steel truss girders terminating in a canted wall at two sides. The roof is clad in metal covered in a sprayed sealant. The exterior walls are formed of corrugated metal panels. At the northwest end is a set of telescoping metal hangar doors that run on steel tracks, allowing them to slide fully into a housing apparatus that extends beyond the mass of the building. A horizontal row of three-by-three windows is set into the hangar doors. It appears that an additional set of hangar doors at the southeast façade has been replaced with a corrugated metal wall and two large vehicle bays with roll-up corrugated doors and two flat metal personnel doors. The interior of the hangar is a single open space with a concrete floor and exposed steel trusses. Lighting consists of regularly spaced rows of pendant industrial fixtures.

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

*P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking northwest at the southeast façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

ca 1955

Ontario International Airport Master Plan, 1963

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 1, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ Contingency and Post-Closure Plan, Hazardous Waste Management Unit, GE Aircraft Engine Maintenance Center, Ontario International Airport. Prepared for General Electric Aircraft Engines by Dames & Moore, February 14, 1992.



Image 1. View looking north at the southeast and northeast façades.



Image 2. View looking east at the northwest and southwest façades.



Image 3. Detail view looking at the northwest façade.



Image 4. Interior view looking southeast.



Image 5. View of the interior looking northwest.



Image 6. Detail view of the interior looking west.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: GE Storage Hangars

P1. Other Identifier: GE Aircraft Engine District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W 1/4 of 1/4 of Sec S.B. B.M.

c. Address 2043 E. Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444029.46 mE/ 3767987.90 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The GE Storage Hangers are located at 2043 E. Avion St. at Ontario International Airport. They consist of two adjoining single-story front-gabled hangar-type buildings with long, rectangular plans set on poured-concrete foundations. The moderately pitched roofs are covered in sheet asphalt and have narrow eaves. Vents and other utilities are visible on the roof. The buildings are clad in corrugated metal. Fenestration includes corrugated metal sliding barn-style doors and personnel doors on the southwest and northeast facades. At the southeast façade is a series of personnel doors and windows, as well as a row of freestanding exterior lighting fixtures on steel posts. A scale mechanism is located near the northeast façade. Three smaller buildings constructed at a later date adjoin the hangars on the northwest, obscuring the northwest façade of the hangars. The buildings were used for parts storage. The interiors were not accessible at the time of survey.

(continued on page 3)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View northeast at the southwest façades.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

ca 1955

Ontario International Airport Authority records

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking north at the southwest and southeast façades.



Image 2. View looking west at the northeast and southeast façades.



Image 3. View looking south at the northwest and northeast façades.



Image 4. Detail view looking west at the southeast façade.



Image 5. View looking west at the northeast façade.



Image 6. Detail view of scale mechanism to the northeast of the storage hangars.

Page 1 of 7

***Resource Name or #:** Terminal One Historic District

D1. Historic Name: Terminal One, Baggage Claim B, Control Tower, FAA office building

D2. Common Name: Terminal One, Baggage Claim B, Control Tower, FAA office building

***D3. Detailed Description:** (Describe overall coherence of the district, its setting, visual characteristics, and minor features. List all elements of district.)

The Terminal One Historic District consists of a group of related buildings north of the primary runway at Ontario International Airport (ONT). Terminal One replaced an earlier terminal and was constructed in 1956-1960. The terminal building contained a full complement of passenger services, including a lobby, ticket counters, and a restaurant. Designed for expansion, the Terminal One building was enlarged extensively in two phases in the 1960s, and again in the 1970s. In 1983 and 1993, the terminal received two more additions. The complex includes the 1953 control tower, built adjacent to the prior terminal. In 1965, a freestanding single-story Federal Aviation Authority (FAA) office building was added to the complex. Terminal One was vacated in 1998, when the current ONT terminals two and four were opened to the east.

(Continued on page 2)

***D4. Boundary Description:** (Describe limits of district and attach map showing boundary and district elements.)

The Terminal One Historic District is located at 1820-1822 East Moore Way and 525 South Vineyard Avenue, and south of East Airport Drive. The boundary includes Terminal One, the control tower, and FAA building.

***D5. Boundary Justification:**

The boundary of the Terminal One Historic District encompasses the core of the extant buildings that served passenger travel during the period of significance.

D6. Significance: Theme Civil Aviation

Area Early Passenger Travel

Period of Significance 1950-1967, 1955-1970

Applicable Criteria NRHP Criterion A, CRHR Criterion 1, and Local District Criteria 1-3;
NRHP Criterion C, CRHR Criterion 3, and Local District Criterion 1

(Discuss district's importance in terms of its historical context as defined by theme, period of significance, and geographic scope. Also address the integrity of the district as a whole.)

The Terminal One Historic District was evaluated under the context of Aviation in Ontario; theme Civil Aviation, 1946-1967; and sub-themes Early Passenger Travel, 1950-1967; and Aviation and Modernism, 1950-1970, according to the guidelines established in the *Ontario International Airport Historic Context Statement*, prepared by ASM Affiliates, Inc., for the City of Ontario. Eligible properties under this sub-theme include historic districts that retain the buildings and structures, and their spatial relationships, from the period of significance. Eligible districts include buildings that serve specialized functions, including a prominent terminal with a control tower that overlooks facilities and runways; vehicle access for picking up and dropping off passengers; associated baggage claim and handling facilities including physical association with passenger, ticketing, and aircraft loading; buildings and structures located adjacent to aircraft aprons and runways; paved surfaces surrounding buildings and structures; parking closely associated with terminals; and landscaping associated with terminals and administrative and office buildings.

As noted in the Historic Context Statement registration requirements, the Terminal One Historic District represents important patterns and trends in early passenger travel at ONT, contains a grouping of buildings and structures typical of a passenger aviation support facility, retains a majority of the buildings/structures present during the period of significance, and retains most of its character-defining features and essential aspects of integrity. ASM recommends the Terminal One Historic District eligible under national and state Criteria A/1 and local District Criteria 1 through 3 for its association with civil aviation and early passenger travel at ONT during the period of significance.

(Continued on page 2)

***D8. Evaluator:** Shannon Davis and Marilyn Novell **Date:** June 2017

Affiliation and Address: ASM Affiliates, Inc., 20 N. Raymond Avenue, Pasadena, CA 91103

D3. Detailed Description: (Continued from page 1)

Prior to construction of Terminal One, Bonanza Air Lines began services out of the prior terminal in 1955, and continued to operation in the new Terminal One. In the 1950s, nonstop flights by Western and Bonanza airlines did not travel farther than Las Vegas. In 1962, Western began nonstop flights to San Francisco, and Bonanza began nonstop F27 flights to Phoenix in 1967 (USACE 1998:3-4). By 1967, Bonanza and Western were joined by Los Angeles Airways (a helicopter airmail service to downtown Los Angeles and LAX).

On October 18, 1967, a contract was signed by the City of Los Angeles and the City of Ontario agreeing to jointly contribute to the further expansion and development of ONT. The City of Ontario would benefit economically from a larger airport but lacked the necessary funds to expand, which the City of Los Angeles was able to provide. Los Angeles also agreed to promote and manage the airport (Agreement 1967).

On November 1, 1967, ONT was officially added to the Los Angeles Department of Airports (LADOA) regional network of satellite airports, which included Van Nuys and Palmdale as well (Figure 27). At that time, development at ONT was already fully under way, with the 1960 terminal already being doubled to accommodate increased traffic, 350 acres acquired at the east end for runway expansion, and plans for additional extensions of runways. As the only airport in the eastern Los Angeles metro area capable of serving large commercial jetliners, and with existing facilities including a fully equipped passenger terminal and six airlines with daily scheduled service, ONT was ideally situated for inclusion in a regional airport system based at LAX. The Civil Aeronautics Board approved service that would allow all domestic airlines serving LAX to provide similar service out of ONT (LADOA 1967).

Since the new terminals were opened east of Terminal One, the complex has been a popular location for filming. Classic Mid-Century-Modern in style, Terminal One has stood in for a number of airports, both fictional and real, in movies and television shows. ONT represented Miami International Airport, Tehran Airport, Las Vegas Airport, and LAX in the 1960s, among other airports.

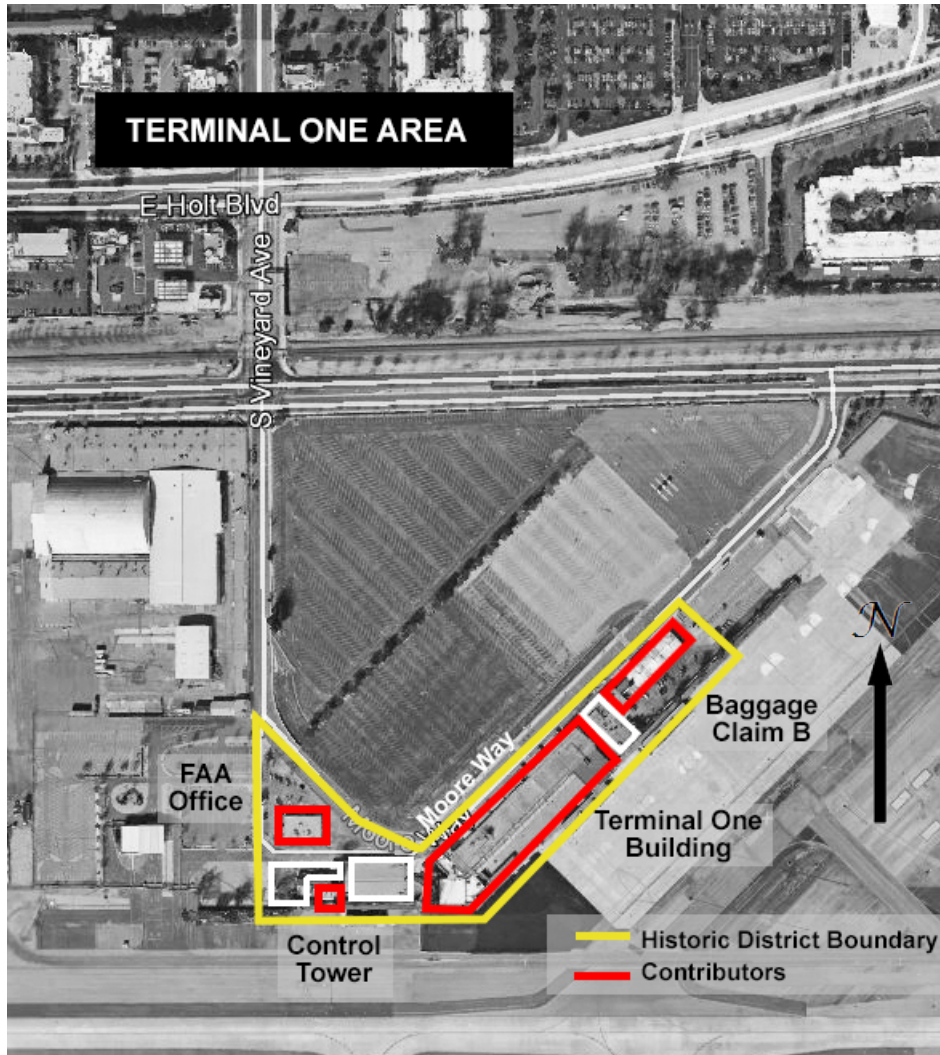
D6. Significance (Continued from page 1):

Recommended contributors to the Terminal One Historic District are the portions of the terminal building built during the period of significance, the baggage claim building to the northeast of the terminal, the control tower, and the FAA services building, as representing an important association with early passenger travel during the period of significance. The terminal building and the control tower were also found individually eligible under this sub-theme under national and state Criteria A/1 and Local Criteria 3 a and b.

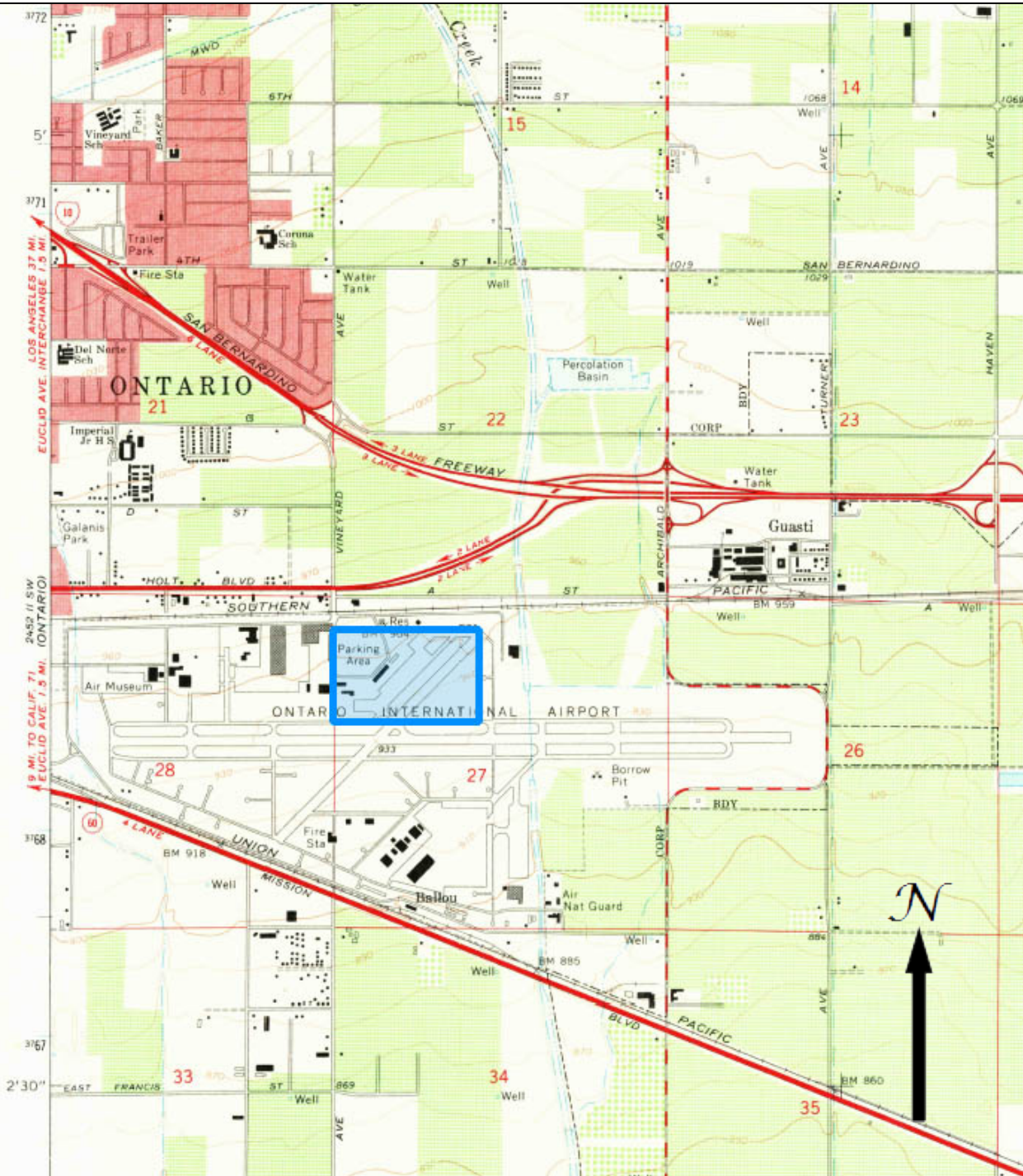
The terminal building and the control tower were also evaluated as individually eligible under the context of Aviation in Ontario; theme of Aviation and Architecture; and sub-theme of Modernism and Aviation, 1955-1970. The two buildings are good representations of Mid-Century-Modern architecture designed by a known local architect and represent the use of the style for prominent buildings visible to and used by the public. They were constructed during the period of significance and retain most of the character-defining features of the style to convey their historical association. Although Terminal One has been altered over time by additions to expand the space as passenger travel increased, the core of the original building retains integrity and remains clearly identifiable as the central portion of the terminal. The Control Tower retains all aspects of integrity. Therefore, the Terminal One building and the Control Tower are found individually eligible under national and state Criteria C/3 and Local Criteria 3 c-d, f-h.

D7. References (Give full citations including the names and addresses of any informants, where possible.):

- Los Angeles Department of Airports (LADOA). 1983. *Ontario International Airport Information: Service, Economics, Improvements and Growth Potential* (Quarterly Report). Ontario, CA: Los Angeles Department of Airports.
- National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.
- National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.
- Ontario International Airport Historic Context Statement*. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
- "Ontario Airport Adds New Aviation Facilities." *Los Angeles Times*, June 21, 1950.



Map of the Terminal One Historic District showing contributors, other resources surveyed, and boundary.
Source: ASM Affiliates, Inc., June 20, 2017.



Location map of the Terminal One area at ONT. (USGS Guasti, 1966).



Image 1. View of Terminal One and ancillary buildings looking east from the Control Tower cab.



Image 2. View of Terminal One primary facade looking east. The historic core of the Terminal One building is recommended as a contributor to the historic district.



Image 3. View of Control Tower and ancillary buildings looking east. Only the Control Tower is recommended as a contributor to the historic district.



Image 4. Historic view of Terminal One in original configuration. Source: HCM brochure, Ontario City Library Robert E. Ellingwood Model Colony Room. Undated.

Page 6 of 7 *Resource Name or # (Assigned by recorder) Terminal One Historic District
Recorded by: Shannon Davis and Marilyn Novell Date: June 2017



Image 5. View of Control Tower looking west from runway. As a later addition to the terminal, this portion of the building does not contribute to the historical significance of the building.



Image 6. Detail view looking north at the southwest façade of Baggage Claim B. This building is recommended as a contributor to the historic district.



Image 6. View looking south at the north façade of the FAA building. This building is recommended as a contributor to the historic district.

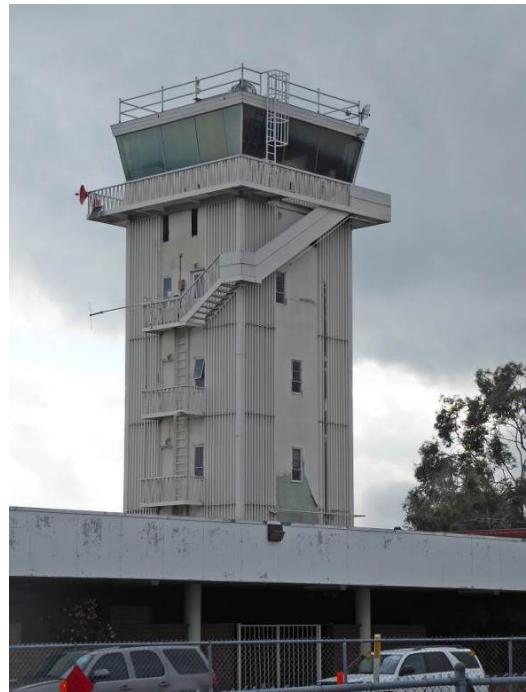


Image 7. View looking southwest at the east and north façades of the Control Tower. The Control Tower is recommended as a contributor to the historic district.

D3. Detailed Description: (Continued from page 1)

Prior to construction of Terminal One, Bonanza Air Lines began services out of the prior terminal in 1955, and continued to operation in the new Terminal One. In the 1950s, nonstop flights by Western and Bonanza airlines did not travel farther than Las Vegas. In 1962, Western began nonstop flights to San Francisco, and Bonanza began nonstop F27 flights to Phoenix in 1967 (USACE 1998:3-4). By 1967, Bonanza and Western were joined by Los Angeles Airways (a helicopter airmail service to downtown Los Angeles and LAX).

On October 18, 1967, a contract was signed by the City of Los Angeles and the City of Ontario agreeing to jointly contribute to the further expansion and development of ONT. The City of Ontario would benefit economically from a larger airport but lacked the necessary funds to expand, which the City of Los Angeles was able to provide. Los Angeles also agreed to promote and manage the airport (Agreement 1967).

On November 1, 1967, ONT was officially added to the Los Angeles Department of Airports (LADOA) regional network of satellite airports, which included Van Nuys and Palmdale as well (Figure 27). At that time, development at ONT was already fully under way, with the 1960 terminal already being doubled to accommodate increased traffic, 350 acres acquired at the east end for runway expansion, and plans for additional extensions of runways. As the only airport in the eastern Los Angeles metro area capable of serving large commercial jetliners, and with existing facilities including a fully equipped passenger terminal and six airlines with daily scheduled service, ONT was ideally situated for inclusion in a regional airport system based at LAX. The Civil Aeronautics Board approved service that would allow all domestic airlines serving LAX to provide similar service out of ONT (LADOA 1967).

Since the new terminals were opened east of Terminal One, the complex has been a popular location for filming. Classic Mid-Century-Modern in style, Terminal One has stood in for a number of airports, both fictional and real, in movies and television shows. ONT represented Miami International Airport, Tehran Airport, Las Vegas Airport, and LAX in the 1960s, among other airports.

D6. Significance (Continued from page 1):

Recommended contributors to the Terminal One Historic District include a prominent terminal, a control tower, and an FAA services building, as representing an important association with early passenger travel during the period of significance. The terminal building and the control tower were also found individually eligible under this sub-theme under national and state Criteria A/1 and local Individual Criteria 3 a and b.

The terminal building and the control tower were also evaluated as individually eligible under the context of Aviation in Ontario; theme of Aviation and Architecture; and sub-theme of Modernism and Aviation, 1955-1970. The two buildings are good representations of Mid-Century-Modern architecture designed by a known local architect and represent the use of the style for prominent buildings visible to and used by the public. They were constructed during the period of significance and retain most of the character-defining features of the style to convey their historical association. Although Terminal One has been altered over time by additions to expand the space as passenger travel increased, the core of the original building retains integrity and remains clearly identifiable as the central portion of the terminal. The Control Tower retains all aspects of integrity. Therefore, the Terminal One building and the Control Tower are found individually eligible under national and state Criteria C/3 and local Individual Criteria 3 c-h.

D7. References (Give full citations including the names and addresses of any informants, where possible.):

- Los Angeles Department of Airports (LADOA). 1983. *Ontario International Airport Information: Service, Economics, Improvements and Growth Potential* (Quarterly Report). Ontario, CA: Los Angeles Department of Airports.
- National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.
- National Park Service. 1997. *How to Complete the National Register Nomination Form*. National Register Bulletin No. 16A. Washington, D.C.
- Ontario International Airport Historic Context Statement*. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.
- "Ontario Airport Adds New Aviation Facilities." *Los Angeles Times*, June 21, 1950.

State of California — The Resources Agency
 DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3D

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 4 *Resource Name or #: FAA Flight Services Building
 P1. Other Identifier: Flight Services Building; Terminal One Historic District, Ontario International Airport
 *P2. Location: Not for Publication Unrestricted
 *a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)
 *b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____
 c. Address 525 South Vineyard Avenue City Ontario Zip 91761
 d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443649.93 mE/ 3768906.91 mN; _____
 e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____
 *P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The FAA Building is a freestanding Mid-Century Modern office building located in the Terminal One area of Ontario International Airport (ONT) and designed by Harnish Morgan and Causey Architects, who were also responsible for Terminal One. The 4,050-square-foot building contained offices for general aviation and the systems maintenance sector, as well as the chief of the combined station and tower.¹ It is located at 525 South Vineyard Avenue across from the old control tower. It is a flat-roofed horizontally oriented building with a rectangular plan set on a poured-concrete foundation. The north and south façades are characterized by a deep horizontal overhanging fascia spanning the façade. The west and east façades are composed of prominent red-brick wing walls that extend

(continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP6, 1-3 story commercial building
 *P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)
 P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)
View looking northeast at the west and south façades.

*P6. Date Constructed/Age and Source:
 Historic Prehistoric Both
1965
Architectural drawings, Los Angeles World Airports (LAWA) archives

*P7. Owner and Address:
Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)
Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____

¹ "Ontario Airport Study to Be Presented Today." *Los Angeles Times*. May 28, 1964.



Image 1. View looking south at the north façade.

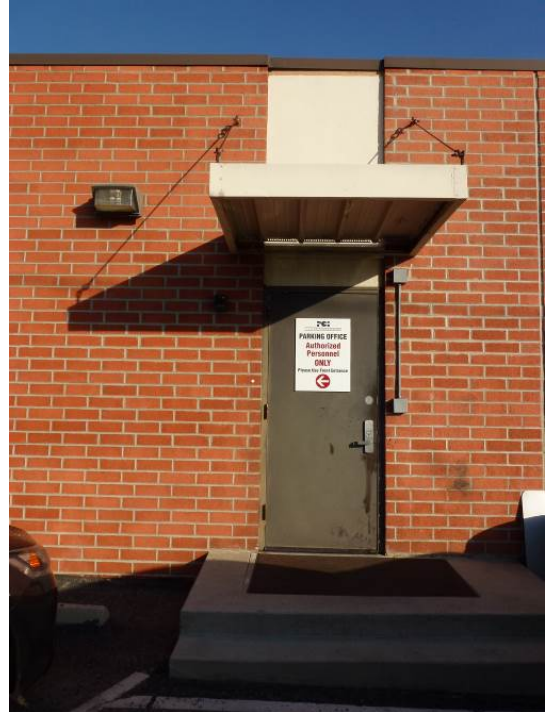


Image 2. Detail view of secondary entrance on west façade.



Image 3. View looking southwest at the east and north façades.



Image 4. Detail view looking at the north façade.

Page 3 of 4 *Resource Name or # (Assigned by recorder) FAA Flight Services Building
 Recorded by: Shannon Davis and Marilyn Novell Date: December 2016



Image 5. Interior view looking west of office.



Image 6. Interior view.

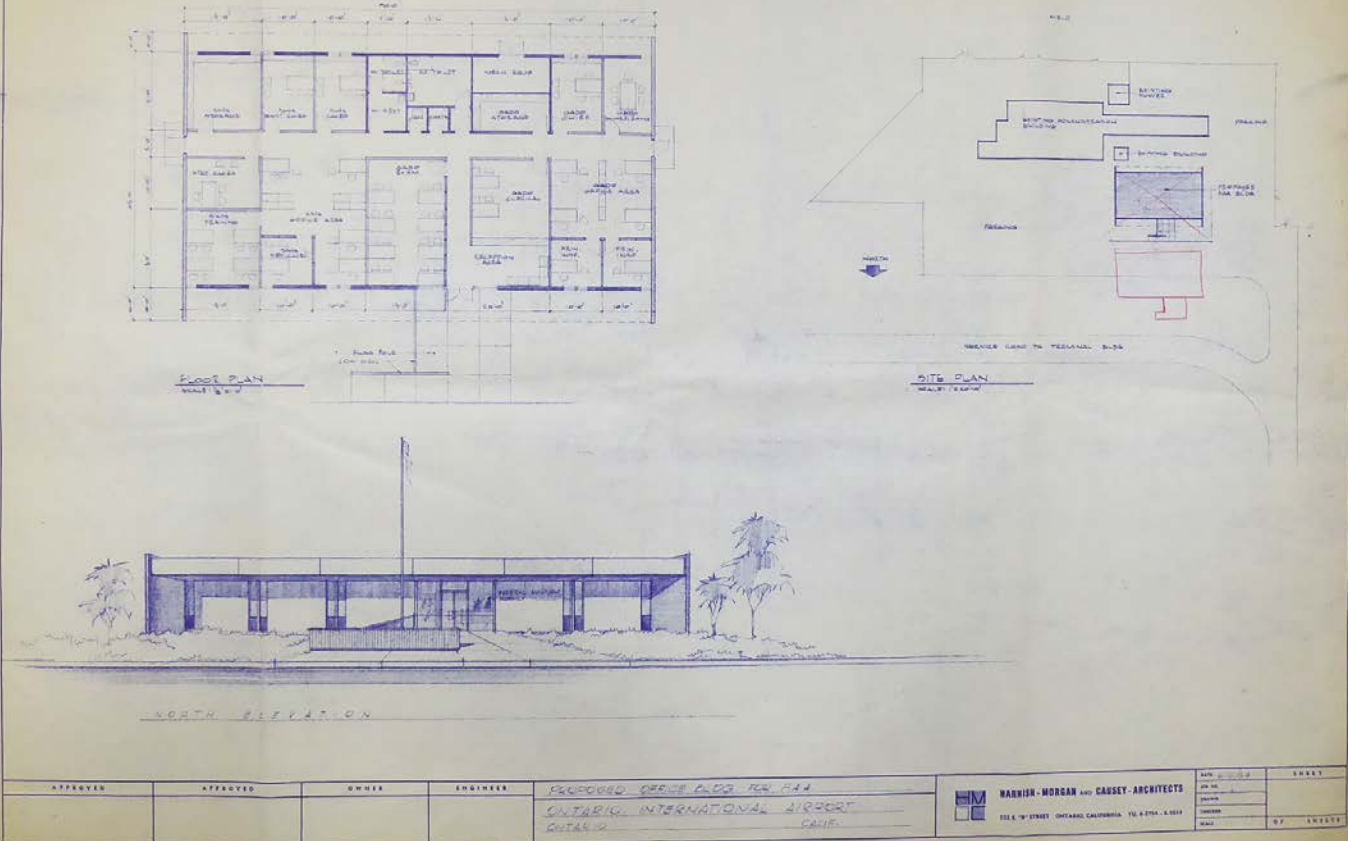


Image 7. Architectural drawing showing floor plan and façade drawing.
 [architectural drawing dated 1965, Ontario International Airport Authority records]

State of California — The Resources Agency
DEPARTMENT OF PARKS AND RECREATION
PRIMARY RECORD

Primary # _____
HRI # _____
Trinomial _____
NRHP Status Code 3D

Other Listings _____
Review Code _____ Reviewer _____ Date _____

Page 1 of 3

*Resource Name or #: Baggage Claim B

P1. Other Identifier: Baggage Claim Building; Terminal One Historic District, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec _____ S.B. _____ B.M. _____

c. Address 525 South Vineyard Avenue City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 443649.93 mE/ 3768906.91 mN; _____

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries) _____

Baggage Claim B is a Mid-Century Modern building in the Terminal One area of Ontario International Airport (ONT). Although it was likely constructed concurrent with Terminal One, which was designed by Harnish Morgan and Causey Architects, no original architectural plans or construction history of the baggage claim were found to confirm the architect or year built. The building was originally freestanding and located to the northeast of Terminal One, at the time of survey it was connected via a corridor to Terminal One and ancillary buildings to the southwest. The roof of the building displays a unique technology in which three cast-concrete segments are each supported at the center by a large, square concrete column. The four-part roofs slope down toward the walls of the building, creating a tent-like effect. The roof is supported atop a red-brick wall at the primary façade by a concrete horizontal beam connected to the wall by thin posts, leaving a gap between the wall and roof that creates a floating effect. On the exterior, the roofs

(Continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP6. 1-3 story commercial building

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking east at the northwest and southwest façades

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both
ca 1959

Architectural drawings, Los Angeles World Airports (LAWA) archives

*P7. Owner and Address:

Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking north at the southwest façade.



Image 2. Detail view looking north at the southwest façade.



Image 3. Detail view looking north at the southwest façade.



Image 4. Interior view looking west.



Image 5. Interior view looking west.



Image 6. Detail view of interior of northwest façade.

***P3a. Description:** (continued from page 1)

have four prominent standing seams. At the runway side of the building, a half-height concrete block screen sits below the slightly undulating edges of the roofline. At the primary façade, a flat canopy supported by square concrete posts shelters the entrances. The interior floor is composed of poured concrete. Four elongated oval-shaped conveyor systems clad in stainless steel pierce the back wall of the building for loading and circulate to the interior for passenger access to baggage.

State of California — The Resources Agency
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PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3S

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 4

*Resource Name or #: Aerojet-General Hangar

P1. Other Identifier: United States Postal Service facilities, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 301 South Hellman Avenue City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444500.89 mE/ 3769053.34 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.)

Located within Ontario International Airport

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Aerojet-General Hangar is located at 301 S. Hellman Av. adjacent to the Cucamonga Creek channel on the north side of the Ontario International Airport. Constructed in 1958, the building has an irregular plan and is set on a poured-concrete foundation. The nearly 19,000-square-foot hangar has a moderately pitched front-gabled roof clad in corrugated metal with regularly spaced inset corrugated fiberglass skylights. The exterior walls are formed of corrugated metal panels. At the west façade is a set of telescoping corrugated metal doors that run on steel tracks set into the concrete pavement, allowing them to retract fully from the mass of the building. The doors are hung on a steel structure that projects beyond the sides of the building. At the center below the gable is an opening for the aircraft tail with a roll-up metal door. At the east façade is an array of utilities in a chain-link-fenced area and vents attached to the exterior walls. A single-story flat-roofed concrete masonry unit building, originally built to accommodate offices, a lobby, radio testing facility, engine storage, and repair shop, is attached to the north façade.¹

(continued on page 4)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP11. Engineering structure

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



*P5b. Description of Photo: (view, date, accession#)

View looking east at the west façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both
1958

Los Angeles Times; historicaerials 1959

*P7. Owner and Address:

Ontario International Airport Authority
1923 E. Avion St.
Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell
ASM Affiliates, Inc.
2034 Corte Del Nogal
Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.") Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List):

¹ "\$95,000 facility rising at airport." *Los Angeles Times*, September 21, 1958.



Image 1. View looking northeast at the west and south façades.



Image 2. View looking southwest at the east and north façades.



Image 3. Detail view looking north at the rails on the west façade.



Image 4. Interior view looking southeast.



Image 5. Interior view looking southwest.



Image 6. Interior view of south wing.

***P3a. Description:** (continued from page 1)

A flat-roofed warehouse area with a bay door and a metal personnel door adjoins the hangar on the south façade. The interior of the hangar is a single open space with exposed steel frame. Hanging fluorescent tubing fixtures augment the lighting on the interior. The building was used for aircraft maintenance and modification.

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 4 of 4

*NRHP Status Code 3S

*Resource Name or # (Assigned by recorder) Aerojet-General Hangar

B1. Historic Name: Aerojet-General Hangar

B2. Common Name: United States Postal Service Hangar

B3. Original Use: Hangar and offices for aircraft maintenance operations

B4. Present Use: Post office hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1958

The Aerojet-General Hangar and adjoining single-story concrete masonry block offices were constructed in 1958. A wing housing a storage area was added to the south sometime before 1980 (historicaerials.com 1966, 1980).

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron

B9a. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture Area: Developments in Construction Technology

Period of Significance: 1958 Property Type: Aircraft hangar and offices Applicable Criteria: C/3/3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Aerojet-General Hangar is an example of construction technology eligible under the context of Aviation in Ontario, Theme of Aviation and Architecture, 1942–1975, and the Sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features of the style, including a front-gabled roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction and embodies the distinctive characteristics of the type of hangar during the period of significance. Although the use of the hangar and offices has changed, it is the only historic hangar of the simple gable-roof type at ONT that retains all seven aspects of integrity. After careful consideration, ASM recommends the Aerojet-General Hangar eligible for listing at the federal, state, and local level under Criterion C/3/3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks:

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



State of California — The Resources Agency
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PRIMARY RECORD

Primary # _____
 HRI # _____
 Trinomial _____
 NRHP Status Code 3S

Other Listings _____
 Review Code _____ Reviewer _____ Date _____

Page 1 of 9 *Resource Name or #: Air National Guard Hangar

P1. Other Identifier: Air National Guard Area, Ontario International Airport

*P2. Location: Not for Publication Unrestricted

*a. County: San Bernardino and (P2c, P2e, and P2b or P2d. Attach a Location Map as necessary.)

*b. USGS 7.5' Quad Guasti Date 2015 T 1S R 7W ¼ of ¼ of Sec S.B. B.M.

c. Address 2475 East Avion Street City Ontario Zip 91761

d. UTM: (give more than one for large and/or linear resources) Zone 11S, 444499.47 mE/ 3767880.54 mN;

e. Other Locational Data: (e.g. parcel#, directions to resource, elevation, etc.) _____

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries)

The Air National Guard Hangar, located at 2475 E. Avion St. at Ontario International Airport, is a complex comprising an aircraft hangar with a two-story office/workshop complex known as a "lean-to" adjoining the hangar on three sides. In addition, there are two wings on the south façade, housing a boiler room and a diesel tank facility. The hangar is a multi-story building of steel frame construction enclosing a single open space to accommodate aircraft. It is rectangular in form and sits on a poured-concrete foundation that opens on the north to the aircraft apron. The hangar is composed of corrugated metal and is capped by a low-pitched front-gabled roof. A continuous row of steel-frame six-over-three fixed windows spans three sides of the hangar. At the north façade is a double set of telescoping hangar-type doors that retract into enclosed housing at the sides. A continuous row of multi-light windows spans the width of the doors. At the interior of the hangar the steel truss construction is visible on the ceiling and walls. The floor is smooth poured concrete, and lighting is provided by regularly spaced industrial pendant fixtures, as well as the rows of windows on all sides.

(continued on page 7)

*P3b. Resource Attributes: (List attributes and codes) HP8. Industrial building; HP34. Military property

*P4. Resources Present: Building Structure Object Site District Element of District Other (Isolates, etc.)

P5a. Photograph or Drawing (Photograph required for buildings, structures, and objects.)



P5b. Description of Photo: (view, date, accession#)

View looking south at the north façade.

*P6. Date Constructed/Age and Source:

Historic Prehistoric Both

1955

*P7. Owner and Address:

Ontario International Airport Authority

1923 E. Avion St.

Ontario, CA. 91761

*P8. Recorded by: (Name, affiliation, and address)

Shannon Davis and Marilyn Novell

ASM Affiliates, Inc.

2034 Corte Del Nogal

Carlsbad, CA 92011

*P9. Date Recorded: December 6, 2016

*P10. Survey Type: (Describe) Pedestrian Intensive

*P11. Report Citation: (cite survey report and sources, or enter "none.")

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

*Attachments: NONE Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record
 Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record
 Artifact Record Photograph Record Other (List): _____



Image 1. View looking southeast at the north and west façades, with Fire Station in the foreground.



Image 2. View looking east at the west façade.



Image 3. Detail view looking southeast at the north and west façades.



Image 4. View looking southwest at the east and north façades.



Image 5. View looking north at the south façade.



Image 6. View looking northwest at the south and east façades.



Image 7. View looking northeast at the west and south façades.



Image 8. View looking west at the east façade.



Image 5. Looking southwest at the north and east façades.



Image 6. View looking northwest at the south and east façades of the Diesel Tanks room.



Image 7. Detail view looking east at the west façade of the Diesel Tanks room.



Image 8. Detail view of the entrance on the east façade of the Diesel Tanks room.



Image 9. Detail view looking northwest at the south façade of the Hangar.



Image 10. Interior view of the hangar looking northeast.



Image 11. Interior view of the hangar looking northwest.



Image 13. Interior view of the hangar looking southeast.



Image 14. Detail view of the interior of the hangar ceiling.



Image 15. Detail interior view of the hangar doors looking east.



Image 16. Detail view of the hangar door track mechanism.

Primary # _____

HRI # _____

Trinomial _____

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*Resource Name or # (Assigned by recorder)

Air National Guard Hangar

Recorded by: Shannon Davis and Marilyn Novell

Date: December 2016

Continuation Update

***P3a. Description:** (continued from page 1)

The flat-roofed two-story wings, or "lean-tos," as described in the technical specifications for the hangar,¹ are clad in corrugated metal with a red brick water line. A continuous row of steel-frame windows resembling those on the hangar have both fixed and awning-style operable portions. All of the windows on the south façade and approximately half of the windows on the other façades have been painted over. The west wing functioned as offices for operations, and the east wing housed maintenance and shops. The interiors of the wings were not accessible at the time of the survey.

Two wings extend from the south façade of the hangar complex. A large flat-roofed corrugated steel industrial-style building set on a concrete foundation is located toward the east side of the south façade. A second, much smaller, flat-roofed wing extends from approximately the center of the south façade. The industrial building is clad in red brick and sits on a poured concrete foundation. Fenestration includes a set of double doors with a vent in a transom above and a single door, with three horizontal lights each, located on the east façade; a single door with similar lights is located on the west façade. Windows are two-by-five in a combination of fixed and operable portions.

¹ "Technical Specifications, Part IV, for Hangar Building—with two-story lean-to with exterior Utilities and Facilities. California Air National Guard, Ontario, California, June 6, 1952. [from Model Colony Room archives]

BUILDING, STRUCTURE, AND OBJECT RECORD

Page 8 of 9

*NRHP Status Code 3B

*Resource Name or # (Assigned by recorder) Air National Guard Hangar

B1. Historic Name: Air National Guard Hangar

B2. Common Name: _____

B3. Original Use: Hangar and offices/workshops to support Air National Guard operations

B4. Present Use: Hangar

*B5. Architectural Style: Utilitarian

*B6. Construction History: (Construction date, alterations, and date of alterations) 1955

The Air National Guard Hangar was constructed in 1955. Technical specifications were prepared by the California Air National Guard, Ontario, in 1952. The hangar and lean-to buildings are minimally altered

*B7. Moved? No Yes Unknown Date: _____ Original Location: N/A

*B8. Related Features: Aircraft apron

B9a. Architect: Unknown b. Builder: Unknown

*B10. Significance: Theme Aviation and Architecture

Area: Developments in Construction Technology

Period of Significance: 1955-1975

Property Type: Aircraft hangar and offices Applicable Criteria: NRHP/CRHR Criteria C/3; Local Individual Criteria 3 d, f-h

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity.)

The Air National Guard Hangar is an example of construction technology considered within the context of Aviation in Ontario under the theme of Aviation and Architecture, 1942–1975, and the sub-theme of Developments in Construction Technology, 1942–1975. The hangar displays character-defining features one style typical of Air National Guard facilities during the period of significance, including a front-gabled roof, multi-leaved hangar door and tail cut, and a large open space to accommodate aircraft enabled by steel truss construction. The multi-leaved telescoping hangar doors with extensive fenestration and the mass of the building formed by the pop-up center section with clerestory windows and the lower “lean-to” sections all original features (Aaron 2011). Although the National Guard no longer occupies the site, suggesting some change in use, both the interior and exterior of the building reflect all seven aspects of integrity. After careful consideration, ASM recommends the Air National Guard Hangar individually eligible for listing at the federal, state or local level under Criteria C/c or Local Individual Criteria 3 d, f-h.

B11. Additional Resource Attributes: (List attributes and codes) HP39. Aircraft apron

***B12. References:**

Aaron, Jayne. 2011. *Historical and Architectural Overview of Aircraft Hangars of the Reserves and National Guard Installations from World War I through the Cold War*. Prepared for the Department of Defense Legacy Resource Management Program.

National Park Service. 1997. *How to Apply the National Register Criteria for Evaluation*. National Register Bulletin No. 15. Washington, D.C.

Ontario International Airport Historic Context Statement. Prepared by ASM Affiliates, Inc., for City of Ontario. 2017.

B13. Remarks: _____

*B14. Evaluator: ASM Affiliates, Inc. (Shannon Davis and Marilyn Novell)

*Date of Evaluation: June 2017

(This space is reserved for official comments)

Sketch Map with north arrow required.



