CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION SPECIAL MEETING AGENDA

December 20, 2021

Ontario City Hall 303 East "B" Street, Ontario, California 91764

6:30 PM

WELCOME to a meeting of the Ontario Planning/Historic Preservation Commission.

All documents for public review are on file in the Planning Department located at 303 E. B Street, Ontario, CA 91764.

- Anyone wishing to speak during public comment or on a particular item should fill out a green slip and submit it to the Secretary.
- Comments will be limited to 5 minutes. Speakers will be alerted when their time is up. Speakers are then to return to their seats and no further comments will be permitted.
- In accordance with State Law, remarks during public comment are to be limited to subjects within the Commission's jurisdiction. Remarks on other agenda items will be limited to those items.
- Remarks from those seated or standing in the back of the chambers will not be permitted. All those wishing to speak including Commissioners and Staff need to be recognized by the Chair before speaking.
- The City of Ontario will gladly accommodate disabled persons wishing to communicate at a public meeting. Should you need any type of special equipment or assistance in order to communicate at a public meeting, please inform the Planning Department at (909) 395-2036, a minimum of 72 hours prior to the scheduled meeting.
- Please turn off <u>all</u> communication devices (phones and beepers) or put them on non-audible mode (vibrate) so as not to cause a disruption in the Commission proceedings.

ROLL CALL						
Anderson	Dean	DeDiemar	Gage	Lampkin	Ricci	Willoughby
PLEDGE OF	<u>ALLEGIA</u>	NCE TO THE	FLAG			

ANNOUNCEMENTS

- 1) Agenda Items
- 2) Commissioner Items

PUBLIC COMMENTS

Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

CONSENT CALENDAR ITEMS

All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of November 23, 2021, approved as written.

PUBLIC HEARING ITEMS

For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak, unless there are a number of person's wishing to speak and then the Chairperson will allow only three (3) minutes, to accommodate for more persons. The Planning/Historic Preservation Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

PLANNING & HISTORIC PRESERVATION COMMISSION ITEMS

B. ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT20-005: A Tentative Parcel Map (TPM 20253) to subdivide 56.94 acres of land into 27 parcels generally bordered by Guasti Road to the north, Old Guasti Road to the south, Turner Avenue to the east and Archibald Avenue to the west, within Planning Area 2 of the Guasti Plaza Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with an amendment to the Guasti Plaza Specific Plan, File No. PSPA08-006, for which a Supplemental Environmental Impact Report (State Clearinghouse No. 2008111072) was certified by the City Council on May 3, 2011. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-192-11 and 0211-201-15) submitted by Ontario Airport Venture, LLC.

1. CEQA Determination

No action necessary – Use of previously certified EIR

2. File No. PMTT20-005 (TPM 20253) (Tentative Parcel Map)

Motion to continue to January 25, 2022 meeting.

C. ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT21-012: A Tentative Parcel Map (TPM 20387) to subdivide 0.31-acre of land into 2 parcels located at the southwest corner of Euclid Avenue and Maitland Street, at 1004 South Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-563-10) submitted by United Construction Company.

1. CEQA Determination

No action necessary – Exempt: <u>CEQA Guidelines Section § 15315</u>

2. File No. PMTT21-012 (TPM 20387) (Tentative Parcel Map)

Motion to Approve/Deny

D. ENVIRONMENTAL ASSESSMENT, **CONDITIONAL** PERMIT, USE DEVELOPMENT PLAN REVIEW FOR FILE NOS. PCUP21-004 AND PDEV21-012: A Conditional Use Permit (File No. PCUP 21-004) and Development Plan (File No. PDEV21-012) to construct and establish a 2,370 square foot fast food restaurant with drive-thru (Sonic) on 0.72acre of land located at the southwest corner of Mountain Avenue and Sixth Street, within the Main Street land use district of the Mountain Village Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1008-431-25) submitted by Coast to Coast Commercial, LLC.

1. CEQA Determination

No action necessary – Exempt: CEQA Guidelines Section § 15332

2. File No. PCUP21-004 (Conditional Use Permit)

Motion to Approve/Deny

3. File No. PDEV20-012 (Development Plan)

Motion to Approve/Deny

- Ε. **ENVIRONMENTAL** ASSESSMENT. **TENTATIVE PARCEL** MAP. AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT21-010 AND PDEV21-018: A Tentative Parcel Map (TPM 20147) (File No. PMTT21-010) to subdivide 95.35 acres of land into three parcels, in conjunction with a Development Plan (File No. PDEV21-018) to construct two industrial buildings totaling 168,772 square feet on 13.07 acres of land located at the southeast corner of Jurupa Street and Milliken Avenue, at 1425 South Toyota Way, within the Industrial Mixed Use and Warehouse/Distribution land use districts of the Toyota Ontario Business Park Specific Plan. An Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008104410) was prepared. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0238-121-75) submitted by Pamela Steele, MIG, Inc.
 - 1. **CEQA Determination** (Addendum to a Certified EIR)

Motion to Approve/Deny

2. File No. PMTT21-010 (TPM 20147) (Tentative Parcel Map)

Motion to Approve/Deny

3. File No. PDEV21-018 (Development Plan)

Motion to Approve/Deny

- F. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN AND CERTIFICATE OF APPROPRIATENESS REVIEW FOR FILE NO. PDEV21-030 AND PHP21-015: A Development Plan (File No. PDEV21-030) to construct a 174,603 -square-foot industrial building on 7.47 acres of land, in conjunction with a Certificate of Appropriateness (File No. PHP21-015) to facilitate the demolition of an existing Tier III historic eligible structure (1914 Craftsman Single-Family Residence) to accommodate the proposed industrial development located on the west side of Bon View Avenue, approximately 132 feet north of Cedar Street, within the IG (General Industrial) zoning district. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport, and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 1050-441-05, 1050-441-09, 1050-441-11, 1050-441-12, 1050-441-13, 1050-441-14, 1050-441-15, 1050-441-16, 1050-441-17, 1050-441-18, 1050-441-19, 1050-441-20, 1050-441-21, 1050-441-22, 1050-441-23, 1050-441-24, 1050-441-25, 1050-441-26, 1050-441-27, 1050-441-28, 1050-441-29, 1050-441-30, 1050-441-31, 1050-441-32, 1050-441-33, 1050-441-34, 1050-441-35, 1050-441-36, 1050-441-37, 1050-441-38, 1050-441-39, 1050-441-40, 1050-441-41, 1050-441-42, 1050-441-43, 1050-441-44, 1050-441-45, 1050-441-46, 1050-441-47, 1050-441-48, 1050-441-49, 1050-441-50, 1050-441-51, 1050-441-52, 1050-441-53, 1050-441-54, 1050-441-55, 1050-441-56, 1050-441-57, 1050-441-58, 1050-441-59, 1050-441-60, 1050-441-61, 1050-441-62, and 1050-441-73) submitted by Bon View Land 10, LLC & BV Investments 10, LLC as tenantsin-common.
- G. <u>ENVIRONMENTAL ASSESSMENT AND ZONE CHANGE REVIEW FOR FILE NO.</u> <u>PZC21-001</u>: A Zone Change (File No. PZC21-001) amending the zoning designation on 2.77 acres located at 1948 South Bon View Avenue, from IL (Light Industrial) to IG (General

Industrial). Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1050-441-05) submitted by Bon View Land 10, LLC & BV Investments 10, LLC. City Council action is required.

1. CEQA Determination (Addendum to a certified EIR)

Motion to recommend Approval/Denial

2. File No. PHP21-015 (Certificate of Appropriateness)

Motion to Approve/Deny

3. File No. PDEV21-030 (Development Plan)

Motion to Approve/Deny

4. File No. PZC21-001 (Zone Change)

Motion to recommend Approval/Denial

- H. ENVIRONMENTAL ASSESSMENT, GENERAL PLAN AMENDMENT, ZONE CHANGE, AND DEVELOPMENT CODE AMENDMENT FOR FILE NOS. PGPA21-004, PZC21-002, AND PDCA21-001: A General Plan Amendment (File No. PGPA21-004) for the Housing Element update to the Policy Plan (General Plan) component of The Ontario Plan addressing State mandates and the 6th Cycle Regional Housing Needs Allocation (RHNA) and to modify the Policy Plan Land Use Plan (Exhibit LU-01) to establish the following: 1) a Zone Change (File No. PZC21-002) to modify the Zoning Map to establish an AH (Affordable Housing) zoning district; 2) create an Affordable Housing Overlay; and 3) a Development Code Amendment (File No. PDCA21-001) revising section 6.01.035 for the purpose of adding provisions to establish an AH (Affordable Housing) Overlay District. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); City initiated. City Council action is required.
 - 1. CEQA Determination (Addendum to a certified EIR)

Motion to recommend Approval/Denial

2. <u>File No. PGPA21-004</u> (General Plan Amendment)

Motion to recommend Approval/Denial

3. File No. PZC21-002 (Zone Change)

Motion to recommend Approval/Denial

4. File No. PDCA21-001 (Development Code Amendment)

Motion to recommend Approval/Denial

MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION

- 1) Old Business
 - Reports From Subcommittees
 - Historic Preservation (Standing): Met on December 9, 2021.
- 2) New Business
- 3) Nominations for Special Recognition

DIRECTOR'S REPORT

1) Monthly Activity Report

If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.

If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.

I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **December 16, 2021**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.

Gwen Berendsen, Secretary Pro Tempore

Rudy Zeledon, Planning Director Planning/Historic Preservation Commission Secretary

CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING

MINUTES

November 23, 2021

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CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING

MINUTES

November 23, 2021

REGULAR MEETING: City Hall, 303 East B Street

Called to order by Chairman Gage at 6:30 PM

COMMISSIONERS

Present: Chairman Gage, Anderson, Dean, Lampkin, and Ricci

Absent: Vice-Chairman Willoughby, DeDiemar

OTHERS PRESENT: Planning Director Zeledon, City Attorney Maldonado, Principal

Planner Mercier, Principal Planner Ruddins, Assistant Planner Vaughn, Community Development Administrative Officer Womble, Senior Landscape Planner Richardson, Assistant City

Engineer Lee, and Planning Secretary Berendsen

PLEDGE OF ALLEGIANCE TO THE FLAG

The Pledge of Allegiance was led by Commissioner Lampkin.

ANNOUNCEMENTS

Mr. Zeledon stated that Items B & C will be taken together.

PUBLIC COMMENTS

No one responded from the audience.

CONSENT CALENDAR ITEMS

Motion was made by Ricci and seconded by Lampkin to pull Agenda Item A-02 off the Consent Calendar for separate discussion.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of October 26, 2021, approved as written.

It was moved by Ricci, seconded by Dean, to approve the Planning Commission Minutes of October 26, 2021, as written. The motion was carried 5 to 0.

PUBLIC HEARING ITEMS

A-02. REVIEW OF THE ONTARIO CONCEPTUAL 'GREAT' PARK MASTER PLAN AND PRELIMINARY PARK DESIGN FOR PHASE 1 FOR FILE NO. PADV21-004: A request for the approval of the City of Ontario Conceptual 'Great' Park Master Plan and Preliminary Park Design for Phase 1. The Ontario 'Great' Park is approximately 370 acres that extend over 3-1/2 miles in an east-west direction from Haven Avenue to Campus Avenue and has varying widths anticipated between 280 to 1,600 feet. The Conceptual 'Great' Park Master Plan will guide the future phased improvements and development of the 'Great' Park, seeking to provide a major amenity for Ontario residents as a key organizational element of land use development. The 'Great' Park is envisioned to accommodate passive and recreational uses, cultural amenities, outdoor performance venues, gardens, ponds and waterways, a network of trails, bike paths, and greenways. The Conceptual 'Great' Park Master Plan will provide a flexible framework and be used by the City of Ontario as the basis for the review and approval of the future development of the Ontario 'Great' Park. The Preliminary Park Design for Phase 1 of the 'Great' Park encompasses approximately 130 acres of land bordered by Grand Park Street to the north, Eucalyptus Avenue to the south, Haven Avenue on the east, and Archibald Avenue on the west. The concept for Phase 1 includes an amphitheater, a central arroyo, meadows, fields as dual-use areas, and a hierarchy of trail systems. (Phase 1 APNs: 0218-241-58, 0218-241-49, 0218-241-39, 0218-241-45, & 0218-241-47) submitted by the City of Ontario. City Council action is required.

Senior Landscape Planner Richardson, gave the background on the "Great" Park, the project location of Phase I, community outreach and introduced the consultant for the project, SWA and they did a presentation, regarding the Preliminary Plan for Phase I.

SWA Consultants presented the "Great" Park framework, the conceptual site plan, and the details of Phase I.

Ms. Richardson then stated that staff is recommending the Planning Commission recommend approval of File No. PADV21-004, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Gage asked about a name for the park.

Mr. Zeledon stated we are working with Community Life and Culture on that. He also thanked the SWA consultants for all their work on the project.

Ms. Anderson wanted to know how long it would take to do the first phase completely.

Mr. Zeledon stated that it was probably a 2 or 3 year phase, but would depend on funding and working with CLC and program demands. He stated it is never going to be complete, which is the beauty of the design, it will always be evolving.

Mr. Gage wanted to know if they were recommending any pepper trees.

Ms. Richardson stated they were at Significant Ontario entrances.

PUBLIC TESTIMONY

No one responded.

As there was no one else wishing to speak, Chairman Gage closed the public testimony

- Mr. Ricci spoke in favor of the project.
- Mr. Lampkin thanked the staff and consultants and spoke in favor of the project.
- Mr. Gage spoke in favor of the project.
- Mr. Dean thanked the consultant for their thorough presentation.

PLANNING COMMISSION ACTION

It was moved by Ricci, seconded by Lampkin, to recommend adoption of a resolution to approve the Preliminary Park Design for Phase I, File No., PADV21-004, subject to conditions of approval. Roll call vote: AYES, Anderson, Dean, Gage, Lampkin, and Ricci; NOES, none; RECUSE, none; ABSENT, DeDiemar, Willoughby. The motion was carried 5 to 0.

- B. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT AGREEMENT REVIEW FOR FILE NO. PDA21-004: A Development Agreement (File No. PDA21-004) between the City of Ontario and LHC Ontario Holdings, LLC., to establish the terms and conditions for the development of Tentative Tract Map 18916 (File No. PMTT21-011), a 11.05 acre property located near the southwest corner of Archibald and Chino Avenues, within Neighborhood 7 of the Countryside Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Countryside Specific Plan (File No. PSP04-001) Environmental Impact Report (State Clearinghouse No. 2004071001) certified by the City Council on April 18, 2006. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-131-34) submitted by LHC Ontario Holdings, LLC. City Council action is required.
- ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP REVIEW FOR FILE NO. PMTT21-011 (TTM 18916): A Tentative Tract Map (TTM 18916) to subdivide 11.05 acres of land into 36 numbered lots and two lettered lots to facilitate future residential development, located near the southwest corner of Archibald and Chino Avenues, within Neighborhood 7 of the Countryside Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Countryside Specific Plan (File No. PSP04-001), Environmental Impact Report (State Clearinghouse No. 2004071001) certified by the City Council on April 18, 2006. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0218-131-34) submitted by LHC Ontario Holdings, LLC.

Assistant Planner Vaughn, presented the staff report. She stated that staff is recommending the Planning Commission recommend approval of File No. PDA21-004 and approve File No. PMTT21-011, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

No one responded.

PUBLIC TESTIMONY

Craig Cristina with Richland Communities stated he was available to answer questions and thanked staff.

Mr. Gage asked if Mr. Cristina agreed to the Conditions of Approval.

Mr. Cristina stated yes.

As there was no one else wishing to speak, Chairman Gage closed the public testimony

There was no Planning Commission deliberation.

PLANNING COMMISSION ACTION

It was moved by Ricci, seconded by Anderson, to recommend adoption of a resolution to approve the Development Agreement, File No., PDA21-004, subject to conditions of approval. Roll call vote: AYES, Anderson, Dean, Gage, Lampkin, and Ricci; NOES, none; RECUSE, none; ABSENT, DeDiemar, Willoughby. The motion was carried 5 to 0.

It was moved by Lampkin, seconded by Ricci, to adopt a resolution to approve the Tentative Tract Map, File No., PMTT21-011, subject to conditions of approval. Roll call vote: AYES, Anderson, Dean, Gage, Lampkin, and Ricci; NOES, none; RECUSE, none; ABSENT, DeDiemar, Willoughby. The motion was carried 5 to 0.

MATTERS FROM THE PLANNING COMMISSION

Old Business Reports From Subcommittees

Historic Preservation (Standing): This subcommittee met on November 10, 2021

Development Code Review (Ad-hoc): This subcommittee did not meet.

Zoning General Plan Consistency (Ad-hoc): This subcommittee did not meet.

New Business

Mr. Lampkin spoke about the Housing Element Subcommittee meeting.

Mr. Ricci spoke about the upcoming Christmas parade.

NOMINATIONS FOR SPECIAL RECOGNITION

None at this time.

DIRECTOR'S REPORT

Mr.	Zeledon	stated	the]	Monthl	y A	ctivity	y Re	ports	are	in '	the 1	packe	et.

ADJOURNMENT

Ricci motioned to adjourn, seconded by Dean. special December 20, 2021 meeting.	The meeting was adjourned at 7:25 PM, to the
	Secretary Pro Tempore
	Chairman, Planning Commission



MEMORANDUM

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

TO: Chairman and Members of the Planning Commission

FROM: Rudy Zeledon – Planning Director

DATE: December 20, 2021

SUBJECT: ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT20-005: A Tentative Parcel Map (TPM 20253) to subdivide 56.94 acres of land into 27 parcels generally bordered by Guasti Road to the north, Old Guasti Road to the south, Turner Avenue to the east and Archibald Avenue to the west, within Planning Area 2 of the Guasti Plaza Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with an amendment to the Guasti Plaza Specific Plan, File No. PSPA08-006, for which a Supplemental Environmental Impact Report (State Clearinghouse No. 2008111072) was certified by the City Council on May 3, 2011. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-192-11 and 0211-201-15) submitted by Ontario Airport Venture, LLC.

Staff is requesting that File No. PMTT20-005 be continued to the January 25, 2022 Planning Commission meeting, to allow departmental conditions to be finalized.



PLANNING COMMISSION STAFF REPORT

December 20, 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NO: PMTT21-012 (TPM 20387)

SUBJECT: A Tentative Parcel Map (TPM 20387) to subdivide 0.31-acre of land into two parcels generally located at the southwest corner of Euclid Avenue and Maitland Street, at 1004 South Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts: (APN: 1049-563-10) **submitted by United Construction Company.**

PROPERTY OWNER: Ontario Housing Authority

RECOMMENDED ACTION: That the Planning Commission consider and approve File No. PMTT21-012 (TPM 20387), pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site is comprised of 0.31-acre of land located at the southwest corner of Euclid Avenue and Maitland Street at 1004 South Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue

Overlay) zoning districts, and is depicted in Figure 1: Project Location. The Project site is currently vacant and consists of one rectangular-shaped lot-oriented east to west, towards Euclid Avenue. The site was previously developed with a residence and accessory building, with vehicular access via a drive approach on Euclid Avenue. The residence and accessory structure were demolished in 1975, the drive approach remains at the original location. The Project site is surrounded by residential land uses to the north, east, south, and west. The existing surrounding land uses, zoning, and general plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.



Figure 1: Project Location

Case Planner:	Elly Antuna, Associate Planner
Planning Director Approval:	
Submittal Date:	07/01/2021

Hearing Body	Date	Decision	Action
DAB	12/20/2021	Approval	Recommend
PC	12/20/2021		Final
CC			

PROJECT ANALYSIS: The proposed Tentative Parcel Map will subdivide the Project site into two rectangular-shaped numbered lots (see Exhibit B—Tentative Parcel Map, attached). The Project site is currently comprised of one rectangular-shaped lot, oriented east to west towards Euclid Avenue. The subdivision will create two lots (one interior lot and one corner lot) and reconfigure the orientation of the lots north to south, with the frontages located along Maitland Street. The parcels are each 6,573 square feet, with lot widths of 80.9 feet, and lot depths of 81.25 feet. The Development Code requires lots established by a Small Lot Traditional Single-Family Subdivision to have a minimum lot size of 4,000 square feet for interior lots and 4,500 square feet for corner lots, a minimum lot width of 40 feet for interior lots and 45 feet for corner lots and lot depths of 75 feet. In addition, the Project is consistent with the Small Lot Infill Subdivisions Development Code standards (Section 6.01.010.G) that requires that lots have a minimum area of 480 square feet and a minimum lot width of 16 feet. The proposed lots exceed these minimum requirements.

Approval of the Tentative Parcel Map will facilitate the future construction of two new single-family dwelling units on the lots. The infill Project will front onto Maitland Street (Exhibit C—Conceptual Site Plan, attached), with enhanced architecture on the Euclid Avenue and Maitland Street elevations (Exhibit D—Conceptual Elevations, attached). Attached garages will be oriented north, with access from the Maitland Street. Conditions of Approval have been imposed on the Project to ensure that future development of the lot will be designed in a manner which safeguards Euclid Avenue's position on the National Register of Historic Places. The development of the lots will require administrative approval through the City's Building Department Plan Check process.

On December 20, 2021, the Development Advisory Board ("DAB") reviewed the application and recommended that the Planning Commission approve the proposed Project, subject to the department conditions of approval included with this report.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

- (1) <u>City Council Goals</u>.
 - Invest in the Growth and Evolution of the City's Economy
 - Operate in a Businesslike Manner
 - Focus Resources in Ontario's Commercial and Residential Neighborhoods
- (2) <u>Vision</u>.

Distinctive Development:

Commercial and Residential Development

> Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) <u>Governance</u>.

Decision Making:

- <u>Goal G1</u>: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
- ➤ <u>G1-2 Long-term Benefit</u>. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

Land Use Element:

- <u>Goal LU1</u>: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-1 Strategic Growth</u>. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
- ➤ <u>LU1-6 Complete Community</u>: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
 - Goal LU2: Compatibility between a wide range of uses.
- ➤ <u>LU2-6</u>: <u>Infrastructure Compatibility</u>: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- <u>Goal H2</u>: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
- ➤ <u>H2-5 Housing Design</u>. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.
- ➤ <u>CE1-6 Diversity of Housing</u>. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.
- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
- ➤ <u>CE2-1 Development Projects</u>. We require new development and redevelopment to create unique, high-quality places that add value to the community.
- ➤ <u>CE2-2 Development Review</u>. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
- ➤ <u>CE2-4 Protection of Investment</u>. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- ➤ <u>CE2-5 Private Maintenance</u>. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
- > <u>\$1-1</u> Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- <u>Goal CD1</u>: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
- ➤ <u>CD1-1 City Identity</u>. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
- ➤ <u>CD1-3 Neighborhood Improvement</u>. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- <u>Goal CD2</u>: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
- ➤ <u>CD2-1 Quality Architecture</u>. We encourage all development projects to convey visual interest and character through:
- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- ➤ <u>CD2-2 Neighborhood Design</u>. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:
- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
 - Landscaped parkways, with sidewalks separated from the curb.
- ➤ <u>CD2-7 Sustainability</u>. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

- ➤ <u>CD2-8 Safe Design</u>. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- ➤ <u>CD2-9 Landscape Design</u>. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- ➤ <u>CD2-13 Entitlement Process</u>. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
- <u>Goal CD3</u>: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.
- ➤ <u>CD3-1 Design</u>. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.
- ➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.
- ➤ <u>CD3-3 Building Entrances</u>. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- ➤ <u>CD3-5 Paving</u>. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- ➤ <u>CD3-6 Landscaping</u>. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
- <u>Goal CD5</u>: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- ➤ <u>CD5-1 Maintenance of Buildings and Property</u>. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

> <u>CD5-2 Maintenance of Infrastructure</u>. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AlrPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines, which consists of the division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division is in conformance with the General Plan and zoning, no variances or exceptions are required, all services and access to the proposed parcels to local standards are available, the parcel was not involved in a division of a larger parcel within the previous two years, and the parcel does not have an average slope greater than 20 percent.

CONDITIONS OF APPROVAL: See attached department reports.

Planning Commission Staff Report File No.: PMTT21-012 (TPM 20387)

December 20, 2021

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	Existing Land Use	General Plan Designation	Zoning Designation
Site	Vacant	Low Density Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay)
North	Multiple-Family Dwellings	Low Density Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay)
South	Single-Family Dwellings	Low Density Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay)
East	Vacant/Single-Family Dwellings	Low-Medium Density Residential	MDR-11 (Medium Density Residential – 5.1 to 11.0 du/ac) and EA (Euclid Avenue Overlay)
West	Multiple-family Dwellings	Low Density Residential	LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac)

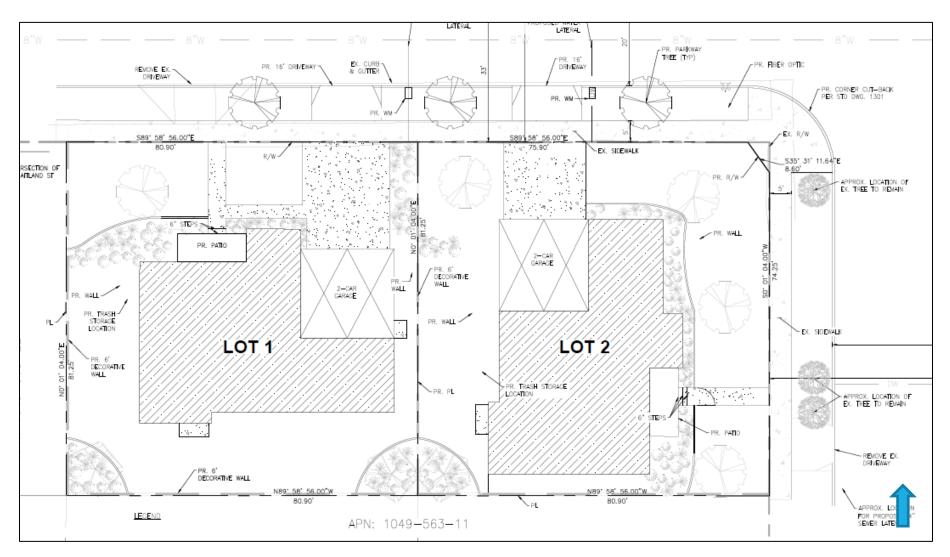
General Site & Building Statistics

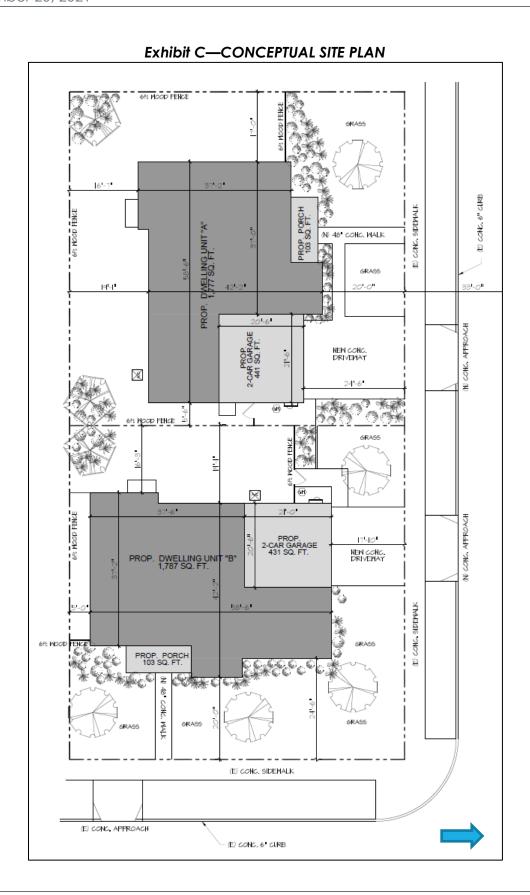
Item	Required Min./Max.	Provided (Ranges)	Meets Y/N
Project area (in acres):	One acre, or; an existing lot of record that is substandard as to minimum "project" area shall be permitted the development rights of the zone in which it is located, except that the maximum density shall be limited to the minimum allowed within the density range.	.31	Y
Maximum project density (dwelling units/ac):	2.1 to 5.0 dwelling units/ac	2.1 to 5.0 dwelling units/ac	Υ
Minimum lot size (in SF):	4,000 SF – Interior Lot 4,500 SF – Corner Lot	6,573 SF	Y
Minimum lot depth (in FT):	75 FT	81.25 FT	Y
Minimum lot width (in FT):	40 FT – Interior Lots 45 FT – Corner Lots	80.9 FT	Υ

Exhibit A—PROJECT LOCATION MAP



Exhibit B—TENTATIVE PARCEL MAP





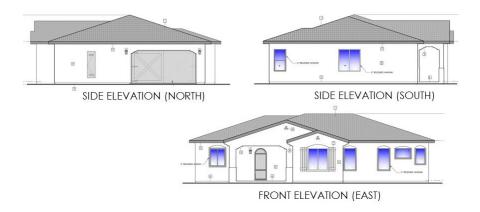
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Exhibit D—CONCEPTUAL ELEVATIONS









RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT21-012, A TENTATIVE PARCEL MAP (TPM 20387) TO SUBDIVIDE 0.31-ACRE OF LAND INTO TWO PARCELS LOCATED AT THE SOUTHWEST CORNER OF EUCLID AVENUE AND MAITLAND STREET, AT 1004 SOUTH EUCLID AVENUE, WITHIN THE LDR-5 (LOW DENSITY RESIDENTIAL – 2.1 TO 5.0 DU/AC) AND EA (EUCLID AVENUE OVERLAY) ZONING DISTRICTS, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1049-563-10.

WHEREAS, UNITED CONSTRUCTION COMPANY, ("Applicant") has filed an Application for the approval of a Tentative Parcel Map, File No. PMTT21-012, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.31-acre of land located at the southwest corner of Euclid Avenue and Maitland Street, at 1004 South Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts, and is presently vacant; and

WHEREAS, the property to the north of the Project site, across Maitland Street, is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts and is developed with multiple-family dwellings. The properties to the east, across Euclid Avenue, are within the MDR-11 (Medium Density Residential – 5.1 to 11.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts and are vacant or developed with single-family dwellings. The properties to the south are within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts and are developed with single-family dwellings. The property to the west, is within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) zoning district and is developed with multiple-family dwellings; and

WHEREAS, the proposed Tentative Parcel Map will subdivide the Project site into two rectangular-shaped lots that are 6,573 square feet in size, 80.9 feet in width and 81.25 feet in depth; and

WHEREAS, the proposed Tentative Parcel map will reconfigure the orientation of the lots from east to west, to north to south, with the frontages located along Maitland Street; and

WHEREAS, the Tentative Parcel Map will facilitate the future construction of new single-family dwelling units on the lots; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-057, recommending that the Planning Commission approve the Application; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: **Environmental Determination and Findings.** As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (2) The Project is categorically exempt from environmental review pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines, which consists of the division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division is in conformance with the General Plan and zoning, no variances or exceptions are required, all services and access to the proposed parcels to local standards are available, the parcel was not involved in a division of a larger parcel within the previous two years, and the parcel does not have an average slope greater than 20 percent; and
- (3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and
- (4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 3: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of

Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 4</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

- The proposed Tentative Parcel Map is consistent with the goals. (1) policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments. The proposed Tentative Parcel Map is located within the LDR (Low Density Residential) land use district of the Policy Plan Land Use Map, and the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts. The proposed subdivision is consistent with the goals. policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing "a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life" (Goal LU1). Furthermore, the Project will promote the City's policy to "incorporate a variety of land uses and building types that contribute to a complete community where residents at all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU1-6 Complete Community).
- (2) The design or improvement of the proposed Tentative Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments. The proposed Tentative Parcel Map is located within the LDR (Low Density Residential) land use district of the Policy Plan Land Use Map, and the LDR-5 (Low Density Residential 2.1 to 5.0 du/ac)

and EA (Euclid Avenue Overlay) zoning districts. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing "[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct" (Goal CD2). Furthermore, the Project will promote the City's policy to "create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb." (Policy CD2-2 Neighborhood Design).
- (3) The site is physically suitable for the type of development proposed. The Project site meets the minimum lot area and dimensions of the LDR-5 (Low Density Residential 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.
- (4) The site is physically suitable for the density/intensity of development proposed. The Project site is proposed for residential development at a density of 2.1 to 5.0 du/ac. The Project site meets the minimum lot area and dimensions of the LDR-5 (Low Density Residential 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts and is physically suitable for this proposed density / intensity of development.
- (5) The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat. The Project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.
- (6) The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems. The design of the proposed subdivision, and the single-family residential improvements proposed on the Project site,

are not likely to cause serious public health problems, as the Project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or Project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the Project site.

(7) The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision. The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

<u>SECTION 5</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 6</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the Applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 7</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 8</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

_ _ _ _ _ _ _ _ _ _ _ _ _ _ _ _

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PMTT21-012 (TPM 20387) December 20, 2021 Page 8	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
I, Gwen Berendsen, Secretary Pro Tel City of Ontario, DO HEREBY CERTIFY that duly passed and adopted by the Planning C special meeting held on December 20, 2021, I	Commission of the City of Ontario at their
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A:

File No. PMTT21-012 Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 12/15/2021

File No: PMTT21-012 (TPM 20387)

Project Description: A Tentative Parcel Map (TPM 20387) to subdivide 0.31 acres of land into 2 parcels generally located at the southwest corner of Euclid Avenue and Maitland Street, at 1004 South Euclid Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 du/ac) and EA (Euclid Avenue Overlay) zoning districts.; (APN: 1049-563-10); **submitted by United Construction Company.**

Prepared By: Elly Antuna, Associate Planner

<u>Phone</u>: 909.395.2414 (direct) <u>Email</u>: eantuna@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 <u>Time Limits</u>.

(a) Tentative Parcel Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Parcel Map shall be in conformance with the approved Tentative Parcel Map on file with the City. Variations rom the approved Tentative Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

- **(b)** Tentative Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.
- (c) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.
- **2.3** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.
- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.4 Landscaping.

- (a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).
- **(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.
- **(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.
- **(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.
- **2.5** <u>Walls and Fences</u>. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

2.7 Mechanical and Rooftop Equipment.

- (a) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.
- **2.8** <u>Security Standards</u>. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).
- **2.9** <u>Sound Attenuation</u>. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.10 Disclosure Statements.

- (a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:
- (i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

2.11 Environmental Review.

- (a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines, which consists of the division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division I in conformance with the general Plan and zoning, no variances or exceptions are required, all services and access to the proposed parcels to local standards are available, the parcel was not involved in a division of a larger parcel within 2 years, and the parcel does not have an average slope greater than 20 percent.
- **(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).
- **(c)** If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a

qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.12 <u>Indemnification</u>. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.13 Additional Fees.

- ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.
- **(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.

2.14 Additional Requirements.

- (a) Development of Lots 1 and 2 shall be constructed in conformance with conceptual plans provided with the Tentative Parcel Map.
- **(b)** Dwellings shall be constructed in the Mediterranean Revival architectural style, or any other appropriate architectural style as determined by the Planning Director.
 - (c) Primary entrances shall be enhanced with covered front porches.
- (d) Floor plans shall be designed to accommodate focal windows on primary elevations.
- **(e)** Development on Lot 2 shall be setback a minimum of 20 feet from Euclid Avenue, feature enhanced architectural treatment on both the Euclid Avenue (east) and Maitland Street (north) elevations, and Incorporate features such as recessed windows and doors, arched openings, windowsills and enhanced porch columns.
 - **(f)** Lots 1 and 2 shall be oriented towards Maitland Street.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

☐ DEVELOPMENT PLAN	⊠ PARCE	L MAP	TRACT MAP
OTHER	☐ FOR C	ONDOMINIUM P	URPOSES
PROJE	CT FILE NO.	PMTT21-012/PM-	<u>-20387</u>
RELATED	FILE NO(S).		
⊠ OR	RIGINAL	REVISED:/_/	_
CITY PROJECT ENGINEER &	R PHONE NO:	Eric Woosley, P.E.	(909) 395-2134
CITY PROJECT PLANNER &	PHONE NO:	Elly Antuna	(909) 395-2414
DAB MEETING DATE:		December 20, 2021	
PROJECT NAME / DESCRIPTION:		PM-20387, a Tentati subdivide 0.31 acre parcels within the lo residential land use	es of land into two (2) ow-density
LOCATION:		Southwest corner of Maitland Street	of Euclid Avenue and
APPLICANT:		United Construction	
REVIEWED BY:		Rayalh	n 12/8/21
APPROVED BY:		Raymond Lee, P.E. Assistant City Engi Khoi Do, P.E. City Engineer	Date 12-8-21 Date

Last Revised: 12/7/2021

Date: 11/30/2021



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1.	PRIC	OR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check Whe Complete	n
	1.01	Dedicate to the City of Ontario, the right-of-way, described below:	
		feet on	
		Property line corner 'cut-back' required at the intersection of	
	1.02	Dedicate to the City of Ontario, the following easement(s):	
	1.03	Restrict vehicular access to the site as follows:	
	1.04	Vacate the following street(s) and/or easement(s):	
		 All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. 	
	1.05	Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.	
	1.06	Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.	
	1.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
	1.08	File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.	
		(1)	
		(2)	
\boxtimes	1.09	Prepare a fully executed Subdivision Agreement (on City approved format and forms) with	



accompanying security as required, or complete all public improvements.

\boxtimes	1.10	Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.	
\boxtimes	1.11	Provide a preliminary title report current to within 30 days.	
	1.12	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	1.13	Ontario Ranch Developments:	
		☐ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.	
		 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents). 	
		3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).	
	1.14	Other conditions:	
2.	PRIO	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:	
2.	A. GE	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL its includes Grading, Building, Demolition and Encroachment)	
2.	A. GE	NERAL	
	A. GE	NERAL its includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance	
\boxtimes	A. GE (Perm 2.01	NERAL hits includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.	
\boxtimes	A. GE (Perm 2.01 2.02	NERAL hits includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario	
\boxtimes	A. GE (Perm 2.01 2.02 2.03	NERAL hits includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the	
\boxtimes	A. GE (Perm 2.01 2.02 2.03 2.04	NERAL hits includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.	
\boxtimes	A. GE (Perm 2.01 2.02 2.03 2.04	NERAL hits includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No. 20387 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972. Apply for a:	



	2.06	Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.	
	2.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
	2.08	Submit a soils/geology report.	
\boxtimes	2.09	Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:	
		State of California Department of Transportation (Caltrans) (For any work within Euclid Avenue ROW, including sewer laterals, ADA access ramp, etc.) San Bernardino County Road Department (SBCRD) San Bernardino County Flood Control District (SBCFCD) Federal Emergency Management Agency (FEMA) Cucamonga Valley Water District (CVWD) for sewer/water service United States Army Corps of Engineers (USACE) California Department of Fish & Game Inland Empire Utilities Agency (IEUA) Other:	
	2.10	Dedicate to the City of Ontario the right-of-way described below: feet on	
		Property line corner 'cut-back' required at the intersection ofand	
	2.11	Dedicate to the City of Ontario the following easement(s):	
	2.12	Vacate the following street(s) and/or easement(s):	
		 All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. 	
	2.13	Ontario Ranch Developments:	
		☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.	
		2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay	



	2.17	Other conditions: a. Submit a Final Utilities Systems Map (USM) as part of the precise grading plan submittal that conforms to the Conceptual USM emailed on 11/10/2021 with a "last plotted" date of 11/10/2021 and meets all of the City's USM requirements. These requirements include to show and label all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, etc.), sizes, points of connection, and any easements. The final utility design shall comply with all Division of Drinking Water (CCR §64572) Separation Requirements which include a minimum 10' horizontal separation, outside wall to outside wall, between water and sewer and a minimum 5' horizontal separation, outside wall to outside wall, between water and all other utilities.	
\boxtimes	2.16	Pay all Development Impact Fees (DIF) to the Building Department.	
	2.15	The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.	
	2.14	Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.	
		☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.	
		any applicable fees as set forth by said agreement.	

 \boxtimes



B. PUBLIC IMPROVEMENTS (See attached Exhibit 'A' for plan check submittal requirements.)	

2.17	Design and construct full public improvements in accordance with the City of Ontario Municipal
	Code, current City standards and specifications, master plans and the adopted specific plan for
	the area, if any. These public improvements shall include, but not be limited to, the following
	(checked boxes):

Improvement	Euclid Avenue	Maitland Street	Street 3	Street 4
Curb and Gutter	New; 80-ft. from C/L to replace drive approach (match existing) Replace damaged	New; 18-ft. from C/L to replace drive approach (match existing) Replace damaged	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace
AC Pavement	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Drive Approach	New Remove and replace with full height curb, parkway landscape and sidewalk	New (2) Remove and replace with full height curb	New Remove and replace	New Remove and replace
Sidewalk	New Remove and replace damaged panels	New Remove and replace damaged panels	New Remove and replace	New Remove and replace
ADA Access Ramp	New Remove and replace	New Remove and replace	New Remove and replace	New Remove and replace
Parkway	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)



Raised Landscaped Median	New Remove and replace			
Fire Hydrant	New / Upgrade Relocation			
Sewer (see Sec. 2.C)	☐ Main ☐ Lateral	Main Lateral	Main Lateral	Main Lateral
Water (see Sec. 2.D)	Main Service	Main Service (2)	Main Service	Main Service
Recycled Water (see Sec. 2.E)	Main Service	Main Service	Main Service	Main Service
Traffic Signal System (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Traffic Signing and Striping (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Street Light (see Sec. 2.F)	New / Upgrade Relocation			
Bus Stop Pad or Turn-out (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Storm Drain (see Sec. 2G)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Fiber Optics (see Sec. 2K)	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances
Overhead Utilities	Underground Relocate	Underground Relocate	Underground Relocate	Underground Relocate
Removal of Improvements				
Other Improvements				

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		Specific notes for improvements listed in item no. 2.17, above:	
\boxtimes	2.18	Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): Maitland Street, from centerline to curb.	
	2.19	Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.	
	2.20	Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.	
	2.21	Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.	
	2.22	Other conditions:	
	C. SE	WER CONTROL OF THE CO	
	2.23	An 8-inch and a 10-inch sewer main is available for connection by this project in Maitland Street and Euclid Avenue, respectively. (Ref: Sewer plan bar code: S10275, unknown)	
	2.24	Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately feet away.	
	2.25	Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.	
	2.26	Other conditions: a. Construct a new sewer lateral for Lot 1 along E Maitland Street and a new sewer lateral for Lot 2 along Euclid Avenue, as shown on the Conceptual USM last plotted on 11/10/2021. The proposed sewer laterals shall not be under any stormwater infiltration.	
	D. W	ATER	
	2.27	An 8-inch water main is available for connection by this project in Maitland Street. (Ref: Water plan bar code: W12430)	
	2.28	Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately feet away.	
	2.29	 Other conditions: a. Construct two potable water services (one service for each lot) in Maitland Street, as shown on the Conceptual USM last plotted on 11/10/2021. b. Comply with Division of Drinking Water (DDW) Separation Requirements and California Code of Regulations (CCR) § 64572. Potable water services shall be a minimum of 10 feet horizontally from sewer. Potable water mains and services shall be a minimum of 25 horizontal feet of the nearest edge of any facilities for storm water retention and storm water infiltration. c. Abandon existing fire hydrant, lateral and valve back to the water main located at the southwest corner of Euclid Avenue and Maitland Street, and construct a new fire hydrant, lateral and valve. The new fire hydrant shall be located at a minimum of 5' back of the BCR. 	



	E. RE	CYCLED WATER	
\boxtimes	2.30	No recycled water main is available for connection by this project in Maitland Street or Euclid Avenue.	
	2.31	Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.	
	2.32	Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.	
	2.33	Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.	
		Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.	
	2.34	Other conditions:	
	F. TR	AFFIC / TRANSPORTATION	
	2.35	Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 1. On-site and off-site circulation 2. Traffic level of service (LOS) at 'build-out' and future years 3. Impact at specific intersections as selected by the City Engineer	
	2.36	New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.	
	2.37	Other conditions: a. Existing driveways to be closed on Euclid Avenue and Maitland Street shall be infilled with curb and parkway including sidewalk, per City Standards and to the satisfaction of the City Engineer. 	
		b. The Applicant/Developer shall be responsible to replace any existing street light fixtures along its project frontage of Euclid Avenue with the current City approved LED equivalent fixture per the Traffic and Transportation Design Guidelines.	
		 Design and construct proposed driveways in accordance with City of Ontario Standard Drawing No. 1203 for Residential Driveway Approach I along Maitland Street. 	
	G. DR	AINAGE / HYDROLOGY	
\boxtimes	2.38	No storm drain main is available to accept flows from this project in Maitland Street or Euclid Avenue.	
	2.39	Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.	

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	2.40	An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.	
	2.41	Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.	
	2.42	Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.	
	2.43	Other conditions:	
		ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM	
	(NPDE	S)	
	2.44	401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.	
		engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.	
\boxtimes	2.45	Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp .	
	2.46	Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.	
П	2.47	Other conditions:	



	J. SP	ECIAL DISTRICTS	
	2.48	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	2.49	Other conditions:	
	K. FIE	SER OPTIC	
\boxtimes	2.50	No fiber optic line is available for connection by this project.	
	2.51	Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall generally be installed in the parkway along the frontages of Euclid Avenue and Maitland Street, with a handhole at each end of the property limits, see Fiber Optic Exhibit herein, Exhibit 'B'.	
\boxtimes	2.52	Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.	
	L. INT	EGRATED WASTE	
\boxtimes	2.52	Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at: https://www.ontarioca.gov/OMUC/IntegratedWaste	
		nttps://www.ortanoca.gov/ontoo/integratedwaste	
	2.53	Other conditions:	
3.	PRIO	R TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:	
	3.01	Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.	
	3.02	Complete all requirements for recycled water usage.	
		☐ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.	
		☐ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.	
		☐ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.	
	3.03	The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.	

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	3.04	Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.	
\boxtimes	3.05	Confirm payment of all Development Impact Fees (DIF) to the Building Department.	
	3.06	Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).	
4.	PRIO	R TO FINAL ACCEPTANCE, APPLICANT SHALL:	
\boxtimes	4.01	Complete all Conditions of Approval listed under Sections 1-3 above.	
	4.02	Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.	
	4.03	The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.	
	4.04	Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.	

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EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: Parcel Map No. 20387

1.	\boxtimes	A copy of this check list
2.	\boxtimes	Payment of fee for Plan Checking
3.	\boxtimes	One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.
4.	\boxtimes	One (1) copy of project Conditions of Approval
5.	\boxtimes	Include a PDF (electronic submittal) of each required improvement plan at every submittal.
6.	sho	Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations wing low, average and peak water demand in GPM for the proposed development and proposed water er size).
7.		Three (3) sets of Public Street improvement plan with street cross-sections
8.		Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average peak water demand in GPM for the proposed development and proposed water meter size)
9.	low,	Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing average and peak water demand in GPM for the proposed development and proposed water meter size an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.		Four (4) sets of Public Sewer improvement plan
11.		Five (5) sets of Public Storm Drain improvement plan
12.		Three (3) sets of Public Street Light improvement plan
13.		Three (3) sets of Signing and Striping improvement plan
14.	\boxtimes	Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15.	ultin clea	Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and nate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall trances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 6. Include Auto CAD electronic submittal)
16.	mod	Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with diffied Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal edifications.
17.	⊠ Pre	Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved liminary WQMP (PWQMP).
18.	\boxtimes	One (1) copy of Hydrology/Drainage study
19.	\boxtimes	One (1) copy of Soils/Geology report
20.	\boxtimes	Payment for Final Map/Parcel Map processing fee

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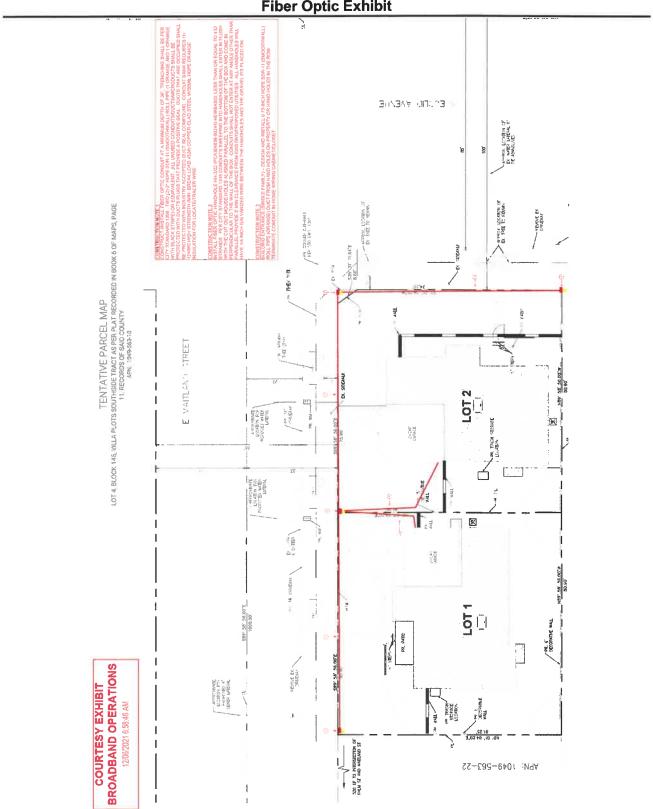


- 21. Three (3) copies of Final Map/Parcel Map
- 22. One (1) copy of approved Tentative Map
- 23. One (1) copy of Preliminary Title Report (current within 30 days)
- 24. One (1) copy of Traverse Closure Calculations
- 25. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. 🛛 One (1) Final Utilities System Map (USM)



EXHIBIT 'B'

BROADBAND DEPARTMENT Fiber Optic Exhibit



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CITY OF ONTARIO

LANDSCAPE PLANNING DIVISION

303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL

Sign Off	
Q.P.	11/29/2021
Jamie Richardson, Sr. Landscape Planner	Date

	Reviewer's Name: Jamie Richardson, Sr. Landscape Planner Phone: (909) 395-2615				
РМТ	3. File No.: Related Files:	Case Planner: Elly Antuna			
Sub	ct Name and Location: divide .31 acres into 2 parcels 4 S Euclid Ave.				
Unite 9500	cant/Representative: ed Construction Company O 7 th Street Suite U cho Cucamonga, CA 91730				
\boxtimes	A Tentative Tract Map (dated10/28/2021) has been approved c following conditions below be met upon submittal of the lands				
	A Tentative Tract Map (dated) has not been approved. Correct required before DAB approval.	ions noted below are			
CORRECTIONS REQUIRED					

Conditions of Approval

- 1. Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width, and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed; mitigation may be required. Include existing trees within 15' of adjacent property affected by new walls, footings, or on-site tree planting. There are existing heritage trees on Euclid Ave.
- 2. Stormwater infiltration devices located in landscape areas shall not exceed 40% of the front yard landscape area's width. Transition from basin to a meandering dry stream bed and coordinate with a landscape architect for the design. The landscape has to be 55% living landscape materials, and non-living ornamental features (boulders, gravel, dry stream beds, etc.) may comprise up to 5% of the landscape and shall be a permeable material. Remove the "mow curb" and provide landscape.
- 3. Bio-retention areas to engineered soil (65% sand, 20% sandy loam, 15% organic matter by volume) with 40% void spaces and capable of supporting vegetation.
- 4. Show existing utilities: Relocate utilities to minimum clearances to allow parkway trees. Parkway trees are to be 30' apart, and where residential driveways occur, a maximum 45' apart. Show and note a 10' parkway tree space, 5' clearance each side of tree from any utility or hardscape including water, sewer, drain lines, and driveways; and min. 10' clear from street

Grading or Utility Construction Plans shall address the following:

- 5. Stormwater infiltration devices located in parkways or other landscape areas shall be routed to this department to be reviewed and approved before permit approval or installation.
- 6. Note for compaction to not be greater than 85% at landscape areas; all finished grades 1 ½" below finished surfaces; landscaped slopes to max 3:1.

- 7. Show infiltrating catch basins with two ¾" dia. holes in bottom set on 12" square of filter fabric wrapped gravel, located 5' or greater from buildings and 24" from sidewalk, add detail.
- 8. Show or note transformers shall be located in planter areas and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformers greater than 4' high. Locate on level grade. Coordinate with landscape plans.
- 9. Provide a utility clear space 8' wide in parkways 30' apart for street trees. Move water meters, drain lines, light standards to the minimum spacing to allow street trees.
- 10. Show light standards 15' away from required tree locations.
- 11. Wall footings shall not restrict landscape; max 12" in front of footing with 12" of cover.
- 12. Provide a solid surface path from the driveway to the side yard gate for entry and trash bin access
- 13. AC units shall be located in residential side yards, opposite the main back yard access path with gate, or a second gate and solid surface path on the opposite side added for access.
- 14. Stormwater infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division before installation.
- 15. Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width, and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings, or on-site tree planting. Add tree protection notes on construction and demo plans.
- 16. Add notes for any tree removal to occur outside of typical nesting season (February 1 through August 31) or per the specific plan EIR mitigation Measures.
- 17. After a project's entitlement approval, the applicant shall pay all applicable fees at a rate established by City Council.



CITY OF ONTARIO MEMORANDUM

TO: Elly Antuna, Associate Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: July 6, 2021

SUBJECT: PMTT21-012 - A Parcel Map to subdivide .31 acres of land into 2 parcels

located at the southwest corner of Maitland Street and Euclid Avenue (1004 South Euclid Avenue), within the LDR5 (Low Density Residential-2.1 to 5.0 DU/Acre) and EA (Euclid Avenue Overlay) zoning districts

(APN: 1049-563-10).

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2019 CBC Type of Construction: Type V-B wood frame

B. Type of Roof Materials: non-rated

C. Ground Floor Area(s): Various Unit A = 1,777 Sq. Ft. / Unit B = 1,787 Sq. Ft.

D. Number of Stories: One Story

E. Total Square Footage: 4,642 Sq. Ft.

F. 2019 CBC Occupancy Classification(s): R-3, U

CONDITIONS OF APPROVAL:

1.0 GENERAL

- I.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario website at www.ontarioca.gov/Fire/Prevention.

2.0 FIRE DEPARTMENT ACCESS

- ∑ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.

3.0 WATER SUPPLY

4.0 FIRE PROTECTION SYSTEMS

■ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13 D. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ∑ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ∑ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ∑ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.



PLANNING COMMISSION STAFF REPORT

December 20, 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NOS: PDEV21-012 and PCUP21-004

SUBJECT: A Development Plan (File No. PDEV21-012) to construct a 2,370-square-foot commercial building for a fast-food restaurant (Sonic) in conjunction with a Conditional Use Permit (File No. PCUP21-004) to establish a drive-thru facility on 0.72-acre of land located at the southwest corner of Mountain Avenue and Sixth Street, within the Main Street land use district of the Mountain Village Specific Plan (APN: 1008-431-25); **submitted by Coast to Coast Commercial, LLC.**

PROPERTY OWNER: Pacific/Lewis Proprieties and Kendrew Investments, LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve File Nos. PDEV21-012 and PCUP21-004, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 0.72-acre of land located at the southwest corner of Mountain Avenue and Sixth Street, within the Main Street land use

district of the Mountain Village Specific Plan, and is depicted in Figure 1: Project Location, right. The property surrounding the Project site is characterized by general commercial land uses and is developed with a Carl's Jr. fast-food restaurant to the south, a multi-story office building and ancillary off-street parking to the west, and a combination of commercial retail/restaurant uses and Edwards Theater to the north, across Sixth Street. The property to the east, across Mountain Avenue is developed with a commercial office building. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table



Figure 1: Project Location

Case Planner:	Jeanie Irene Aguilo
Planning Director Approval:	
Submittal Date:	03/15/21

Hearing Body	Date	Decision	Action
DAB	12/20/21	Approved	Recommend
PC	12/20/21		Final
CC			

December 20, 2021

located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) <u>Background</u> — The Mountain Village Specific Plan (File No. 5104-SP) and related Environmental Impact Report (EIR No. 97-1) were adopted by the City Council on December 2, 1997. The Mountain Village Specific Plan encompasses approximately 60 acres of land generally located along both sides of Mountain Avenue, bounded by I-10 (San Bernardino Freeway) to the north, portions of Palmetto and Mountain Avenues to the east, Fifth Street to the south, and portions of Elderberry Court to the west. The purpose of the Specific Plan was to guide in the redevelopment of blighted and underutilized properties at one of Ontario's major gateways. The majority of the Specific Plan is presently built-out with a combination of commercial, office, fast-food restaurants, and residential uses, with a few vacant lots remaining, including the Project Site, where the applicant is proposing to construct a Sonic fast-food restaurant.

Sonic restaurants were founded in Oklahoma in the early 1950s, initially as a walk-up root beer stand, and over time, began serving hamburgers, hot dogs, drinks, and frozen desserts. Similar to many diners in the 1950s and 60s, Sonic transformed their ordering process by providing a carhop dining service (commonly known as "carhops"). However, unlike other carhops where servers came to the vehicle to take your order, Sonic installed speakers at each parking stall, allowing customers to place food orders and a server in roller skates would then bring restaurant orders directly to people in their cars. Sonic continues to evolve and presently the majority of their restaurants also include outdoor patio seating and drive-thru lanes to serve their customers. Currently there are over 3,500 Sonic restaurants located throughout 46 states, with the closest restaurant located in Rancho Cucamonga on Fourth Street, directly north of Ontario Mills.

On March 15, 2021, the applicant submitted a Development Plan (File No. PDEV21-012) requesting to construct a 2,370-square-foot fast-food restaurant, in conjunction with a Conditional Use Permit (File No. PCUP21-004) to establish the drive-thru facility.

(2) <u>Site Design/Building Layout</u> — The proposed fast-food restaurant and drive-thru is centered along the northern portion of the property, oriented in an east-west configuration, with the primary entrance facing east, toward Mountain Avenue and a secondary entrance facing north, towards Sixth Street. The building is setback 76 feet from the south property line, 87 feet from the west property line, 63 feet from the east property line (Mountain Avenue), and 17 feet from the north property line (Sixth Street). A plaza has been provided at the northeast corner of the project site and parking for employees and customers has been provided immediately to the south of the building.

The entrance to the drive-thru is located at the southeast corner of the Project site and will circulate from north to west, turning around the building south to east in a counterclockwise direction and terminating towards the east end of the building. The

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Project provides drive-thru lane stacking for 16 vehicles (see Exhibit B—Site Plan, attached).

To accommodate Sonic's carhop dining service, the Project is proposing two covered canopies within the parking lot to provide a total of 14 vehicle dine-in parking stalls. Each oversized parking stall (12 feet by 18 feet) will be equipped with an intercom speaker to accommodate Sonic's vehicle dine-in carhop service.

- (3) <u>Proposed Use</u> Sonic is a fast-food restaurant that provides drive-thru, dine-in, outdoor dining, and carhop services with a menu that offers breakfast, lunch, happy hour (2:00 PM to 5:00 PM), dinner, and frozen treats. Proposed business hours for dine-in, outdoor dining, and carhop services are Sunday through Thursday, from 8:00 AM to 12:00 AM, and Friday through Saturday, from 8:00 AM to 2:00 AM. The drive-thru will operate 24-hours per day. The restaurant will operate with approximately 10 employees per shift, for a total of 50 employees.
- (4) <u>Floor Plan</u> The restaurant is divided into six general areas, that include an indoor dining area that accommodates 36 patrons, kitchen, cooler, and freezer area, janitor and storage area, restrooms, and a 571-square-foot outdoor patio located on the east side of the building (see Exhibit C—Floor Plan, attached).
- (5) <u>Site Access/Circulation</u> There are three points of vehicular access proposed to serve the project site. Primary access will be taken from Mountain Avenue via an existing 26-foot-wide driveway located off-site, on the adjoining parcel directly south of the project site that is presently developed with a Carl's Jr. fast-food restaurant. The Project is proposing to construct a 26-foot-wide drive-aisle located at the southeast corner of the site that will connect to the existing Mountain Avenue driveway and require off-site improvements on the adjoining Carl's Jr. property. The off-site improvements include the removal of three to four parking stalls and the reconfiguration/removal of the landscape planter located along Carl's Jr. north property line that is necessary to provide access to the Project site. There is an existing reciprocal access easement agreement between the two properties that grants pedestrian, ingress, egress, and vehicular access to the Project site from Carl's Jr. (see Exhibit E—Reciprocal Easement Agreement). The final design of the Carl's Jr. off-site improvements has been conditioned to be subject to Planning Director review and approval.

A second point of vehicular access will be provided from Sixth Street, via a 28-foot-wide driveway located at the northwest corner of the Project site. A third access point will be provided at the southwest corner of the project site, via an existing 26-foot-wide driveway that connects to a 24-foot-wide drive-aisle, which runs north-south between the Project site and the office commercial properties to the west.

(6) <u>Parking</u> — The Project has been parked in accordance with the "restaurant" parking standards of the Mountain Village Specific Plan, which requires that a minimum of one off-street parking space for each 75 square feet of "public service area," with

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outdoor dining area exempt up to 25 percent of the restaurant's built floor area. The Project provides a total of 570 square feet of indoor dining and 517 square feet of outdoor dining, requiring a minimum of 8 off-street parking spaces. The Project proposes to provide a total of 22 off-street parking spaces, exceeding the minimum off-street parking requirements, as shown in the table below.

Parking Summary Table						
Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided		
Restaurant	570 SF	1 off-street parking space per 75 square feet of "public service area"	8	22		
Outdoor Dining Area	571 SF	1 off-street parking space per 75 square feet of "public service area" (Outdoor dining area is exempt from the parking requirement, up to 25 percent of the restaurants-built floor area)	0	0		
		* 593 SF of outdoor dining is allowed (25% of 2,370 total building SF)				
OTAL				22		

<u>Architecture</u> — The proposed Project incorporates a contemporary architectural style that is consistent with the requirements of the Mountain Village Specific Plan and complements the existing commercial development to the north (Edwards Theatre) and south (Carl's Jr., Raising Cane's, and Wal-Mart). The project incorporates design elements consistent with the design guidelines for commercial developments and drive-thru facilities, which are contained in the Specific Plan. The design guidelines require that a building should be designed to ensure that it's massing and proportion, along with its colors and architectural detailing, are consistent on all building walls, giving a four-sided (360-degree architecture) appearance. The exterior walls are treated with a combination of horizontal fiber reinforced cement panels, stone veneer, stucco, and a contrasting color palette that includes red, blue, tan, brown, and beige tones. The eastern elevation, facing Mountain Avenue, incorporates a tower element with a red metal canopy that projects over the main entrance and continues along the north elevation, facing Sixth Street. The drive-thru canopy, located along the south elevation, has been designed to complement the architectural style of the building and includes columns with a stone veneer and an overhead red metal canopy (see Exhibit D— Elevations).

The mechanical equipment will be roof-mounted and obscured from public view by parapet walls and, if necessary, equipment screens, which will incorporate design features consistent with the building's architecture. Staff believes that the proposed project illustrates the type of high-quality architecture promoted by the Development Code. This is exemplified through the use of:

 Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas;

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- Articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall;
- A mix of exterior colors, materials, finishes and fixtures; and
- Incorporation of base and top treatments defined by changes in color, materials, and recessed wall areas. Designed to ensure that it's massing and proportion, along with its colors and architectural detailing, are consistent on all building walls, giving a four-sided (360-degree) appearance.
- (8) <u>Landscaping</u> The Specific Plan requires a minimum 15 percent landscape coverage and 17.2 percent landscape coverage has been provided (see Exhibit E—Landscape Plan, attached). The Project provides substantial landscaping along Mountain Avenue and throughout the Project site. A combination of 48-inch, 36-inch box, and 15-gallon accent and shade trees are proposed including Mexican Blue Palms, Red Push Chinese Pistache and California Sycamore. Existing street trees along Mountain Avenue (Crape Myrtle) and Sixth Street (Crape Myrtle and London Plane) will be protected in place. A variety of shrubs and groundcovers are also being provided, which are low water usage or drought tolerant. Additionally, accent and pedestrian lighting will be provided at key locations pursuant to the requirements of the Specific Plan.

The Mountain Village Specific Plan requires a Village Wall and plazas with entry gateways to be constructed at key locations to create a sense of identity along Mountain Avenue at a scale that can relate to both motorists and pedestrians. The Project will construct a plaza, entry gateway and complete the final portion of the Village Wall along Mountain Avenue and Sixth Street, consistent with the Specific Plan and properties located south of the Project site. On Mountain Avenue the Village Wall will feature a sand color split face block wall with decorative pilasters and matching cap, with an overall height of four feet. At the intersection of Mountain Avenue and Sixth Street, the entry gateway will feature two 12-foot-high split-face pilasters that will frame the entrance of the corner plaza and tie into the Village Wall. The Project has been conditioned to connect the two 12-foot-high pilasters with a heavy timber trellis and gateway sign consistent with the Specific Plan. On Sixth Street, the Village Wall will be constructed of metal panels (greenscreen) and split-face pilasters spaced 15 feet apart, with an overall height of four feet. The Project has also been conditioned to provide a minimum 2-foot-wide landscape planter north of the Sixth Street Village Wall to provide adequate landscaping for screening the wall and continue the landscape plant palette and design from Mountain Avenue.

The Specific Plan requires the plaza at the intersection of Mountain Avenue and Sixth Street to be a minimum of 4,356 square feet (0.1-acre) in size and the Project is providing a 5,300 square foot plaza area. To accommodate the proposed drive-thru facility, the plaza was designed in a rectangular shape instead of the traditional square shape design that exist on the properties to the north. The plaza provides two separate seating areas divided by a portion of the drive-thru lane, decorative pavement, seating, tables, and extensive landscape areas. The Project has been conditioned to provide decorative

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paving within portions of the drive-thru lane to frame the pedestrian walkway connecting the two seating areas.

- (9) <u>Signage</u> The project is required to be consistent with the design guidelines and requirements of the Mountain Village Specific Plan, which allows for the installation of freestanding signage incorporated into the Village Wall. The Project has been conditioned to submit a sign plan for review and approval prior to the installation of any signage. The conceptual sign location and design have been included into the proposed building elevations for reference.
- (10) <u>Utilities (drainage, sewer)</u> Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The on-site drainage will be conveyed via a catch basin and on-site storm drain to an underground basin located on the south side of the project site underneath the parking area and any overflow drainage will be conveyed to the curb and gutter along Mountain Avenue.
- (11) Conditional Use Permit (File No. PCUP21-004) The Mountain Village Specific Plan requires approval of a Conditional Use Permit to establish a drive-thru facility in conjunction with a restaurant. A Conditional Use Permit review is required to ensure the compatibility of the proposed use with adjacent existing and proposed uses by identifying potential nuisance activities and establishing measures for appropriate mitigation accordingly. The Project site is located within the Main Street District of the Mountain Village Specific Plan and the land uses immediately south of the Project Site have been developed as fast-food restaurants with drive-thru facilities (Carl's Jr. and Raising Canes), consistent with the proposed Project. Staff believes that the Project's overall site plan and drive-thru facility has been designed to provide adequate stacking of up to 16 vehicles to sufficiently mitigate any potential negative impacts that may be associated with the proposed use. Additionally, the nearby businesses within and surrounding area will not be exposed to any impacts beyond those that would normally be associated with any other fast-food restaurant with a drive-thru facility.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

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(1) <u>City Council Goals</u>.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

(2) Vision.

Distinctive Development:

- Commercial and Residential Development
- > Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
- ➤ <u>G1-2 Long-term Benefit</u>. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

Land Use Element:

- <u>Goal LU1</u>: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-1 Strategic Growth</u>. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
- ➤ <u>LU1-6 Complete Community</u>: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
 - Goal LU2: Compatibility between a wide range of uses.

➤ <u>LU2-6</u>: <u>Infrastructure Compatibility</u>: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
- ➤ <u>CE2-1 Development Projects</u>. We require new development and redevelopment to create unique, high-quality places that add value to the community.
- ➤ <u>CE2-2 Development Review</u>. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
- ➤ <u>CE2-4 Protection of Investment</u>. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- ➤ <u>CE2-5 Private Maintenance</u>. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
- > <u>\$1-1</u> Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- <u>Goal CD1</u>: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
- ➤ <u>CD1-1 City Identity</u>. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
- ➤ <u>CD1-2 Growth Areas</u>. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

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- ➤ <u>CD1-3 Neighborhood Improvement</u>. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- <u>Goal CD2</u>: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
- ➤ <u>CD2-1 Quality Architecture</u>. We encourage all development projects to convey visual interest and character through:
- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- ➤ <u>CD2-7 Sustainability</u>. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- ➤ <u>CD2-8 Safe Design</u>. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- ➤ <u>CD2-9 Landscape Design</u>. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- ➤ <u>CD2-10 Surface Parking Areas</u>. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
- ➤ <u>CD2-11 Entry Statements</u>. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

- ➤ <u>CD2-12 Site and Building Signage</u>. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.
- ➤ <u>CD2-13 Entitlement Process</u>. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
- <u>Goal CD3</u>: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.
- ➤ <u>CD3-1 Design</u>. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.
- ➤ <u>CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas</u>. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.
- ➤ <u>CD3-3 Building Entrances</u>. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- ➤ <u>CD3-5 Paving</u>. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- ightharpoonup CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
- <u>Goal CD5</u>: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- ➤ <u>CD5-1 Maintenance of Buildings and Property</u>. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.
- > <u>CD5-2 Maintenance of Infrastructure</u>. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not

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one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AlrPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, meeting each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services.

CONDITIONS OF APPROVAL: See attached department reports.

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TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

301100110	sorrounding zoning and tand use.					
	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use		
Site	Vacant	General Commercial (GC)	Mountain Village Specific Plan	Main Street District		
North	Retail	General Commercial (GC)	Mountain Village Specific Plan	Entertainment District		
South	Carl's Jr. Restaurant	GC (General Commercial)	Mountain Village Specific Plan	Main Street District		
East	Office	GC (General Commercial)	Mountain Village Specific Plan	Sixth Street District		
West	Medical Offices	GC (General Commercial)	Mountain Village Specific Plan	Main Street District		

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	0.72-acre	N/A	Υ
Lot/Parcel Size:	0.72-acre	N/A	Υ
Building Area:	2,370 SF	N/A	Υ
Floor Area Ratio:	0.10	0.4 (Max.)	Υ
Building Height:	30 FT	35 FT (Max.)	Υ

Off-Street Parking:

on-sheer ranking.						
Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided		
Restaurant	570 SF	One off-street parking space per 75 square feet of "public service area"	8	22		
Outdoor Dining Area	571 SF	One off-street parking space per 75 square feet of "public service area" (Outdoor dining area is exempt from the parking requirement, up to 25 percent of the restaurants-built floor area) * 593 SF of outdoor dining is allowed (25% of 2,370 total building SF)	0	0		
TOTAL			8	22		

Exhibit A—PROJECT LOCATION MAP



Exhibit B—SITE PLAN



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Exhibit C—FLOOR PLAN

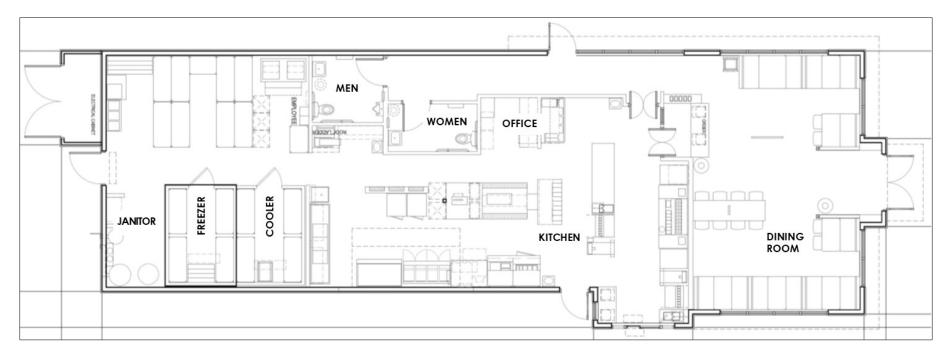
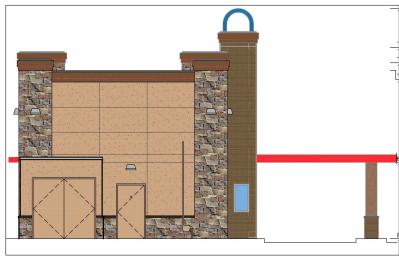




Exhibit D—ELEVATIONS



North Elevation



West Elevation

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Exhibit D—ELEVATIONS CONTINUED

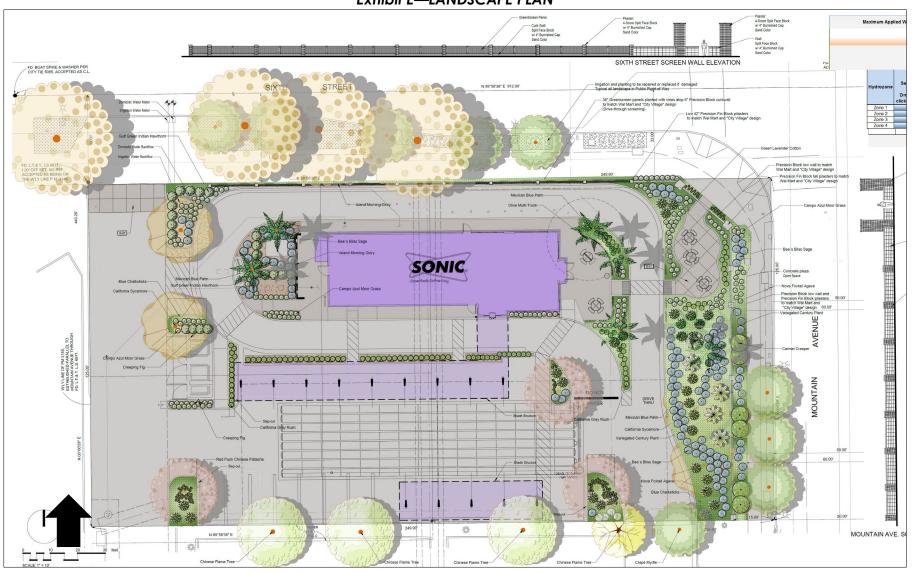


North Elevation



East Elevation

Exhibit E—LANDSCAPE PLAN



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Exhibit F—RECIPROCAL EASEMENT AGREEMENT

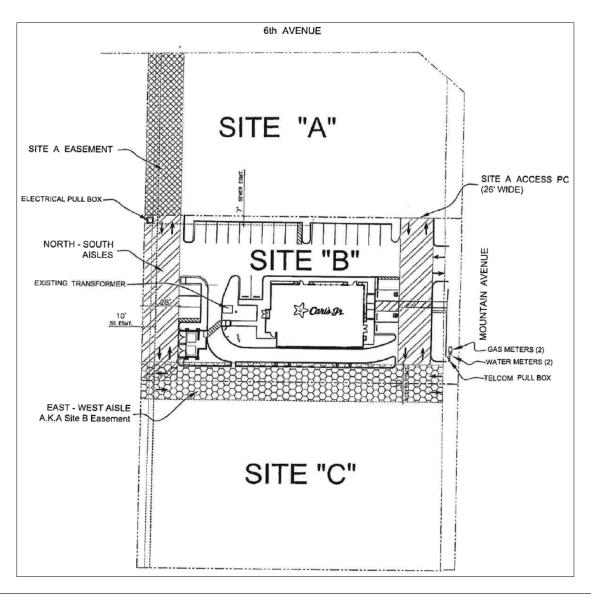


Exhibit G-MOUNTAIN AVENUE VILLAGE WALL

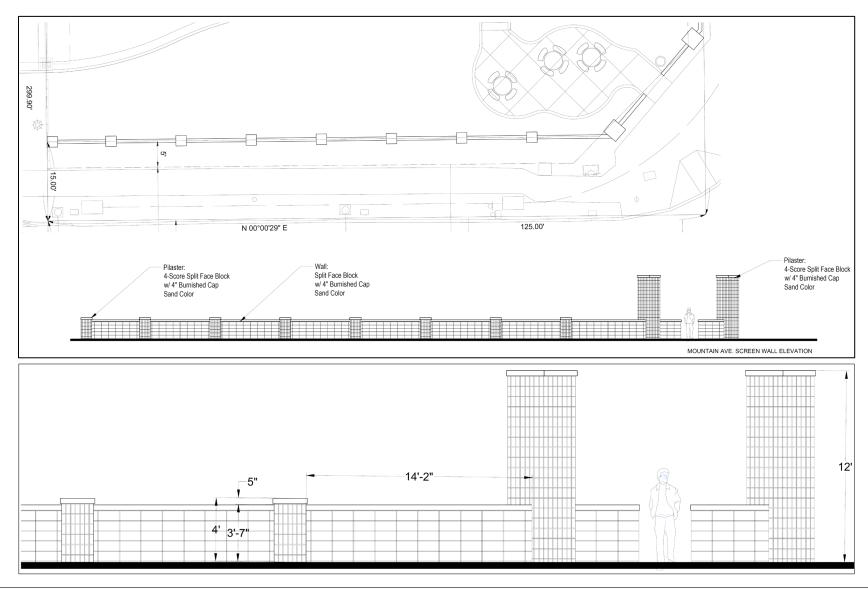
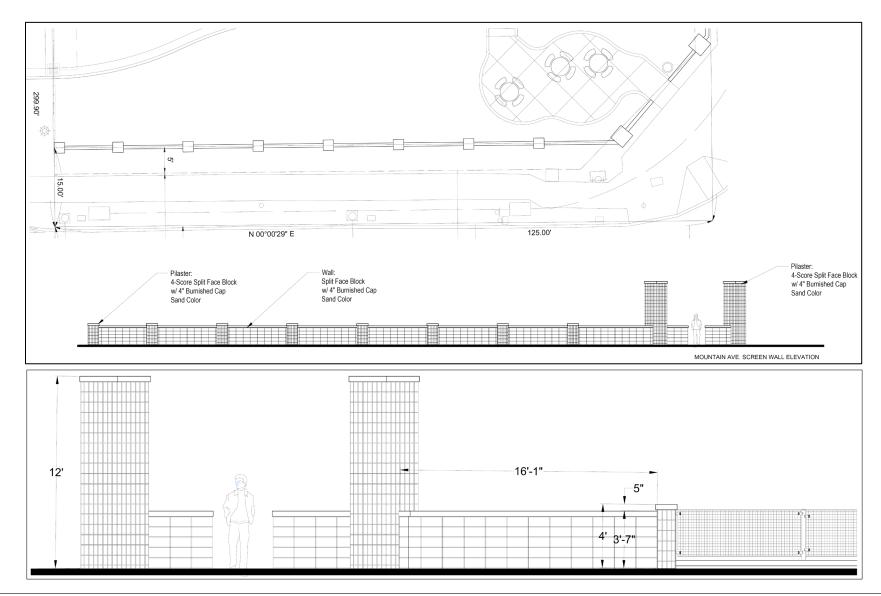


Exhibit H—SIXTH STREET VILLAGE WALL



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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PCUP21-004, A CONDITIONAL USE PERMIT TO ESTABLISH A DRIVE-THRU FACILITY IN CONJUNCTION WITH A 2,370 SQUARE FOOT COMMERCIAL BUILDING FOR A FAST-FOOD RESTAURANT (SONIC) ON 0.72-ACRE OF LAND LOCATED AT THE SOUTHWEST CORNER OF MOUNTAIN AVENUE AND SIXTH STREET, WITHIN THE MAIN STREET DISTRICT OF THE MOUNTAIN VILLAGE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1008-431-25.

WHEREAS, COAST TO COAST COMMERCIAL, LLC ("Applicant") has filed an Application for the approval of a Conditional Use Permit, File No. PCUP21-004, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.72 acre of land located at the southwest corner of Mountain Avenue and Sixth Street within the Main Street District of the Mountain Village Specific Plan, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Main Street District of the Mountain Village Specific Plan, and is developed with commercial retail/restaurant uses and a movie theatre. The property to the east is within the Sixth Street District of the Mountain Village Specific Plan and is developed with medical offices. The property to the south is within the Main Street District of the Mountain Village Specific Plan and is developed with a Carl's Jr. fast-food restaurant. The property to the west is within the Main Street District of the Mountain Village Specific Plan, and is developed with medical offices; and

WHEREAS, a Development Plan (File No. PDEV21-012) to construct a 2,370-square-foot fast-food restaurant (Sonic) was filed in conjunction the Conditional Use Permit (File No. PCUP21-004) to construct and establish a drive-thru facility; and

WHEREAS, the Mountain Village Specific Plan (File No. 5104-SP) and related Environmental Impact Report (EIR No. 97-1) were adopted by the City Council on December 2, 1997. The Mountain Village Specific Plan encompasses approximately 60 acres of land generally located along both sides of Mountain Avenue, bounded by I-10 (San Bernardino Freeway) to the north, portions of Palmetto and Mountain Avenues to the east, Fifth Street to the south, and portions of Elderberry Court to the west. The purpose of the Specific Plan was to guide in the redevelopment of blighted and underutilized properties at one of Ontario's major gateways. The majority of the Specific Plan is presently built-out with a combination of commercial, office, fast-food restaurants, and residential uses, with a few vacant lots remaining, including the Project Site; and

WHEREAS, the proposed fast-food restaurant and drive-thru is centered along the northern portion of the property, oriented in an east-west configuration, with the primary entrance facing east, toward Mountain Avenue and a secondary entrance facing north, towards Sixth Street. The building is setback 76 feet from the south property line, 87 feet from the west property line, 63 feet from the east property line (Mountain Avenue), and 17 feet from the north property line (Sixth Street). A plaza has been provided at the northeast corner of the project site and parking for employees and customers has been provided immediately to the south of the building; and

WHEREAS, the entrance to the drive-thru is located at the southeast corner of the Project site and will circulate from north to west, turning around the building south to east in a counterclockwise direction and terminating towards the east end of the building. The Project provides drive-thru lane stacking for 16 vehicles; and

WHEREAS, Sonic is a fast-food restaurant that provides drive-thru, dine-in, outdoor dining, and carhop services with a menu that offers breakfast, lunch, happy hour (2:00 PM to 5:00 PM), dinner, and frozen treats. Proposed business hours for dine-in, outdoor dining, and carhop services are Sunday through Thursday, from 8:00 AM to 12:00 AM, and Friday through Saturday, from 8:00 AM to 2:00 AM. The drive-thru will operate 24-hours per day. The restaurant will operate with approximately 10 employees per shift, for a total of 50 employees; and

WHEREAS, the Project has been parked in accordance with the "restaurant" parking standards of the Mountain Village Specific Plan, which requires that a minimum of one off-street parking space for each 75 square feet of "public service area," with outdoor dining area exempt up to 25 percent of the restaurant's-built floor area. The Project provides a total of 570 square feet of indoor dining and 517 square feet of outdoor dining, requiring a minimum of 8 off-street parking spaces. The Project proposes to provide a total of 22 off-street parking spaces, exceeding the minimum off-street parking requirements; and

WHEREAS, the Mountain Village Specific Plan requires approval of a Conditional Use Permit to establish a drive-thru facility in conjunction with a restaurant. A Conditional Use Permit review is required to ensure the compatibility of the proposed use with adjacent existing and proposed uses by identifying potential nuisance activities and establishing measures for appropriate mitigation accordingly. The Project site is located within the Main Street District of the Mountain Village Specific Plan and the land uses immediately south of the Project Site have been developed as fast-food restaurants with drive-thru facilities (Carl's Jr. and Raising Canes), consistent with the proposed Project. The Project's overall site plan and drive-thru facility has been designed to provide adequate stacking of up to 16 vehicles to sufficiently mitigate any potential negative impacts that may be associated with the proposed use. Additionally, the nearby businesses within and surrounding area will not be exposed to any impacts beyond those

that would normally be associated with any other fast-food restaurant with a drive-thru facility; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-058 recommending that the Planning Commission approve the Application; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, meeting each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services; and
- (3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and
- (4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is

not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seg.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 4</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

- (1) The scale and intensity of the proposed land use would be consistent with the scale and intensity of land uses intended for the particular zoning or land use district. The proposed location of the Conditional Use Permit is in accord with the objectives and purposes of the City of Ontario Development Code and the Main Street District of the Mountain Village Specific Plan, and the scale and intensity of land uses intended for the zoning district in which the use is proposed to be located. Furthermore, the proposed fast-food restaurant with drive-thru facility will be established and operated consistent with the objectives and purposes, and development standards and guidelines, of the Main Street District of the Mountain Village Specific Plan; and
- (2) The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed fast-food restaurant with drive-thru

facility will be located within the General Commercial land use district of the Policy Plan Land Use Map, and the Main Street District of the Mountain Village Specific Plan. The development standards, and the conditions of approval under which the proposed land use will be established, operated, and maintained, are consistent with the goals, policies, plans, and exhibits of the Vision, City Council Priorities, and Policy Plan (General Plan) components of The Ontario Plan; and

- (3) The proposed use at the proposed location, and the manner in which it will be operated and maintained, is consistent with the objectives and requirements of the Development Code and any applicable specific plan or planned unit development. The proposed fast-food restaurant with drive-thru facility is located within the Main Street District of the Mountain Village Specific Plan, and has been reviewed and conditioned to ensure the establishment, operation and maintenance of the proposed land use, consistent with all applicable objectives, purposes, standards, and guidelines of the Development Code and Mountain Village Specific Plan; and
- (4) The establishment, maintenance, and operation of the proposed use at the proposed location would not be detrimental or injurious to property and improvements within the vicinity, nor would it be detrimental to the health, safety, or general welfare of persons residing or working in the surrounding neighborhood. The Project site is located within the Main Street District of the Mountain Village Specific Plan, for which a drive-thru facility in conjunction with a restaurant is a conditionally permitted use. The project will be conditioned to ensure that it will operate and be properly maintained, therefore the project will not be detrimental or injurious to surrounding property and improvements. The Development Advisory Board has required certain safeguards, and imposed certain conditions of approval, which have been established to ensure that: [i] the purposes of the Mountain Village Specific Plan are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; and [iv] the Project will be in harmony with the surrounding area in which it is proposed to be located.

<u>SECTION 5</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 6</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 7</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 8</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

- - - - - - - - - - - - -

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PCUP21-004 December 20, 2021 Page 8	
STATE OF CALIFORNIA COUNTY OF SAN BERNARDINO CITY OF ONTARIO)))
City of Ontario, DO HEREBY CERTIF	Pro Tempore of the Planning Commission of the Y that foregoing Resolution No was duly ommission of the City of Ontario at their special / the following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A:

File No. PCUP21-004 Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 12/7/2021

File No: PCUP21-004

Related Files: PDEV21-012

Project Description: A Conditional Use Permit to establish a 2,370 square foot fast food restaurant with drive-thru (Sonic) on 0.72-acre of land located at the southwest corner of Mountain Avenue and Sixth Street, within the Main Street land use district of the Mountain Village Specific Plan (APN: 1008-431-25); **submitted by Coast to Coast Commercial, LLC.**

Prepared By: Jeanie Irene Aguilo, Associate Planner

<u>Phone</u>: 909.395.2418 (direct) <u>Email</u>: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:
- 2.1 <u>Time Limits.</u> Conditional Use Permit approval shall become null and void two years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director, except that a Conditional Use Permit approved in conjunction with a Development Plan shall have the same time limits as said Development Plan. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.
- **2.2** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility, and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.
- **2.3** <u>Security Standards</u>. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.4 Environmental Review.

- (a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:
- **(i)** The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;
- (ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;
- (iii) The project site has no value as habitat for endangered, rare, or threatened species;
- (iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and
- (v) The Project site can be adequately served by all required utilities and public services.
- **(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).
- (c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.
- **2.5** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.6 Additional Fees.

- ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.
- **(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.

2.7 Additional Requirements.

(a) All applicable Conditions of Approval from other City departments shall be met and addressed by the applicant.

AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.:	PDEV21-012 & P	CUP21-004		Reviewed By:	
Address:	SWC Mountain Ave & 6th Street			Lorena Mejia	
APN:	1008-431-25			Contact Info:	
Existing Land Use:	and Vacant			909-395-2276	
Proposed Land Commercial drive-thru restaurant 2,370 SF (Sonics) Use:			Project Planner: Jeanie Aguilo		
Site Acreage:	Site Acreage: 0.72 Proposed Structure Height: 30 FT			Date: July 6, 2021	
ONT-IAC Projec	t Review: n/a			CD No.: 2021-023	
Airport Influence	Area: ON	T		PALU No.: n/a	
TI	ne project is	impacted by the follow	ing ONT ALUCP Compa	tibility Zones:	
Safe		Noise Impact	Airspace Protection	Overflight Notification	
Zone 1 Zone 1A Zone 2 Zone 3 Zone 4 Zone 5 Zone 1	Zone		High Terrain Zone FAA Notification Surfaces Airspace Obstruction Surfaces Airspace Avigation Easement Area Allowable Height: 200 FT + owing Chino ALUCP Sate Zone 4 Zone 4		
Allowable Heig	Jnt:				
CONSISTENCY DETERMINATION					
This proposed Project is:					
The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.					
Airport Planner Signature:					



CITY OF ONTARIO MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: March 25, 2021

SUBJECT: PCUP21-004 - A Conditional Use Permit to establish one (1) commercial

drive-thru restaurant building totaling 2,370 square feet on 0.72 acres of land located at the southwest corner of Mountain Avenue and 6th Street, within the Main Street land use district of the Mountain Village Specific

Plan (APN(s): 1008-431-25). Related File(s): PDEV21-012.

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

Standard Conditions of Approval apply. See conditions under PDEV21-012.



CITY OF ONTARIO

MEMORANDUM

то:	Scott Murphy, Community Devel Rudy Zeledon, Planning Director Diane Ayala, Advanced Planning Charity Hernandez, Economic D Matt Montieth, Building Departm Raymond Lee, Engineering Dep Jamie Richardson, Landscape P Dennis Mejia, Municipal Utility C Gabriel Gutierrez, Police Depart Mike Gerken, Deputy Fire Chief/ Jay Bautista, T. E., Traffic/Trans Lorena Mejia, Airport Planning Eric Woosley, Engineering/NPDI Angela Magana, Community Im Jimmy Chang, IPA Department	r (Copy of memo only) g Division (Copy of memo only) evelopment eent artment Planning Division company ment Fire Marshal eportation Manager ES eprovement (Copy of memo only)		
FROM:	Jeanie Irene Aguilo, Assoc	iate Planner	REVISION NO. 2	
DATE:	October 22, 2021		b	
SUBJECT:	FILE #: PCUP21-004	Finance Acct#:		
of your DAB	3 report to the Planning Departmen	r review. Please send one (1) copy and by . Permit to establish a 2,370 square		
restaurant wand Sixth St	vith drive-thru (Sonic) on 0.72-acre	e of land located at the southwest con se district of the Mountain Village Sp	rner of Mountain Avenue	
The pla	an does adequately address the de	partmental concerns at this time.		
☐ No comments				
See previous report for Conditions				
Report attached (1 copy and email 1 copy)				
	Standard Conditions of Approva	I apply		
The pla	an does not adequately address the	e departmental concerns.		
	The conditions contained in the a	attached report must be met prior to	scheduling for	

Department Discon Date Title Date

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV21-012, A DEVELOPMENT PLAN TO CONSTRUCT A 2,370 SQUARE FOOT COMMERCIAL BUILDING FOR A FAST-FOOD RESTAURANT (SONIC) WITH A DRIVE-THRU FACILITY ON 0.72-ACRE OF LAND LOCATED AT THE SOUTHWEST CORNER OF MOUNTAIN AVENUE AND SIXTH STREET, WITHIN THE MAIN STREET DISTRICT OF THE MOUNTAIN VILLAGE SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1008-431-25.

WHEREAS, COAST TO COAST COMMERCIAL, LLC ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-012, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 0.72 acre of land located at the southwest corner of Mountain Avenue and Sixth Street within the Main Street District of the Mountain Village Specific Plan, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Main Street District of the Mountain Village Specific Plan, and is developed with commercial retail/restaurant uses and a movie theatre. The property to the east is within the Sixth Street District of the Mountain Village Specific Plan and is developed with medical offices. The property to the south is within the Main Street District of the Mountain Village Specific Plan and is developed with a Carl's Jr. fast-food restaurant. The property to the west is within the Main Street District of the Mountain Village Specific Plan, and is developed with medical offices; and

WHEREAS, the Mountain Village Specific Plan (File No. 5104-SP) and related Environmental Impact Report (EIR No. 97-1) were adopted by the City Council on December 2, 1997. The Mountain Village Specific Plan encompasses approximately 60 acres of land generally located along both sides of Mountain Avenue, bounded by I-10 (San Bernardino Freeway) to the north, portions of Palmetto and Mountain Avenues to the east, Fifth Street to the south, and portions of Elderberry Court to the west. The purpose of the Specific Plan was to guide in the redevelopment of blighted and underutilized properties at one of Ontario's major gateways. The majority of the Specific Plan is presently built-out with a combination of commercial, office, fast-food restaurants, and residential uses, with a few vacant lots remaining, including the Project Site, where the applicant is proposing to construct a Sonic fast-food restaurant; and

WHEREAS, a Development Plan (File No. PDEV21-012) to construct a 2,370-square-foot fast-food restaurant (Sonic) was filed in conjunction the Conditional Use Permit (File No. PCUP21-004) to construct and establish a drive-thru facility; and

WHEREAS, the Mountain Village Specific Plan (File No. 5104-SP) and related Environmental Impact Report (EIR No. 97-1) were adopted by the City Council on December 2, 1997. The Mountain Village Specific Plan encompasses approximately 60 acres of land generally located along both sides of Mountain Avenue, bounded by I-10 (San Bernardino Freeway) to the north, portions of Palmetto and Mountain Avenues to the east, Fifth Street to the south, and portions of Elderberry Court to the west. The purpose of the Specific Plan was to guide in the redevelopment of blighted and underutilized properties at one of Ontario's major gateways. The majority of the Specific Plan is presently built-out with a combination of commercial, office, fast-food restaurants, and residential uses, with a few vacant lots remaining, including the Project Site; and

WHEREAS, the proposed fast-food restaurant and drive-thru is centered along the northern portion of the property, oriented in an east-west configuration, with the primary entrance facing east, toward Mountain Avenue and a secondary entrance facing north, towards Sixth Street. The building is setback 76 feet from the south property line, 87 feet from the west property line, 63 feet from the east property line (Mountain Avenue), and 17 feet from the north property line (Sixth Street). A plaza has been provided at the northeast corner of the project site and parking for employees and customers has been provided immediately to the south of the building; and

WHEREAS, the entrance to the drive-thru is located at the southeast corner of the Project site and will circulate from north to west, turning around the building south to east in a counterclockwise direction and terminating towards the east end of the building. The Project provides drive-thru lane stacking for 16 vehicles; and

WHEREAS, the Project is providing two covered canopies within the parking lot to provide a total of 14 vehicle dine-in parking stalls each equipped with an intercom speaker to accommodate Sonic's carhop dining service; and

WHEREAS, Sonic is a fast-food restaurant that provides drive-thru, dine-in, outdoor dining, and carhop services with a menu that offers breakfast, lunch, happy hour (2:00 PM to 5:00 PM), dinner, and frozen treats. Proposed business hours for dine-in, outdoor dining, and carhop services are Sunday through Thursday, from 8:00 AM to 12:00 AM, and Friday through Saturday, from 8:00 AM to 2:00 AM. The drive-thru will operate 24-hours per day. The restaurant will operate with approximately 10 employees per shift, for a total of 50 employees; and

WHEREAS, the restaurant is divided into six general areas, that include an indoor dining area that accommodates 36 patrons, kitchen, cooler, and freezer area, janitor and storage area, restrooms, and a 571-square-foot outdoor patio located on the east side of the building; and

WHEREAS, there are three points of vehicular access proposed to serve the project site. Primary access will be taken from Mountain Avenue via an existing 26-footwide driveway located off-site on the adjoining parcel directly south of the project site that is presently developed with a Carl's Jr. fast-food restaurant. There is an existing reciprocal access easement agreement between the two properties that grants pedestrian, ingress, egress, and vehicular access to the Project site from Carl's Jr.; and

WHEREAS, the final design of the Carl's Jr. off-site improvements to provide access to the Project site shall be subject to Planning Director review and approval; and

WHEREAS, a second point of vehicular access will be provided from Sixth Street, via a 28-foot-wide driveway located at the northwest corner of the Project site. A third access point will be provided at the southwest corner of the project site, via an existing 26-foot-wide driveway that connects to a 24-foot-wide drive-aisle, which runs north-south between the Project site and the office commercial properties to the west; and

WHEREAS, the Project has been parked in accordance with the "restaurant" parking standards of the Mountain Village Specific Plan, which requires that a minimum of one off-street parking space for each 75 square feet of "public service area," with outdoor dining area exempt up to 25 percent of the restaurant's-built floor area. The Project provides a total of 570 square feet of indoor dining and 517 square feet of outdoor dining, requiring a minimum of 8 off-street parking spaces. The Project proposes to provide a total of 22 off-street parking spaces, exceeding the minimum off-street parking requirements; and

WHEREAS, the proposed Project incorporates a contemporary architectural style that is consistent with the requirements of the Mountain Village Specific Plan. The building has been designed to ensure that it's massing and proportion, along with its colors and architectural detailing, are consistent on all building walls, giving a four-sided (360-degree architecture) appearance. The building's exterior walls are treated with a combination of horizontal fiber reinforced cement panels, stone veneer, stucco, and a contrasting color palette that includes red, blue, tan, brown, and beige tones; and

WHEREAS, the drive-thru canopy, located along the south elevation, has been designed to complement the architectural style of the building and includes columns with a stone veneer and an overhead red metal canopy; and

WHEREAS, the Specific Plan requires a minimum 15 percent landscape coverage and 17.2 percent landscape coverage has been provided; and

WHEREAS, the Mountain Village Specific Plan requires a Village Wall and plazas with entry gateways to be constructed at key locations to create a sense of identity along Mountain Avenue at a scale that can relate to both motorists and pedestrians. The Project

will construct a plaza, entry gateway and complete the final portion of the Village Wall along Mountain Avenue and Sixth Street, consistent with the Specific Plan and properties located south of the Project site; and

WHEREAS, the Specific Plan requires the plaza at the intersection of Mountain Avenue and Sixth Street to be a minimum of 4,356 square feet (0.1-acre) in size and the Project is providing a 5,300 square foot plaza area; and

WHEREAS, public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The on-site drainage will be conveyed via a catch basin and on-site storm drain to an underground basin located on the south side of the project site underneath the parking area and any overflow drainage will be conveyed to the curb and gutter along Mountain Avenue; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (hereinafter referred to as "CEQA"); and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan

(hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-059, recommending that the Planning Commission approve the Application; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The administrative record has been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (2) The Project is categorically exempt from environmental review pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines, meeting each of the following conditions: [1] the Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations; [2] the proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses; [3] the project site has no value as habitat for endangered, rare, or threatened species; [4] approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and [5] the Project site can be adequately served by all required utilities and public services; and

- (3) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and
- (4) The determination of CEQA exemption reflects the independent judgment of the Planning Commission.

SECTION 2: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

- <u>SECTION 4</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:
- (1) The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and

City Council Priorities components of The Ontario Plan. The proposed Project is located within the General Commercial land use district of the Policy Plan Land Use Map, and the Main Street District of the Mountain Village Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

- (2) The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located. The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Main Street District of the Mountain Village Specific Plan, including standards relative to the particular land use proposed (drive-thru restaurant), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and
- (3) The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project. The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Mountain Village Specific Plan are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Mountain Village Specific Plan; and
- (4) The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development. The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Mountain Village Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular commercial land use being proposed. As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Mountain Village Specific Plan.

<u>SECTION 5</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 6</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 7</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 8</u>: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PDEV21-012 December 20, 2021 Page 10	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
City of Ontario, DO HEREBY CERTIFY	Tempore of the Planning Commission of the that foregoing Resolution No was duly mmission of the City of Ontario at their special he following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore
	occidity i to i chipole

ATTACHMENT A:

File No. PDEV21-012 Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 12/7/2021

File No: PDEV21-012

Related Files: PCUP21-004

Project Description: A Development Plan Review to construct a 2,370 square foot fast food restaurant with drive-thru (Sonic) on 0.72-acre of land located at the southwest corner of Mountain Avenue and Sixth Street, within the Main Street land use district of the Mountain Village Specific Plan (APN: 1008-431-25); **submitted by Coast to Coast Commercial, LLC.**

Prepared By: Jeanie Irene Aguilo, Associate Planner

<u>Phone</u>: 909.395.2418 (direct) <u>Email</u>: jaguilo@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:
- **2.1** <u>Time Limits.</u> Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.
- **2.2** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility, and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

- (a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).
- **(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.
- **(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.
- **(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.
- **2.4** <u>Walls and Fences</u>. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

- (a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).
- **(b)** All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.
- (c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.
- (d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

- **(e)** Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).
- **(f)** Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).
- (g) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.
- **(h)** Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.6 <u>Mechanical and Rooftop Equipment.</u>

- (a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.
- **(b)** All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.
- **2.7** <u>Security Standards</u>. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).
- **2.8** <u>Signs</u>. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).
- **2.9** <u>Sound Attenuation</u>. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.10 Environmental Review.

- (a) The proposed project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to **Section 15332 (Class 32, In-Fill Development Projects)** of the CEQA Guidelines, meeting the following conditions:
- **(i)** The Project is consistent with the applicable general plan designation and all applicable general plan policies, as well as the applicable zoning designation and regulations;

- (ii) The proposed development occurs within city limits, on a project site of no more than five acres, and is substantially surrounded by urban uses;
- (iii) The project site has no value as habitat for endangered, rare, or threatened species;
- (iv) Approval of the Project will not result in any significant effects relating to traffic, noise, air quality, or water quality; and
- (v) The Project site can be adequately served by all required utilities and public services.
- **(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).
- (c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.
- **2.11** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.12 Additional Fees.

- ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.
- **(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.

2.13 <u>Additional Requirements</u>.

(a) The final design of the Carl Jr's off-site parking lot improvements (parking space removal and a landscape planter removal/reconfiguration) shall require Planning Director review and approval. The off-site parking lot improvements shall be designed to be in accordance with the existing reciprocal access between Carl's Jr. and the Project site.

- **(b)** Additional striping and signage shall be provided at the drive-thru lane emergency exit area located on the northwest corner of the Project site to prevent vehicles from entering the drive-thru lane improperly.
- **(c)** The drive-thru lane shall incorporate decorative paving at the drive-thru entrance and on both sides of the pedestrian walkway connecting the plaza patio to the restaurant patio.
- **(d)** The 12-foot-high gateway entrance pilasters shall be re-designed to include a heavy timber trellis and gateway sign consistent with the Mountain Village Specific Plan. The final design of the gateway entrance shall require Planning Director review and approval.
- **(e)** A 2-foot-wide landscape planter shall be installed along the north side of the Sixth Street Village Wall. The landscape planting palette shall be similar to the landscape planter located along Mountain Avenue.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

□ DEVELOPMENT PLAN □ OTHER		EL MAP	TRACT MAP
PF	OJECT FILE	NO. <u>PDEV21-01</u>	2
RELA	TED FILE NO	O(S). <u>PCUP21-00</u>	4
⊠ OR	IGINAL 🗌	REVISED:/_/	
CITY PROJECT ENGINEER 8	R PHONE NO:	Antonio Alejos	(909) 395-2384
CITY PROJECT PLANNER &	PHONE NO:	Jeanie Aguilo	(909) 395-2418
DAB MEETING DATE:		December 20 th , 202	1
PROJECT NAME / DESCRIP	ΓΙΟΝ:	PDEV21-012, a Deviconstruct a 2,370 so restaurant with driv 0.72-acre of land with Village Specific Pla	quare foot fast food re-thru (Sonic) on thin the Mountain
LOCATION:		Southwest Corner o	of Mountain Avenue
APPLICANT:		Coast to Coast Con	nmercial, LLC
REVIEWED BY:		Rayworth	ne 1248/2
APPROVED BY:		Raymond Lee, P.E. Assistant City Engi Khoi Do, P.E. City Engineer	Date 12-8-21 Date

Last Revised: 12/7/2021



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1.	PRIC	OR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check Whe Complete	n
	1.01	Dedicate to the City of Ontario, the right-of-way, described below:	
		feet on	
		Property line corner 'cut-back' required at the intersection of	
	1.02	Dedicate to the City of Ontario, the following easement(s):	
	1.03	Restrict vehicular access to the site as follows:	
	1.04	Vacate the following street(s) and/or easement(s):	
		 All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. 	
	1.05	Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.	
	1.06	Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.	
	1.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
	1.08	File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.	
		(1)	
		(2)	
	1.09	Prepare a fully executed Subdivision Agreement (on City approved format and forms) with	

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		accompanying security as required or complete all public improvements.	
	1.10	Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ontarioca.gov) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.	
	1.11	Provide a preliminary title report current to within 30 days.	
	1.12	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	1,:13	Ontario Ranch Developments:	
		☐ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.	
		 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents). 	
		☐ 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).	
	1.14	Other conditions:	
		R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:	
2.	PRIO		
	PRIO	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:	
	PRIO A. GE (Permi	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance	
	PRIO A. GE (Permi	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.	
	PRIO A. GE (Permi	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per LLA06-016	
	PRIO A. GE (Permi) 2.01 2.02 2.03	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per LLA06-016 Instrument No. 2007-0116480. Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the	
	PRIO A. GE (Permi) 2.01 2.02 2.03 2.04	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per LLA06-016 Instrument No. 2007-0116480. Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.	
	PRIO A. GE (Permi) 2.01 2.02 2.03 2.04	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: NERAL ts includes Grading, Building, Demolition and Encroachment) Record Parcel Map/Tract Map No pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code. Submit a PDF of the recorded map to the City Engineer's office. Note that the subject parcel is a recognized parcel in the City of Ontario per LLA06-016 Instrument No. 2007-0116480. Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972. Apply for a:	



	2.06	Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, and drive approaches, as applicable to the project.	
	2.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
\boxtimes	2.08	Submit a soils/geology report.	
\boxtimes	2.09	Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:	
		State of California Department of Transportation (Caltrans) San Bernardino County Road Department (SBCRD) San Bernardino County Flood Control District (SBCFCD) Federal Emergency Management Agency (FEMA) Cucamonga Valley Water District (CVWD) for sewer/water service United States Army Corps of Engineers (USACE) California Department of Fish & Game Inland Empire Utilities Agency (IEUA) Other: Non-interference Letter required from each easement holder listed in the Title Report.	
	2.10	Dedicate to the City of Ontario the right-of-way described below:	
		feet on	
		Property line corner 'cut-back' required at the intersection ofand	
	2.11	Dedicate to the City of Ontario the following easement(s):	
	2.12	Vacate the following street(s) and/or easement(s): a. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. b. The southerly 3 feet of right-of-way along a portion of the Sixth Street property frontage.	
	2.13	Ontario Ranch Developments:	
		☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.	
		☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.	



		☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.	
\boxtimes	2.14	Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.	
	2.15	The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.	
\boxtimes	2.16	Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$16,000, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.	
	2.17	 Other conditions: a. Submit a final Utilities Systems Map (USM) as part of the precise grading plan submittal that conforms to the Conceptual USM dated on 11/4/2021 and meets all of the City's USM requirements. The USM shall show and label all existing/proposed utilities (including all appurtenances such as backflow devices, DCDAs, etc.), sizes, points of connection, and any easements. The final utility design shall comply with all Division of Drinking Water (CCR §64572) Separation Requirements which include a minimum 10' horizontal separation, outside wall to outside wall, between water & sewer and a minimum 5' horizontal separation, outside wall to outside wall, between water & all other utilities. b. The applicant/developer shall provide a copy of an executed reciprocal access easement to be used for the shared driveway approach, between the westerly parcel (APN 1008-431-24) and the easterly parcel (APN 1008-431-25). c. The applicant/developer shall submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project. 	



B. PUBLIC IMPROVEMENTS (See attached Exhibit 'A' for plan check submittal requirements.)

	(000)	actoriou Extinote / 10	pidir onoon odonii	ttai requirements.)		
	2.17	Design and constru Code, current City s the area, if any. The (checked boxes):	standards and spec	ifications, master p	lans and the adopte	ed specific plan for
		Improvement	Mountain Av	Sixth St	Drive Aisle	Street 4
		Curb and Gutter	New; ft. from C/L Replace damaged	New; ft. from C/L Replace damaged	New; ft. from C/L Replace damaged	New; ft. from C/L Replace damaged
		AC Pavement	Replacement Widen additional feet along frontage, including paym't	Replacement Widen additional feet along frontage,	Replacement Widen additional feet along frontage,	Replacement Widen additional feet along frontage,

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V					
Recycled Water	Main Main	Main	Main	Main	
(see Sec. 2.E)	Service	Service	Service	Service	
Traffic Signal	New	New	New	New	
System	Modify	Modify	Modify	Modify	
(see Sec. 2.F)	existing	existing	existing	existing	
Traffic Signing	New	New	New	New	
and Striping	Modify	Modify	Modify	Modify	
(see Sec. 2.F)	existing	existing	existing	existing	
	New /	New /	New /	New /	
Street Light	Upgrade	Upgrade	Upgrade	Upgrade	
(see Sec. 2.F)	Relocation	Relocation	Relocation	Relocation	
Pun Stan Dad or	New	New	New	New	
Bus Stop Pad or Turn-out	Modify	Modify	Modify	Modify	
(see Sec. 2.F)	existing	existing	existing	existing	
Storm Drain	Main	Main	Main	Main	
(see Sec. 2G)	Lateral	Lateral	Lateral	Lateral	
Fiber Optics	Conduit /	Conduit /	Conduit /	Conduit /	
(see Sec. 2K)	Appurtenances	Appurtenances	Appurtenances	Appurtenances	
Overhead Utilities	Underground Relocate	Underground Relocate	Underground Relocate	Underground Relocate	
Removal of Improvements					
Other Improvements					
the current corporation	nprovements listed lity mains/services City procedure. All a stops at the existir with the main.	no longer to be use services/laterals sh	ed shall be abandor nall be abandoned l	by shutting off the	
Construct a 2" asphal	t concrete (AC) grind	and overlay on the	following street(s): _		
Reconstruction of the 1011, may be require limits of reconstruction	d based on the existi	ng pavement conditi	on and final street de	esign. Minimum	
Make arrangements v ☐ sewer service to t provide documentatio	he site. This property	y is within the area s	served by the CVWE	and Applicant shall	
Overhead utilities sha (Ordinance No. 2804 undergrounding of util	4 and 2892). Deve	eloper may pay in-	lieu fee, approxima	tely , for	
Other conditions:					

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2.182.19

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	C. SE	WER CONTROL OF THE CO	
\boxtimes	2.23	An 8-inch sewer main is available for connection by this project in the drive aisle. (Ref: Sewer Drawing Number: <u>S11753</u>)	
	2.24	Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately feet away.	
	2.25	Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.	
	2.26	Other conditions: a. All proposed sewer upstream of the existing manhole located at the southwest corner of the project site shall be privately owned and maintained. b. The applicant/developer shall install a grease interceptor with a sample box downstream of the grease interceptor per the latest City of Ontario Design Guidelines/Standard Drawings. c. The applicant/developer or occupant shall apply for a Wastewater Discharge Permit for their establishment and comply with all the requirements of the Wastewater Discharge Permit application (https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/fse wastewater permit application fillable.pdf). For wastewater discharge permit application questions, please contact: Michael Birmelin, Environmental Programs Manager omucenvironmental@ontarioca.gov Phone: (909) 395-3661	
	D. WA	Phone: (909) 395-2661	
	2.27	A 12-inch water main is available for connection by this project in Sixth Street. (Ref: Water Drawing Number: <u>W10302</u>)	
	2.28	Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately feet away.	
	2.29	Other conditions:	
		CYCLED WATER	
	2.30	Ainch recycled water main is available for connection by this project in (Ref: Recycled Water plan bar code:)	
	2.31	Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.	
	2.32	Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.	
	2.33	Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.	
	0.04	Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.	
Ш	2.34	Other conditions:	
	2.35	Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 1. On-site and off-site circulation 2. Traffic level of service (LOS) at 'build-out' and future years 3. Impact at specific intersections as selected by the City Engineer	



Ш	2.36	New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.	
	2.37	 Other conditions: a. The applicant/developer shall install parkway landscaping with an irrigation system per the Landscape Planning Requirements. Installation of a 2-ft landscape buffer between the perimeter wall and sidewalk may result in the reconstruction of the sidewalk panels along the Sixth Street property frontage. b. All landscaping, block walls and other obstructions in the parkway shall be compatible with the stopping sight distance requirements per City Standard Drawing Number 1309. 	
	G. DF	RAINAGE / HYDROLOGY	
\boxtimes	2.38	A 42-inch storm drain main is available to accept flows from this project in Sixth Street. (Ref: Storm Drawing Number: <u>D11046</u>)	
	2.39	Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.	
\boxtimes	2.40	An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.	
	2.41	Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.	
		A 1 M 4 A A 4 A 1 M 4 M = 0 = 0 = 0	_
	2.42	Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.	
	2.43	project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. Other conditions:	
	2.43 H. ST	project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. Other conditions: ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM	
	2.43	project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. Other conditions: ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM	
	2.43 H. ST (NPDE	project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. Other conditions: CORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (SS) 401 Water Quality Certification/404 Permit — Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.	
	2.43 H. ST (NPDE 2.44	project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program. Other conditions: ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM S) 401 Water Quality Certification/404 Permit — Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130. Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at:	



	J. SP	ECIAL DISTRICTS	
	2.48	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	2.49	Other conditions:	
	K. FIE	BER OPTIC	
	2.50	Afiber optic line is available for connection by this project in	
	2.51	(Ref: Fiber Optic plan bar code:) Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system. Generally located along Sixth Street and Mountain Avenue, see Fiber Optic Exhibit herein	
\boxtimes	2.52	Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.	
	L. INT	EGRATED WASTE	
\boxtimes	2.52 2.53	Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at: https://www.ontarioca.gov/OMUC/IntegratedWaste Other conditions: a. Prior to the issuance of a Building Permit, the applicant/developer shall comply with the following:	
		i. Submit a Solid Waste Handing Planning (SWHP) & Integrated Waste Management Report (IWMR) with the Precise Grading Plan and Trash Enclosure Detail with the Building (Architectural) Plan for the Ontario Municipal Utilities Company (OMUC) to review and approve. Additionally email a PDF copy of the plans after the building permit number is created to <u>bishii@ontarioca.gov</u> (if files are larger than 20MB, then send a link to download files).	
		ii. Size the trash enclosure for a minimum of three (3) 4CY bins (one 4CY bin for refuse, one 4CY bin for recycling, and one 4CY bin for organics) plus any other bins/storage or other handling required by the SWHP and IWMR.	
		For questions on the SWHP and IWMR submittal requirements, please contact: Blaine Ishii, Integrated Waste Assistant Division Manager bishii@ontarioca.gov Phone: (909) 395-2775	



3.	PRIC	OR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:	
\boxtimes	3.01	Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.	
	3.02	Complete all requirements for recycled water usage.	
		1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.	
		2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.	
		☐ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.	
	3.03	The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.	
	3.04	Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.	
\boxtimes	3.05	Confirm payment of all Development Impact Fees (DIF) to the Building Department.	
	3.06	Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).	
4.	PRIO	R TO FINAL ACCEPTANCE, APPLICANT SHALL:	
\boxtimes	4.01	Complete all Conditions of Approval listed under Sections 1-3 above.	
\boxtimes	4.02	Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.	
	4.03	The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.	
\boxtimes	4.04	Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.	



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV21-012

The following items are req	uired to be i	ncluded with the	first plan	check submittal:
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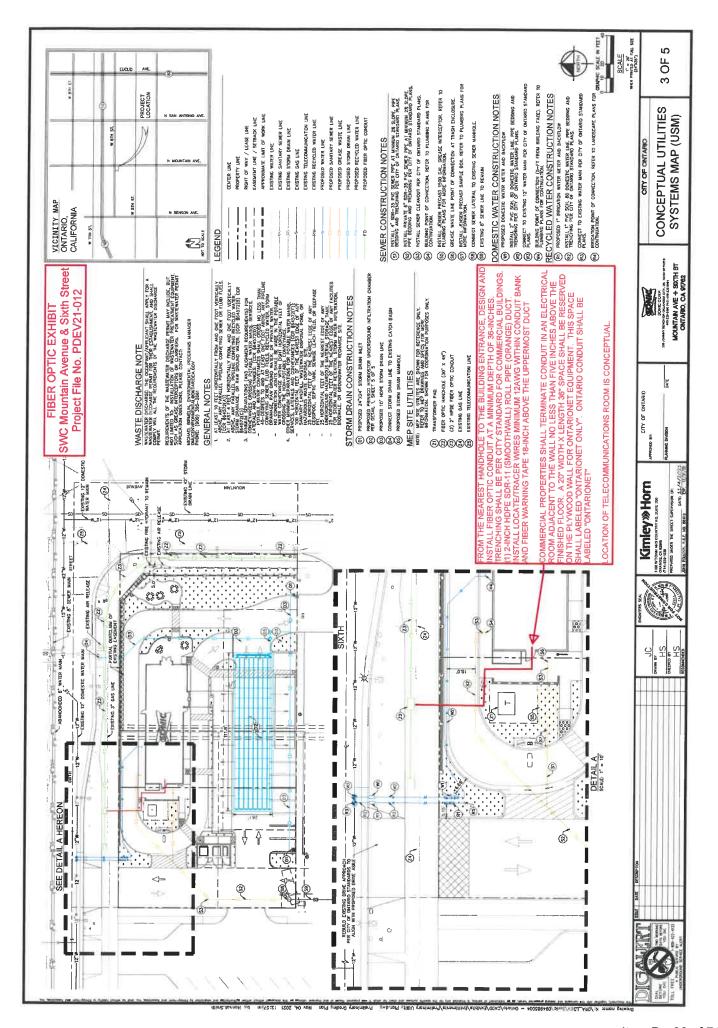
1.	☑ A copy of this check list
2.	☑ Payment of fee for Plan Checking
3.	☑ One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp
4.	☑ One (1) copy of project Conditions of Approval
5.	☑ Include a PDF (electronic submittal) of each required improvement plan at every submittal
6.	☑ Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
7.	☐ Three (3) sets of Public Street improvement plan with street cross-sections
8.	Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9.	Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.	☐ Four (4) sets of Public Sewer improvement plan
11.	Five (5) sets of Public Storm Drain improvement plan
12.	☐ Three (3) sets of Public Street Light improvement plan
13.	☐ Three (3) sets of Signing and Striping improvement plan
14.	☐ Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15.	☐ Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16.	☐ Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications
17.	☑ Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP)
18.	☐ One (1) copy of Hydrology/Drainage study
19.	☑ One (1) copy of Soils/Geology report
20.	☐ Payment for Final Map/Parcel Map processing fee

Last Revised 12/7/2021 Page 12 of 13



21.	I hree (3) copies of Final Map/Parcel Map
22.	☐ One (1) copy of approved Tentative Map
23.	One (1) copy of Preliminary Title Report (current within 30 days)
24.	One (1) copy of Traverse Closure Calculations
25.	☐ One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
26.	Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
27.	☐ Other:

Last Revised 12/7/2021 Page 13 of 13



AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.:	PDEV21-012 & PC	UP21-004		Reviewed By:		
Address:	Lorena Mejia					
APN:	1008-431-25			Contact Info:		
Existing Land Use:	Vacant			909-395-2276		
Proposed Land Use:	Commercial drive-tl	hru restaurant 2,370 SF (Sonics)		Project Planner: Jeanie Aguilo		
Site Acreage:	0.72	Proposed Structure Heig	ht: 30 FT	Date: July 6, 2021		
ONT-IAC Project	t Review: n/a	_		CD No.: 2021-023		
Airport Influence	Area: ONT	,		PALU No.: n/a		
TI	ne proiect is i	mpacted by the follow	ing ONT ALUCP Compa	ntibility Zones:		
Safe		Noise Impact	Airspace Protection	Overflight Notification		
Zone 1 Zone 1A Zone 2 Zone 3 Zone 4 Zone 5 Zone 1	Zone		High Terrain Zone FAA Notification Surfaces Airspace Obstruction Surfaces Airspace Avigation Easement Area Allowable Height: 200 FT + owing Chino ALUCP Sate Zone 4 Zone 4			
Allowable Heig	jnt: —————					
		CONSISTENCY	DETERMINATION			
This proposed Pro	oject is: Exemp	ot from the ALUCP	sistent Consistent with Cor	nditions		
The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.						
Airport Planner S	Yanur Ufgic Airport Planner Signature:					



CITY OF ONTARIO

MEMORANDUM

TO:	Scott Murphy, Community Development Director Rudy Zeledon, Planning Director (Copy of m Diane Ayala, Advanced Planning Division (Coharity Hernandez, Economic Development Matt Montieth, Building Department Raymond Lee, Engineering Department Jamie Richardson, Landscape Planning Dividentis Mejia, Municipal Utility Company Gabriel Gutierrez, Police Department Mike Gerken, Deputy Fire Chief/Fire Marsha Jay Bautista, T. E., Traffic/Transportation Matter Mejia, Airport Planning Eric Woosley, Engineering/NPDES Angela Magana, Community Improvement (Jimmy Chang, IPA Department	emo only) sopy of memo only) sion I anager				
FROM:	Jeanie Irene Aguilo, Associate Plann	er	•	REVISION NO.	2	
DATE:	October 22, 2021			UEAISION NO.		
SUBJECT:	FILE #: PDEV21-012	Finance Acct#:	_			
The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .						
with drive-thr Street, within	ESCRIPTION: A Development Plan to construit (Sonic) on 0.72-acre of land located at the solution the Main Street land use district of the Moun PCUP21-004.	southwest corner of Mount	tain A	venue and Sixth		
The plar	n does adequately address the departmental	concerns at this time.				
	No comments					
	See previous report for Conditions					
Ø	Report attached (1 copy and email 1 copy)					
	Standard Conditions of Approval apply					
The plan	n does not adequately address the departmer	ital concerns.				
	The conditions contained in the attached rep Development Advisory Board.	oort must be met prior to so	chedu	uling for		

Department Dus was Title and Date | 1/12/2

CITY OF ONTARIO LANDSCAPE PLANNING DIVISION

Sign Off

11/12/2021

Jamie Richardson, Sr. Landscape Planner

Date

CONDITIONS OF APPROVAL

303 East "B" Street, Ontario, CA 91764

DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Reviewer's Name: Phone: Jamie Richardson, Sr. Landscape Planner (909) 395-2615 D.A.B. File No.: Case Planner: PDEV21-012 Jeanie Aguilo Project Name and Location: Commercial drive-thru SW corner of Mountain Ave and 6th Street Applicant/Representative: Coast to Coast Commercial, LLC 25400 La Alameda, Suite 100 Mission Viejo, CA 92691 A Preliminary Plan (dated 10/22/2021) meets the Standard Conditions for New X Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents. A Preliminary Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval. A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE. Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov

Civil/ Site Plans

1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020. A total of \$3,800 in monetary valve will be paid prior to plan check approval.

Landscape Plans

- 2. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See http://www.ontarioca.gov/landscape-planning/standards
- 3. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council.



CITY OF ONTARIO MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: March 25, 2021

SUBJECT: PDEV21-012 - A Development Plan to construct one (1) commercial drive-

thru restaurant building totaling 2,370 square feet on 0.72 acres of land located at the southwest corner of Mountain Avenue and 6th Street, within the Main Street land use district of the Mountain Village Specific Plan

(APN(s): 1008-431-25). Related File(s): PCUP21-004.

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2019 CBC Type of Construction: V

B. Type of Roof Materials: Ordinary

C. Ground Floor Area(s): 2,370 Sq. Ft.

D. Number of Stories: 1

E. Total Square Footage: 2,370 Sq. Ft.

F. 2019 CBC Occupancy Classification(s): A-2

CONDITIONS OF APPROVAL:

1.0 GENERAL

- I.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.

2.0 FIRE DEPARTMENT ACCESS

- ∑ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.

- ≥ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.

3.0 WATER SUPPLY

4.0 FIRE PROTECTION SYSTEMS

- ✓ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per <u>Standard #C-001</u>. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ∑ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ∑ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department.
 All Knox boxes shall be monitored for tamper by the building fire alarm system. See <u>Standard</u> #H-001 for specific requirements.
- ∑ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.



CITY OF ONTARIO MEMORANDUM

TO: Jeanie Irene Aguilo, Associate Planner

FROM: Officer Bill Lee, Police Department

DATE: April 19, 2021

SUBJECT: PDEV21-012 - A DEVELOPMENT PLAN TO CONSTRUCT ONE

COMMERCIAL DRIVE-THRU RESTAURANT BUILDING TOTALING 2,370 SQUARE FOOT LOCATED AT THE SOUTHWEST CORNER OF MOUNTAIN AVENUE AND 6TH STREET. RELATED FILE: PCUP21-

004.

The "Standard Conditions of Approval" contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions.
- The Applicant shall install a video surveillance system on the site. Cameras shall cover at a minimum all entry doors, all cash registers, and at least one camera shall capture any vehicle utilizing the drive-thru. Cameras shall be positioned so as to maximize the coverage of patrons and vehicles in these areas. Cameras shall record at least 15 frames per second and at a minimum of 720p of resolution. Recordings shall be stored for a minimum of 30 days and made available upon request to any member of the Ontario Police Department.
- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal, or shall be

covered/painted over with a color reasonably matching the color of the existing building, structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

• Maintain all landscaping on property to a standard that all ground covering shrubbery and hedges are no taller than 2 feet (24") and the lower canopy of all trees is no lower than 6 feet (72").

The Applicant is invited to call Bill Lee at (909) 408-1672 regarding any questions or concerns.



PLANNING COMMISSION STAFF REPORT

December 20, 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NOS: PMTT21-010 and PDEV21-018

SUBJECT: A Tentative Parcel Map (TPM 20147) (File No. PMTT21-010) to subdivide 95.35 acres of land into three parcels, in conjunction with a Development Plan (File No. PDEV21-018) to construct two industrial buildings totaling 168,772 square feet on 13.07 acres of land located at the southeast corner of Jurupa Street and Milliken Avenue, at 1425 South Toyota Way, within the Industrial Mixed Use and Warehouse/Distribution land use districts of the Toyota Ontario Business Park Specific Plan; (APN: 0238-121-75) **submitted by MIG, Inc.**

PROPERTY OWNER: Toyota Motor Sales, U.S.A., Inc.

RECOMMENDED ACTION: That the Planning Commission take the following actions:

- (1) Consider and adopt the resolution approving the use of an Addendum to The Ontario Plan Environmental Impact Report;
- (2) Consider and adopt a resolution approving Tentative Parcel Map No. 20147, File
- No. PMTT21-010, subject to the conditions of approval contained in the attached departmental reports; and
- (3) Consider and adopt a resolution approving the Development Plan, File No. PDEV21-018, subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 95.35 acres of land located at 1425 South Toyota Way, within the Toyota Ontario Business Park Specific Plan zoning district, depicted in Figure 1: Project Location, right. The properties to the north and west of the Project site are within the Commercial/Food/Hotel and Light Industrial land use districts of the



Figure 1: Project Location

Case Planner:	Edmelynne V. Hutter, AICP
Planning Director Approval:	
Submittal Date:	May 10, 2021

Hearing Body	Date	Decision	Action
DAB	12/20/2021	Approval	Recommend
PC	12/20/2021		Final
CC			

December 20, 2021

California Commerce Center Specific Plan, and are improved with warehousing, light industrial, commercial, and retail developments. The properties to the south are within the Entratter Industrial Specific Plan and the IG (General Industrial) zoning district, are developed with warehouses. To the east of the Project site is the Interstate 15 freeway. The existing surrounding land uses, zoning, and general plan and specific plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

PROJECT ANALYSIS:

(1) <u>Background</u> — The Toyota Ontario Business Park Specific Plan was established in 1993 by the City Council to govern the development of the Specific Plan area. Subsequently, the specific plan area was developed in 1994 with a warehouse and distribution facility for Toyota North American Parts that includes warehouse space, ancillary office, parking lot, a truck yard, landscaping, a private street (Toyota Way), and other associated site improvements. The northwest portion of the Specific Plan area, however, was left vacant and undeveloped.

On June 2, 2020, the City Council adopted 1) Resolution No. R2020-063 approving the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by the Ontario City Council on January 27, 2010, and 2) Resolution No. R2020-064, approving a Specific Plan Amendment to the Toyota Ontario Business Park Specific Plan (File No. PSPA19-004) to allow Industrial Mixed-Use land uses and update the landscape palette to incorporate drought tolerant plant species.

On May 10, 2021, the Applicant submitted a Tentative Parcel Map and Development Plan applications requesting approval for the development and the construction of two industrial buildings with associated site improvements on the remaining vacant portion of the Project site.

(2) Addendum — The related Applications establish a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an Initial Study/Addendum has been prepared to determine possible environmental impacts. Although the proposed project could have a significant effect on the environment, however all potentially significant effects have been analyzed adequately in an earlier Certified EIR, and have been avoided or mitigated pursuant to that earlier Certified EIR, including revisions or mitigation measures that are imposed on the proposed project, so nothing further is required. The Project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Environmental Impact Report are a condition of project approval and are incorporated in the Initial Study/Addendum (see Attachment A—EIR Addendum, attached).

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(3) <u>Tentative Parcel Map No. 20147 (File No. PMTT21-010)</u> — The Applicant is proposing to subdivide the 95.35-acre Project site into three numbered lots (see Exhibit B—Parcel Map and Exhibit C—Site Plan, both attached). Parcel 1 will be 81.67 acres in size and encompass the area already developed with the Toyota warehouse and distribution facility, Toyota Way (private street), and existing landscaped areas. Parcel 2 is the northwest parcel, occupying the corner of Milliken Avenue and Jurupa Street, and will be 8.24 acres in size. Parcel 3 is located to the south of Parcel 2 and located at the northeast corner of Milliken Avenue and Toyota Way, encompassing 3.79 acres of land. Parcels 2 and 3 comprise the existing vacant land in the northwest portion of the Project site. The lot sizes proposed by the tentative parcel map exceed the one-acre minimum lot size required by the Toyota Ontario Business Park Specific Plan.

(4) <u>Development Plan (File No. PDEV21-018)</u>

(a) <u>Site Design/Building Layout</u> — The Applicant is proposing to construct two industrial buildings and associated site improvements on the vacant portion of the Project site, at the northwest corner (see Exhibit C—Site Plan, attached) of the Specific Plan area. The subject area is triangular in shape and 13.07 acres in size. It is bounded by Jurupa Street to the north, Toyota Way to the south and east, and Milliken Avenue to the west. There is a City well site located on a separate parcel located along the Jurupa Street frontage, midway between the Milliken Avenue and Toyota Way intersections, and is not part of the Project site. No new development is proposed in the area south and east of Toyota Way, as this area is already developed with an 807,067-square-foot industrial building, including 48,929 square feet of office space.

The proposed Building A on Parcel 2 is the larger of the two new buildings, at a total of 118,067 square feet, including 10,000 square feet of ancillary office space. This building is rectangular in shape and is designed with main entrances on the northwest and northeast corners of the building. The parking lot areas surround this building on four sides, providing a total of 131 parking spaces. The truck yard area is located along the south elevation and provides 25 truck dock positions, which will be screened from public view and gated.

Proposed Building B on Parcel 3 is a 50,705-square-foot industrial building, with 5,000 square feet of office space. The office area is located at the northwest corner of the building. The parking lot areas are on the north, south, and west sides of the building, with a total of 63 parking spaces. This industrial building also provides eight truck dock positions, which are located on the north side of the building. Consistent with Building A, the truck yard for Building B will be gated and screened from public view.

(b) <u>Site Access/Circulation</u> — The Project site has existing access via Toyota Way, which is a private street that intersects with Milliken Avenue on the west and Jurupa Street to the north. The new industrial buildings will have access from new driveway approaches on Milliken Avenue and Toyota Way. Direct access from Jurupa Street is not proposed. The primary passenger vehicle and truck access is provided via a shared

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driveway on Milliken Avenue, approximately 400 feet south of Jurupa Street. Secondary access is available at two locations along Toyota Way.

Internal circulation is provided by minimum 24-foot-wide drive aisles along the building perimeter. The truck yard for Building A has two gated access points, one each on the east and west ends of the yard. The truck yard for Building B has one access point, which is located at the west end of the yard, near the primary driveway approach on Milliken Avenue.

Pedestrian access paths that lead to the building entrances are at three locations on the Project site. Two paths are located along Milliken Avenue, and the third path is along Jurupa Street.

(c) <u>Parking</u> — The Project is required to provide 69 passenger vehicle parking spaces for Building A and 36 passenger vehicle parking spaces for Building B. The Project provides 131 vehicle parking spaces for Building A and 63 parking spaces for Building B, exceeding the passenger vehicle parking space requirements. Table 1 provides a parking summary for the Project.

Building A Building B Total Use & Required Parking Ratio Required Provided Required **Provided** Required **Provided** Warehouse (one space / 1,000 SF <20,000 SF and one space / 2,000 SF 69 131 36 63 105 194 ≥20,000 SF) Office (4 spaces / 1,000 SF for portion of building in excess of 10% 0 0 0 0 0 0 of GFA) Trailer Parking (one trailer space / 4 7 7 2 2 9 9 dock-high loading doors/spaces)

Table 1: Parking Summary

In addition to passenger vehicle parking spaces, the Project is required to provide truck-trailer parking spaces. The proposed site design provides the minimum number of truck-trailer parking spaces required for the Project.

(d) <u>Architecture</u> — The architectural design of the proposed buildings is based on the existing warehouse building design to present a cohesive architectural style among the three industrial buildings. The new buildings will be of concrete tilt-up construction with painted and scored accents. The building elevations will have variations in the glazing and paint color, which are proposed to be a color scheme of white with medium and gray accents, and blue trim (see Exhibit D—Exterior Elevations, attached). The building design also incorporates recessed primary entry doors, two story glazing, concrete trellis with metal fins, and variations in parapet height, all of which are designs that can be found on the existing warehouse building on the Project site.

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- (e) <u>Landscaping</u> Landscaping is proposed along all street frontages, including Toyota Way, and will maintain the existing mature trees along Jurupa Street and Milliken Avenue. Areas that are not developed with structures, parking lot or other site amenity will be landscaped in accordance with City requirements. The Project is required to provide landscape areas over at least 15 percent of the lot area. The Project exceeds this requirement, with 29 percent landscape coverage on Parcel 2, where Building A is located, and 35 percent landscape coverage on Parcel 3, where Building B is located.
- (f) <u>Signage</u> Project signage is not proposed as part of this Project. As conditioned, signage shall be required to comply with the Ontario Development Code Division 8.1 (Sign Regulations).
- (g) <u>Utilities (drainage, sewer)</u> Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

- (1) <u>City Council Goals</u>.
 - Invest in the Growth and Evolution of the City's Economy
- (2) Vision.

Distinctive Development:

- Commercial and Residential Development
- > Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

December 20, 2021

(3) Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
- ➤ <u>G1-2 Long-term Benefit</u>. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) <u>Policy Plan (General Plan)</u>

Land Use Element:

- <u>Goal LU1</u>: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-1 Strategic Growth</u>. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
- ➤ <u>LU1-6 Complete Community</u>: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
 - Goal LU2: Compatibility between a wide range of uses.
- ➤ <u>LU2-6</u>: <u>Infrastructure Compatibility</u>: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
- ➤ <u>CE2-1 Development Projects</u>. We require new development and redevelopment to create unique, high-quality places that add value to the community.
- ➤ <u>CE2-2 Development Review</u>. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

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➤ <u>CE2-4 Protection of Investment</u>. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ <u>CE2-5 Private Maintenance</u>. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
- > <u>\$1-1</u> Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.
- > <u>\$1-2 Entitlement and Permitting Process</u>. We follow state guidelines and the California Building Code to determine when development proposals must conduct geotechnical and geological investigations.

Community Design Element:

- <u>Goal CD1</u>: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
- ➤ <u>CD1-1 City Identity</u>. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
- ➤ <u>CD1-2 Growth Areas</u>. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.
- <u>Goal CD2</u>: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
- ➤ <u>CD2-1 Quality Architecture</u>. We encourage all development projects to convey visual interest and character through:
- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and

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- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- ➤ <u>CD2-7 Sustainability</u>. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- ➤ <u>CD2-8 Safe Design</u>. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- ➤ <u>CD2-9 Landscape Design</u>. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- ➤ <u>CD2-10 Surface Parking Areas</u>. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
- ➤ <u>CD2-11 Entry Statements</u>. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- ➤ <u>CD2-12 Site and Building Signage</u>. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.
- ➤ <u>CD2-13 Entitlement Process</u>. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
- <u>Goal CD3</u>: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

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- ➤ <u>CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas</u>. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.
- ➤ <u>CD3-3 Building Entrances</u>. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- ➤ <u>CD3-5 Paving</u>. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.
- ➤ <u>CD3-6 Landscaping</u>. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.
- <u>Goal CD5</u>: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- ➤ <u>CD5-1 Maintenance of Buildings and Property</u>. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.
- > <u>CD5-2 Maintenance of Infrastructure</u>. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AlrPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

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ENVIRONMENTAL REVIEW: On January 27, 2010, the City Council certified The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008104410), in conjunction with File No. PGPA06-001. Staff prepared an Addendum to The Ontario Plan Environmental Impact Report ("Certified EIR") for the proposed Project, which found that all potential adverse direct, indirect, and cumulative environmental impacts were thoroughly analyzed and discussed in the Certified EIR, including, but not limited to potential aesthetic, air quality, greenhouse gas emissions, noise, transportation, and tribal cultural resources impacts, and all feasible mitigation has been identified and will be incorporated into the proposed Project. This Project does not contemplate any actions that would require the preparation of a subsequent or supplemental environmental document under State CEQA Guidelines Section 15162 or 15163, as it is consistent with the development scenario identified within the Certified EIR. Furthermore, this project introduces no new significant environmental impacts and no further environmental review is required. A copy of the Addendum has been included in the environmental action resolution provided with this report.

CONDITIONS OF APPROVAL: See attached department reports.

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TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

301100110	soffooffallig Zoffling and Land ose.					
	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use		
Site	Warehouse, Vacant	Industrial	Toyota Ontario Business Park Specific Plan	Industrial Mixed Use, Warehouse/Distribution		
North	Warehouse/Distribution	Industrial	California Commerce Center Specific Plan	Light Industrial, Commercial/Food/Hotel		
South	Warehouse	Industrial	Entratter Industrial Specific Plan, General Industrial	Industrial		
East	Interstate 15 Freeway	Interstate 15 Freeway	Interstate 15 Freeway	Interstate 15 Freeway		
West	Office/Business Park	Industrial	California Commerce Center Specific Plan	Light Industrial, Commercial/Food/Hotel		

General Site & Building Statistics

Item	Proposed	Min./Max. Standard	Meets Y/N
Project Area:	95.35 acres	N/A	
Lot/Parcel Size:	Parcel 1 - 81.67 acres Parcel 2 - 8.24 acres Parcel 3 - 3.79 acres	1 acre (Min.)	Y
Building Area:	Parcel 1 / 1425 Toyota Wy 807,067 SF Parcel 2 / Building A - 118,067 SF Parcel 3 / Building B - 50,705 SF	N/A	
Floor Area Ratio:	Parcel 1 – 23% Parcel 2 – 33% Parcel 3 – 31%	55% (Max.)	Y
Building Height:	41 FT	65 FT (Max.)	Υ

Off-Street Parking (Parcels 2 and 3):

on sincer ranking (ranceis 2 and b).						
Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided		
Warehouse	168,772 SF	1 space / 1,000 SF <20,000 SF and 1 space / 2,000 SF <u>></u> 20,000 SF	95	194		
Office	1 7 1 1 1 1 1 1 1	4 spaces / 1,000 SF for portion of building in excess of 10% of GFA	0	0		
Trailer Parking	33 Docks	1 trailer space / 4 dock-high loading doors/spaces	9	9		

EXHIBIT A: PROJECT LOCATION MAP

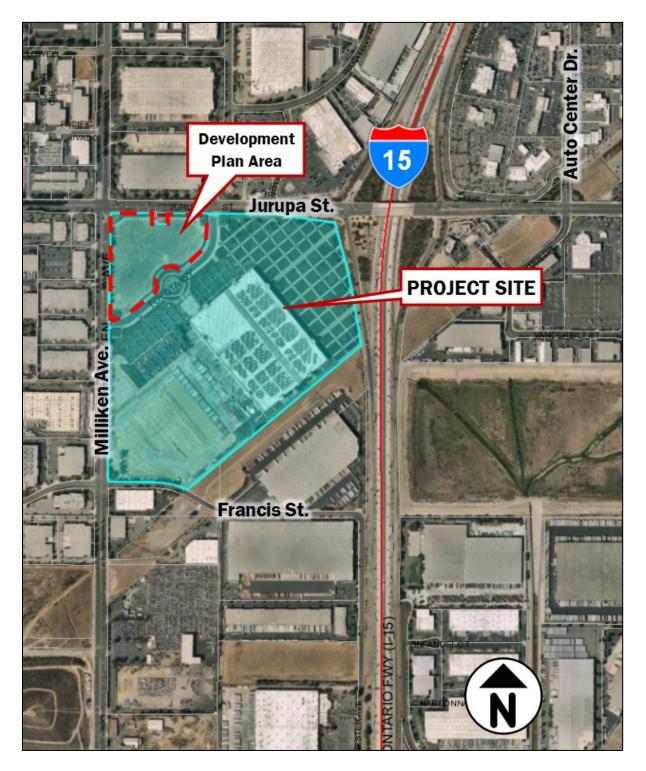


EXHIBIT B: TENTATIVE PARCEL MAP NO. 20147

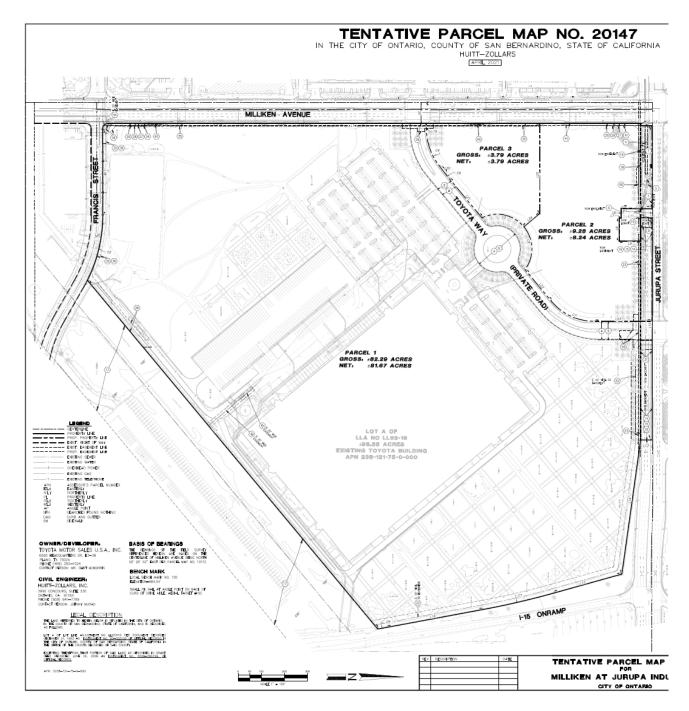


EXHIBIT C: SITE PLAN

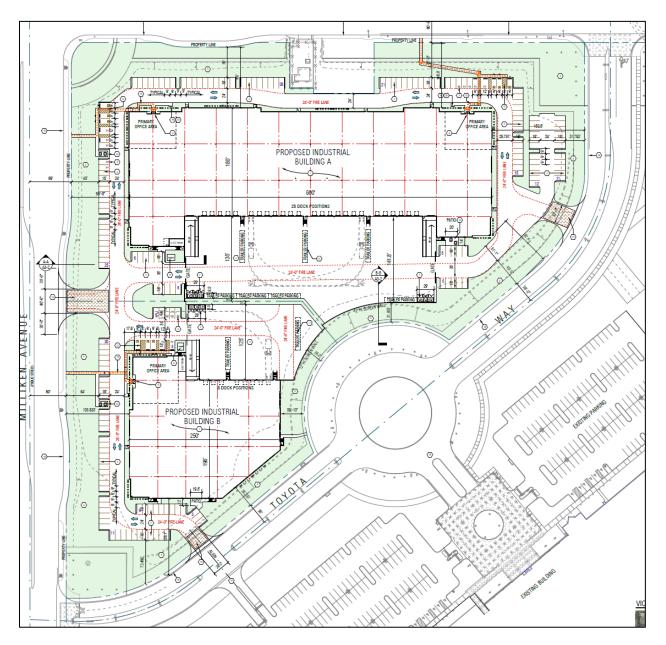


EXHIBIT D: BUILDING ELEVATIONS

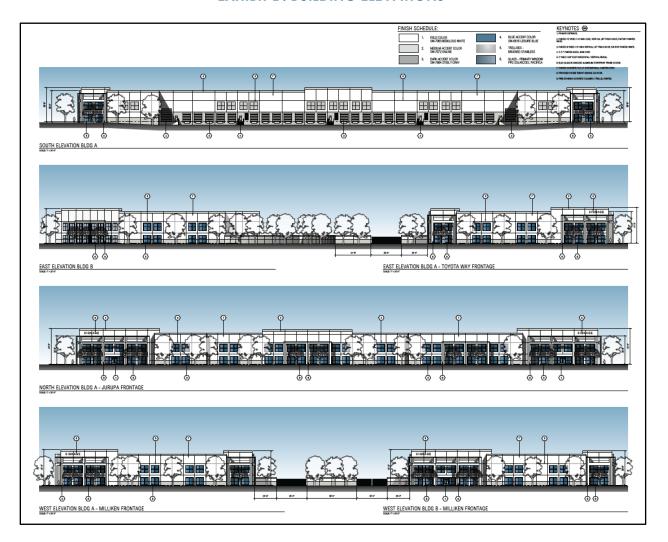


EXHIBIT D: BUILDING ELEVATIONS (CONTINUED)



1425 South Toyota Way



Building A

FINISH SCHEDULE:

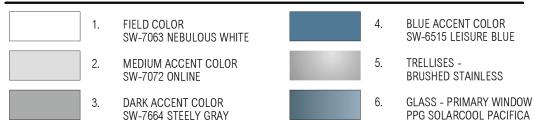
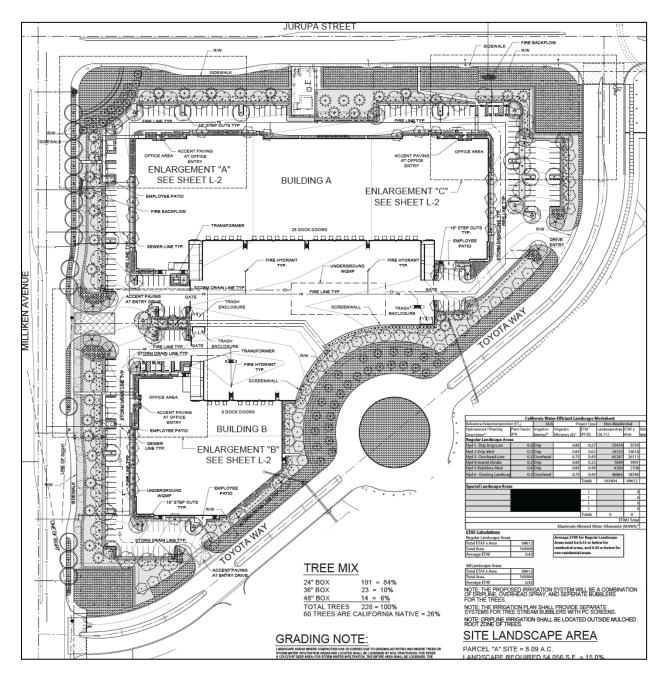


EXHIBIT F: CONCEPTUAL LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO APPROVING AN ADDENDUM TO THE ONTARIO PLAN ENVIRONMENTAL IMPACT REPORT, PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NOS. PMTT21-010 AND PDEV21-018.

WHEREAS, MIG, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Tentative Parcel Map and Development Plan, File Nos. PMTT21-010 AND PDEV21-018, respectively, which consists of a Tentative Parcel Map to subdivide 95.35 acres of land into three parcels, in conjunction with a Development Plan to construct two industrial buildings totaling 168,772 square feet on 13.07 acres of land located at the southeast corner of Jurupa Street and Milliken Avenue, at 1425 South Toyota Way, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008104410) was certified on January 27, 2010, (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the decision-making authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project is on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008104410), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and
- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

- (5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.
- <u>SECTION 2</u>: **Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- <u>SECTION 3</u>: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby finds

that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 5</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 6</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File Nos. PMTT21-010 and PDEV21-018 December 20, 2021 Page 6	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
City of Ontario, DO HEREBY CERTIFY th	Tempore of the Planning Commission of the nat foregoing Resolution No was duly mission of the City of Ontario at their special e following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen
	Secretary Pro Tempore

ATTACHMENT A:

Addendum to The Ontario Plan Environmental Impact Report

(Addendum to follow this page)



California Environmental Quality Act

Addendum to The Ontario Plan Environmental Impact Report

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

- 1. Project Title/File No.: PMTT21-010 and PDEV21-018
- 2. Lead Agency: City of Ontario, 303 East B Street, Ontario, California 91764, (909) 395-2036
- 3. Contact Person: Edmelynne V. Hutter, Senior Planner, Phone: (909) 395-2429, Email: ehutter@ontarioca.gov
- 4. Project Sponsor: MIG, Inc. ATTN: Pamela Steele; 1650 Spruce Street, Suite 106, Riverside, CA 92507
- 5. Project Location: The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 and 2, below, the project site is located at 1425 South Toyota Way, on approximately 95 acres, at the southeast corner of Jurupa Street and Milliken Avenue.

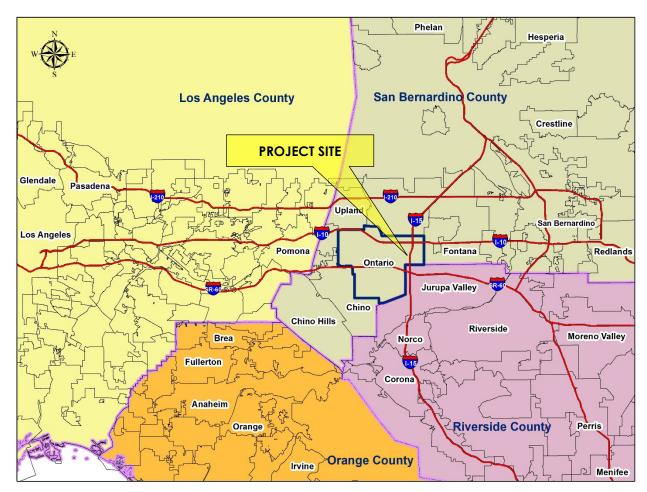


Figure 1: REGIONAL LOCATION MAP

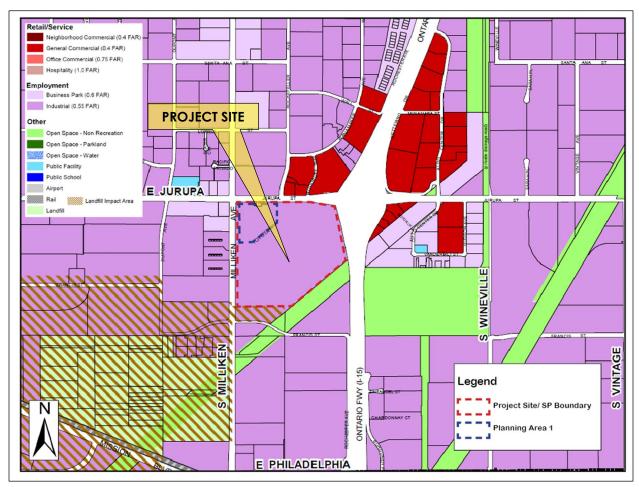


Figure 2: VICINITY MAP



Figure 3: AERIAL PHOTOGRAPH

- 6. Policy Plan (General Plan) Designation: Industrial
- 7. Zoning Designation: Toyota/Ontario Business Park Specific Plan Industrial Mixed Use
- 8. Description of Project: A Tentative Parcel Map (TPM No. 20147), submitted as part of the application, would subdivide the Specific Plan area into three parcels: Parcel 1 would include the Specific Plan area south of Toyota Way, which is already developed with an 807,068 square feet warehouse and distribution facility and no development for this area is proposed under this Project; Parcels 2 and 3 would be the area north of Toyota Way and include the northwest portion of the Project site where new development is proposed. As shown in Figure 4, the proposed Project would involve the construction of two industrial warehouse buildings totaling 168,722 square feet and associated site improvements.

Building A. Building A, located on the northern portion of Project site, would total 118,067 square feet and include 108,067 square feet of warehouse space and 10,000 square feet of office space. The office component of the building would be two stories and would be located in the northwest and northeast corners of the building. Twenty-five truck loading docks would be located along the southern side of the building. A summary of development specifications are outlined in Tables 1 and 2.

Building B. Building B, located on the southern portion of the Project site, would be located south of Building A and would total 50,705 square feet which would include 45,705 square feet of warehouse space with 5,000 square feet of office use. The office component of the building would be two stories and would be located in the northwest corner of the building. Eight truck loading docks would be located along the northern side of the building. A summary of development specifications are outlined in Tables 1 and 2.

Table 1: Development Specifications								
	1425 S. Toyota Wy. (existing) /	Building A /	Building B /					
	Parcel 1	Parcel 2	Parcel 3					
Office Space	48,929 sf	10,000 sf	5,000 sf					
Warehouse Space	758,139 sf	108,067 sf	45,705 sf					
Total Building Area	807,068 sf	118,067 sf	50,705 sf					
Site Area (sf)	3,557,545 sf	358,934 sf	165,092 sf					
Site Area (acres)	81.67 ac	8.24 ac	3.79 ac					

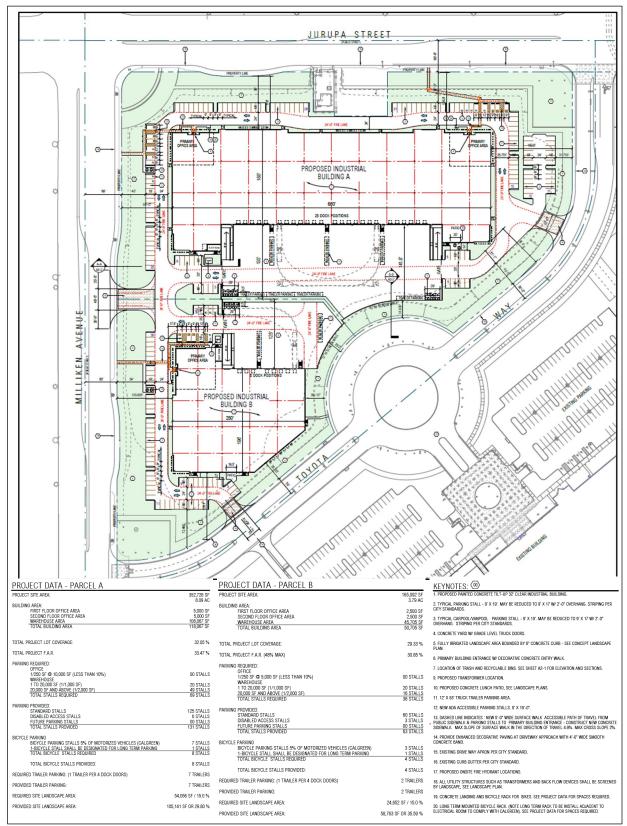


Figure 4: DEVELOPMENT PLAN SITE PLAN

Table 2: Applicable Development Standards									
Development Standards	Required	Parcel 1/ 1425 S. Toyota Wy.	Parcel 2/ Building A	Parcel 3/ Building B					
Maximum FAR	55%	22.68%	32.89%	30.71%					
Minimum Parcel Size	1 acre	81.67 acres	8.24 acres	3.79 acres					
Maximum Building Height	65 feet	No change	41 feet	41 feet					
Minimum Building Setbacks Milliken Avenue Jurupa Street Toyota Way	45 feet 40 feet 25 feet	No change	95 feet 100 feet 87.4 -173.6 feet	103.8 feet N/A 38.3 to 126 feet					
Warehousing and Distribution	on Parking Requireme	nts							
Office-1 space/250 sf less than 10,000 sf	20 spaces								
Warehouse-1 space/1,000 sf to 20,000 sf	Bldg A = 20 spaces Bldg B = 20 spaces	No change	Total Spaces:	Total Spaces:					
Plus 1 space/2,000 sf 20,000 sf and above	Bldg A = 44 spaces Bldg B = 23 spaces	140 Change	131	63					
Parking Space Totals	Bldg A = 84 spaces Bldg B = 63 spaces								
Plus 1 tractor trailer	1 space/4 dock	No change	25 dock doors	8 dock doors					
parking space	doors	ino change	7 spaces	2 spaces					
Source: Discretionary Permi	t Application, May 202	21; Project Site Plan, A	ugust 2021.						

Architecture. The proposed buildings would be concrete tilt-up construction with painted and scored accents. The design provides glazing and color variation along the length of the buildings, and would utilize a combination of materials and colors. The main colors of the buildings would be Nebulous White with accents colors of Online (medium) and Steely Gray (dark), with Leisure Blue trimming. The primary window blue reflective glass Solarcool Pacifica is located on the wall panels, the building corners, and office areas. There are trellises and free-standing concrete columns painted with a brushed Stainless color to provide accents to the buildings. The proposed screen walls would match the paint and color variations of the buildings. Conceptual elevations are shown in Figure 5.

The buildings designs would also incorporate the following features:

- Recessed primary entry doors
- Two story glazing
- Punched square first and second floor windows with thin concrete legs in a pattern to create an office look
- Concrete trellis with metal fin with canopy top
- Higher parapets at the primary entry to create hierarchy
- Higher parapets at the secondary corners to create additional hierarchy

Landscaping. Landscaping is proposed along all streets frontages, along Toyota Way, on the front and side setbacks adjacent to the buildings, and throughout the parking areas. The Project provides landscaping which exceeds the 15 percent landscaping requirement, and includes 29.80 percent for Building A and 35.59 percent for Building B. A landscaping plan is shown in Figure 6.

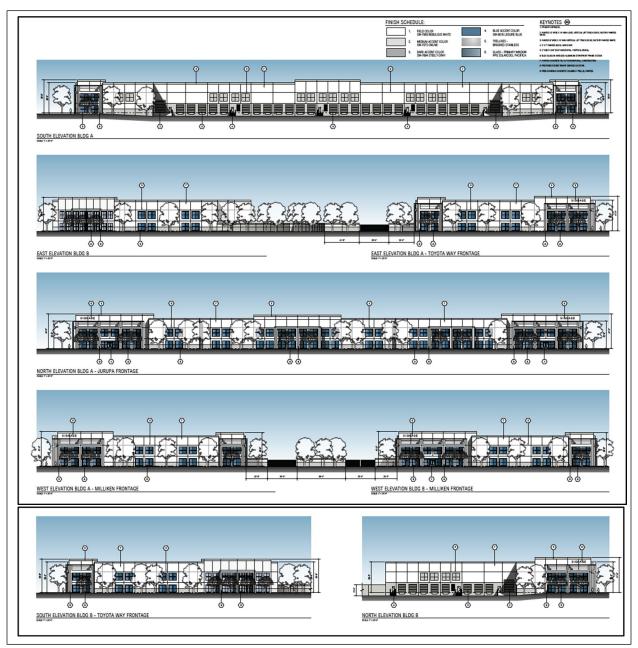


Figure 5: CONCEPTUAL BUILDING ELEVATIONS

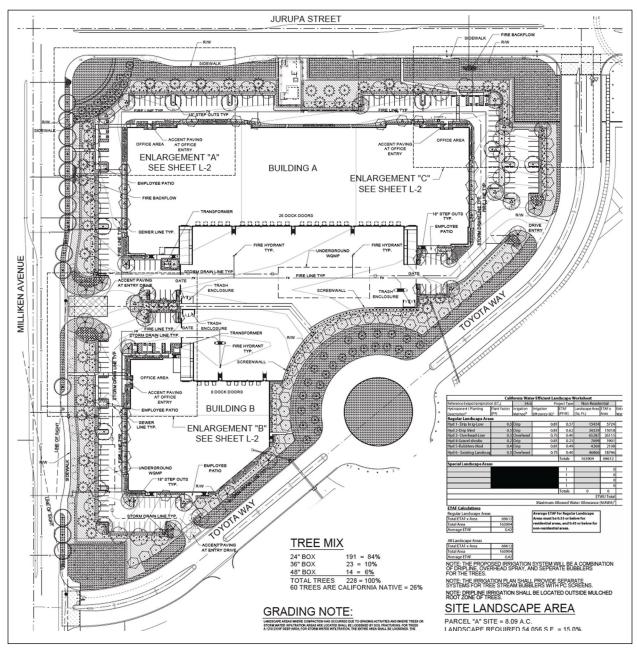


Figure 6: CONCEPTUAL LANDSCAPE PLAN

Drainage. For Building A, runoff from the east portion of the site, along with the northerly landscaped area, would be conveyed through storm drain Line A. Runoff from the west half of the site along with the parking lot along the south of the building would be conveyed through storm drain Line B. These storm drain lines would discharge into an underground infiltration chamber system located in the central portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume, as well as mitigate discharge to existing conditions for the Project site. The system would have a footprint of approximately 54-feet by 160-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line A, which would convey and then discharge to the existing private storm drain line in Toyota Way.

For Building B, runoff from north of the building would be conveyed through storm drain Line C. Runoff from south of the building would be conveyed through storm drain Line D. The

proposed storm drain lines would discharge into an underground infiltration chamber system located in the southwest portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume. The system would have a footprint of approximately 24-feet by 105-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line D and release in Line E, which would convey and then discharge to the existing private storm drain line in Toyota Way.

Circulation. There are two driveways off Toyota Way that would provide access to the site. A 35-foot wide driveway, located east of Building A, would be for automobile and truck use. To the south of Building B there would be a 29-foot wide driveway for automobile use only. A 40-foot wide driveway located along Milliken Avenue would provide direct access to the truck courts of both buildings.

Emergency vehicle access would be provided around the two buildings with 24-foot wide drive aisles through the parking areas and truck court. The majority of the automobile parking would be located along the perimeter of the proposed buildings.

Prior Environmental Analysis of Project Site. The Project site is located within the boundary of the Toyota/Ontario Business Park Specific Plan (Specific Plan), which was adopted in August 1993. The Specific Plan area is Assessor's Parcel Number (APN) 0238-121-75, totaling 95.35 gross acres

On January 27, 2010, the Ontario City Council adopted The Ontario Plan (TOP). TOP serves as the framework for the City's business plan and provides a foundation for the City to operate as a municipal corporation that consists of six distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report was prepared for TOP (TOP FEIR) (SCH # 2008101140) and certified by the City Council on January 27, 2010 and included Mitigation Measures, Findings, and a Statement of Overriding Considerations pursuant to CEQA. TOP FEIR analyzed the direct and physical changes in the environment that would be caused by implementation of TOP, focusing on changes to land use associated with the buildout of the proposed land use plan, and the associated population and employment growth in the City. The Project site was analyzed in TOP FEIR as industrial (See Exhibit A, TOP EIR Figure 1-3, Proposed Land Use Plan) to be consistent with the industrial uses to the north, west, and south of the Project site, Interstate I-15 freeway to the east, and the Project site's location under the landing path of the Ontario International Airport. The significant unavoidable adverse impacts that were identified in TOP FEIR included: agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise and transportation/traffic.

In 2019, Toyota requested a Specific Plan Amendment to change the zoning of the Project site (identified as Planning Area 1 in the Specific Plan) from Office/Research & Development (Office/R&D) to Industrial Mixed Use, which would allow for both Office/R&D and warehouse/distribution/manufacturing uses on the site. The Specific Plan Amendment also updated the landscape palette to conform to current drought tolerant landscape practices, which apply to any development in the Specific Plan area. Revisions to the Specific Plan also included:

- Any graphic that showed Planning Area 1 zoning as Office/R&D was changed to Industrial Mixed Use.
- Text in the Specific Plan that referred to Office/R&D was revised to reflect the new

- designation of Industrial Mixed Use.
- Permitted Uses under Industrial Mixed Use was updated to include warehouse and distribution uses.
- Graphic and text references to Rockefeller Drive within the Specific Plan area were changed to reflect the actual street name: Toyota Way.
- The landscape palette and graphic representations of landscape were updated to reflect more drought tolerant materials.
- Specific Design Guidelines for Office/R&D district were retitled to Industrial Mixed Use.

To evaluate the environmental impacts associated with the Specific Plan Amendment, the City prepared an Addendum to TOP FEIR. The Specific Plan Amendment, File No. PSPA 19-004 was approved in 2020 with an environmental Addendum Resolution No. 2020-063, which included the review and approval of three updated technical studies including a Traffic Trip Generation Comparison Letter, Greenhouse Gas Emissions CEQA Thresholds and Screening Tables, and a Cultural Resources Report.

Use of an Addendum. According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously certified EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162, requiring the preparation of a subsequent Negative Declaration or EIR, have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

1. Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.

Substantial changes are not proposed by the Project, and Project construction and operation would not require revisions to TOP FEIR. TOP FEIR analyzed the environmental impacts that would be caused by implementation of TOP; focusing on changes to land use associated with the buildout of the proposed land use plan. The Project site is located in the Toyota/Ontario Business Park Specific Plan area, with a zoning designation of Industrial Mixed Use, which would allow warehouse/distribution uses along with the Office/R&D uses. As described in the Specific Plan and, therefore, analyzed in TOP FEIR, maximum development within Planning Area 1 could be up to 300,000 square feet. The proposed Project would include 168,722 square feet of warehouse use on the site, which would be significantly less development than assumed at the site in TOP FEIR. Therefore, the proposed Project would result in the less development than TOP FEIR analyzed at buildout.

Because the proposed Project would result in reduction in development, compared to the site development assumptions originally included in TOP FEIR analysis, no revisions to TOP FEIR are required. A trip generation comparison was conducted by Ganddini Group, and reviewed by the City's Traffic Engineering Division, which compares trip generation at the Project site under the original zoning versus the development proposed under the Specific Plan Amendment. As shown below in Table 3, the Specific Plan Amendment zoning would result in 2,528 fewer daily personal car equivalent (PCE) trips compared to the development allowed under the prior zoning included in TOP FEIR. It should also be noted that the proposed Project would include less square footage than analyzed within the Ganddini Group trip generation comparison, so the reduction in trips would likely be greater than what was identified in Table 3.

Table 3: Trip Generation Comparison									
				Trip	Gene	eration			
		AM	Peak I	Hour	P۸	1 Peak I	Hour	Daily	
Zoning	Quantity	In	Out	Total	In	Out	Total	Daily	
Original Zoning (Office)	300,000 SF	299	49	348	55	290	345	2,922	
Proposed (Warehouse)	173,247 SF	35	14	49	16	36	52	394	
Trip Generation Comparison (Proposed Project – Original Zoning)		-264	-35	-299	-39	-254	-293	-2,528	
Source: Ganddini Grou	ıp Inc, 2019.								

In addition, all previously adopted mitigation measures of TOP FEIR are applicable to the Project and are incorporated herein by reference. Additionally, City Standard Conditions of Approval, and Development and Performance Standards included in the Specific Plan, would be applicable to the proposed Project. Construction and operation of the proposed Project would not result in new significant environmental effects or a substantial increase in the severity of previously identified effects. The attached Initial Study provides an analysis of the proposed Project and verification that the Project would not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

 Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.

Substantial changes have not occurred that would require major revisions to TOP FEIR. TOP FEIR evaluated the Project site as Industrial with a maximum FAR of 0.55 (see attached Exhibit "A"- TOP EIR Figure 1-3, Proposed Land Use Plan), consistent with the surrounding industrial properties to the north, west, and south. A Specific Plan Amendment was adopted by the City Council and an Addendum to TOP FEIR was prepared and adopted in 2020. The proposed Project would result in the construction of two warehouse buildings, and associated site improvements, and would be consistent with the Industrial Mixed Use land use designation included in the Specific Plan Amendment. No proposed changes or revisions to TOP FEIR are required. In addition, all previously adopted mitigation measures of TOP FEIR are applicable to the Project and are incorporated herein by reference. Lastly, City Standard Conditions of Approval, and Development and Performance Standards included in the Specific Plan, would be applicable to the proposed Project. The attached Initial Study provides an analysis of the proposed Project and verification that the Project would not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

3. Required Finding. No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.

No new information has been provided that would indicate the proposed Project would result in any new significant effects not previously discussed in TOP FEIR. As stated above in Section 2, no substantial changes have occurred with respect to the circumstances under which the Project was undertaken. TOP FEIR evaluated the site as Industrial with a maximum FAR of 0.55 (see attached Exhibit "A"- TOP EIR Figure 1-3, Proposed Land Use Plan), consistent with the surrounding industrial properties to the north, west, and south. Since adoption of the 1992 General Plan and the 2010 TOP FEIR, the Project site and

surrounding area have been planned for and remained industrial use (see attached Exhibit "B"-1992 General Plan Land Use Map). Therefore, no proposed changes or revisions to TOP FEIR are required. In addition, all previously adopted mitigation measures of TOP FEIR are applicable to the Project and are incorporated herein by reference. Finally, City Standard Conditions of Approval, and Development and Performance Standards included in the Specific Plan, would be applicable to the proposed Project. The attached Initial Study provides an analysis of the proposed Project and verification that the Project would not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

CEQA Requirements for an Addendum. If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

- 1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- 2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- 3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:
 - i. The project will have one or more significant effects not discussed in the previous negative declaration;
 - ii. Significant effects previously examined will be substantially more severe than shown in the previous EIR;
 - iii. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
 - iv. Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the proposed Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to TOP FEIR.

Conclusion. TOP FEIR, certified by City Council on January 27, 2010, was prepared as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). TOP FEIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment associated with implementation of TOP. Consequently, TOP FEIR focused on impacts from changes to land use associated with buildout of the City's Land Use Plan, and impacts from the resulting population and employment growth in the City. The proposed Project is consistent with the existing uses of the properties and uses within the surrounding areas. As described above, the amount of development associated with the proposed Project would be lower for Planning Area 1 than TOP FEIR analyzed.

Accordingly, and based on the findings and information contained in the previously certified TOP FEIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in TOP FEIR. No changes or additions to TOPF EIR analyses are necessary, nor is there a need for any additional mitigation measures. Therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to TOP FEIR.

9. Project Setting: The Project site is relatively flat with minimal topographic variation, with the majority of the area being developed with the warehouse building, parking lot and landscaping. The vacant area of the project site being proposed for new development is primarily covered in grass. Mature palm trees are planted at various locations along the perimeter of the site with shrubs located along the Toyota Way frontage. Existing sidewalks are located along the Milliken Avenue and Jurupa Street frontages. Street lighting is also located along the perimeter of the site. A city well and structure is south of Jurupa Street immediately adjacent to the Project site; however, this area is not included within the Project site. The Project site is located in a developed, urban area of the City of Ontario, and is surrounded by business park uses, warehouses, and distribution facilities.

The Project site is located within the Toyota/Ontario Business Park Specific Plan (Specific Plan), which identifies the land use of the site as Industrial Mixed Use and Warehouse/Distribution. As described in the Specific Plan, which was adopted in August 1993, the purpose of the document is to assure the systematic implementation of the goals and policies contained in the Ontario General Plan. The Specific Plan contains development standards for the classifications of land use within the Project site, and addresses transportation and circulation, streetscape and landscape guidelines, and infrastructure and public services.

The proposed new industrial buildings would be located on approximately 13 acres of vacant land in the northwest corner of the Project site and bounded by Jurupa Street to the north, Toyota Way to the south and east, and Milliken Avenue to the west.

10. Surrounding Land Uses:

	Existing Land Use	General Plan Designation	Zoning Designation	Specific Plan Land Use
Site:	Warehouse, vacant	Industrial	Toyota Ontario Business Park Specific Plan	Industrial Mixed Use, Warehouse/Distribution
North:	Warehouse/ Distribution	Industrial	California Commerce Center Specific Plan	Light Industrial

Addendum to The Ontario Plan Environmental Impact Report File Nos.: PMTT21-010 and PDEV21-018

		Existing Land Use		neral Plan signation	Zoning Desig	gnatio	<u>n Sr</u>	ecific P	lan Land Use
Soi	uth:	Warehouse	Ir	ndustrial	Entratter Inc Specific F		ıl	Inc	lustrial
Ea	st:	Interstate 15 Freeway		erstate 15 reeway	Interstate Freewo			_	state 15 eeway
We	est:	Office/Business Park	Ir	ndustrial	Californ Commerce Specific I	Cente	er Co		ndustrial, al/Food/Hotel
11.		er public agencies whicipation agreement): 1			required (e.g	J., pe	rmits, f	inancin	g approval or
12.		e California Native Am a requested consultatio							
	If "y	es", has consultation be	egur	ı\$			□ Yes	□No	□ Completed
EN	VIRO	NMENTAL FACTORS POT	ENTI	ALLY AFFECTE	D				
at I	east	ironmental factors chec one impact that is a "F g pages.							
	Aest	thetics		Agriculture/Fo	orestry		Air Qu	uality	
	Biolo	ogical Resources		Cultural Reso	urces		Geolo	ogy / So	ils
	Gre	enhouse Gas Emissions		Hazards & Ha Materials	zardous		Hydro	ology/W	ater Quality
	Land	d Use / Planning		Mineral Resou	urces		Noise		
	Рор	ulation / Housing		Public Service	es		Recre	ation	
	Tran	sportation		Utilities / Servi	ce Systems			latory Fi cance	ndings of
	Tribo	al Cultural Resources		Wildfire			Energ	У	
DET	ERM	INATION (To be comple	ted	by the Lead A	Agency)				
On	the	basis of this initial evalue	atio	n:					
	☐ I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.								
		d that although the property will not be a significe			_				

made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.

- ☐ I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- □ I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- ☑ I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or NEGATIVE DECLARATION pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or NEGATIVE DECLARATION, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Signature: Edmelynne V. Hitter	Date: November 16, 2021
Printed Name: Edmelynne V. Hutter, Senior Planner	For: City of Ontario

EVALUATION OF ENVIRONMENTAL IMPACTS

- 1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2. All answers must take account of the whole action involved, including off-site as well as onsite, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4. "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).
- 5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

- a. Earlier Analyses Used. Identify and state where they are available for review.
- b. Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
- c. Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
- 7. Supporting Information Sources. A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.
- 8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9. The explanation of each issue should identify:
 - a. The significance criteria or threshold, if any, used to evaluate each question; and
 - b. The mitigation measure identified, if any, to reduce the impact to less than significance.

(Note: Example explanations have been provided. Add, remove, or replace as needed.)

		Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
1.		THETICS. Except as provided in Public Resources Code tion 21099, would the project:				
	a)	Have a substantial adverse effect on a scenic vista?				×
	b)	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?				
	c)	In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?)				
	d)	Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?				
2.		RICULTURE AND FOREST RESOURCES. In determining ether impacts to agricultural resources are significant				

		Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
	Cali Asse of C imp whe are refe Dep state Ran Asse met	ironmental effects, lead agencies may refer to the ifornia Agricultural Land Evaluation and Site essment Model (1997) prepared by the California Dept. Conservation as an optional model to use in assessing acts on agriculture and farmland. In determining ether impacts to forest resources, including timberland, significant environmental effects, lead agencies may enter to information compiled by the California partment of Forestry and Fire Protection regarding the etal inventory of forest land, including the Forest and age Assessment Project and the Forest Legacy essment project; and forest carbon measurement thodology provided in Forest protocols adopted by the iffornia Air Resources Board. Would the project:				
	a)	Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?				⊠
	b)	Conflict with existing zoning for agricultural use, or a Williamson Act contract?				
	c)	Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?				
	d)	Result in the loss of forest land or conversion of forest land to non-forest use?				×
	e)	Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?				X
3.	estc distr	QUALITY. Where available, the significance criteria ablished by the applicable air quality management rict or air pollution control district may be relied upon to see the following determinations. Would the project:				
	a)	Conflict with or obstruct implementation of the applicable air quality plan?				
	b)	Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?				×
	c)	Expose sensitive receptors to substantial pollutant concentrations?				×
	d)	Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?				⊠
4.	BIO	LOGICAL RESOURCES. Would the project:				
	a)	Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish				⊠

Issues and Game or U.S. Fish and Wildlife Service?	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?				⊠
c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				×
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				⊠
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?				⊠
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?				×
5. CULTURAL RESOURCES. Would the project:				
 a) Cause a substantial adverse change in the significance of a historical resource pursuant to § 15064.5? 				⊠
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?				⊠
c) Disturb any human remains, including those interred outside of dedicated cemeteries?				⊠
6. ENERGY. Would the project:				
a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?				×
b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?				⊠
7. GEOLOGY AND SOILS. Would the project:				
a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:				
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.				⊠
ii) Strong seismic ground shaking?				×
iii) Seismic-related ground failure, including				\boxtimes

		Issues liquefaction?	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
		iv) Landslides?				\boxtimes
	b)	Result in substantial soil erosion or the loss of topsoil?				
	c)	Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?				×
	d)	Be located on expansive soil, as defined in Table 18- 1-B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?				×
	e)	Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?				×
	f)	Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?				×
8.	GRE	EENHOUSE GAS EMISSIONS. Would the project:				
	a)	Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?				⊠
	b)	Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?				×
9.	HAZ	ARDS AND HAZARDOUS MATERIALS. Would the project:				
	a)	Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?				⊠
	b)	Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?				×
	c)	Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?				×
	d)	Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				×
	e)	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?				⊠
	f)	Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				×
	g)	Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death				×

		Issues involving wildland fires?	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
10.	HYC	DROLOGY AND WATER QUALITY. Would the project:				
	a)	Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?				×
	b)	Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?				X
	c)	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:				
		i) Result in substantial erosion or siltation on- or off- site;				
		ii) Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;				×
		iii) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or				⊠
	d)	Impede or redirect flood flows?				×
	e)	In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?				×
	f)	Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?				×
11.	LAN	ID USE AND PLANNING. Would the project:				
	a)	Physically divide an established community?				×
	b)	Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?				☒
12.	MIN	ERAL RESOURCES. Would the project:				
	a)	Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?				×
	b)	Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?				×
13.	NOI	SE. Would the project result in:				
	a)	Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?				X
	b)	Generation of excessive groundborne vibration or groundborne noise levels?				⊠

	Issue	s	Potentially Significant	Less Than Significant with	Less Than Significant	No New Impact /
			Impact	Mitigation	Impact	No Impact
	airstrip or an airport lan plan has not been add public airport or public	within the vicinity of a private d use plan or, where such a opted, within two miles of a use airport, would the project or working in the project area s?				⊠
14.	POPULATION AND HOUSING. Would the project:					
	an area, either directly new homes and bu	anned population growth in (for example, by proposing (sinesses) or indirectly (for tension of road or other				×
	b) Displace substantial nu housing, necessitating replacement housing el					⊠
15.	PUBLIC SERVICES. Would the	project:				
	associated with the praltered governmental physically altered governmental construction of which environmental impact acceptable service rate	adverse physical impacts ovision of new or physically facilities, need for new or overnmental facilities, then could cause significant its, in order to maintain tios, response times or other of or any of the public services:				
	i) Fire protection?					×
	ii) Police protection?					×
	iii) Schools?					×
	iv) Parks?					×
	v) Other public facilitie	es\$				×
16.	RECREATION.					
	neighborhood and recreational facilities su	crease the use of existing regional parks or other uch that substantial physical acility would occur or be				×
	require the construction	de recreational facilities or or expansion of recreational n adverse physical effect on				⊠
17.	TRANSPORTATION. Would the project:					
		m plan, ordinance or policy ion system, including transit, bedestrian facilities?				⊠
	b) Conflict or be inconsist section 15064.3, subdivision	stent with CEQA Guidelines sion (b)?				⊠
	design feature (e.g.,	nazards due to a geometric sharp curves or dangerous mpatible uses (e.g., farm				⊠
	d) Result in inadequate en	nergency access?				×

Issues		Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact	
18.	TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:					
	a)	Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or				⊠
	b)	A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.				⊠
19.	UTIL	ITIES AND SERVICE SYSTEMS. Would the project:				
	a)	Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?				×
	b)	Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?				X
	c)	Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?				×
	d)	Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?				×
	e)	Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?				×
20.	land	WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
	a)	Substantially impair an adopted emergency response plan or emergency evacuation plan?				
	b)	Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?				×
	c)	Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may				×

	Issues		Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	No New Impact / No Impact
		exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?				
	d)	Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?				☒
21.	MANDATORY FINDINGS OF SIGNIFICANCE. (State CEQA Guidelines section 15065(a).)					
	a)	Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?				⊠
	b)	Does the project have the potential to achieve short- term environmental goals to the disadvantage of long-term environmental goals?				
	c)	Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)				X
	d)	Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?				×

Note: Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.

Reference: Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083.3, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; Sundstrom v. County of Mendocino (1988) 202 Cal.App.3d 296; Leonoff v. Monterey County Board of Supervisors (1990) 222 Cal.App.3d 1337; Eureka Citizens for Responsible Govt. v. City of Eureka (2007) 147 Cal.App.4th 357; Protect the Historic Amador Waterways v. Amador Water Agency (2004) 116 Cal.App.4th 1099, 1109; San Franciscans Upholding the Downtown Plan v. City and County of San Francisco (2002) 102 Cal.App.4th 656.

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

<u>Discussion of Effects</u>: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, the Policy Plan (Policy CD1-5) requires all major require north-south streets be designed and redeveloped to feature views of the San Gabriel Mountain. The project site is located along Milliken Avenue and Jurupa Street, both principal arterials, as identified in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan.

The proposed Project would allow for the construction of warehouses within an area that has warehouse, business park, and industrial development. While construction of the Project may block some public views to the north, intermittent views of the San Gabriel Mountains would still be available, and the Project would not result in significant adverse environmental impacts with

regard to views of the San Gabriel Mountains; no significant adverse impacts on a scenic vista would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Substantially damage scenic resources, including, but not limited to, tress, rock outcroppings and historic buildings within a state scenic highway?

<u>Discussion of Effects</u>: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east-west direction. I-15 traverses the northeastern portion of the City in a north-south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Substantially degrade the existing visual character or quality of the site and its surroundings?

<u>Discussion of Effects</u>: The project would not degrade the existing visual character or quality of the site or its surroundings. The project site is located in an urban area that is characterized by industrial development and is surrounded by urban land uses. Development of the proposed Project would be required to meet policies of the TOP Community Design Element and zoning designations on the property. Furthermore, the proposed Project would be required to be consistent with the design guidelines and development standards of the Toyota/Ontario Business Park Specific Plan. The Project site is zoned for Industrial Mixed Use, and the Project would not conflict with regulations governing scenic quality.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

<u>Discussion of Effects</u>: New lighting will be introduced to the site with the development of the project. Pursuant to the requirements of the City's Development Code, project on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Furthermore, as required by the Toyota/Ontario Business Park Specific Plan, a Master Lighting Plan would be submitted to the City of Ontario for review and approval. The Master Plan would contain criteria and standards governing lighting along Toyota Way, lighting within parking lots and access drives, and lighting improvements for pedestrian walkways. The Master Plan would also establish minimum illumination criteria consistent with City of Ontario policies on exterior illumination. Site lighting would be directed inward and downward, to avoid spillover of light and glare onto the adjacent freeway, nearby public streets or onto adjacent properties.

Additionally, the Project would be required to adhere to the following performance standard included in the Toyota/Ontario Business Park Specific Plan:

5.11.3 Light and Glare: No lighting fixture shall create any illumination which exceeds five foot candles on adjacent parcels of land, whether such illumination is direct or indirect. Glare levels shall be measured with a photoelectric photometer following standard spectral luminous efficiency curves adopted by the International Commission of Illumination.

Site lighting plans are subject to review by the Planning Department and Ontario Police Department prior to issuance of building permits (pursuant to the City's Building Security Ordinance); no significant adverse impacts would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

- 2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:
- a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

<u>Discussion of Effects</u>: The proposed development site is presently vacant and does not contain any agricultural uses. Further, the site is identified as Urban and Built-up Land on the map prepared by the California Resources Agency, pursuant to the Farmland Mapping and Monitoring Program. As a result, no adverse environmental impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

<u>Discussion of Effects</u>: The project site is not zoned for agricultural use. The project site zoned is Industrial Mixed Use (Toyota Ontario Business Park Specific Plan). The proposed project is consistent with the development standards and allowed land uses of the proposed zone. Furthermore, there is no Williamson Act contract in effect on the subject site. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with existing or Williamson Act contracts.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section

4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g)?

<u>Discussion of Effects</u>: Impacts to forest land were not analyzed in TOP FEIR, but this topic has since been included as part of the revisions to the State CEQA Guidelines. The Project site is zoned Industrial Mixed Use. Development of the proposed Project would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production as those land use designations do not exist within the City of Ontario. No impacts to forest or timberland would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not conflict with existing zoning or cause rezoning of forest land, timberland, or timberland zoned Timberland Production. No impact would occur and no further analysis of impacts to forest resources or timberland is required in TOP FEIR; no changes or additions to TOP FEIR analysis are necessary.

d. Result in the loss of forest land or conversion of forest land to non-forest use?

<u>Discussion of Effects</u>: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither The Ontario Plan nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in a loss of forest land or conversion of forest land to non-forest use. No impact would occur and no further analysis of impacts to forest resources is required in TOP FEIR; no changes or additions to TOP FEIR analyses are necessary.

e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

<u>Discussion of Effects</u>: Construction of the proposed Project would not result in changes to the existing environment that would result in the loss of farmland. While conversion of farmland increases the potential for adjacent areas to also be converted from farmland to urban uses, there are no agricultural uses occurring onsite or in the immediate vicinity of the Project site; the Project does not directly or indirectly result in conversion of farmland. No new cumulative impacts beyond those identified in the Certified TOP FEIR would result from Project construction. As a result, the Project would not result in loss of Farmland to non-agricultural use.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. The proposed Project would not result in the conversion of forest land to non-forest use.

<u>Mitigation Required</u>: No additional mitigation is required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

- 3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:
 - a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects: As described in TOP FEIR, TOP would not be consistent with the South

Coast Air Quality Management District's Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of TOP Land Use Plan would exceed estimates of population, employment, and VMT for Ontario; these emissions are not included in the regional emissions inventory for the SoCAB used for the analysis within TOP FEIR. Therefore TOP was considered inconsistent with the AQMP resulting in a significant unavoidable impact.

Because the proposed Project would result in a decrease in the amount of development than was anticipated at the Project site under TOP FEIR, construction and operation of the proposed Project would not increase the identified significant air quality impact associated with implementation of TOP

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

<u>Discussion of Effects</u>: As described in TOP FEIR, both construction and operational air quality impacts associated with implementation of TOP Land Use Plan were deemed to be significant. Mitigation measures were identified, but the impacts were still considered significant and unavoidable. These mitigation measures are applicable to the proposed Project, and are included below:

- 3-1: The City of Ontario Building Department shall require that all new construction projects incorporate all feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:
 - Requiring fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as:
 - Requiring use of nontoxic soil stabilizers to reduce wind erosion.
 - Applying water every four hours to active soil-disturbing activities.
 - Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials.
 - Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 or higher exhaust emission limits.
 - Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards.
 - Limiting nonessential idling of construction equipment to no more than five consecutive minutes.
 - Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.agmd.gov/prdas/brochures/Super-Compliant_AIM.pdf.
- 3-2: The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g., sidewalks).

Because the proposed Project would result in a decrease in the amount of development anticipated at the Project site compared to what was assumed in TOP FEIR, construction and

operation of the proposed Project would not increase the anticipated air quality impacts identified within TOP FEIR.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

<u>Discussion of Effects</u>: As described in TOP FEIR, eight high vehicle count intersections were evaluated for CO emissions, and it was determined that sensitive receptors in the area would not be significantly adversely affected by CO emissions generated at buildout of the Land Use Plan. Localized air quality impacts related to mobile-source emissions were identified as being less than significant.

In addition, as noted in TOP FEIR, the California Air Resource Board (CARB) developed and approved the Air Quality and Land Use Handbook: A Community Health Perspective in May 2005 to address the siting of sensitive land uses in the vicinity of freeways, distribution centers, rail yards, ports, refineries, chrome-plating facilities, dry cleaners, and gasoline-dispensing facilities. This guidance document was developed to assess compatibility and associated health risks when placing sensitive receptors near existing pollution sources.

As detailed in TOP FEIR Table 5.3-9, CARB provides the following recommendations for siting new sensitive land uses for distributions centers:

- Avoid siting new sensitive land uses within 1,000 feet of a distribution center (that
 accommodates more than 100 trucks per day, more than 40 trucks with operating
 transport refrigeration units [TRUs] per day, or where TRU unit operations exceed 300 hours
 per week).
- Take into account the configuration of existing distribution centers and avoid locating residences and other sensitive land uses near entry and exit points.

The Project site is located within an industrial area, and is not located in close proximity to any sensitive receptors.

The proposed warehouse would not be located within 1,000 feet of any sensitive receptor, and is not anticipated to generate 100 truck trips per day; the trip generation analysis provided by Ganddini Group estimated 61 truck trips per day. The proposed Project would not place sensitive receptors near existing pollution sources. Therefore, impacts related to the exposure of sensitive receptors to substantial pollutant concentrations would not occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people)?

<u>Discussion of Effects</u>: As described in TOP FEIR, construction activity would require the operation of equipment that may generate exhaust from either gasoline or diesel fuel. Construction and development would also require the application of paints and the paving of roads, which could generate odors from materials such as paints and asphalt. As these odors are short-term in nature and quickly disperse into the atmosphere, this is not considered significant. Additionally, commercial, industrial, and residential projects, associated with implantation of TOP are also required to comply with SCAQMD Rule 402 to prevent occurrence of public nuisances. As a result, Project-related odors are required to avoid the creation of a public nuisance. Odorous emissions attributable to implementation of TOP are not considered a significant adverse impact to air quality.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

<u>Discussion of Effects</u>: The Project site is within an urbanized area, characterized by industrial development, and lacks native habitat. However, the United States Department of Fish and Wildlife (USFWS) has identified the Project vicinity as an area where the endangered Delhi Sands flower-loving fly (*Rhaphiomidas terminates abdominalis*) is known to currently or to have at one time existed. Because the Delhi Sands flower-loving fly requires a specific habitat type, this species requires site specific considerations, protection and enhancement of its limited habitat type, and species specific management to maintain the habitat and populations. To avoid potential impacts to this species, the proposed Project conducted pre-construction surveys for Delhi Sands flower-loving fly to determine if the species or its habitat are currently located on the Project site. The pre-construction surveys of the site determined that there is currently no extant habitat for the species nor were any individuals of the species observed on the Project site. Therefore, impacts to species identified as a candidate, sensitive, or special status species will not occur as a result of the proposed Project.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

<u>Discussion of Effects</u>: The Project site is within an urbanized area and lacks native habitat. According to the United States National Wetlands Inventory (NWI), no riparian habitat is found within the Project site. Therefore, no significant impact would occur to riparian or other sensitive natural communities as a result of construction and operation of the proposed Project.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

<u>Discussion of Effects</u>: The Project site is within an urbanized area and lacks any wetlands. According to the United States National Wetlands Inventory (NWI), no wetlands occur within the Project site; no significant wetlands impact would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

<u>Discussion of Effects</u>: As described in TOP FEIR, no regional wildlife movement corridors have been identified in the City, and most of the City is ill-suited for the purposes of wildlife movement. Furthermore, the Project site is within an urban area with industrial uses and is bounded on all sides by development. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no significant wildlife corridor impacts would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

<u>Discussion of Effects</u>: As described in TOP EIR, while the City of Ontario does not have any municipal ordinances for the protection of trees on private property, Municipal Code Sections 10-1.25 and 10-2.05 prohibit the damaging or destruction of trees on City property, except under conditions specified in the Municipal Code. Additionally, TOP Policy ER5-2 notes that the City will comply with state and federal regulations regarding protected species. The proposed Project would not conflict with any local policies or ordinances protection biological resources.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

<u>Discussion of Effects</u>: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

- 5. CULTURAL RESOURCES. Would the project:
 - a. Cause a substantial adverse change in the significance of a historical resource as defined in Section 15064.5?

<u>Discussion of Effects</u>: The Project site is vacant and does not contain any buildings or structures constructed more than 50 years ago (generally, structures need to be at least 50 years or older to be considered historical resources). As such, there are no structures onsite eligible for listing in the California Register of Historic Resources.

A Cultural Resources Records Search was conducted on July 16, 2019 at the California Historic Resource Inventory System at the South Central Coastal Information Center (CHRIS-SCCIC). The records search indicated that there are no cultural resources (prehistoric, historic, or built environments) recorded within the Project boundaries. There was one historic resource (CA-SBR-008857H) located within a one-half mile radius of the Project site. The historic resource is a section of the Southern California Edison Company's Lugo-Mira No. 1 500kv Transmission Line. The

transmission line was determined eligible for listing in the National Register of Historic Place (NRHP) under Criteria A and C, and therefore, is eligible for listing in the California Register of Historic Resources. However, this historic resource would not be impacted (directly or indirectly) by the proposed Project. Therefore, no significant impacts would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

<u>Discussion of Effects</u>: As described in TOP FEIR, a records review at the Archaeological Information Center at San Bernardino County Museum (SBCM) indicated no known prehistoric archaeological resources in the City of Ontario; however, only about 10 percent of the City has been adequately surveyed for prehistoric or historic archaeology. Figure 5.5-2 of TOP FEIR shows that the Project site has not been surveyed for archeological resources. The CHRIS-SCCIC records search, noted in subsection "a" above, did not identify prehistoric, historic, or historic built environments within or adjacent to the Project boundaries. Additionally, an NAHC Sacred Lands File search failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site.

The Project site has been highly disturbed by modern human activities including agricultural use from the 1940's through the 1960's and the development and construction of the Toyota Motors North American Parts Center and supporting infrastructure that would have displaced potential surface and subsurface archaeological resources. The proposed Project would not impact cultural (prehistoric, historic, or historic built environments) resources and no mitigation measures are recommended.

While no adverse impacts to archeological resources are anticipated at the site due to its urbanized nature, the following City Standard Conditions of Approval would be applied should unanticipated archaeological resources be discovered during excavation or construction:

5.2 If any archeological or paleontological resources are found during Project grading, excavation, or construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

<u>Mitigation</u>: No additional mitigation is required. The Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Disturb any human remains, including those interred outside of dedicated cemeteries?

<u>Discussion of Effects</u>: The proposed Project is located in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area, and human remains are not expected to be encountered during construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, the following City Standard Conditions of Approval would be applied in the event of unanticipated discoveries of human remains are identified during excavation and construction activities:

5.1 If human remains are found during Project grading, excavation, or construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed (if deemed applicable).

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

6. ENERGY. Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

<u>Discussion of Effects</u>: Energy was not analyzed in TOP FEIR, but was included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the proposed Project would increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. A detailed discussion is provided below:

Electricity

Construction. Temporary electric power would be required for lighting and electronic equipment (e.g., computers) located in trailers used by the construction crew. However, the electricity used for such activities would be temporary and would have a negligible contribution to the Project's overall energy consumption.

Operation. Operation of the proposed Project would require electricity for multiple purposes, such as: building heating and cooling, lighting, appliances, and electronics. Although electricity consumption would increase at the site under implementation of the proposed Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The Project would also be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the Project would implement additional measures, as detailed in the GHG reduction measures screening table, which would further reduce electricity consumption. For these reasons, the electricity that would be consumed by the Project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Natural Gas

Construction. Natural gas consumption is not anticipated during construction of the Project. Fuels used for construction would generally consists of diesel and gasoline, which are discussed in the next subsection. Any amounts of natural gas that may be consumed during Project construction would be nominal and would have a negligible contribution to the Project's overall energy consumption.

Operational. The operational phase of the proposed Project would require natural gas consumption for various purposes, such as building heating and cooling. While natural gas consumption would increase at the site with construction of the proposed Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The Project would be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. For these reasons, the natural gas that would be consumed by the Project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Diesel and Gasoline Fuel

Construction. Diesel and gasoline fuels, also referred to as petroleum in this subsection, would be consumed throughout construction of the Proposed Project. Fuel consumed by construction equipment would be the primary energy resource consumed over the course of construction, and vehicle miles traveled (VMT) associated with the transportation of construction materials (e.g., deliveries to the site) and worker trips to and from the site would also result in petroleum consumption. Whereas on-site, heavy-duty construction equipment and delivery trucks would predominantly use diesel fuel, construction workers would generally rely on gasoline-powered vehicles. Construction-related vehicles would be required to comply with CARB's Airborne Toxic Control Measures, which restricts heavy-duty diesel vehicle idling to five minutes. Since petroleum use during construction would be temporary and required to conduct development activities, it would not be wasteful or inefficient, and impacts would be less than significant.

Operational. Fuel consumption associated with development pursuant to the proposed Project's operational phase would primarily be attributable to workers commuting to and from the Project and the operation of large, diesel-powered trucks (e.g., semi-trucks) needed to transport goods. Over the lifetime of the Project, the fuel efficiency of the vehicles being used by the employees is expected to increase. As such, the amount of petroleum consumed as a result of vehicular trips to and from the Project site during operation is anticipated to decrease over time. Operation of the Project is expected to decrease the amount of petroleum it consumes in the future due to advances in fuel economy. Although the proposed Project would increase petroleum use in the region during construction and operation, the use would be a small fraction of the statewide use and, due to efficiency increases, would diminish over time. As such, petroleum consumption associated with the Project would not be considered inefficient or wasteful and would result in a less-than-significant impact.

<u>Mitigation</u>: The Project would not result in any new significant impacts. No changes or additions to TOP FEIR analyses are necessary.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

<u>Discussion of Effects</u>: The proposed Project would not conflict with or obstruct a state or local plan adopted for the purposes of increasing the amount of renewable energy or energy efficiency. The California Title 24 Building Code contains energy efficiency standards for non-residential buildings. These standards address electricity and natural gas efficiency in lighting, water, heating, and air conditioning, as well as the effects of the building envelope (e.g., windows, doors, walls and rooves, etc.) on energy consumption. As described above, the Project would be required to comply with the 2019 CALGreen standards, and would implement additional measures identified in the City's Greenhouse Gas Reduction Measures Screening Threshold Table for Commercial and Industrial Development. Given the above, the proposed Project would not conflict with nor obstruct a state or local plan for renewable energy or energy efficiency. This impact would be less than significant.

<u>Mitigation</u>: The Project would not result in any new significant impacts. No changes or additions to TOP FEIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

- a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:
- i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other

substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

<u>Discussion of Effects</u>: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Ontario Plan FEIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

ii. Strong seismic ground shaking?

<u>Discussion of Effects</u>: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Land Use Plan (Figure LU-6) of the Policy Plan (General Plan) FEIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will be in compliance with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

iii. Seismic-related ground failure, including liquefaction?

<u>Discussion of Effects</u>: As identified in the TOP FEIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

iv. Landslides?

<u>Discussion of Effects</u>: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the city) makes the chance of landslides remote. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

<u>Discussion of Effects</u>: Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, the following City Standard Conditions of Approval would reduce the potential for substantial erosion:

3.67 Prior to Grading Plan approval and the issuance of a grading permit, an Erosion and Sediment Control Plan shall be submitted to, and approved by, the Engineering Department. The Erosion and Sediment Control Plan shall specifically identify the Best Management Practices (BMPs) that will be implemented on the Project during construction, to reduce the discharge of sediment and other pollutants into the City's storm drain system.

Compliance with the California Building Code, review of grading plans, and approval and implementation of the Erosion and Sediment Control Plan by the Engineering Department would ensure no significant erosion impacts occur. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, California Building Code, and Ontario Municipal code would reduce impacts to a less than significant level.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

<u>Discussion of Effects</u>: Please see Section 7.a.iii for a discussion of liquefaction and Section 7.a.iv for a discussion of landslides.

Lateral spreading is a form of horizontal displacement of soil toward an open channel or other "free" face, such as an excavation boundary. Lateral spreading may also occur where open banks and unsupported cut slopes provide a free face. Ground subsidence is the gradual settling or sinking of the ground surface with little or no horizontal movement, and most often results from human activities such as the extraction of oil, gas, or groundwater. Effects of subsidence include fissures, sinkholes, depressions, and disruption of surface drainage.

As described in TOP FEIR, projects developed pursuant to TOP would be required to meet the most current seismic safety requirements in the California Building Code (CBC). Chapter 16 of the CBC contains requirements for design and construction of structures to resist loads, including earthquake loads. Chapter 18 contains requirements for excavation, grading, and fill; load-bearing values of soils; and foundations, footings, and piles. Compliance with those requirements would ensure that there would not be substantial impacts related to ground shaking, liquefaction, or seismic settlement. TOP Policy S1-1 would require that all new habitable structures be designed in accordance with the most recent Building Code adopted by the City, including provisions regarding lateral forces and grading. Implementation of TOP strategies, California Building Code, and Ontario Municipal code would reduce impacts to a less than significant level.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and

addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

<u>Discussion of Effects</u>: Soils containing expansive clay minerals can shrink or swell substantially as the moisture content decreases or increases. Structures built on these soils may experience shifting, cracking, and breaking damage as soils shrink and subside or expand.

As described in TOP FEIR, expansive soils are likely in the southern parts of the City, where there are silts, sandy silts, and silty clays. Near-surface soils in the northern and central parts of the City are primarily granular, that is, silty sand, sand, and gravel; such sediments are usually non-expansive or have very low expansion potential. Projects in the southern part of the City considered for approval under TOP could expose persons or structures to potentially significant hazards from expansive soils. However, compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure no significant impacts would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

<u>Discussion of Effects</u>: The Project area is served by the local sewer system and the use of septic tanks or alternative wastewater disposal systems is not proposed.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

g.

<u>Discussion of Effects</u>: A paleontological resources records search prepared for the Project site commissioned through the Natural History Museum of Los Angeles County (NHMLAC) did not identify previously recorded vertebrate fossil localities or unique geological features within the Project Site or within a one-mile radius. There are two previously recorded fossil localities (LACM 7811: Masticophis and LACM 1207: Odocoileus) located within a five-mile radius of the Project site that were discovered within the same sedimentary deposits at depths that extends into the Project area. The results of the literature review and the search at the NHMLAC indicated that the Project site has surficial sediments composed of younger Quaternary Alluvium, derived as alluvial fan deposits from the San Gabriel Mountains to the north or as dune sands. These deposits typically do not contain significant vertebrate fossils, at least in the uppermost layers, but they may be underlain by older sedimentary materials at estimated depths greater than 9 feet. Therefore, MIG concluded that the proposed Project would not result in a significant impact to paleontological resources or unique geological features, and no mitigation measures are recommended.

While no paleontological resources are anticipated on the Project site, the following City Standard Conditions of Approval would be applied should unanticipated paleontological resources be uncovered during excavation and construction activities:

5.2: If any archeological or paleontological resources are found during Project grading, excavation, or construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by

a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

<u>Discussion of Effects</u>: The impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Environmental Impact Report ("EIR") for the Policy Plan (General Plan). According to the EIR, this impact would be significant and unavoidable. (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases.

The Project applicant has prepared a Greenhouse Gas Reduction Measures Screening Threshold Table for Commercial and Industrial Development, which identifies the GHG reduction measures that have been incorporated into the Project. As noted in the instructions for completing the table, the Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature. The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions

As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 103 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required and the Project GHG impact is considered less than significant. No changes or additions to TOP FEIR analysis are necessary.

<u>Mitigation Required</u>: No new mitigation measures are required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

<u>Discussion of Effects</u>: The proposed project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in accordance with regional, state and federal regulations. In addition, the proposed project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15%), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6. Therefore, the

proposed project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

As described in more detail in Section 8.a (above), the proposed Project includes enough GHG reduction features to be considered consistent with the reduction quantities anticipated in the City's Climate Action Plan. Construction and operation of the proposed Project would not result in a significant conflict with an applicable plan, policy or regulation developed to reduce GHG emissions

<u>Mitigation Required</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

<u>Discussion of Effects</u>: Construction of the proposed Project would likely involve the use and disposal of chemical agents, solvents, paints, and other hazardous materials associated with construction activities. The amount of these chemicals typically present during construction would be limited, would be in compliance with existing government regulations, and would not be considered a significant hazard.

It is possible that activities associated with operation of the proposed Project would involve transport, use or disposal of hazardous materials. As described in TOP FEIR, current federal and state regulations, City ordinances, and TOP policies would regulate the handling of hazardous substances to reduce potential releases; exposure; and risks of transporting, storing, treating, and disposing of hazardous materials and wastes. Hazardous waste transport, use, and/or disposal that would occur would be less than significant with adherence to the existing regulations.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

<u>Discussion of Effects</u>: Operation of the proposed Project may involve the transport, storage, use or disposal of hazardous materials. With existing federal, State and local regulation and oversight of hazardous materials, the risk to the public or the environment from upset and accident conditions involving the release of hazardous materials would be a less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

<u>Discussion of Effects</u>: The proposed Project is a warehouse facility, and is not anticipated to generate or emit hazardous emissions or materials as part of its regular operations. It is possible that the facility could store hazardous materials. However, as described in TOP FEIR, current federal

and state regulations, City ordinances, and TOP policies would regulate the handling of hazardous substances to reduce potential releases; exposure; and risks of transporting, storing, treating, and disposing of hazardous materials and wastes. Any hazardous waste transport, use, and/or disposal that would occur with operation of the Project would be less than significant with adherence to the existing regulations.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

<u>Discussion of Effects</u>: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

<u>Discussion of Effects</u>: The proposed Project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport (ONT). An airport influence area includes areas in which current or future airport-related safety, noise, airspace protection, or overflight factors may significantly affect land uses or necessitate restrictions on those uses. The entirety of the City of Ontario is within the ONT influence area. The Project site is located within the ONT airport influence area but outside the airport safety zones. The Project site lies outside the boundaries of the Chino Airport Influence Area.

The Project was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT. The Project is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy. Any potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

f. For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?

<u>Discussion of Effects</u>: The project site is not located within the vicinity of a private airstrip. Therefore, no impacts are anticipated.

Mitigation: None required.

g. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

<u>Discussion of Effects</u>: The proposed Project does not include any changes to surrounding

roadways. The City's Safety Element, as included within TOP, includes policies and procedures to be administered in the event of a disaster. TOP seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to, and recover from every day and disaster emergencies. In addition, the proposed Project would comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the Project will comply with all applicable City codes, any impacts would be reduced to a less than significant level.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

h. Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands?

<u>Discussion of Effects</u>: The Project site is located within an urban industrial area and does not include any changes to roadways. Additionally, according to CalFire mapping, the Project site is not located in or near a State Responsibility Area (SRA), nor is it located in or near lands identified as a Fire Hazard Severity Zone (FHSZ). The closest FHSZ is approximately three miles from the Project site. Therefore, no impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR; no significant wildland fire impact would occur.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any other water quality standards or waste discharge requirements or potential for discharge of storm water pollutants from areas of material storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing), waste handling, hazardous materials handling or storage, delivery areas or loading docks, or other outdoor work areas?

<u>Discussion of Effects</u>: The project site is served by City water and sewer service and will not affect water quality standards or waste discharge requirements. Discharge of storm water pollutants from areas of materials storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing, waste handling, hazardous materials handling or storage, delivery areas or loading docks, or other outdoor work) areas could result in a temporary increase in the amount of suspended solids, trash and debris, oil and grease, organic compounds, pesticides, nutrients, heavy metals and bacteria pathogens in surface flows during a concurrent storm event, thus resulting in surface water quality impacts. The site is required to comply with the statewide National Pollutant Discharge Elimination System (NPDES) General Industrial Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario's Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)). This would reduce any impacts to below a level of significance.

Additionally, the Project applicant would be required to submit a Water Quality Management Plan (WQMP), which would establish the site's compliance with storm water discharge and water quality management requirements. The WQMP will include site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, biotreatment and evapotranspiration. Furthermore, prior to Grading Plan approval and the issuance of a grading permit, an Erosion and Sediment Control Plan must be approved by the Engineering Department. The Erosion and Sediment Control Plan will specifically identify the Best Management Practices (BMPs) that will be implemented on the Project during construction, to

reduce the discharge of sediment and other pollutants into the City's storm drain system. Adherence to federal and state regulations, City ordinances, and TOP policies would result in a less-than-significant impact.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?

<u>Discussion of Effects</u>: No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property will be negligible. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface.

As described in the City's Urban Water Management Plan, the Chino Basin is the City's main source of water supply. Chino Basin encompasses about 235 square miles of the upper Santa Ana River watershed and lies within portions of San Bernardino, Riverside, and Los Angeles counties. The Chino Basin has approximately 5 to 7 million acre feet of water in storage, and an estimated 1 million acre-feet of additional unused storage capacity.

TOP FEIR assumed development on the Project site would be an industrial use. The proposed Project includes warehouse uses, which would generally have a lower water demand than Office/Research & Development (Office/R&D), which was the prior zoning of the Project site. Therefore, this would result in a decrease in demand on groundwater supplies compared to what was evaluated within the TOP FEIR.

While construction of the proposed Project would result in an increase in impervious surfaces, which would decrease the amount of groundwater recharge that occurs on the Project site, given the total size of the Chino Basin, this decrease in pervious surface would not be considered to interfere substantially with groundwater recharge. This potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site or volume of storm water runoff to cause environmental harm or potential for significant increases in erosion of the project site or surrounding areas?

<u>Discussion of Effects</u>: The Project site is currently undeveloped, and construction of the proposed Project would change the existing drainage pattern of the Project site. However, the Project applicant would be required to submit an Erosion and Sediment Control Plan which would be approved by the Engineering Department. The Erosion and Sediment Control Plan would specifically identify the Best Management Practices (BMPs) that would be implemented on the Project site during construction to reduce the discharge of sediment and other pollutants into the City's storm drain system. Furthermore, stormwater generated by the Project would be discharged

in compliance with the statewide NPDES Permit and San Bernardino County MS4 permit requirements. Implementation of a Storm Water Pollution Prevention Plan (SWPPP), the BMPs included in the SWPPP, and an Erosion and Sediment Control Plan would reduce any impacts to below a level of significance. No streams or streambeds are present on the site.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site or potential for significant changes in the flow velocity or volume of storm water runoff to cause environmental harm?

<u>Discussion of Effects</u>: For Building A, runoff from the east portion of the site, along with the northerly landscaped area, would be conveyed through storm drain Line A. Runoff from the west half of the site along with the parking lot to the south of the building would be conveyed through storm drain Line B. These storm drain lines would discharge into an underground infiltration chamber system located in the central portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume, as well as mitigate discharge to existing conditions for the project site. The system would have a footprint of approximately 54-feet by 160-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line A, which would convey and then discharge to the existing private storm drain line in Toyota Way.

For Building B, runoff from the area to the north of the building would be conveyed through storm drain Line C. Runoff from the area to the south of the site would be conveyed through storm drain Line D. The proposed storm drain lines would discharge into an underground infiltration chamber system located in the southwest portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume. The system would have a footprint of approximately 24-feet by 105-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line D and release in Line E, which would convey and then discharge to the existing private storm drain line in Toyota Way. On- or off-site flooding impacts would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. Create or contribute runoff water which would exceed the capacity of existing or planned storm water drainage systems or provide substantial additional sources of polluted runoff (a&b) during construction and/or post-construction activity?

<u>Discussion of Effects</u>: Please see Section 10.b.ii for a discussion of on-site storm drainage facilities. The proposed Project's underground infiltration system would be sized to capture and retain the required water quality design capture volume as well as mitigate discharge to existing conditions for the Project site.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

f. Otherwise substantially degrade water quality or potential for discharge of storm water to

affect the beneficial uses of receiving water?

<u>Discussion of Effects</u>: Activities associated with the construction period, could result in a temporary increase in the amount of suspended solids in surface flows during a concurrent storm event, thus resulting in surface water quality impacts. The site is required to comply with the statewide NPDES General Construction Permit and the City of Ontario's Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)) to minimize water pollution. Thus it is anticipated that there is no potential for discharges of stormwater during construction that will affect the beneficial uses of the receiving waters. However, with the General Construction Permit requirement and implementation of the policies in The Ontario Plan, any impacts associated with the project would be less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

g. Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map?

<u>Discussion of Effects</u>: The project site is not located within a 100-year flood hazard area as mapped on a Federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map. Therefore, no impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

h. Place within a 100-year flood hazard area, structures that would impede or redirect flood flows?

<u>Discussion of Effects</u>: As identified in the Safety Element (Exhibit S-2) of the Policy Plan (General Plan), the site lies outside of the 100-year flood hazard area. Therefore, no adverse impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

i. Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?

<u>Discussion of Effects</u>: As identified in the Safety Element (Exhibit S-2) of The Ontario Plan, the site lies outside of the 100-year flood hazard area. No levees or dams are located near the project site. Therefore, no adverse impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

j. Expose people or structures to inundation by seiche, tsunami or mudflow?

<u>Discussion of Effects</u>: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. According to FEMA mapping, the Project site is not located in a known floodplain. No

wetlands have been mapped on the Project site according to the NWI. The Project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunamizone. Additionally, the Project site is not next to a large body of water. Therefore, the Project would not have a significant risk of flood hazard, tsunami, seiche zones.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

- 11. LAND USE & PLANNING. Would the project:
 - a. Physically divide an established community?

<u>Discussion of Effects</u>: The physical division of an established community typically refers to the construction of a physical feature (such as an interstate highway or railroad tracks) or removal of a means of access (such as a local road or bridge) that would impair mobility within an existing community or between a community and outlying area. The Project site is located in an area that is currently developed with urban land uses. This Project would be of similar design and size to surrounding development, and would not change any public roadways. The proposed Project would not physically divide an established community.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Conflict with applicable land use plan, policy or regulation of agencies with jurisdiction over the project (including, but not limited to general plan, airport land use compatibility plan, specific plan, or development code) adopted for the purpose of avoiding or mitigation an environmental effect?

<u>Discussion of Effects</u>: The proposed project is consistent with The Ontario Plan and does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Conflict with any applicable habitat conservation plan or natural community conservation plan?

<u>Discussion of Effects</u>: There are no adopted habitat conservation plans in the project area. As such no conflicts or impacts are anticipated.

Mitigation: None required.

- 12. MINERAL RESOURCES. Would the project:
- a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

<u>Discussion of Effects</u>: As shown in Figure 5.11-1 of TOP FEIR, the Project site is located within an area identified as Mineral Resource Zone 3 (MRZ-3). As described in TOP FEIR, a designation of MRZ-3 indicates the significance of mineral deposits cannot be determined from available data.

The Project site is located within a mostly developed area surrounded by urban land uses

and, as noted in TOP FEIR, development in a MRZ-3 would not result in significant impacts as mineral resources of statewide or local importance are not identified in the California Geological Survey PC maps. Therefore, this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

<u>Discussion of Effects</u>: The Project site is not identified as a locally important mineral resource recovery site on a local general plan, specific plan, or other land use plan, and there are no known mineral resources on the Project site. No mineral resource impacts would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

13. NOISE. Would the project result in:

a. Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

<u>Discussion of Effects</u>: Construction Noise. As described in TOP FEIR, there are two types of short-term noise impacts could occur during construction. First, the transport of workers and movement of materials to and from the site could incrementally increase noise levels along local access roads. However, the amount of construction traffic is typically small in relation to the total daily traffic volumes on those roadway segments.

The second type of short-term noise impact is related to demolition, site preparation, grading, and/or physical construction. Construction is performed in distinct steps, each of which has its own mix of equipment, and, consequently, its own noise characteristics. However, despite the variety in the type and size of construction equipment, similarities in the dominant noise sources and patterns of operation allow construction-related noise ranges to be categorized by work phase.

Construction of individual developments associated with buildout of TOP Land Use Plan would temporally increase the ambient noise environment. However, the City of Ontario restricts the hours of construction activities to the least noise-sensitive portions of the day. According to the Municipal Code, construction activities are restricted to the weekday hours of 7:00 AM to 6:00 PM and 9:00 AM to 6:00 PM on Saturday and Sunday. However, construction activities may occur outside of these hours if the City determines that the maintenance, repair, or improvement is necessary to maintain public services or cannot feasibly be conducted during normal business hours, or if construction activities comply with the stationary source noise standards of the Municipal Code (see Table 5.12-4 of TOP FEIR). Because construction activities associated with any individual development may occur near noise-sensitive receptors and noise disturbances may occur for prolonged periods of time, construction noise impacts from buildout of the Land Use Plan are considered significant.

The Project site is identified as industrial use in TOP Land Use Plan. The industrial land use designation is generally not considered a noise sensitive land use. The Project is located in an industrial area and there are no noise-sensitive receptors in the immediate vicinity that may be disturbed by construction of the proposed Project. In addition, the following mitigation measure from TOP FEIR would be applicable:

Mitigation Measures 12-4: Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing nonessential idling of construction equipment to no more than five minutes shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.

Operational Noise. As described in TOP FEIR, the northeastern portion of the City of Ontario is characterized by industrial warehousing operations. In addition to on-site mechanical equipment, warehousing and industrial land uses generate substantial truck traffic that results in additional sources of noise on local roadways in the vicinity of industrial operations.

The City of Ontario regulates noise sources within the City through the City's Municipal Code (Title 5, Chapter 29, Noise). The City of Ontario Municipal Code has established noise standards for stationary source noise levels, as shown in Table 5.12-4 of TOP FEIR, City of Ontario Maximum Permissible Exterior Noise Levels, at various categories of land uses in the City. The City applies the Noise Control Ordinance standards to non-transportation noise sources. These standards do not gauge the compatibility of developments in the noise environment, but provide restrictions on the amount and duration of noise generated at a property, as measured at the property line of the noise receptor.

Industrial noise is less intermittent and can have moderate to high levels on a continual basis. TOP proposes 159,998,711 square feet of industrial land uses at buildout. As shown in Figure 3-6 for TOP FEIR, proposed industrial areas are centered around the Ontario International Airport (ONT) and Chino Airport. In general, new industrial areas would be buffered by business park uses or located around existing major noise sources that would mask most industrial noise (e.g., freeways, Chino Airport, ONT). The siting of new industrial developments may increase noise levels to nearby uses. This can be due to the continual presence of heavy trucks used for the pick-up and delivery of goods and supplies, or from the use of noisy equipment used in the manufacturing or machining process. While vehicle noise on public roadways is exempt from local regulation, for the purposes of the planning process, it may be regulated as a stationary-source noise while operating on private property. To regulate stationary-source noise created by industrial machinery and tools from affecting sensitive land uses, the City of Ontario requires industrial operations to limit noise to no greater than the maximum allowable noise levels as described in the Noise Ordinance.

As shown in TOP FEIR Figure 5.12-6, future noise levels from surface transportation are expected to be between 65 and 75 dBA CNEL, depending on the location within the Project site. Part of the Project site would be expected to experience a 3 to 4 dBA CNEL increase associated with buildout of TOP land use plan.

The proposed Project would be required to comply with the City's Noise Ordinance (Title 5, Chapter 29, Noise). Compliance with the noise ordinance would result in noise levels that are acceptable to the City and would result less than significant noise impacts from stationary sources.

<u>Mitigation</u>: No additional mitigation required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?

<u>Discussion of Effects</u>: The Proposed Project would involve a warehouse use, and excessive

groundborne vibration or groundborne noise levels are not anticipated with operation of the Project.

Groundborne vibration may occur as part of the construction of the proposed Project. As described in TOP FEIR, construction operations can generate varying degrees of ground vibration, depending on the construction procedures and equipment. Operation of construction equipment generates vibrations that spread through the ground and diminish with distance from the source. The effect on buildings in the vicinity of the construction site varies depending on soil type, ground strata, and receptor-building construction. The results from vibration can range from no perceptible effects at the lowest vibration levels, to low rumbling sounds and perceptible vibrations at moderate levels, and slight structural damage at the highest levels.

The Project site is located within an industrial area, and there are no noise-sensitive receptors in the immediate vicinity. As specific construction equipment use is not known at this time, the following mitigation measure included in TOP FEIR would be applicable to the Project site, and reduce the potential impact to a less-than-significant level:

Mitigation Measures 12-2: Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers, occurring near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g., drilled piles to eliminate use of vibration-intensive pile driver).

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?

<u>Discussion of Effects</u>: The project will not be a significant noise generator and will not cause a substantial permanent increase in ambient noise levels because of the limited size and scope of the project. Moreover, the proposed use will be required to operate within the noise levels permitted for commercial development, pursuant to City of Ontario Development Code. Therefore, no increases in noise levels within the vicinity of the project are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?

<u>Discussion of Effects</u>: Temporary construction activities will minimally impact ambient noise levels. All construction machinery will be maintained according to industry standards to help minimize the impacts. Normal activities associated with the project are unlikely to increase ambient noise levels.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. For a project located within the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

<u>Discussion of Effects</u>: According to the Safety Element in The Ontario Plan, the proposed site is located within the airport land use plan. However, the project is located outside of the 65CNEL noise contour. Therefore, no impacts are anticipated.

Mitigation: None required.

f. For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?

<u>Discussion of Effects</u>: The project site is not located within the vicinity of a private airstrip. Therefore, no impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

<u>Discussion of Effects</u>: This proposed Project includes construction of two warehouse buildings and associated site improvements. The Project does not include new housing or a large employment generator, which could directly induce substantial unplanned population growth. Additionally, the proposed Project does not include the expansion of infrastructure, which could indirectly cause unplanned population growth. No population growth impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere?

<u>Discussion of Effects</u>: The Project site is currently vacant and does not include any housing units. Construction of the Project would not displace substantial numbers of existing people or housing, necessitating the construction of replacement housing; no impact would occur.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?

<u>Discussion of Effects</u>: The project site is currently undeveloped. Therefore, no impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and

addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

15. PUBLIC SERVICES. Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

<u>Discussion of Effects</u>: The Ontario Fire Department provides fire service to the Project site. The Ontario Fire Department has ten fire stations, including the Ontario International Airport (ONT) fire station. These fire stations house nine 4-person paramedic engine companies, three 4-person truck companies, an 8-person ARFF station, one fire investigation supervisor, and two battalion chiefs. The closest fire station to the Project site is Ontario Fire Department Station 7, located at 4901 Vanderbilt Street, which is approximately one mile from the Project site.

In 2020, the Ontario Fire Department responded to over 20,000 calls for service, approximately 55 calls per day, ranging from medical emergencies to a traffic collisions to large commercial fires. Ontario Fire Department has 221 personnel comprised of 182 sworn firefighters and 39 professional staff members serving our community across five bureaus – Operations, Fire Prevention, Support Services/Airport Operations, EMS, and Administrative Services.

As described in TOP FEIR, firefighter staffing needs are determined by the Ontario Fire Department by the number of calls and requests for fire services within the service area. The Fire Department also reviews service contracts with the California Department of Forestry to ensure fire services in times of emergency. To ensure the provision of adequate fire protection services, the City of Ontario has established a Development Impact Fee Program to provide funding for services within the City. Fees collected from developers are placed in a fire services fund that can be expended for the acquisition or construction of new fire services facilities and for the improvement or expansion of the City's existing fire service capabilities, provided that such expenditure from the fund has been authorized by the City Council. The Project applicant would be required to pay all appropriate fire service development fees.

Additionally, as described in detail in TOP FEIR, future growth in accordance with TOP is expected to increase the demand for fire services throughout the city but especially in the New Model Colony (NMC). The Development Impact Fee and Nexus Schedule (2005) recommends that two new stations would be built in the Old Model Colony (OMC) to replace stations number 3 and 7 and that four new stations be built in the NMC. The funding needed to build these stations has been assessed and incorporated into the fee schedule and it would be adequate for the proposed development and relocation of stations. Various localized environmental impacts related to construction of new fire stations could occur; however environmental review would occur once site specific plans have been developed.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the TOP FEIR. No changes or additions to TOP FEIR analyses are necessary.

ii. Police protection?

<u>Discussion of Effects</u>: The Ontario Police Department is a full-service police agency providing a wide range of crime suppression, education, and prevention services to the community. The Ontario Police Department has three main service bureaus: the Uniform Bureau,

Investigations Bureau, and Service Bureau. Within these bureaus, the department comprises the Police Administration, Air Support Unit, Community Oriented Problem Solving unit, Special Weapons and Tactics Team, Traffic Division, Communications Division, Investigation Division, and Crime Prevention Division.

The Ontario Police Department has implemented a geographical based policing program. As part of this "Geo-Policing" program, the city has been divided into three geographical areas: West, East, and South. Each area has an assigned Lieutenant as Area Commander. The Area Commander is responsible for the delivery of police services in their area of control with an emphasis on the preservation and improvement of the quality of life, safety, and economic value of those who live and do business in the city. Each area has dedicated teams of officers and corporals, headed by police sergeants, who work day-to-day (24/7) patrol operations; traffic officers; Community Oriented Problem Solving (C.O.P.S.) officers, who work special projects; narcotics investigators; and detectives. The Project site is located within the East Area Command.

As described in TOP FEIR, buildout of TOP would result in an increase in demand for police protection services within the City. New facilities, equipment, and personnel may be necessary to maintain adequate levels of service. Development within the City would be subject to development impact fees that would pay for police services. The police services required to cover the new development and population growth for the City of Ontario would be assessed and acquired appropriately based on the needs of the City. The police services would receive adequate funding through the City's general fund to cover Project needs.

Buildout of TOP would result in an impact on the Ontario Police Department and their ability to deliver police services in a timely manner. Buildout of TOP would require the hiring of new staff and the building of new facilities. Environmental impacts would result from the construction of these facilities and each project would have to complete environmental review under CEQA. Various localized environmental impacts related to construction of new police facilities could occur; however environmental review would occur once site specific plans have been developed and potential environmental impacts would be reduced to less than significant through mitigation and compliance with existing regulations.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

iii. Schools?

<u>Discussion of Effects</u>: The proposed Project does not include residential uses and is not expected to directly increase demand for school services. The proposed Project would be required to pay applicable development impact fees to the Cucamonga School District and the Chaffey Joint Union High School District, which provide school services to the Project site and surrounding area. Currently, the Cucamonga School District collects a development fee of \$0.228 per square foot of Industrial/Warehouse/Manufacturing Use. Based on a revenue sharing agreement, the Chaffey Joint Union High School District receives approximately 31 percent of the school impact fee collected. Payment of school impact fees, as allowed by Government Code 65996, are meant to offset increased student enrollment and has been deemed by the State legislature (per Government Code Section 65995(h)) to constitute full and complete mitigation of impacts of a development project on the provision of adequate school facilities. No impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered

and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

iv. Parks?

<u>Discussion of Effects</u>: This proposed Project includes construction of two warehouse buildings and associated site improvements. The Project does not include new housing or a large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities; this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

v. Other public facilities?

<u>Discussion of Effects</u>: This proposed Project includes construction of two warehouse buildings and associated site improvements. The Project does not include new housing or a large employment generator that would cause an increase in the use of public facilities, such as libraries; this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

16. RECREATION. Would the project:

a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

<u>Discussion of Effects</u>: This proposed Project includes construction of two warehouse buildings and associated site improvements. The Project does not include new housing or a large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities; this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

<u>Discussion of Effects</u>: The Project does not include construction of recreational facilities. Additionally, the Project is not proposing any new housing or a large employment generator that would require the construction or expansion neighborhood parks or other recreational facilities; this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

17. TRANSPORTATION/TRAFFIC. Would the project:

a. Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited?

<u>Discussion of Effects:</u> The project is in an area that is mostly developed with all street improvements existing. The number of vehicle trips per day is not expected to be increased as a result of this Project. Less than significant impacts are anticipated.

<u>Mitigation:</u> No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Conflict with an applicable congestion management program, including, but not limited to, level of service standard and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?

Discussion of Effects: TOP FEIR concluded that trips generated as a result of buildout TOP Land Use Plan would cause a deficient level of service (LOS) for existing area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of TOP Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that is already projected to occur in the future. TOP FEIR Mitigation Measure 16-1 includes development of more enhanced intersections throughout the City, as identified in Table 5.16-6 of TOP FEIR, and construction of additional turn and through lanes. As further described in TOP FEIR, implementation of these improvements would result in LOS E or above at all intersections during both AM and PM peak hours. With implementation of the mitigation measure, impacts to local roadways would be less than significant. However, buildout of TOP Land Use Plan would result in additional traffic volume that would significantly cumulatively contribute to mainline freeway segment impacts. The City's development impact fees cannot be used for improvements to roadway facilities under the sole jurisdiction of the California Department of Transportation (Caltrans), such as freeway mainline segments, and the City cannot widen the freeway itself. Consequently, impacts to freeway segments within the City were identified as significant and unavoidable.

As described in more detail in Section 17.b, below, the proposed Project is forecast to generate fewer trips than the original zoning of the site, which was incorporated in TOP FEIR. The traffic impacts associated with the proposed Project would be considered consistent with and less than the traffic impacts projected and analyzed in TOP FEIR. The proposed Project would not create an increase in the number of vehicle trips, traffic volume, or congestion at intersections than has already been evaluated in TOP FEIR.

As described in TOP FEIR, the Mobility Element would introduce and implement various strategies and approaches to accommodate multiple modes of travel. The plan accounts for improvements and enhancements to roadways (for passenger cars, trucks, buses, and bicycles), rail lines (for freight and passenger rail), and trails and walkways (for bicycles and pedestrians). The strategies and approaches to improvements to public transit and nonmotorized transportation would ensure that this impact would be less than significant. Construction of the proposed Project would not impede implementation of the strategies identified in the Mobility Element, and the impact would be considered less than significant.

<u>Mitigation:</u> No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Result in a change in air traffic patterns, including either an increase in traffic levels or a

change in location that results in substantial safety risks?

<u>Discussion of Effects</u>: The project will not create a substantial safety risk or interfere with air traffic patterns at Ontario International Airport as it is under such height restrictions. No impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

<u>Discussion of Effects</u>: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines update as part of the implementation of SB 743, which requires local jurisdictions use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. As part of the implementation of SB 743, local jurisdiction were given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. TOP FEIR was certified prior to adoption of SB 743. As such, the analysis of traffic impacts within TOP FEIR is based on Level of Service (LOS) methodologies, not Vehicle Miles Travelled (VMT). As TOP FEIR relies on LOS for the analysis of the transportation impacts, this Addendum also includes a discussion of LOS.

TOP FEIR concluded that trips generated as a result of buildout of TOP Land Use Plan would cause a deficient LOS for the existing area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of TOP Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that were projected to occur in the future. TOP FEIR Mitigation Measure 16-1 includes development of more enhanced intersections throughout the City, as identified in Table 5.16-6 of the TOP FEIR, and construction of additional turn and through lanes. As further described in TOP FEIR, implementation of these improvements would result in LOS E or above at all intersections during both AM and PM peak hours. Under the City's development impact fee program, project applicants for new developments can either contribute their fair share toward traffic improvements or make the improvements as part of the project. Additionally, the City of Ontario has a Capital Improvement Program that details the implementation of regional improvements. With implementation of the mitigation measure, impacts to local roadways would be less than significant. However, buildout of TOP Land Use Plan would result in additional traffic volume that would significantly cumulatively contribute to mainline freeway seament impacts. The City's development impact fees cannot be used for improvements to roadway facilities under Caltrans' sole jurisdiction, such as freeway mainline segments, and the City cannot widen the freeway itself. Consequently, impacts to freeway segments within the City were identified as significant and unavoidable.

TOP FEIR analysis assumed that the Project site would have an Industrial land use. The Toyota/Ontario Business Park Specific Plan identified the site as having a maximum development potential of 300,000 square feet. The Specific Plan Amendment to the Toyota/Ontario Business Park Specific Plan adopted by the City Council in 2020 changed the zoning of the site from Office/Research & Development (Office/R&D) to Industrial Mixed Use. The proposed Project includes development of two warehouse buildings totaling 168,772 square feet, which includes approximately 15,000 square feet of office space and approximately 153,772 square feet of warehouse space.

To evaluate the potential change in trips associated with the Specific Plan Amendment for the Project site, Ganddini evaluated the trip generation associated with the original Office/Research and Development zoning designation, as well as the development for the Specific Plan Amendment. It should be noted that for the Specific Plan Amendment development

analysis, the Ganddini Group evaluated a development scenario that was slightly larger (approximately 4,475 square feet) than what the Project proposes. As shown in Table 5, the Proposed Project would result in a reduction in the number of trips generated on the Project site compared to the prior zoning designation.

Table 5: Trip Generation Comparison								
		Trip Generation						
		AM Peak Hour			PM Peak Hour			Daily
Zoning	Quantity	In	Out	Total	In	Out	Total	Daily
Original Zoning (Office)	300,000 SF	299	49	348	55	290	345	2,922
Proposed (Warehouse)	173,247 SF ^(A)	35	14	49	16	36	52	394
Trip Generation Comparison (Proposed – Original)		-264	-35	-299	-39	-254	-293	-2,528

Source: Ganddini Group Inc, 2019.

(A) While the proposed Project includes 168,772 square feet of floor area, the Ganddini report analyzed impacts from 173,247 square feet of floor area.

The proposed Project is forecast to generate fewer trips than the original zoning of the site. The traffic impacts associated with the proposed Project would be considered consistent with and less than the traffic impacts projected and analyzed with TOP FEIR. The proposed Project would not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections than has already been evaluated within the FEIR.

<u>Mitigation:</u> No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

<u>Discussion of Effects</u>: The Project is in an area that is mostly developed, and no alterations are proposed for adjacent intersections or arterials. The Project would not create a substantial increase in hazards due to a design feature; this potential impact would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

f. Result in inadequate emergency access?

<u>Discussion of Effects</u>: The proposed Project would not include any changes to adjacent roadways. Additionally, development of the proposed Project includes fire lanes and would provide access for all emergency vehicles. The proposed Project would not result in a significant impact related to inadequate emergency access.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

g. Result in inadequate parking capacity?

<u>Discussion of Effects</u>: The project is required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

h. Conflict with adopted policies, plans, or programs supporting alternative transportation (e.g., bus turnouts, bicycle racks)?

<u>Discussion of Effects</u>: The project does not conflict with any transportation policies, plans or programs. Therefore, no impacts are anticipated.

Mitigation: None required.

18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

<u>Discussion of Effects</u>: The results of the records research compiled from the CHRIS-SCCIC, the Scared Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (TCR) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (PRC): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. In compliance with AB 52, it is the responsibility of the Public Agency (e.g. Lead Agency) to consult with Native American tribes early in the CEQA process to allow tribal governments, lead agencies, and project proponents to discuss the appropriate level of environment review, identify and address potential adverse impacts to TCRs, and reduce the potential for delay and conflict in the environmental review process (see PRC Section 2108.3.2). Specifically, government-to-government consultation may provide "tribal knowledge" of the Project Area that can be used in identifying TCRs that cannot be obtained through other investigative means.

The Project Site has been highly disturbed by modern human activities to include agricultural use from the 1940's through the 1960's and the development and construction of the Toyota Motors North American Parts Center and supporting infrastructure that would have displaced surface and subsurface archaeological resources. Therefore, it is concluded that the proposed Project would not impact Tribal Cultural Resources or Native America artifacts relating to TCR's and as such, no mitigation measures are recommended.

<u>Mitigation</u>: No new mitigation measures required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

<u>Discussion of Effects</u>: The subject site is not listed in the California Register of Historic Resources. It is anticipated that during the application process the Lead Agency will notify the tribes of the proposed Specific Plan Amendment and will commence AB 52 Consultations as specified in the regulations. In addition, the results of the records research compiled from the CHRIS-SCCIC, the Scared Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (TCR) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (PRC): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. No impacts are anticipated through Project implementation.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?

<u>Discussion of Effects</u>: The proposed project is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. The project is required to meet the requirements of the Ontario Engineering Department regarding wastewater. No impacts are anticipated.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

<u>Discussion of Effects</u>: The proposed project is served by the City of Ontario sewer system and which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. RP-1 is not at capacity and this project will not cause RP-1 to exceed capacity. The project will therefore not require the construction of new wastewater treatment facilities, or the expansion of existing facilities. No impacts are anticipated.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?

<u>Discussion of Effects</u>: As described in TOP FEIR, the potable water network in Ontario includes 536 miles of pipeline, ranging from 2 to 42 inches in diameter. The City has four pressure zones. Locally obtained water comes from approximately 20 operating groundwater wells in Ontario and the Chino Basin Desalter Authority (CDA). The Project Site would be served by City of Ontario water system and there is a 12-inch water line available for connection in Jurupa Street.

TOP FEIR includes a discussion of the City's sewer system. The City sewer mains are primarily constructed of vitrified clay pipe ranging from 4 to 42 inches in diameter. Approximately 75 percent of the pipes are 8 inches in diameter. The City's sewers are classified into two groups: primary sewers, greater than 15 inches in diameter, and secondary sewers, 15 inches or smaller in diameter. The City has about 375 miles of gravity sewers. The City's wastewater collection system also consists of two City-owned pump stations, one privately owned/City-maintained pump station, over 7,000 feet of associated force mains, and five siphons. The Project would connect to an 8-inch sewer line off Jurupa Street.

Wastewater generated at the Project site would be treated by the Inland Empire Utilities Agency at Regional Water Recycling Plant No. 1 (RP-1). RP-1 is located in the City of Ontario and has undergone several expansions to increase the design hydraulic domestic wastewater treatment capacity to 44 million gallons per day. The plant treats an average influent wastewater flow of approximately 28 million gallons per day. The plant serves areas of Chino, Fontana, Montclair, Ontario, Rancho Cucamonga, Upland, and solids removed from RP-4, located in Rancho Cucamonga. RP-1 has treatment capacity and future development of this Project site would not cause RP-1 to exceed capacity.

The City of Ontario maintains the local stormwater drainage system, which includes regional (major) drainage facilities designed to convey peak 100-year discharge flows and secondary drainage facilities designed for peak 10-, 25-, or 100-year flows that convey locally generated flows to regional facilities. The City's stormwater is collected and diverted into various channels that empty into the Santa Ana River; a small portion is reclaimed in spreading basins for reuse as percolated groundwater. The primary direction of drainage flow in the Chino watershed is from the San Gabriel Mountains southward to the Santa Ana River, then southwest in the river.

For Building A, runoff from the east portion of the site, along with the northerly landscaped area, would be conveyed through storm drain Line A. Runoff from the west half of the site along with the parking lot to the south of the building would be conveyed through storm drain Line B. These storm drain lines would discharge into an underground infiltration chamber system located in the central portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume, as well as mitigate discharge to existing conditions for the Project site. The system would have a footprint of approximately 54-feet by 160-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line A, which would convey and then discharge to the existing private storm drain line in Toyota Way.

For Building B, runoff from the north of the building would be conveyed through storm drain Line C. Runoff from the south of the building would be conveyed through storm drain Line D. The proposed storm drain lines would discharge into an underground infiltration chamber system located in the southwest portion of the site. The underground infiltration system would be sized to capture and retain the required water quality design capture volume. The system would have a footprint of approximately 24-feet by 105-feet and consist of 60-inch diameter perforated corrugated metal pipes surrounded by gravel. The excess flow generated from higher storm events would be designed to back up in proposed Line D and release in Line E, which would convey and then discharge to the existing private storm drain line in Toyota Way.

The proposed Project would not require the construction of new water or wastewater treatment facilities, or the expansion of existing facilities. As discussed in the Energy Section above (Section 6), the Project would have less than significant impacts with regard to electric power and natural gas. In addition, the Project would not have an impact on telecommunications facilities. Construction and operation of the proposed Project would result in a less than significant impact related to utilities services.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

<u>Discussion of Effects</u>: As a water supplier, the City is required to prepare and Urban Water Management Plan (UWMP). A UWMP provides a framework for long term water supply and evaluates existing water conservation efforts. As described in the City's UWMP, purchased water and groundwater supplies are sufficient in meeting the City's water demands under all base years, including during normal, single, and multiple dry years. Sufficient water supplies are available to serve the proposed Project.

<u>Mitigation</u>: No additional mitigation required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

e. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

<u>Discussion of Effects</u>: Wastewater generated at the Project site would be treated by the Inland Empire Utilities Agency at the Regional Water Recycling Plant No. 1 (RP-1). RP-1 is located in the City of Ontario and has undergone several expansions to increase the design hydraulic domestic wastewater treatment capacity to 44 million gallons per day. The plant treats an average influent wastewater flow of approximately 28 million gallons per day. The plant serves areas of Chino, Fontana, Montclair, Ontario, Rancho Cucamonga, Upland, and solids removed from the Regional Water Recycling Plant No. 4 (RP-4), located in Rancho Cucamonga. RP-1 has treatment capacity, and future development of this Project site would not cause RP-1 to exceed capacity.

<u>Mitigation</u>: No additional mitigation required. The proposed Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analyses are necessary.

f. Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?

<u>Discussion of Effects</u>: City of Ontario serves the proposed project. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: None required.

g. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

<u>Discussion of Effects</u>: As described in TOP FEIR, increases in population in the City of Ontario would result in increases in solid waste disposal needs. Buildout of TOP would result in the generation of 2,017 tons per day of solid waste in the City. This would be 1,009 more tons per day

(368,488 more tons per year) of solid waste than in 2007. To reduce waste disposal, AB 939 requires every California city and county to divert 50 percent of its waste from landfills by the year 2000. The City of Ontario has met this waste diversion requirement through local recycling programs and participation in regional recycling programs.

As noted in TOP FEIR, the EI Sobrante landfill has a capacity of 184,930,000 tons and is expected to close in 2030. Growth in the surrounding communities that use EI Sobrante landfill will cause additional increases in waste generation. According to AB 939, jurisdictions are required to begin planning for new landfills when the jurisdiction's primary disposal site reaches its 15-year capacity. To reduce waste disposal, AB 939 also requires every California city and county to divert 50 percent of its waste from landfills by the year 2000. The City of Ontario has exceeded this requirement by diverting approximately 64 percent of waste through local recycling programs and participation in regional recycling programs. Continuation of these recycling programs would ensure compliance with AB 939. All impacts on waste disposal services would be less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

h. Comply with federal, state, and local statutes and regulations related to solid waste?

<u>Discussion of Effects</u>: The proposed Project would comply with all with federal, state, and local statues and regulations regarding solid waste; potential impacts would be considered less than significant.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

- 20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:
- a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

<u>Discussion of Effects</u>: Wildfire was not analyzed in TOP FEIR but has since been included as part of the 2019 revisions to the State CEQA Guidelines. A discussion of potential wildfire impacts is provided below.

The Project site is located within an urban industrial area and does not include any changes to roadways. Additionally, according to CalFire mapping, the Project site is not located in or near a State Responsibility Area (SRA), nor is it located in or near lands classified as a Fire Hazard Severity Zone (FHSZ). The closest FHSZ is approximately three miles from the Project site. The proposed Project would not substantially impair and emergency response or evacuation plan.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new significant impacts. No changes or additions to TOP FEIR analysis are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

<u>Discussion of Effects</u>: The Project site is not located in or near an SRA nor is it located in or near lands classified as a very high FHSZ. The closest FHSZ is approximately three miles from the Project site. This potential impact would be considered less than significant.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new significant impacts. No changes or additions to TOP FEIR analysis are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

<u>Discussion of Effects</u>: The proposed Project would include construction of two warehouse buildings and associated onsite improvements, and would not require the installation of off-site infrastructure, such as roads, fuel breaks, emergency water sources, or other utilities. The Project site is not located in or near an SRA nor is it located in or near lands classified as a very high FHSZ. The closest fire hazard severity zone is approximately three miles from the Project site. This potential impact would be considered less than significant.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new significant impacts. No changes or additions to TOP FEIR analysis are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

<u>Discussion of Effects</u>: The Project site is relatively flat and is located in an urban industrial area. The Project site is not located in or near an SRA nor is it located in or near lands classified as a very high FHSZ. The closest FHSZ is approximately three miles from the Project site. This potential impact would be considered less than significant.

<u>Mitigation</u>: No new mitigation measures are required. The proposed Project would not result in any new significant impacts. No changes or additions to TOP FEIR analysis are necessary.

21. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat or a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

<u>Discussion of Effects</u>: The results of the preceding analysis indicate that the proposed Project would have less-than-significant impacts to sensitive biological, historical, archaeological, and paleontological resources with implementation of the mitigation measures identified in TOP FEIR and the City's Standard Conditions of Approval. Impacts related to degradation of the environment would be less than significant and no additional mitigation is required.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

<u>Discussion of Effects</u>: The proposed Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals. TOP land use

plan has anticipated industrial uses on the Project site, and all applicable TOP FEIR mitigation measures and City Standard Conditions of Approval would be applicable.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

<u>Discussion of Effects</u>: Cumulative impacts resulting from the development of the Project site were included in TOP FEIR analysis. The proposed Project does not include any changes to land use plan designations and thus is generally consistent with the project analyzed in TOP FEIR. The proposed Project's individual contribution to potentially significant cumulative impacts is not considerable and no mitigation is required.

<u>Mitigation</u>: No additional mitigation is required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analysis are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

<u>Discussion of Effects</u>: As supported by the preceding environmental evaluation, the proposed Project would not result in substantial adverse effects on human beings. It has been determined through analysis within TOP FEIR and this Addendum that the proposed Project would not result in a significant substantial adverse effect on human beings.

<u>Mitigation</u>: No additional mitigation required. The proposed Project would not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP FEIR. No changes or additions to TOP FEIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) <u>Earlier Analyses Used</u>. Identify earlier analyzes used and state where they are available for review.
 - a) The Ontario Plan Final Environmental Impact Report
 - b) The Ontario Plan
 - c) City of Ontario Zoning

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

2) <u>Impacts Adequately Addressed</u>. Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal

standards.

MITIGATION MEASURES

(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

The Mitigation Measures contained in the Certified TOP Final Environmental Impact Report adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring and Reporting Program.

No additional mitigation beyond that previously imposed is required.

Additional Reference Materials

- California Department of Conservation. Farmland Mapping and Monitoring Program. San Bernardino County, Important Farmland 2016. Website: https://maps.conservation.ca.gov/dlrp/ciftimeseries/ (accessed September 19, 2021).
- California Department of Forestry and Fire Protection, 2021. Fire Hazard Severity Zones Viewer, https://egis.fire.ca.gov/FHSZ/ (accessed September 22, 2021).
- City of Ontario, 2017. Resolution No. 2017-027, A Resolution of the City Council of the City of Ontario, California, Amending the Standard Conditions of Approval for New Development Projects, and Making Findings in Support Thereof, April 18.
- City of Ontario, 2021. Completed Table 2: Greenhouse Gas Reduction Measures Screening Table for Commercial and Industrial Development, May 6.
- City of Ontario, Fire Department, 2021. https://www.ontarioca.gov/Fire (accessed September 22, 2021).
- City of Ontario, Police Department, 2021. https://www.ontarioca.gov/Police (accessed September 22, 2021).
- City of Ontario, Toyota/Ontario Business Park Specific Plan.
- Cooperative Strategies, 2020. Residential and Commercial/Industrial Development School Fee Justification Study, Cucamonga School District, February 27.
- Federal Emergency Management Agency. Flood Map Service Center. https://msc.fema.gov/portal/home (accessed September 2021).
- Ganddini Group, Inc., 2019. Toyota-Ontario Business Park Specific Plan Amendment Trip Generation Analysis, July 5.
- Gill, Mike, 2021. Completed Table 2: Greenhouse Gas Reduction Measures Screening Table for Commercial and Industrial Development, May 6.
- Huitt-Zollars, Inc., 2021. Preliminary Hydrology Report for Milliken at Jurupa Industrial, April 8.
- Inland Empire Utilities Agency, 2021. Regional Water Recycling Plan No. 1, https://www.ieua.org/facilities/regional-water-recycling-plant-no-1/ (accessed September 24, 2021).
- Mead & Hunt, Inc, 2011. LA/Ontario International Airport Land Use Compatibility Plan, April 19.
- Ontario Airport Planning, 2021. Airport Land Use Compatibility Planning Consistency Determination Report, July 6.
- Purtell, Christopher W., Senior Archaeologist, MIG, 2019. Cultural Resources Search Review for a Proposed Specific Plan Addendum for Toyota Motor Sales, USA, City of Ontario, County of San Bernardino, California, July 25.
- Stetson Engineers, Inc. City of Ontario 2020 Urban Water Management Plan, Final Draft, June.
- United States Fish and Wildlife Service National Wetlands Inventory. https://www.fws.gov/wetlands/data/Mapper.html (accessed September 21, 2021).

Exhibit A
TOP EIR Figure 1-3, Proposed Land Use Plan

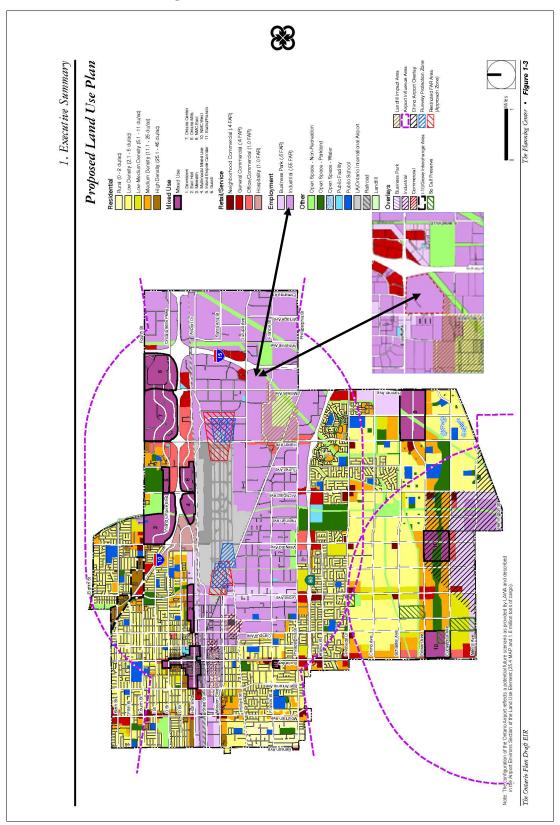
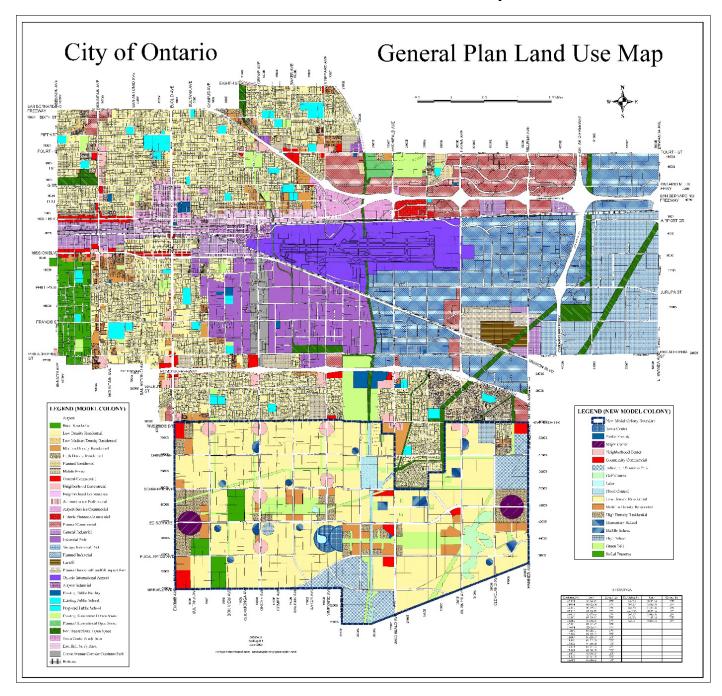


Exhibit B 1992 General Plan Land Use Map



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING TENTATIVE PARCEL MAP NO. 20147, FILE NO. PMTT21-010, TO SUBDIVIDE 95.35 ACRES OF LAND INTO THREE PARCELS TO FACILITATE THE DEVELOPMENT OF TWO INDUSTRIAL BUILDINGS AND ASSOCIATED SITE IMPROVEMENTS, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0238-121-75.

WHEREAS, MIG, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of Tentative Parcel Map No. 20147, File No. PMTT21-010, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 95.35 acres of land generally located at the southeast corner of Jurupa Street and Milliken Avenue, at 1425 South Toyota Way within the Toyota Ontario Business Park Specific Plan zoning district, and is presently partially improved with a warehouse and distribution facility; and

WHEREAS, the properties to the north are within the California Commerce Center Specific Plan zoning district and are developed with warehouse and commercial uses. The properties to the south are within the Entratter Industrial Specific Plan and the IG (General Industrial) zoning district, and are developed with warehouse uses. The land to the east is the Interstate 15 freeway. The properties to the west are within the California Commerce Center Specific Plan and are developed with light industrial and business park uses; and

WHEREAS, The Applicant is proposing to subdivide the Project site into three parcels. Parcel 1 will be 81.67 acres in size and encompass the area already developed with the Toyota warehouse and distribution facility, Toyota Way (private street), and existing landscaped areas. Parcel 2 is the northwest parcel, occupying the corner of Milliken Avenue and Jurupa Street, and will be 8.24 acres in size. Parcel 3 is located to the south of Parcel 2 and located at the northeast corner of Milliken Avenue and Toyota Way, encompassing 3.79 acres of land. Parcels 2 and 3 comprise the existing vacant land in the northwest portion of the Project site; and

WHEREAS, the lot sizes proposed by Tentative Parcel Map No. 20147 exceed the one-acre minimum lot size required by the Toyota Ontario Business Park Specific Plan; and

WHEREAS, Tentative Parcel Map No. 20147 was filed in conjunction with a Development Plan, File No. PDEV21-018, to construct two industrial warehouse buildings totaling 168,772 square feet on Parcels 2 and 3; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to analyze possible environmental impacts; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010, (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside,

and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Application and concluded said hearing on that date, voting to issue Decision No. DAB21-060, recommending that the Planning Commission approve the Application; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission adopted a Resolution approving the use of the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PMTT21-010 and PDEV21-018; and

- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and
- (5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.
- <u>SECTION 2</u>: **Subsequent or Supplemental Environmental Review Not Required.** Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The Project will have one or more significant effects not discussed in the Certified EIR; or

- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

- (1) The proposed Tentative Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments. The proposed Tentative Parcel Map is located within the Industrial land use district of the Policy Plan Land Use Map, and the Toyota Ontario Business Park Specific Plan zoning district. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to the establishment of "[a] dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses" (Goal CD1). Furthermore, the project will promote the City's policy to "take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods" (Policy CD1-1 City Identity).
- The design or improvement of the proposed Tentative Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments. The proposed Tentative Parcel Map is located within the Industrial land use district of the Policy Plan Land Use Map, and the Toyota Ontario Business Park Specific Plan zoning district. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will provide "[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct (Goal CD2). Furthermore, the project will promote the City's policy to "collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques" (Policy CD2-7 Sustainability).
- (3) The site is physically suitable for the type of development proposed. The project site meets the minimum lot area and dimensions of the Toyota Ontario Business Park Specific Plan zoning district, and is physically suitable for the type of industrial development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.
- (4) The site is physically suitable for the density/intensity of development proposed. The project site is proposed for industrial development at a floor-area ratio

ranging from 0.23 to 0.33. The project site meets the minimum lot area and dimensions of the Toyota Ontario Business Park Specific Plan zoning district, and is physically suitable for this proposed density / intensity of development.

- (5) The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat. The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.
- (6) The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems. The design of the proposed subdivision, and the industrial improvements existing or proposed on the project site, are not likely to cause serious public health problems, as the project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site.
- (7) The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision. The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

<u>SECTION 6</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 7</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 8</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 9</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon Planning Director and Secretary to the Planning Commission

Planning Commission Resolution File No. PMTT21-010 (TPM 20147) December 20, 2021 Page 9	
STATE OF CALIFORNIA COUNTY OF SAN BERNARDINO CITY OF ONTARIO)))
City of Ontario, DO HEREBY CERTIFY	ro Tempore of the Planning Commission of the / that foregoing Resolution No was duly ommission of the City of Ontario at their special the following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen
	Secretary Pro Tempore

ATTACHMENT A:

File No. PMTT21-010 (TPM 20147) Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 11/18/2021

File No: PMTT21-010

Related Files: PDEV21-018

Project Description: A Parcel Map to subdivide 95.35 acres of land into three parcels to facilitate the construction of two industrial buildings on land located at 1425 South Toyota Way, on the southeast corner of Jurupa Street and Milliken Avenue, within the Industrial Mixed use and Warehouse/Distribution land use districts of the Toyota Ontario Business Park Specific Plan; (APN: 0238-121-75) **submitted by MIG, Inc.**

Prepared By: Edmelynne V. Hutter, Senior Planner

<u>Phone</u>: 909.395.2429 (direct) Email: ehutter@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Parcel Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 <u>Subdivision Map.</u>

(a) The Final Parcel Map shall be in conformance with the approved Tentative Parcel Map on file with the City. Variations rom the approved Tentative Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

- **(b)** Tentative Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.
- (c) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.
- **2.3** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.
- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.
- **2.4** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.
- (a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.
- **(b)** The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.
 - (c) CC&Rs shall ensure reciprocal parking and access between parcels.
- (d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:
 - (i) Landscaping and irrigation systems within common areas;
- (ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines

of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02:

- (iii) Shared parking facilities and access drives; and
- (iv) Utility and drainage easements.
- **(e)** CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.
- (f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.
- (g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.5 Environmental Review.

- (a) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.
- **(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).
- (c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.
- **2.6** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.7 <u>Additional Fees</u>.

(a) Within 5 days following final application approval, the Notice of Determination ("NOD") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.



CITY OF ONTARIO

MEMORANDUM

TO:	Scott Murphy, Community Develor Rudy Zeledon, Planning Director Diane Ayala, Advanced Planning Charity Hernandez, Economic De Matt Montieth, Building Departme Raymond Lee, Engineering Departme Raymond Lee, Engineering Departme Richardson, Landscape Poennis Mejia, Municipal Utility Construction of Gabriel Gutierrez, Police Department Mike Gerken, Deputy Fire Chief/ Jay Bautista, T. E., Traffic/Transport Lorena Mejia, Airport Planning Eric Woosley, Engineering/NPDE Angela Magana, Community Impulmony Chang, IPA Department	r (Copy of memo only) g Division (Copy of memo only) evelopment ent artment Planning Division ompany ment Fire Marshal portation Manager	
FROM:	Edmelynne Hutter, Senior F	Planner	Davisias #
DATE:	October 07, 2021		Revision #
SUBJECT:	FILE #: PMTT21-010	Finance Acct#:	
	g project has been resubmitted for report to the Planning Departmen	r review. Please send one (1) copy and it by .	email one (1) copy
at southeast Warehouse/	t corner of Jurupa Street and Millike	ubdivide 13.07 acres of land into three (en Avenue, within the Office/R-D and e Toyota Business Park Specific Plan (A	
The pla	n does adequately address the de	partmental concerns at this time.	
	No comments		
	See previous report for Condition	าร	
	Report attached (1 copy and ema	ail 1 copy)	
ightharpoons	Standard Conditions of Approval	apply	
The pla	n does not adequately address the	e departmental concerns.	
	The conditions contained in the a Development Advisory Board.	attached report must be met prior to sch	eduling for

Department Signature Title Date Date



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

DEVELOPMENT PLAN OTHER	⊠ PARCE	L MAP TRAC	SES			
PF	PROJECT FILE NO. PMTT21-010					
RELA	RELATED FILE NO(S). PDEV21-018					
⊠ OR	RIGINAL F	REVISED:/_/_				
CITY PROJECT ENGINEER 8	R PHONE NO:	Michael Bhatanawin, P.E. (9	09) 395-2130			
CITY PROJECT PLANNER &	PHONE NO:	Edmelynne Hutter (909) 395-2429				
DAB MEETING DATE:		December 20, 2021				
PROJECT NAME / DESCRIPTION:		PM-20147, a Tentative Parci subdivide 13.07 acres of lar (3) parcels within the Office Warehouse/Distribution lan districts of The Toyota Bus Specific Plan	nd into three /R-D and d use			
LOCATION:		Southeast corner of Jurupa Milliken Avenue	Street and			
APPLICANT:		MIG, Inc.				
REVIEWED BY: APPROVED BY:		Earphort Ree	11/29/21			
		Raymond Lee, P.E. Assistant City Engineer Khoi Do, P.E. City Engineer	Date - 30-21 Date			

Last Revised: 11/24/2021

Date: December 20, 2021



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1.	PRIC		ck When plete
	1.01	Dedicate to the City of Ontario, the right-of-way, described below: feet on	
		Property line corner 'cut-back' required at the intersection of	
	1.02	Dedicate to the City of Ontario, the following easement(s):	
	1.03	Restrict vehicular access to the site as follows:	
\boxtimes	1.04	Vacate the following street(s) and/or easement(s):	
		A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.	
	1.05	Submit a copy of a recorded private reciprocal use agreement or easement. The agreement easement shall ensure, at a minimum, common ingress and egress and joint maintenance common access areas and drive aisles.	ent or of all
	1.06	Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicate the project and as approved by the City Attorney and the Engineering and Plan Departments, ready for recordation with the County of San Bernardino. The CC&Rs provide for, but not be limited to, common ingress and egress, joint maintenance responsifier all common access improvements, common facilities, parking areas, utilities, mediar landscaping improvements and drive approaches, in addition to maintenance requirent established in the Water Quality Management Plan (WQMP), as applicable to the project CC&Rs shall also address the maintenance and repair responsibility for primprovements/utilities (sewer, water, storm drain, recycled water, etc.) located within space/easements. In the event of any maintenance or repair of these facilities, the City only restore disturbed areas to current City Standards.	nning shall ibility n and nents . The public open
	1.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the spe boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the prodeveloper/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Discletter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclerequirements under California Civil Code Section 1102 et seq. This may include notifications i Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer disclosures. Additional information on the plume is available from the Santa Ana Regional Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004	operty osure osure in the ir and Water
	1.08	File an application for Reapportionment of Assessment, together with payment of a reapportion processing fee, for each existing assessment district listed below. Contact the Financial Ser Department at (909) 395-2124 regarding this requirement.	nment vices
		(1)	
		(2)	

Project File No. PM-20147, PMTT21-010, PDEV21-018 Project Engineer: Michael Bhatanawin, P.E. Date: December 20, 2021



\boxtimes	1.09	Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.	
\boxtimes	1.10	Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.	
\boxtimes	1.11	Provide a preliminary title report current to within 30 days.	
	1.12	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	1.13	Ontario Ranch Developments:	
		☐ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.	
		 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents). 	
		☐ 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).	
\boxtimes	1.14	Other conditions:	
		 A. Provide private easements for utilities, cross lot drainage, blanket emergency access and reciprocal access across all parcels in favor of all parcels (as needed). B. Provide a private access easement across Parcel 1/Toyota Way (private) in favor of Parcels 2 and 3 for ingress and access purposes. 	
2.	PRIO	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:	
		NERAL hits includes Grading, Building, Demolition and Encroachment)	
\boxtimes	2.01	Record Parcel Map No. 20147 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.	
\boxtimes	2.02	Submit a PDF of the recorded map to the City Engineer's office.	
	2.03	Note that the subject parcel is a recognized parcel in the City of Ontario per	
	2.04	Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.	
	2.05	Apply for a:	
		☐ Certificate of Compliance with a Record of Survey;	
		☐ Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six	

Last Revised 11/24/2021 Page 3 of 14



	months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);	
2.06	☐ Make a Dedication of Easement. Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.	
2.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
2.08	Submit a soils/geology report.	
2.09	Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:	
	State of California Department of Transportation (Caltrans) San Bernardino County Road Department (SBCRD) San Bernardino County Flood Control District (SBCFCD) Federal Emergency Management Agency (FEMA) Cucamonga Valley Water District (CVWD) for sewer/water service United States Army Corps of Engineers (USACE) California Department of Fish & Game Inland Empire Utilities Agency (IEUA) Other:	
2.10	Dedicate to the City of Ontario the right-of-way described below:	
	Property line corner 'cut-back' required at the intersection ofand	
2.11	Dedicate to the City of Ontario the following easement(s):	
2.12	Vacate the following street(s) and/or easement(s):	
	All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.	
2.13	Ontario Ranch Developments:	
	☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.	
	☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary	

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		use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.	
		☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.	
	2.14	Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.	
	2.15	The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.	
\boxtimes	2.16	Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$270,471, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.	
	2.17	Other conditions:	



L	B. PUBLIC IMPROVEMENTS	
((See attached Exhibit 'A' for plan check submittal requirements.)	

Improvement	Jurupa Street	Milliken Avenue	Street 3	Street 4
Curb and Gutter	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New;ft. from C/L Replace damaged Remove and replace
AC Pavement	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Drive Approach	New Remove and replace			
Sidewalk	New Remove and replace			
ADA Access Ramp	New Remove and replace			
Parkway	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)
Raised Landscaped Median	New Remove and replace			
Fire Hydrant	New / Upgrade Relocation			

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Sewer (see Sec. 2.C)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Water (see Sec. 2.D)	Main Service	Main Service	Main Service	Main Service
Recycled Water (see Sec. 2.E)	Main Service	Main (A) Service	Main Service	Main Service
Traffic Signal System (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Traffic Signing and Striping (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Street Light (see Sec. 2.F)	Upgrade Relocation	Upgrade Relocation	New / Upgrade Relocation	New / Upgrade Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Storm Drain (see Sec. 2G)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Fiber Optics (see Sec. 2K)	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances
Overhead Utilities	Underground Relocate	Underground Relocate	Underground Relocate	Underground Relocate
Removal of Improvements				
Other Improvements				

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listed in COA 2.17 above.

2.18

 \boxtimes

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	2.19	Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.	
	2.20	Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.	
	2.21	Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.	
\boxtimes	2.22	Other conditions:	
		 A. Pay an in-lieu fee for PCC pavement installation at the following signalized intersections: Jurupa Street & Toyota Way Jurupa Street & Milliken Avenue Toyota Way & Milliken Avenue The fee will be based on an engineer's cost estimate that will be reviewed and approved by the City. The Applicant/Developer will be responsible for their fair share which is equivalent to 25% of the total cost. 	
	C. SE	WER	
\boxtimes	2.23	A 18 inch sewer main is available for connection by this project in Milliken Ave. (Ref: Sewer plan bar code: S10006)	
	2.24	Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately feet away.	
	2.25	Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.	
\boxtimes	2.26	Other conditions:	
		A. Sewer Monitoring Manhole: Each building is to have its own monitoring manhole, on private property, immediately back of the property line.	
	D. WA	TER	
\boxtimes	2.27	A 16 inch water main is available for connection by this project in Jurupa St and Milliken Ave. (Ref: Water plan bar code: W11713, W10016)	
	2.28	Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately feet away.	
	2.29	Other conditions:	
	E. RE	CYCLED WATER	
	2.30	A 8 inch recycled water main is available for connection by this project in Jurupa St. (Ref: Recycled Water plan bar code: P10294)	
	2.31	Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.	
	2.32	Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.	

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\boxtimes	2.33	Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.	
		Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.	
\boxtimes	2.34	Other conditions:	
		A. Recycled Water Main Extension: Extend by constructing a recycled water main from the intersection of Milliken Avenue and Jurupa Street, south to serve Building A and B, specifically Building B with recycled water.	
	F. TR	AFFIC / TRANSPORTATION	
	2.35	Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 1. On-site and off-site circulation 2. Traffic level of service (LOS) at 'build-out' and future years 3. Impact at specific intersections as selected by the City Engineer	
	2.36	New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.	
\boxtimes	2.37	Other conditions:	
		 A. Replace any existing street light fixtures project frontages of Milliken Avenue and Jurupa Street with the current City approved LED equivalent fixture per the Traffic and Transportation Design Guidelines B. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309. C. Provide reciprocal access with the between the project site and all other adjacent properties. D. Design and construct proposed driveway in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway along Milliken Avenue. 	
	G. DR	AINAGE / HYDROLOGY	
	2.38	Ainch storm drain main is available to accept flows from this project in (Ref: Storm Drain plan bar code:)	
	2.39	Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.	
	2.40	An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of predevelopment peak flows, in accordance with the approved hydrology study and improvement plans.	
\boxtimes	2.41	Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.	

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2.42	Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.			
2.43	Other conditions:			
H. STO	ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM			
(NPDE	S)			
2.44	401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.			
	Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.			
2.45	Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp .			
2.46	Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.			
2.47	Other conditions:			
J. SPE	ECIAL DISTRICTS			
2.48	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.			
2.49	Other conditions:			
K. FIBER OPTIC				
2.50	A fiber optic line is available for connection by this project in (Ref: Fiber Optic plan bar code:)			
2.51	Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole located at the southeast corner of Jurupa St and Milliken Ave.			

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	2.52	Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.	
	L. IN	TEGRATED WASTE	
\boxtimes	2.53	Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:	
		https://www.ontarioca.gov/OMUC/IntegratedWaste	
\boxtimes	2.54	Other conditions:	
		 A. Organics Separation and Collection: This development shall comply with the Requirements of State Assembly Bill AB341 and AB1826, which requires organic waste to be diverted and collected separately from recycling and other refuse wastes. Additional information can be obtained at through the City website at https://www.ontarioca.gov/IntegratedWaste: B. Three 4-CY Bins Trash Enclosure: Each trash enclosure is to have three 4-cy bins with their respective gate, and a solid roof. C. Final Solid Waste Handling Plan (SWHP): Prior to approval of any building permits, revised as stated directly above and submit Final SWHP and be submitted with the Precise Grading Plan for review and approval of the Integrated Waste Department. D. Integrated Waste Management Report (IWMR): Prior to approval of the Precise Grading Plan, submit to OMUC for review and approval, an Integrated Waste Management Report (IWMR) that complies with the IWMR Requirements. 	
3.	PRIO	R TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:	
\boxtimes	3.01	Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.	
\boxtimes	3.02	Complete all requirements for recycled water usage.	
		☑ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.	
		☑ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.	
		☑ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.	
	3.03	The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.	
	3.04	Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.	
\boxtimes	3.05	Confirm payment of all Development Impact Fees (DIF) to the Building Department.	
\boxtimes	3.06	Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).	

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4.	PRIO	R TO FINAL ACCEPTANCE, APPLICANT SHALL:	
\boxtimes	4.01	Complete all Conditions of Approval listed under Sections 1-3 above.	
\boxtimes	4.02	Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.	
	4.03	The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.	
\boxtimes	4.04	Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.	

Date: December 20, 2021



EXHIBIT 'A'

ENGINEERING DEPARTMENTFirst Plan Check Submittal Checklist

Project Number: PMTT21-010, PDEV21-018, and/or Parcel Map No. 20147

The following items are required to be included with the first plan check submittal:					
1.	☑ A copy of this check list				
2.	☑ Payment of fee for Plan Checking				
3.	☑ One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.				
4.	☑ One (1) copy of project Conditions of Approval				
5.	☑ Include a PDF (electronic submittal) of each required improvement plan at every submittal.				
6.	☑ Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).				
7.	☐ Three (3) sets of Public Street improvement plan with street cross-sections				
8.	Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)				
9.	☑ Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)				
10	Four (4) sets of Public Sewer improvement plan				
11.	Five (5) sets of Public Storm Drain improvement plan				
12	Three (3) sets of Public Street Light improvement plan				
13.	☐ Three (3) sets of Signing and Striping improvement plan				
14.	☐ Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)				
15.	☑ Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)				
16.	☐ Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.				
17.	☑ Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).				
18.	☐ One (1) copy of Hydrology/Drainage study				
19.	☑ One (1) copy of Soils/Geology report				

20.

Payment for Parcel Map processing fee

Project Engineer: Michael Bhatanawin, P.E.

Date: December 20, 2021





CITY OF ONTARIO MEMORANDUM

TO: Edmelynne Hutter, Senior Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: May 24, 2021

SUBJECT: PMTT21-010 - A Parcel Map to subdivide 13.07 acres of land into three

(3) parcels located at southeast corner of Jurupa Street and Milliken Avenue, within the Office/R-D and Warehouse/Distribution land use districts of the Toyota Business Park Specific Plan (APN(s): 0238-121-75).

Related File(s): PDEV21-018.

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2019 CBC Type of Construction: Type III-B

B. Type of Roof Materials: Panelized

C. Ground Floor Area(s): Building A = 113,067 Sq. Ft.

Building B = 48,205 Sq. Ft.

D. Number of Stories: 1 with Mezzanine

E. Total Square Footage: Building A = 118,067 Sq. Ft.

Building B = 50,705 Sq. Ft.

F. 2019 CBC Occupancy Classification(s): B/S1/F1

CONDITIONS OF APPROVAL:

1.0 GENERAL

- I.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario website at www.ontarioca.gov/Fire/Prevention.

2.0 FIRE DEPARTMENT ACCESS

- ∑ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.

- ∑ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See <u>Standards #B-003</u>, <u>B-004</u> and <u>H-001</u>.

3.0 WATER SUPPLY

4.0 FIRE PROTECTION SYSTEMS

- □ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- △ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ∑ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ∑ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Homes

- that do not front street shall be provided with an address entry sign at the street. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ∑ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department.
 All Knox boxes shall be monitored for tamper by the building fire alarm system. See <u>Standard</u> #H-001 for specific requirements.
- ∑ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.



CITY OF ONTARIO MEMORANDUM

TO: Edmelynne Hutter, Senior Planner

FROM: Officer Bill Lee, Police Department

DATE: May 19, 2021

SUBJECT: PMTT21-010- A DEVELOPMENT PLAN TO CONSTRUCT TWO (2)

INDUSTRIAL BUILDINGS TOTALING 168,772 SQUARE FEET,

LOCATED AT THE SOUTHEAST CORNER OF JURUPA STREET AND

MILLIKEN AVENUE. RELATED FILE: PDEV21-018.

The "Standard Conditions of Approval" contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Bill Lee at (909) 408-1672 with any questions or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.:	PDEV21-018&	PMTT21-010		Reviewed By:		
Address: SEC Jurupa Street & Milliken Avenue			Lorena Mejia			
APN:	0238-121-75					
Existing Land Use:	Contact Info: 909-395-2276					
	D 1		- 1 1 1 (0 550 GD 1	Project Planner:		
Proposed Land Use:		lan to construct 2 industrial buildings to d Map to subdivide 13.07 acres into 3 p		Edmelynne Hutter		
Site Acreage:	13.07	Proposed Structure Heig	ght: 41 FT	Date: July 6, 2021		
ONT-IAC Projec	t Review:	n/a		CD No.: 2021-030		
Airport Influence	Area:	ONT		PALU No.: n/a		
TI	ne project i	is impacted by the follow	ring ONT ALUCP Compa	tibility Zones:		
Safe	ty	Noise Impact	Airspace Protection	Overflight Notification		
Zone 1		75+ dB CNEL	High Terrain Zone	Avigation Easement		
Zone 1A		70 - 75 dB CNEL	FAA Notification Surfaces	Dedication Recorded Overflight		
Zone 2		65 - 70 dB CNEL	Airspace Obstruction	Notification Real Estate Transaction		
Zone 3		60 - 65 dB CNEL	Surfaces Airspace Avigation	Disclosure		
Zone 4		_	Easement Area			
Zone 5			Allowable 200 FT +			
	The proje	ect is impacted by the fol	lowing Chino ALUCP Sa	fety Zones:		
Zone 1	Z	Zone 3	Zone 4 Zone	Zone 6		
Allowable Heig	jht:					
		CONSISTENCY	DETERMINATION			
This proposed Pr	oject is: OEx	xempt from the ALUCP OCON	nsistent Consistent with Cor	nditions		
	The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) For ONT.					
A: 4BI 6		Lanur	Mejre			

Airport Planner Signature:

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING A DEVELOPMENT PLAN, FILE NO. PDEV21-018, TO CONSTRUCT TWO INDUSTRIAL BUILDINGS TOTALING 168,772 SQUARE FEET AND ASSOCIATED SITE IMPROVEMENTS ON 13.07 ACRES OF LAND LOCATED AT THE SOUTHEAST CORNER OF JURUPA STREET AND MILLIKEN AVENUE, AT 1425 SOUTH TOYOTA WAY, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0238-121-75.

WHEREAS, MIG, INC. (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-018, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 13.07 acres of land generally located at the southeast corner of Jurupa Street and Milliken Avenue, at 1425 South Toyota Way within the Toyota Ontario Business Park Specific Plan, and is presently vacant; and

WHEREAS, the properties to the north are within the California Commerce Center Specific Plan zoning district and are developed with warehouse and commercial uses. The properties to the south are within the Entratter Industrial Specific Plan and the IG (General Industrial) zoning district, and are developed with warehouse uses. The land to the east is the Interstate 15 freeway. The properties to the west are within the California Commerce Center Specific Plan and are developed with light industrial and business park uses; and

WHEREAS, the Applicant is proposing to construct two industrial buildings totaling 168,772 square feet, and associated site improvements, on the vacant portion of the Project site, at the northwest corner of the Specific Plan area; and

WHEREAS, the Application was submitted in conjunction with Tentative Parcel Map No. 20147, File No. PMTT21-010, to subdivide the Project site into three parcels to facilitate the development of the proposed Project with an industrial building on each vacant parcel (Parcels 2 and 3); and

WHEREAS, proposed Building A on Parcel 2 is the larger of the two new buildings, at a total of 118,067 square feet, and proposed Building B on Parcel 3 totals 50,705 square feet; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to analyze possible environmental impacts; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010, (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San

Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Application and concluded said hearing on that date, voting to issue Decision No. DAB21-061, recommending that the Planning Commission approve the Application; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission adopted a Resolution approving the use of the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PMTT21-010 and PDEV21-018; and

- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and
- (5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.
- <u>SECTION 2</u>: **Subsequent or Supplemental Environmental Review Not Required.** Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The Project will have one or more significant effects not discussed in the Certified EIR; or

- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

- (1) The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the Toyota Ontario Business Park Specific Plan zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.
- (2) The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located. The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Toyota Ontario Business Park Specific Plan zoning district, including standards relative to the particular land use proposed (industrial), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.
- (3) The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project. The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Toyota Ontario Business Park Specific Plan are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the California Commerce Center Specific Plan.
- (4) The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development. The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Toyota Ontario Business Park Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed

(industrial). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the California Commerce Center Specific Plan.

<u>SECTION 6</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 7</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 8</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 9</u>: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PDEV21-018 December 20, 2021	
Page 9	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
I, Gwen Berendsen, Secretary Pro Ter City of Ontario, DO HEREBY CERTIFY that passed and adopted by the Planning Commis meeting held on December 20, 2021, by the fo	sion of the City of Ontario at their special
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A:

File No. PDEV21-018 Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 11/18/2021

File No: PDEV21-018

Related Files: PMTT21-010

Project Description: A Development Plan to construct two industrial buildings totaling 168,772 square feet on 13.07 acres of land located at 1425 South Toyota Way, on the southeast corner of Jurupa Street and Milliken Avenue, within the Industrial Mixed Use land use district of the Toyota Ontario Business Park Specific Plan; (APN: 0238-121-75) **submitted by MIG, Inc.**

Prepared By: Edmelynne V. Hutter, Senior Planner

<u>Phone</u>: 909.395.2429 (direct) <u>Email</u>: ehutter@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 <u>Time Limits</u>.

- (a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.
- **2.2** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 <u>Architecture Treatment and Building Design.</u>

- (a) At locations where changes in parapet wall height meet, the taller parapet must return into the building for a minimum distance of 6 FT, so that the actual thickness of the parapet wall cannot be observed or readily discerned.
 - **(b)** Roof access ladders shall be located on the inside of the building.
- **(c)** All building drainage gutters, downspouts, vents, etc., shall be completely concealed from public view or shall be architecturally compatible (decorative) with the exterior building design and color.

2.4 Landscaping.

- (a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).
- **(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.
- **(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.
- **(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.
- **2.5** <u>Walls and Fences.</u> All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

- **(b)** All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.
- **(c)** Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.
- (d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.
- **(e)** Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).
- **(f)** Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.7 <u>Outdoor Loading and Storage Areas.</u>

- (a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).
- **(b)** Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.
- **(c)** Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.
- **(d)** Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:
- (i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or
- (ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.
- **(e)** The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

Screen Wall Height	Minimum Gate Height
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet

8 feet:	8 feet		
6 feet:	6 feet		

2.8 <u>Site Lighting.</u>

- (a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.
- **(b)** Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.9 Mechanical and Rooftop Equipment.

- (a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.
- **(b)** All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.
- **2.10** <u>Security Standards</u>. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).
- **2.11** <u>Signs.</u> All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).
- **2.12** <u>Sound Attenuation</u>. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).
- **2.13** Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.
- (a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.
- **(b)** The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.
 - (c) CC&Rs shall ensure reciprocal parking and access between parcels.

- (d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:
 - (i) Landscaping and irrigation systems within common areas;
- (ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;
 - (iii) Shared parking facilities and access drives; and
 - (iv) Utility and drainage easements.
- **(e)** CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.
- (f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.
- **(g)** A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.14 Environmental Review.

- (a) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.
- **(b)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).
- **(c)** If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.
- **2.15** <u>Indemnification</u>. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding

against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.16 Additional Fees.

- (a) Within 5 days following final application approval, the Notice of Determination ("NOD") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.
- **(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.



CITY OF ONTARIO

MEMORANDUM

TO:

Scott Murphy, Community Development Director

Rudy Zeledon, Planning Director (Copy of memo only)

Diane Ayala, Advanced Planning Division (Copy of memo only)

Charity Hernandez, Economic Development

Matt Montieth, Building Department

Raymond Lee, Engineering Department

Jamie Richardson, Landscape Planning Division

Dennis Mejia, Municipal Utility Company

Gabriel Gutierrez, Police Department

Mike Gerken, Deputy Fire Chief/Fire Marshal

Jay Bautista, T. E., Traffic/Transportation Manager

Lorena Mejia, Airport Planning

Eric Woosley, Engineering/NPDES

Angela Magana, Community Improvement (Copy of memo only)

Jimmy Chang, IPA Department

Revision #2

FROM: Edmelynne Hutter, Senior Planner

DATE:

October 07, 2021

SUBJECT: FILE #: PDEV21-018

Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

PROJECT DESCRIPTION: A Development Plan to construct two (2) industrial buildings totaling 168,772 square feet on 13.07 acres of land located at southeast corner of Jurupa Street and Milliken Avenue, within the Office/R-D and Warehouse/Distribution land use districts of the Toyota Business Park Specific Plan (APN(s): 0238-121-75). Related File(s): PMTT21-010.

The plan does adequately address the departmental concerns at this time.
No comments
See previous report for Conditions
Report attached (1 copy and email 1 copy)
Standard Conditions of Approval apply
The plan does not adequately address the departmental concerns.
The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Department Signature Title Date

CITY OF ONTARIO LANDSCAPE PLANNING DIVISION

CONDITIONS OF APPROVAL

Sign Off

10/6/21

Jamie Richardson, Sr. Landscape Planner Date

303 East "B" Street, Ontario, CA 91764

Reviewer's Name: Phone: Jamie Richardson, Sr. Landscape Planner (909) 395-2615 D.A.B. File No.: Case Planner: PDEV21-018 Edmelynne Hutter Project Name and Location: Toyota Business Center – 2 Industrial Buildings S/W corner of Milliken Ave and Jurupa Street Applicant/Representative: MIG, Inc. - Pamela Steele 1650 Spruce Street, Suite 106 Riverside, CA 92614 Preliminary Plans (dated 6/12/21) meets the Standard Conditions for New Development X and have been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents. Preliminary Plans (dated) have not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE. Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

- 1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
- 2. Locate the employee break areas in an open area near the building rather than the parking lot.
- 3. Stormwater infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division prior to permit issuance. Any stormwater devices in parkway areas shall not displace street trees.
- 4. Show transformers set back 5' from paving all sides. Coordinate with landscape plans.
- 5. Show backflow devices set back 4' from paving all sides. Locate on level grade
- 6. Civil Plans: Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
- 7. Dimension all planters to have a minimum 5' wide inside dimension.
- 8. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.
- 9. Show outdoor employee break area with table or bench and shade trees on the south and west sides
- 10. Add Note to Grading Plans: Landscape areas where compaction has occurred due to grading

activities and where trees or stormwater infiltration areas are located shall be loosened by soil fracturing. For trees, a 12'x12'x18" deep area; for stormwater infiltration, the entire area shall be loosened. Add the following information on the plans: The backhoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The backhoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference, see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

- 11. Provide an arborist report and tree inventory as noted in #1.
- 12. Locate the employee break areas in an open area near the building rather than the parking lot.
- 13. Quercus agrifolia shall be planted in planter spaces greater than 8' wide; use Quercus ilex in planter 5' wide.
- 14. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as masses and duplicate masses in other locations on regular intervals.
- 15. Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans
- 16. Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
- 17. Detail irrigation dripline outside of mulched root zone.
- 18. Show minimum on-site tree sizes per the Landscape Development standards, see the Landscape Planning website. 5% 48" box, 10% 36 box, 30% 24" box, 55% 15 gallon.
- 19. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council.
- 20. Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

DEVELOPMENT PLAN OTHER	⊠ PARCE	L MAP TRAC	SES			
PROJECT FILE NO. PMTT21-010						
RELA	TED FILE NO	(S). PDEV21-018				
⊠ OR	RIGINAL F	REVISED:/_/_				
CITY PROJECT ENGINEER 8	R PHONE NO:	Michael Bhatanawin, P.E. (9	09) 395-2130			
CITY PROJECT PLANNER &	PHONE NO:	Edmelynne Hutter (909) 395-2429				
DAB MEETING DATE:		December 20, 2021				
PROJECT NAME / DESCRIPTION:		PM-20147, a Tentative Parcel Map to subdivide 13.07 acres of land into three (3) parcels within the Office/R-D and Warehouse/Distribution land use districts of The Toyota Business Park Specific Plan				
LOCATION:		Southeast corner of Jurupa Street and Milliken Avenue				
APPLICANT:		MIG, Inc.				
REVIEWED BY:		Earphort Ree	11/29/21			
APPROVED BY:		Raymond Lee, P.E. Assistant City Engineer Khoi Do, P.E. City Engineer	Date			

Last Revised: 11/24/2021

Date: December 20, 2021



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1.	PRIC	OR TO PARCEL MAP APPROVAL, APPLICANT SHALL:	Check When Complete	
	1.01	Dedicate to the City of Ontario, the right-of-way, described below:]
		feet on		
		Property line corner 'cut-back' required at the intersection ofand		
	1.02	Dedicate to the City of Ontario, the following easement(s):]
	1.03	Restrict vehicular access to the site as follows:]
\boxtimes	1.04	Vacate the following street(s) and/or easement(s):]
		A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit interference letter from affected owner/utility company.	non-	
	1.05	Submit a copy of a recorded private reciprocal use agreement or easement. The agreesement shall ensure, at a minimum, common ingress and egress and joint maintenacommon access areas and drive aisles.	reement or ance of all]
	1.06	Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applied the project and as approved by the City Attorney and the Engineering and Departments, ready for recordation with the County of San Bernardino. The CC provide for, but not be limited to, common ingress and egress, joint maintenance restor all common access improvements, common facilities, parking areas, utilities, maintenance improvements and drive approaches, in addition to maintenance requestablished in the Water Quality Management Plan (WQMP), as applicable to the precodens shall also address the maintenance and repair responsibility for improvements/utilities (sewer, water, storm drain, recycled water, etc.) located with space/easements. In the event of any maintenance or repair of these facilities, the only restore disturbed areas to current City Standards.	Planning &Rs shall ponsibility ledian and puirements loject. The lor public lithin open]
	1.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer requirements under California Civil Code Section 1102 et seq. This may include notificat Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property tr disclosures. Additional information on the plume is available from the Santa Ana Regio Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T1000	property 'Disclosure Disclosure ions in the tansfer and onal Water]
	1.08	File an application for Reapportionment of Assessment, together with payment of a reapp processing fee, for each existing assessment district listed below. Contact the Financia Department at (909) 395-2124 regarding this requirement.	ortionment []
		(1)		
		(2)		

Project File No. PM-20147, PMTT21-010, PDEV21-018 Project Engineer: Michael Bhatanawin, P.E. Date: December 20, 2021



\boxtimes	1.09	Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.			
\boxtimes	1.10	Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.			
\boxtimes	1.11	Provide a preliminary title report current to within 30 days.			
	1.12	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.			
	1.13	Ontario Ranch Developments:			
		☐ 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.			
		 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents). 			
		☐ 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).			
\boxtimes	1.14	Other conditions:			
		 A. Provide private easements for utilities, cross lot drainage, blanket emergency access and reciprocal access across all parcels in favor of all parcels (as needed). B. Provide a private access easement across Parcel 1/Toyota Way (private) in favor of Parcels 2 and 3 for ingress and access purposes. 			
2.	PRIO	R TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:			
		NERAL			
	(Perm	its includes Grading, Building, Demolition and Encroachment)			
\boxtimes	2.01	Record Parcel Map No. 20147 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.			
\boxtimes	2.02	Submit a PDF of the recorded map to the City Engineer's office.			
	2.03	Note that the subject parcel is a recognized parcel in the City of Ontario per			
	2.04	Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.			
	2.05	Apply for a:			
		☐ Certificate of Compliance with a Record of Survey;			
		☐ Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six			

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	months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);	
2.06	☐ Make a Dedication of Easement. Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.	
2.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
2.08	Submit a soils/geology report.	
2.09	Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:	
	State of California Department of Transportation (Caltrans) San Bernardino County Road Department (SBCRD) San Bernardino County Flood Control District (SBCFCD) Federal Emergency Management Agency (FEMA) Cucamonga Valley Water District (CVWD) for sewer/water service United States Army Corps of Engineers (USACE) California Department of Fish & Game Inland Empire Utilities Agency (IEUA) Other:	
2.10	Dedicate to the City of Ontario the right-of-way described below:	
	feet on	
	Property line corner 'cut-back' required at the intersection ofand	
2.11	Dedicate to the City of Ontario the following easement(s):	
2.12	Vacate the following street(s) and/or easement(s):	
	All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.	
2.13	Ontario Ranch Developments:	
	☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.	_
	☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary	

Project File No. PM-20147, PMTT21-010, PDEV21-018 Project Engineer: Michael Bhatanawin, P.E. Date: December 20, 2021



		use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.	
		☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.	
	2.14	Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.	
	2.15	The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.	
\boxtimes	2.16	Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$270,471, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.	
	2.17	Other conditions:	



B.	PUBLIC IMPROVEMENTS	
150	attached Exhibit 'A' for plan check submittal requirem	ente \

	2.17	Design and constru Code, current City s the area, if any. The (checked boxes):	standards and spec	ifications, master p	lans and the adopte	ed specific plan for
		Improvement	Jurupa Street	Milliken Avenue	Street 3	Street 4
		Curb and Gutter	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace
		AC Pavement	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions
		PCC Pavement (Truck Route Only)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
		Drive Approach	New Remove and replace			
		Sidewalk	New Remove and replace			
		ADA Access Ramp	New Remove and replace			
		Parkway	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)
		Raised Landscaped Median	New Remove and replace			
		Fire Hydrant	New / Upgrade Relocation			

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 \boxtimes

2.18



Sewer (see Sec. 2.C)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Water (see Sec. 2.D)	Main Service	Main Service	Main Service	Main Service
Recycled Water (see Sec. 2.E)	Main Service	Main (A) Service	Main Service	Main Service
Traffic Signal System (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Traffic Signing and Striping (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Street Light (see Sec. 2.F)	Upgrade Relocation	Upgrade Relocation	New / Upgrade Relocation	New / Upgrade Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Storm Drain (see Sec. 2G)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Fiber Optics (see Sec. 2K)	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances
Overhead Utilities	Underground Relocate	Underground Relocate	Underground Relocate	Underground Relocate
Removal of Improvements				
Other Improvements				
is required p B. All master p Construct a 2" aspha Milliken Ave	linal trenches over per City Std. No. 13 lanned utilities sha alt concrete (AC) gi	150' in length, a mi 06. See COA 2.18 b all be designed and rind and overlay on	nimum 10' wide, 2" elow. installed to the ulti the following stree	mate condition.

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	2.19	Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.	
	2.20	Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.	
	2.21	Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.	
\boxtimes	2.22	Other conditions:	
		 A. Pay an in-lieu fee for PCC pavement installation at the following signalized intersections: Jurupa Street & Toyota Way Jurupa Street & Milliken Avenue Toyota Way & Milliken Avenue The fee will be based on an engineer's cost estimate that will be reviewed and approved by the City. The Applicant/Developer will be responsible for their fair share which is equivalent to 25% of the total cost. 	
	C. SE	WER CONTROL OF THE CO	
\boxtimes	2.23	A 18 inch sewer main is available for connection by this project in Milliken Ave. (Ref: Sewer plan bar code: S10006)	
	2.24	Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately feet away.	
	2.25	Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.	
\boxtimes	2.26	Other conditions:	
		A. Sewer Monitoring Manhole: Each building is to have its own monitoring manhole, on private property, immediately back of the property line.	
	D. WA	TER CONTROL OF THE CO	
\boxtimes	2.27	A 16 inch water main is available for connection by this project in Jurupa St and Milliken Ave. (Ref: Water plan bar code: W11713, W10016)	
	2.28	Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately feet away.	
	2.29	Other conditions:	
	E. RE	CYCLED WATER	
\boxtimes	2.30	A 8 inch recycled water main is available for connection by this project in Jurupa St. (Ref: Recycled Water plan bar code: P10294)	
\boxtimes	2.31	Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.	
	2.32	Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.	

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\boxtimes	2.33	Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.	
		Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.	
\boxtimes	2.34	Other conditions:	
		A. Recycled Water Main Extension: Extend by constructing a recycled water main from the intersection of Milliken Avenue and Jurupa Street, south to serve Building A and B, specifically Building B with recycled water.	
	F. TR	AFFIC / TRANSPORTATION	
	2.35	Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 1. On-site and off-site circulation 2. Traffic level of service (LOS) at 'build-out' and future years 3. Impact at specific intersections as selected by the City Engineer	
	2.36	New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.	
\boxtimes	2.37	Other conditions:	
		 A. Replace any existing street light fixtures project frontages of Milliken Avenue and Jurupa Street with the current City approved LED equivalent fixture per the Traffic and Transportation Design Guidelines B. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309. C. Provide reciprocal access with the between the project site and all other adjacent properties. D. Design and construct proposed driveway in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway along Milliken Avenue. 	
	G. DR	AINAGE / HYDROLOGY	
	2.38	Ainch storm drain main is available to accept flows from this project in (Ref: Storm Drain plan bar code:)	
	2.39	Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.	
	2.40	An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of predevelopment peak flows, in accordance with the approved hydrology study and improvement plans.	
\boxtimes	2.41	Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.	

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	2.42	Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.	
	2.43	Other conditions:	
	H. STO	ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM S)	
	2.44	401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.	
\boxtimes	2.45	Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp .	
	2.46	Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.	
	2.47	Other conditions:	
	J. SPE	ECIAL DISTRICTS	
	2.48	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	2.49	Other conditions:	
	K. FIB	ER OPTIC	
	2.50	A fiber optic line is available for connection by this project in (Ref: Fiber Optic plan bar code:)	
	2.51	Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole located at the southeast corner of Jurupa St and Milliken Ave. Limits of work are generally located along the project frontages of Jurupa St and Milliken Ave.	

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	2.52	Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.	
	L. INT	TEGRATED WASTE	
\boxtimes	2.53	Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:	
		https://www.ontarioca.gov/OMUC/IntegratedWaste	
\boxtimes	2.54	Other conditions:	
		 A. Organics Separation and Collection: This development shall comply with the Requirements of State Assembly Bill AB341 and AB1826, which requires organic waste to be diverted and collected separately from recycling and other refuse wastes. Additional information can be obtained at through the City website at https://www.ontarioca.gov/IntegratedWaste: B. Three 4-CY Bins Trash Enclosure: Each trash enclosure is to have three 4-cy bins with their respective gate, and a solid roof. C. Final Solid Waste Handling Plan (SWHP): Prior to approval of any building permits, revised as stated directly above and submit Final SWHP and be submitted with the Precise Grading Plan for review and approval of the Integrated Waste Department. D. Integrated Waste Management Report (IWMR): Prior to approval of the Precise Grading Plan, submit to OMUC for review and approval, an Integrated Waste Management Report (IWMR) that complies with the IWMR Requirements. 	
3.	PRIO	R TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:	
\boxtimes	3.01	Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.	
\boxtimes	3.02	Complete all requirements for recycled water usage.	
		☑ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.	
		☑ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.	
		☑ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.	
	3.03	The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.	
	3.04	Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.	
\boxtimes	3.05	Confirm payment of all Development Impact Fees (DIF) to the Building Department.	
\boxtimes	3.06	Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).	



4.	PRIO	R TO FINAL ACCEPTANCE, APPLICANT SHALL:	
\boxtimes	4.01	Complete all Conditions of Approval listed under Sections 1-3 above.	
\boxtimes	4.02	Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.	
	4.03	The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.	
\boxtimes	4.04	Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.	

Date: December 20, 2021



EXHIBIT 'A'

ENGINEERING DEPARTMENTFirst Plan Check Submittal Checklist

Project Number: PMTT21-010, PDEV21-018, and/or Parcel Map No. 20147

The foll	owing items are required to be included with the first plan check submittal:
1.	☑ A copy of this check list
2.	☑ Payment of fee for Plan Checking
3.	☑ One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.
4.	☑ One (1) copy of project Conditions of Approval
5.	☑ Include a PDF (electronic submittal) of each required improvement plan at every submittal.
6.	☑ Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7.	☐ Three (3) sets of Public Street improvement plan with street cross-sections
8.	Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9.	☑ Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.	☐ Four (4) sets of Public Sewer improvement plan
11.	☐ Five (5) sets of Public Storm Drain improvement plan
12.	☐ Three (3) sets of Public Street Light improvement plan
13.	☐ Three (3) sets of Signing and Striping improvement plan
14.	☐ Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15.	☑ Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16.	☐ Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17.	☑ Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).
18.	☐ One (1) copy of Hydrology/Drainage study
19.	☑ One (1) copy of Soils/Geology report

20.

Payment for Parcel Map processing fee

Project Engineer: Michael Bhatanawin, P.E.

Date: December 20, 2021





CITY OF ONTARIO MEMORANDUM

TO: Edmelynne Hutter, Senior Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: May 24, 2021

SUBJECT: PMTT21-010 - A Parcel Map to subdivide 13.07 acres of land into three

(3) parcels located at southeast corner of Jurupa Street and Milliken Avenue, within the Office/R-D and Warehouse/Distribution land use districts of the Toyota Business Park Specific Plan (APN(s): 0238-121-75).

Related File(s): PDEV21-018.

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

SITE AND BUILDING FEATURES:

A. 2019 CBC Type of Construction: Type III-B

B. Type of Roof Materials: Panelized

C. Ground Floor Area(s): Building A = 113,067 Sq. Ft.

Building B = 48,205 Sq. Ft.

D. Number of Stories: 1 with Mezzanine

E. Total Square Footage: Building A = 118,067 Sq. Ft.

Building B = 50,705 Sq. Ft.

F. 2019 CBC Occupancy Classification(s): B/S1/F1

CONDITIONS OF APPROVAL:

1.0 GENERAL

- I.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario website at www.ontarioca.gov/Fire/Prevention.

2.0 FIRE DEPARTMENT ACCESS

- ≥ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.

- ∑ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See <u>Standards #B-003</u>, <u>B-004</u> and <u>H-001</u>.

3.0 WATER SUPPLY

- ⊠ 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.

4.0 FIRE PROTECTION SYSTEMS

- □ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- △ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

5.0 BUILDING CONSTRUCTION FEATURES

- ∑ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ∑ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Homes

- that do not front street shall be provided with an address entry sign at the street. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ∑ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department.
 All Knox boxes shall be monitored for tamper by the building fire alarm system. See <u>Standard #H-001</u> for specific requirements.
- ∑ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.



CITY OF ONTARIO MEMORANDUM

TO: Edmelynne Hutter, Senior Planner

FROM: Officer Bill Lee, Police Department

DATE: May 19, 2021

SUBJECT: PMTT21-010- A DEVELOPMENT PLAN TO CONSTRUCT TWO (2)

INDUSTRIAL BUILDINGS TOTALING 168,772 SQUARE FEET,

LOCATED AT THE SOUTHEAST CORNER OF JURUPA STREET AND

MILLIKEN AVENUE. RELATED FILE: PDEV21-018.

The "Standard Conditions of Approval" contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Bill Lee at (909) 408-1672 with any questions or concerns regarding these conditions.

AIRPORT LAND USE COMPATIBILITY PLANNING CONSISTENCY DETERMINATION REPORT



Project File No.:	PDEV21-018&	& PMTT21-010		Reviewed By:	
Address:	SEC Jurupa Street & Milliken Avenue			Lorena Mejia	
APN:	ΔPN: 0238 121 75				
Existing Land Vacant and Industrial Building Use:				Contact Info: 909-395-2276	
Dropoodlond	Davidanment	Plan to construct 2 industrial buildings to	otaling 169 772 SE and	Project Planner:	
Use:		el Map to subdivide 13.07 acres into 3 p		Edmelynne Hutter	
Site Acreage:	13.07	Proposed Structure Heig	ght: 41 FT	Date: July 6, 2021	
ONT-IAC Project	t Review:	n/a		CD No.: 2021-030	
Airport Influence	e Area:	ONT		PALU No.: n/a	
TI	ne project	is impacted by the follow	ing ONT ALUCP Compa	tibility Zones:	
Safe	ty	Noise Impact	Airspace Protection	Overflight Notification	
Zone 1		75+ dB CNEL	High Terrain Zone	Avigation Easement Dedication	
Zone 1A		70 - 75 dB CNEL	FAA Notification Surfaces	Recorded Overflight	
Zone 2		√ 65 - 70 dB CNEL	Airspace Obstruction	Notification	
Zone 3		√ 60 - 65 dB CNEL	Surfaces	Real Estate Transaction Disclosure	
Zone 4		▼ 00 00 00 0112E	Airspace Avigation Easement Area		
Zone 5			Allowable Height: 200 FT +		
	The proj	ect is impacted by the fol		fety Zones:	
Zone 1		Zone 2 Zone 3	Zone 4 Zone		
Allowable Heig	ght:				
		CONSISTENCY	DETERMINATION		
This proposed Project is:					
The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.					
A: 4DI 6		Lanun	Myre		

Airport Planner Signature:



PLANNING COMMISSION STAFF REPORT

December 20, 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NOS.: PZC21-001, PHP21-015, and PDEV21-030

SUBJECT: A Zone Change (File No. PZC21-001) from IL (Light Industrial) to IG (General Industrial) for 2.77 acres of land; a Certificate of Appropriateness (File No. PHP21-015) to facilitate the demolition of an existing Tier III historic eligible structure (1914 Craftsman Single-Family Residence) to accommodate a proposed industrial development; and a Development Plan (File No. PDEV21-030) to construct a 174,603-square-foot industrial building on 7.47 acres of land located on the west side of Bon View Avenue, 132 feet north of Cedar Street, within the IG (General Industrial) zoning district; (APNs: 1050-441-05, 1050-441-09, 1050-441-11, 1050-441-12, 1050-441-13, 1050-441-14, 1050-441-15, 1050-441-16, 1050-441-17, 1050-441-18, 1050-441-19, 1050-441-20, 1050-441-21, 1050-441-22, 1050-441-23, 1050-441-24, 1050-441-25, 1050-441-26, 1050-441-27, 1050-441-28, 1050-441-29, 1050-441-30, 1050-441-31, 1050-441-32, 1050-441-33, 1050-441-34, 1050-441-35, 1050-441-36, 1050-441-37, 1050-441-38, 1050-441-39, 1050-441-40, 1050-441-41, 1050-441-42, 1050-441-43, 1050-441-44, 1050-441-45, 1050-441-46, 1050-441-47, 1050-441-48, 1050-441-49, 1050-441-50, 1050-441-51, 1050-441-52, 1050-441-53, 1050-441-54, 1050-441-55, 1050-441-56, 1050-441-57, 1050-441-58, 1050-441-59, 1050-441-60, 1050-441-61, 1050-441-62, and 1050-441-73) submitted by Bon View Land 10, LLC & BV Investments 10, LLC.

PROPERTY OWNER: BON VIEW LAND 10, LLC & BV INVESTMENTS 10, LLC.

RECOMMENDED ACTION: That the Planning/Historic Preservation Commission take the following actions:

- (1) Consider and adopt a resolution recommending the City Council approve the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140);
- (2) Consider and adopt a resolution recommending the City Council approve the Zone Change, File No. PZC-21-001, pursuant to the facts and reasons contained in the staff report and attached resolution;
- (3) Consider and adopt a resolution approving the Certificate of Appropriateness, File No. PHP21-015, pursuant to the facts and reasons contained in the staff report and attached resolution; and

Case Planner:	Robert Morales, Assistant Planner
Planning Director Approval:	PZ.L
Submittal Date:	12/6/2021

Hearing Body	Date	Decision	Action
DAB	12/20/21	Approval	Recommend
PC	12/20/21		Final
CC			

(4) Consider and adopt a resolution approving the Development Plan, File No. PDEV21-030, pursuant to the facts and reasons contained in the staff report and attached resolution.

PROJECT SETTING: The Project site is comprised of 7.47 acres of land located on the west side of Bon View Avenue, approximately 132 feet north of Cedar Street, and includes a 4.70-acre common lot subdivision with 52 industrial condominium parcels and one 2.77-acre parcel of land, as depicted in adjacent Figure 1: Project Location Map. The site is presently improved with a mix of land uses and building types which includes commercial, industrial, and one single-family residence, which will all be razed to facilitate the construction of the



Figure 1: Project Location Map

proposed Project. Historically, the area was used for residential and agriculture purposes until at least 1960, when the first commercial building was constructed on the site. Throughout the 1960s to the present day, the surrounding area has been predominately developed with industrial land uses.

At the southeast corner of the Project site, fronting onto Bon View Avenue, is a one-story, single-family structure that was constructed in 1914 (est.), in the Craftsman style of architecture. The building is 1,088 square feet in size and possesses the character-defining features of the Craftsman style of architecture, including a front-facing gable roof clad in horizontal wood siding, a porch with 4 square columns that covers the full width of the home and is fully clad in stone, a stoop constructed in stone, and the main entry door flanked with aluminum Chicago-style windows. A unique and distinguishing feature of the building is the stone-clad exterior. Alterations to the building include an enclosed porch addition at the rear and the replacement of wood-framed windows with aluminum. South of the structure is a detached garage that was built prior to 1980.

West of the residence is an 'L'-shaped commercial building constructed during the 1960s, two industrial buildings built after 1966 and before 1980, and an outdoor storage yard. North of the residence are two multi-tenant industrial buildings that were built after 1986, and the remainder of the property is undeveloped. The existing surrounding land uses, zoning, and general plan land use designations are summarized in the "Surrounding Zoning & Land Uses" table located in the Technical Appendix of this report.

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

PROJECT ANALYSIS:

(1) <u>Background</u> — On September 1, 2021, the Applicant submitted File No. PDEV21-030, a Development Plan to construct a 174,603-square-foot industrial building on the 7.47-acre Project site. The subject application was submitted in conjunction with three other application types that are essential to the proposed development, which include:

- A Certificate of Appropriateness (File No. PHP21-015) to allow for the demolition of a Tier III historic resource (a 1,008 square-foot Craftsman single-family structure) that exists on the subject site;
- A Zone Change (File No. PZC21-001) on a 2.77-acre portion of the subject site, changing the zoning designation from IL (Light Industrial) to IG (General Industrial); and
- A Lot Line Adjustment (File No. E202100860) consolidating three lots that comprise
 the Project site, into a single lot. Additionally, the recorded condominium plan over
 a portion of the Project site will be quitclaimed prior to the issuance of a building
 permit to construct the proposed Project.

On December 9, 2021, the Historic Preservation Subcommittee ("HPSC") conducted a hearing to consider the Certificate of Appropriateness and voted to recommend approval of the Certificate of Appropriateness application to the Historic Preservation Commission. The Historic Preservation Commission's approval will not be final and complete until the related Zone Change (File No. PZC21-001) has been approved by the City Council.

On December 20, 2021, the Development Advisory Board (DAB) conducted a hearing to consider the Development Plan and concluded the hearing, voting to recommend that the Planning Commission approve the Application subject to conditions of approval, which are included as attachments to the Planning Commission resolution. The Planning Commission's approval will not be final and complete until the related Zone Change (File No. PZC21-001) has been approved by the City Council.

(2) Zone Change (File No. PZC21-001) — The Applicant is requesting a Zone Change (File No. PZC21-001) to change the zoning designation of the southerly portion of the Project site from IL (Light Industrial) to IG (General Industrial), as depicted in Exhibit F—Zone Change, attached. The proposed Zone Change will serve to promote the orderly development of the subject site through the consolidation of the two adjacent parcels that comprise the Project site, making the site physically suitable for the anticipated future industrial development and related allowed land uses. The adjoining parcel to the south of the Project site will remain within the IL (Light Industrial) zoning district to serve as a buffer between the residential properties to the south, across Cedar Street, and the more intense industrial uses permitted to the north.

(3) <u>Certificate of Appropriateness (File No. PHP21-015)</u> — The Applicant is requesting a Certificate of Appropriateness (File No. PHP21-015) for the demolition of a historic resource. The Development Code establishes criteria for Tier I, Tier II, or Tier III historic resources, with Tier I and II being the most historically significant. The tier system identifies those historic resources that have the highest preservation value in terms of their architectural and/or historical contribution to the City and establishes a method to evaluate the impacts of their loss in the case of major modification or demolition. Major modification or demolition should not occur to Tier I or Tier II historic resources, and preservation and/or avoidance of such historical resources in order to prevent demolition is strongly encouraged. Whereas Tier III historic resources may be modified or demolished under certain circumstances with appropriate mitigation measures in place.

Approval of a Certificate of Appropriateness for the demolition of historic resources, regardless of Tier Determination, and the new "replacement" project is required. On November 10, 2021, the Historic Preservation Subcommittee approved a Tier III Determination for the residence and determined all other buildings were not eligible for listing on the local register of historic resources (see Attachment A: Tier Determination of this report).

Preservation, rehabilitation (adaptive reuse), and relocation of the historic resource as alternatives to demolition were considered. Preserving and maintaining the historic resource on the site adjacent to the new industrial warehouse building is not suitable because the land uses are not compatible. Rehabilitation and adaptive reuse of the historic resource at its current location would render the new development as proposed infeasible. Relocation of the historic resource off-site may be feasible but may not be practical due to the type of stone cladding material used on the residence; however, a condition of approval advertising and offering the residence at no cost for the relocation off-site is required prior to issuance of a demolition permit. It is staff's belief that the demolition of the Tier III historic residence and other buildings on-site and construction of the industrial warehouse building is appropriate with conditions of approval and mitigation measures as attached.

The Planning Commission, serving as the Historic Preservation Commission, must consider and clearly establish certain findings of facts for all Certificate of Appropriateness applications. The demolition of the Tier III historic resource is appropriate for the following reasons:

a) The proposed demolition is necessary because all efforts to restore, rehabilitate, and/or relocate the resource have been exhausted. Restoration nor rehabilitation for adaptive reuse of the residential historic resource is feasible at the Project site due to the proposed development and location within the IL (Light Industrial) zoning district. Such preservation treatments would result in an incompatibility of land uses and building types; however, relocation of the historic resource may be possible under certain conditions. Prior to demolition, the Project conditions of approval require

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

advertisements be placed offering the home at no cost for those who can relocate the home off-site; and

- b) The proposed demolition is necessary because restoration/rehabilitation is not practical because the extensive alterations required would render the resource not worthy of preservation. The proposed industrial development at the Project site is consistent with existing surrounding development and land use. Continuation of the residential use, which is considered a highly sensitive land use, in conjunction with the proposed industrial development, would further intensify adverse impacts due to the incompatibility of land use. Rehabilitation of the residential building for a new industrial use is not practical because State Building Code requirements to ensure health and safety would result in extensive alterations of the residential home that has the potential to render to the resource not worthy of preservation; and
- c) The proposed demolition is necessary because failure to demolish the resource would adversely affect or detract from the character of the District. The project site is not located in a potential, proposed, or designated historic district. The surrounding properties are developed with industrial buildings and are not worthy of preservation; and
- d) The resource proposed to be demolished has been assigned a Tier III designation. The HPSC designated the single-family residence a Tier III historic resource on November 10, 2021, as included in Attachment "A" of this report.

(4) Development Plan (File No. PDEV21-030)

(a) **Site Design/Building Layout.** The proposed industrial building is rectangular in shape and will have an overall height of 45 feet, consistent with the development standards of the IG (General Industrial) zoning district. The building will include 4,500 square feet of office space and 170,173 square feet of warehouse space, having a floor area ratio ("FAR") of 0.54. Passenger vehicle parking is proposed along the north and south sides of the building. The building's main entrance and visitor parking are located adjacent to Bon View Avenue. An outdoor employee patio area is proposed at the northwest corner of the lot.

A yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging is proposed along the north side of the building and will be screened from public view by a combination of landscaping and 14-foot-high tilt-up decorative screen walls. The screen wall height may be reduced based on a sight-line analysis/wall section plan which shows that all roll-up doors, truck trailers, and any items stored outdoors will be completely screened from view from the public street. The proposed screen wall has been designed to match the architecture of the proposed building. In addition, tubular steel fencing at 8 feet in height has been proposed along the north, west, and a portion of the south property lines, transitioning to a 6-foot-high decorative

wall with pilasters which will be constructed along the south-easterly portion of the project site to provide screening for an existing single-family residence.

- (b) **Site Access and Circulation.** The Project site will have two points of access from Bon View Avenue by way of a 40-foot-wide north driveway and a 30-foot-wide south driveway, with truck yard access through a gate-controlled system. A 26-foot drive aisle provides the proposed internal circulation. Pursuant to the conditions of approval, decorative pavement will be provided at all driveway approaches, which will extend from the back of the driveway apron, to the first intersecting drive aisle or parking space.
- (c) **Parking.** The Project has provided the required off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The off-street parking calculations for each building are as follows:

Parking Requirements

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
Warehouse / Distribution	170,173	One space per 1,000 SF (0.001/SF) for portion of GFA <20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF; and One tractor-trailer parking space per 4 dock-high loading doors: 23 dock-high loading doors proposed 19 tractor-trailer parking spaces are provided		100
Office	4,500 SF	4 spaces per 1,000 SF (0.004/SF) of GFA (parking required when "general business offices" and other associated uses, exceed 10 percent of the building GFA (17,000 SF of office allowed unless additional parking is provided)))	0	0
TOTAL	174,603 SF		98	100

The number of off-street parking spaces provided for the building meets the minimum number of parking spaces required by the Development Code for warehouse/distribution facilities. In addition, the City's off-street parking and loading standards require the site to provide a minimum of one tractor-trailer parking space for every four dock-high loading spaces. Therefore, the number of tractor-trailer parking spaces provided for the buildings meets the minimum number required.

(d) **Architecture.** The proposed building will be of concrete-tilt-up construction with enhanced features such as smooth painted concrete, metal siding, inset reveals, storefront windows, anodized clear aluminum mullions, and steel canopies throughout the façade of the building as depicted in Exhibit D—Proposed Elevations, attached. Staff

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

believes the proposed Project illustrates the type of high-quality architecture promoted by the Development Code, which is exemplified through the use of:

- Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas; and
- Articulation in the building parapet/roofline, which serves to accentuate the building's entries and breaks up large expanses of building wall; and
- A mix of exterior materials, finishes, and fixtures; and
- Incorporation of base and top treatments defined by changes in color, materials, and recessed wall areas; and
- The building was designed to ensure that its massing and proportion, along with its colors and architectural detailing, are consistent on all four building elevations.
- (e) Landscaping. The Project provides landscaping along Bon View Avenue and around the project perimeter. The Development Code requires a minimum of 10 percent landscape coverage, which the Project exceeds at 10.14 percent. The proposed on-site and off-site landscape improvements will assist in creating a walkable, safe area for pedestrians to access the project site. The landscape plan incorporates a combination of 48-inch and 24-inch box trees along Bon View Avenue, which includes a mix of Western Toyon, Chinese Flame Tree, Eastern Red Bud, Chinese Pistache, Yew Pine, California Sycamore, and Brisbane Box; in addition, a mix of 15-gallon and 24-inch box accent and shade trees will be provided throughout the project site that includes Island Oak. A variety of shrubs and groundcovers are also being provided, which are low water usage or drought-tolerant (see Exhibit E—Landscape Plan, attached).
- (f) **Health Risk Assessment.** The Project site is within 1,000 feet of existing residential land uses located within Industrial zoning and The Ontario Plan ("TOP") Policy Plan land use designations. As such, the Applicant was required to prepare a Health Risk Assessment ("HRA") to determine whether the proposed Project would pose a health risk to the existing housing units in the vicinity of the project site. The HRA analyzed the cancer burden estimates as well as the Project operational Toxic Air Contaminants ("TACs") impact from Diesel Particulate Matter ("DPM") emissions. Both analyses concluded that these factors would be less than significant; therefore, no mitigation is required for the Project beyond that which was previously analyzed in the TOP Environmental Impact Report (State Clearinghouse No. 2008101140), as certified by the Ontario City Council on January 27, 2010.
- (g) **Utilities (drainage, sewer).** Public utilities (water and sewer) are available to serve the Project. The Applicant submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with stormwater discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration of biotreatment and evapotranspiration. The PWQMP proposes the use of an underground stormwater chamber system for the Project. Any overflow drainage will

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

be conveyed to the 48-inch storm drain located within Bon View Avenue via an 18-inch storm service lateral.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals, and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of TOP. More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) <u>Vision</u>.

Distinctive Development:

- Commercial and Residential Development
- > Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- <u>Goal G1</u>: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
- ➤ <u>G1-2 Long-term Benefit</u>. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

(4) <u>Policy Plan (General Plan)</u>.

Land Use Element:

- ➤ <u>LU1-6 Complete Community</u>. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
 - Goal LU2: Compatibility between a wide range of uses.

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

Community Economics Element:

➤ <u>CE2-1 Development Projects</u>. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- ➤ <u>CE2-2 Development Review</u>. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
- ➤ <u>CE2-4 Protection of Investment</u>. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- ➤ <u>CE2-5 Private Maintenance</u>. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
- > <u>\$1-1</u> Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- ➤ <u>CD1-3 Neighborhood Improvement</u>. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
- > <u>CD2-1 Quality Architecture</u>. We encourage all development projects to convey visual interest and character through:
- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

• Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

- ➤ <u>CD2-7 Sustainability</u>. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- ➤ <u>CD2-8 Safe Design</u>. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- ➤ <u>CD2-9 Landscape Design</u>. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- ➤ <u>CD2-10 Surface Parking Areas</u>. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban runoff capture and infiltration, and pedestrian paths to guide users through the parking field.
- ➤ <u>CD2-11 Entry Statements</u>. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- ➤ <u>CD2-12 Site and Building Signage</u>. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.
- ➤ <u>CD2-13 Entitlement Process</u>. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.
- ➤ <u>CD3-3 Building Entrances</u>. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.
- ➤ <u>CD3-5 Paving</u>. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

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➤ <u>CD3-6 Landscaping</u>. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- <u>Goal CD5</u>: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
- ➤ <u>CD5-1 Maintenance of Buildings and Property</u>. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of TOP, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AlrPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (ALUCP), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The related Applications establish a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an Initial Study/Addendum has been prepared to determine possible environmental impacts. Although the proposed project could have a significant effect on the environment, because all potentially significant effects have been analyzed adequately in an earlier Certified EIR and have been avoided or mitigated pursuant to that earlier Certified EIR, including revisions or mitigation measures that are imposed on the proposed project, nothing further is required. The Project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Environmental Impact Report, are a condition of project approval and are incorporated in the Initial Study/Addendum.

CONDITIONS OF APPROVAL: See attached department reports.

File Nos.: PZC21-001, PHP21-015, and PDEV21-030

December 20, 2021

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

onconding Louing and Land Osc.				
	Existing Land Use	General Plan Designation	Zoning Designation	
Site	Single-Family Residential /Industrial/ Office and Self- storage	Industrial (0.55 FAR)	IL (Light Industrial) IG (General Industrial)	
North	Industrial	Industrial (0.55 FAR)	IG (General Industrial)	
South	Single-Family Residential/ Storage Yard	Industrial (0.55 FAR)	IL (Light Industrial)	
East	Industrial	Industrial (0.55 FAR)	IL (Light Industrial)	
West	Industrial/ Storage Yard	Industrial (0.55 FAR)	IL (Light Industrial) and IG (General Industrial)	

General Site & Building Statistics

ocheral one a boliamy oranshes				
Item	Proposed	Min./Max. Standard	Meets Y/N	
Project Area:	7.47 AC	N/A	Υ	
Lot/Parcel Size:	325, 285 SF	1 AC (Min.)	Y	
Building Area:	174,603 SF	N/A	Υ	
Floor Area Ratio:	0.54	0.55 (Max.)	Y	
Building Height:	45 FT	55 FT (Max.)	Υ	

Off-Street Parking:

Type of Use	Building Area	Parking Ratio	Spaces Required	Spaces Provided
		One space per 1,000 SF (0.001/SF) for portion of GFA <20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF; and		
Warehouse / Distribution	170,173	One tractor-trailer parking space per 4 dock-high loading doors:	98	100
		 23 dock-high loading doors proposed 19 tractor-trailer parking spaces are provided 		
Office	4,500 SF	4 spaces per 1,000 SF (0.004/SF) of GFA (parking required when "general business offices" and other associated uses, exceed 10 percent of the building GFA (4,000 SF of office allowed unless additional parking is provided)))	0	0

Exhibit A—EXISTING SITE

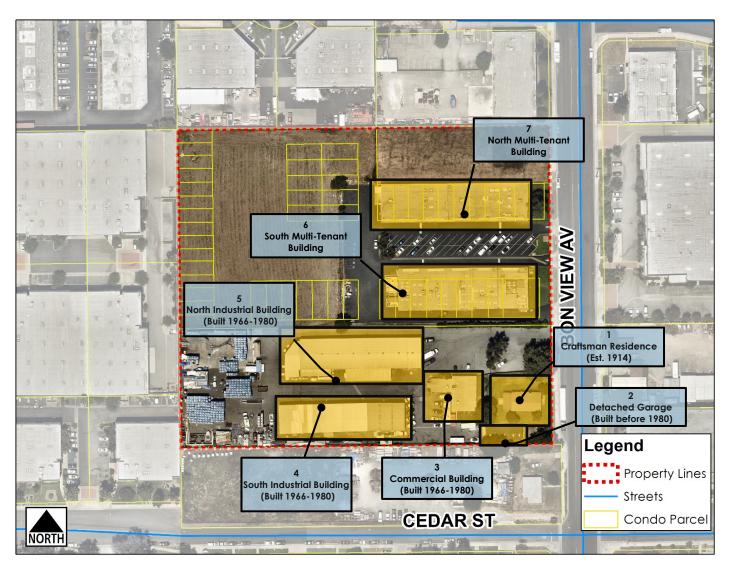


Exhibit B—SITE PHOTOS



Primary Façade – East Elevation



North Elevation



South Elevation



West Elevation



Detached Garage (Built before 1980)



Commercial Building (Built 1966-1980)



Southern Industrial Building (Built 1966-1980)



South Multi-Tenant Building



Northern Industrial Building (Built 1966-1980)



North Multi-Tenant Building

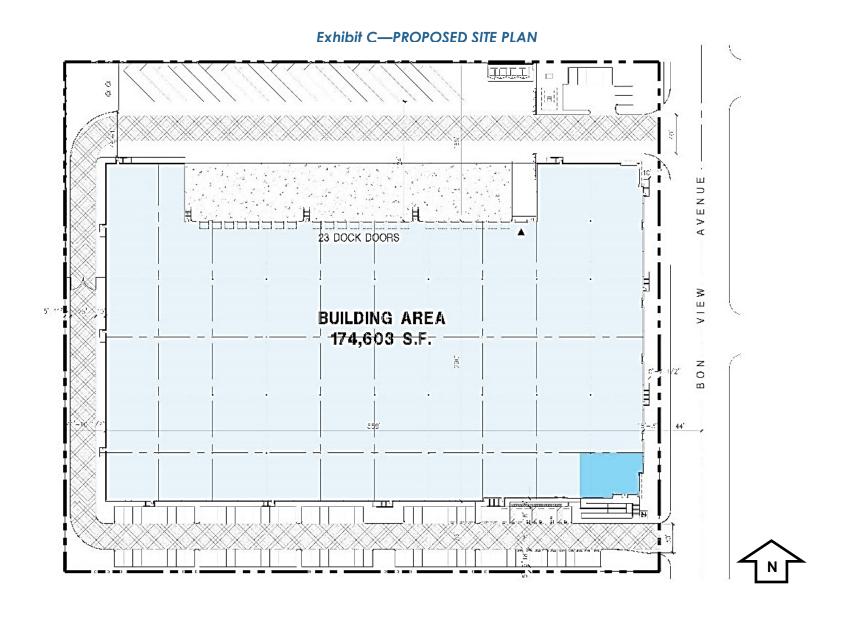


Exhibit D—PROPOSED ELEVATIONS



Exhibit E—LANDSCAPE PLAN

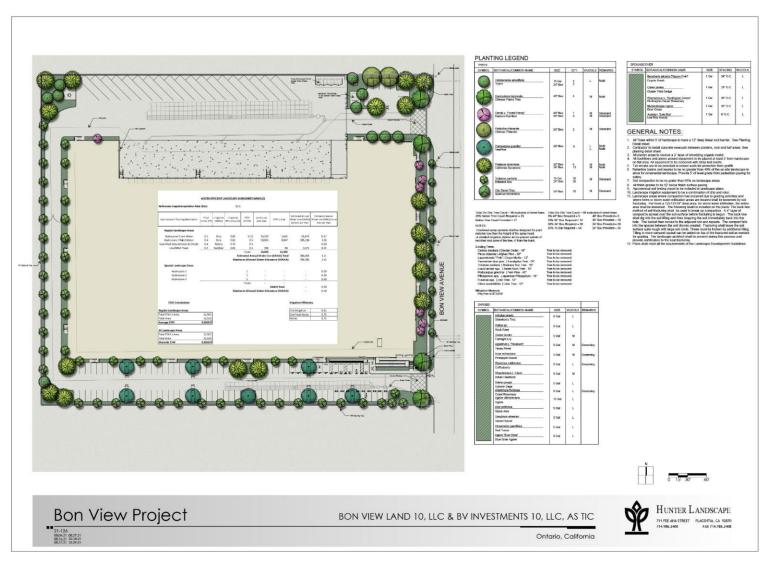
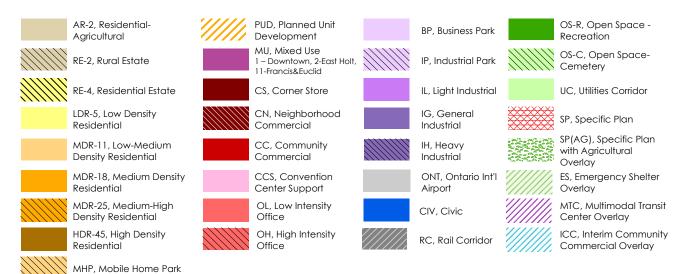
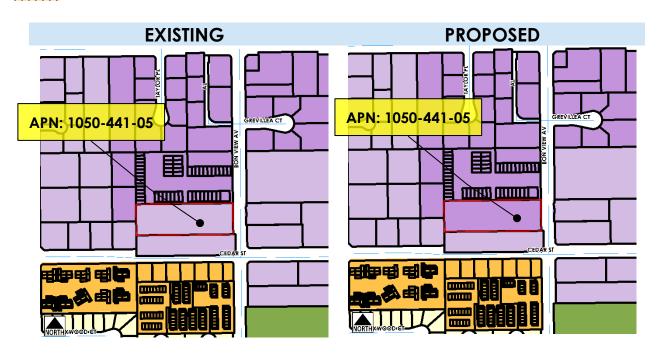


EXHIBIT F—ZONE CHANGE





TOP: Industrial (0.55 FAR) **Zoning:** IL (Light Industrial)

Parcels: (1 Property)

1050-441-05

Industrial (0.55 FAR)

IG (General Industrial)

Planning Commission Staff Report File Nos.: PZC21-001, PHP21-015, and PDEV21-030 December 20, 2021

Attachment A: TIER DETERMINATION

(Document follows this page)

HISTORIC PRESERVATION SUBCOMMITTEE/COMMISSION



TIER DETERMINATION

Date: November 10, 2021

Location: 1948 South Bon View Avenue

Historic Name: None

APN: 1050-441-05

Description:





Decision Date: November 10, 2021

Related Files: PHP21-018

Decision Making Body: HPSC

Tier Determination: III

Current Historic Status: Eligible

The 1,008 square foot residence was constructed in 1914 (est.) in the Craftsman architectural style. The residence is located at the eastern end of the parcel, has a rectangular footprint, a medium-pitched front-facing gabled roof and is clad in stone on all façades. The primary façade of the residence is symmetrical with thin horizontal wood clapboard and a square louver vent in the front-facing gable and triangular knee braces under the eaves. The primary façade is defined by its full-façade porch with four square columns clad in stone that extend from the roofline to the ground level. The primary façade is inset below the primary roofline. The porch is enclosed with an approximately 2-foot-tall wall clad in stone. A stoop constructed of stone, extends from the center of the porch with concrete planters set on top of the end caps. The door is flanked by aluminum Chicago-style windows with sliding sides and a central fixed light. Both windows also have bold concrete headers and wood.

The southern façade has minimal eave overhang. The façade has three single-hung aluminum windows with wood surrounds and concrete headers. The fourth window near the center of the façade has been boarded.

The northern façade has minimal eave overhang and window types including single-hung, fixed, and Chicago-style aluminum. An end-wall chimney constructed with stone is located near the eastern end of the façade.

The western (rear) façade has an enclosed porch addition. There is thin horizontal wood clapboard and a square louver vent in the gable





and three triangular knee braces under the eave.



TIER DETERMINATION

Tier I – Properties which should not be demolished or significantly altered. These properties are the most significant historical or cultural properties and must meet any of the following:

 A property listed on the City's List of Eligible Historical Resources and meets at least 1 of the architectural category and 3 criteria in the history category as listed below;
 A contributing structure in a district where the district meets 1 of the criterion in the architecture category and 3 criterion in the history category.

 Tier II – Properties where demolition should be avoided. These properties must meet any of the following:

Any property listed or determined eligible for listing in the National Register of Historic Places; or

Page 2 of 4 Robert Morales, Assistant Planner

TIER DETERMINATION CONT. Location: 1948 South Bon View Avenue



		Any property listed or determined eligible for listing in the California Register of Historic			
		Resources; or A property listed on the City's List of Eligible Historical Resources and meets at least 2 of			
		the criteria in either the architecture or history categories; or A contributing structure in a Eligible Historic District where the district meets at least 2 of the criteria in either architecture or history categories.			
\boxtimes		I – Properties where demolition should be avoided where possible but may be appropriate certain circumstances. These properties must be one of the following:			
		Designated Historic Landmarks, or Contributing structures in a Designated Historic District, or Eligible Historical Resources as defined in Section 7.01.010.			
TIER (CRITER	RIA			
Archit	tecture	(Check all that apply)			
	examp	ructure is (or the district contains resources which are) a prototype of, or one of the finest bles of a period, style, architectural movement, or construction in the City or a particular of architecture or building type.			
	The structure is (or the district contains resources which are) the first, last, only, or one of the finest examples, notable works, or the best surviving work by an architect or designer or major importance to the City, state or nation.				
Explanation:					
The Craftsman style residence is a fine example of the early Craftsman style of architecture which is evidenced by the survival of the building's character-defining features, such as the wide-open eaves and exposed rafters, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone makes its construction unique within the City. The residence retains its location, design, materials, workmanship, setting, feeling, and association therefore the integrity is high.					
	s its cor	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone nstruction unique within the City. The residence retains its location, design, materials,			
vorkm	s its cor nanship,	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone nstruction unique within the City. The residence retains its location, design, materials,			
vorkm	its connanship, ry (Checon	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone nstruction unique within the City. The residence retains its location, design, materials, setting, feeling, and association therefore the integrity is high.			
vorkm	its connanship, (Checonnanship) It is the City, so	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone instruction unique within the City. The residence retains its location, design, materials, setting, feeling, and association therefore the integrity is high. ck all that apply) e location of an historic event(s) that have had a significant contribution to the history of the			
vorkm Histor	s its connanship, ry (Checon It is the City, so or scient It is idea.	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone instruction unique within the City. The residence retains its location, design, materials, setting, feeling, and association therefore the integrity is high. ck all that apply) e location of an historic event(s) that have had a significant contribution to the history of the tate or nation. sociated with a business, company, or individual that has made a significant, cultural, social,			
vorkm Histor	s its connanship, ry (Checon lt is the City, so or scient lt is ide City, so lt emb	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone instruction unique within the City. The residence retains its location, design, materials, setting, feeling, and association therefore the integrity is high. ck all that apply) e location of an historic event(s) that have had a significant contribution to the history of the tate or nation. sociated with a business, company, or individual that has made a significant, cultural, social, entific contribution to the City, state, or nation. entified with a person(s) who has exerted a major influence on the heritage or history of the			
vorkm Histor	s its connanship, ry (Checon lt is the City, so or scient lt is ide City, so lt embershill embershill lt embershill embershi	rs, large gable style dormer, and wood framed hung, fixed, and its cladding of local stone instruction unique within the City. The residence retains its location, design, materials, setting, feeling, and association therefore the integrity is high. ck all that apply) e location of an historic event(s) that have had a significant contribution to the history of the tate or nation. sociated with a business, company, or individual that has made a significant, cultural, social, entific contribution to the City, state, or nation. entified with a person(s) who has exerted a major influence on the heritage or history of the tate, or nation. sociates the ideals or principles of the "Model Colony" or furthers the ideals or principals			

Page 3 of 4 Robert Morales, Assistant Planner



		The Guasti Winery or the Wine Industry
		The Dairy Preserve, or the Dairy Industry
		The Citrus Context, or the Citrus Industry
	It is re	lated with a business, company or individual significant in the agricultural history of the City.
Explar	nation:.	

Research information on property was limited due to location of property and temporary closure of public offices. As such, no information was found to suggest that the identified previous owners or residents were historic personages, or that any other individuals of historic significance were associated with the property.

Sources include Sapphos Environmental, Inc. 2021 Historic Resource Assessment

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE THE USE OF AN ADDENDUM TO THE ONTARIO PLAN CERTIFIED ENVIRONMENTAL IMPACT REPORT (STATE CLEARINGHOUSE NO. 2008101140), FOR WHICH AN INITIAL STUDY WAS PREPARED, ALL IN ACCORDANCE WITH THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NOS. PZC21-001, PHP21-015, AND PDEV21-030.

WHEREAS, BON VIEW LAND 10, LLC & BV INVESTMENTS 10, LLC., (hereinafter referred to as "Applicant") has filed Applications for the approval of a Zone Change, File No. PZC21-001, Certificate of Appropriateness, File No. PHP21-015, and a Development Plan, File No. PDEV21-030, which consists of: (1) a Zone Change (File No. PZC21-001) from IL (Light Industrial) to IG (General Industrial) on 2.77 acres of land; (2) a Certificate of Appropriateness (File No. PHP21-015) to facilitate the demolition of an existing Tier III historic eligible structure (1914 Craftsman Single-Family Residence) to accommodate a proposed industrial development; and (3) a Development Plan (File No. PDEV21-030) to construct a 174,603-square-foot industrial building on 7.47 acres of land located on the west side of Bon View Avenue, 132 feet north of Cedar Street, in the City of Ontario, California (hereinafter referred to as "Application" or "Project"); and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the City Council on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, prior to the adoption of this Resolution, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an addendum to The Ontario Plan Certified Environmental Impact Report (State Clearinghouse No. 2008101140) for File Nos. PZC21-001, PHP21-015, and PDEV21-030 (hereinafter referred to as "EIR Addendum"), all in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment and identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to California Environmental Quality Act ("CEQA") Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR

if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental Environmental Impact Report would occur from the Project, and that preparation of an addendum to the Environmental Impact Report was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the Zone Change (File No. PZC21-001) and the Planning/Historic Preservation Commission is the decision-making authority for the Certificate of Appropriateness (File No. PHP21-015) and Development Plan (File No. PDEV21-030) applications; and

WHEREAS, the Planning/Historic Preservation Commission's actions on the Certificate of Appropriateness (File No. PHP21-015) and Development Plan (File No. PDEV21-030) applications will not be final and complete until the Zone Change (File No. PZC21-001) has been approved by the City Council; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental Environmental Impact Report have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: **Environmental Determination and Findings.** As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and
- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and
- (5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.
- <u>SECTION 2</u>: **Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- SECTION 3: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby recommend that the City Council approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.
- <u>SECTION 4</u>: *Indemnification.* The Applicant shall agree to defend, indemnify, and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.
- <u>SECTION 5</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.
- <u>SECTION 6</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File Nos. PZC21-001, PHP21-015, and PE December 20, 2021 Page 6	DEV21-030
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
City of Ontario, DO HEREBY CERTIFY t	Tempore of the Planning Commission of the hat foregoing Resolution No, was duly mission of the City of Ontario at their special e following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

Planning Commission Resolution File Nos. PZC21-001, PHP21-015, and PDEV21-030 December 20, 2021 Page 7

Attachment A—EIR Addendum

To view this Attachment Go to the City of Ontario Planning Department Website at
Ontarioca.gov/Planning

(The document was too large to be included in the Agenda Packet.)

RESOLUTION NO.

A RESOLUTION OF THE HISTORIC PRESERVATION COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PHP21-015, A CERTIFICATE OF APPROPRIATENESS TO DEMOLISH A TIER III HISTORIC RESOURCE (A 1,008 SQUARE FOOT CRAFTSMAN SINGLE-FAMILY RESIDENCE) TO ALLOW FOR CONSTRUCTION OF ONE 174.603-SQUARE-FOOT INDUSTRIAL BUILDING ON 7.47 ACRES OF LAND LOCATED ON THE WEST SIDE OF BON VIEW AVENUE, 132 FEET NORTH OF CEDAR STREET, WITHIN THE IG (GENERAL INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF (APNS: 1050-441-05, 1050-441-09, 1050-441-11, 1050-441-12, 1050-441-13, 1050-441-14, 1050-441-15, 1050-441-16, 1050-441-17, 1050-441-18, 1050-441-19, 1050-441-20, 1050-441-21, 1050-441-22, 1050-441-23, 1050-441-24, 1050-441-25, 1050-441-26, 1050-441-27, 1050-441-28, 1050-441-29, 1050-441-30, 1050-441-31, 1050-441-32, 1050-441-33, 1050-441-34, 1050-441-35, 1050-441-36, 1050-441-37, 1050-441-38, 1050-441-39, 1050-441-40, 1050-441-41, 1050-441-42, 1050-441-43, 1050-441-44, 1050-441-45, 1050-441-46, 1050-441-47, 1050-441-48, 1050-441-49, 1050-441-50, 1050-441-51, 1050-441-52, 1050-441-53, 1050-441-54, 1050-441-55, 1050-441-56, 1050-441-57, 1050-441-58, 1050-441-59, 1050-441-60, 1050-441-61, 1050-441-62, and 1050-441-73).

WHEREAS, Bon View Land 10, LLC & BV Investments 10, LLC. (hereinafter referred to as "Applicant") has filed an application for the approval of a Certificate of Appropriateness, File No. PHP21-015, as described in the title of this Resolution (hereinafter referred to as "Project"); and

WHEREAS, the City's character and history are reflected in its cultural, historical, and architectural heritage, with an emphasis on the "Model Colony" as declared by an act of the Congress of the United States and presented at the St. Louis World's Fair in 1904; and

WHEREAS, the City's historical foundations should be preserved as living parts of community life and development in order to foster an understanding of the City's past, so that future generations may have a genuine opportunity to appreciate, enjoy, and understand Ontario's rich heritage; and

WHEREAS, the Community Development and the Aesthetic, Cultural, Open Space, and Recreational Resources Elements of the Policy Plan component of The Ontario Plan sets forth Goals and Policies to conserve Ontario's historic buildings and districts; and

WHEREAS, the Application applies to 7.47 acres of land located on the west side of Bon View Avenue, 132 feet north of Cedar Street, within the IL (Light Industrial) and IG (General Industrial) zoning districts, and is presently improved with a historic 1,008-square-foot single-family residence and detached garage, one commercial building, and four industrial buildings; and

WHEREAS, the property to the north and west of the Project site is within the IG (General Industrial) zoning district and is developed with an industrial use. The property to the east and south is within the IL (Light Industrial) zoning district and is developed with one legal-conforming single-family residence with contractor's storage yard to the south, and industrial building to the east; and

WHEREAS, the one-story, 1,008-square-foot single-family residence built in the Craftsman architectural style was built in 1914 (est.), located at 1948 South Bon View Avenue, met local landmark criteria and was determined by the Historic Preservation Subcommittee, on November 10, 2021, to meet Tier III criteria (File No. PHP21-017); and

WHEREAS, Ontario Development Code Section 4.02.050 requires approval of a replacement structure or development plan by the City of Ontario prior to approval and issuance of a Certificate of Appropriateness for demolition; and

WHEREAS, on September 1, 2021, a Zone Change application (File No. PZC-21-001) to change the zoning designation on a 2.77-acre portion of the Project site, from IL (Light Industrial) to IG (General Industrial), and a Development Plan (File No. PDEV21-030) to construct a 174,603-square-foot industrial building on the Project site in conjunction with the Project; and

WHEREAS, approval of the Project is contingent upon City Council approval of the Zone Change (File No. PZC21-001) application; and

WHEREAS, approval of the Project is contingent upon Planning Commission approval of the Development Plan (File No. PDEV21-030) application; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA"); and

WHEREAS, on January 27, 2010, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the City Council, which determined that demolition of Tier III historic resources results in significant and unavoidable impacts that cannot be fully mitigated to a level of less than significant,

despite the imposition of all feasible mitigation measures identified in the EIR, for which a Statement of Overriding Consideration was documented; and

WHEREAS, the environmental impacts of this Project were previously reviewed in conjunction with The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Historic Preservation Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the proposed Project is located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 9, 2021, the Historic Preservation Subcommittee of the City of Ontario conducted a hearing and issued Decision No. HPSC21-015, recommending the Historic Preservation Commission approve the Application; and

WHEREAS, on December 20, 2021, the Historic Preservation Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, BE IT RESOLVED, that the Historic Preservation Commission of the City of Ontario as follows:

- <u>SECTION 1</u>: *Environmental Determination and Findings.* As the decision-making authority for the Project, the Historic Preservation Commission has reviewed and considered the information contained in the previously certified The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) and supporting documentation, the Historic Preservation Commission finds as follows:
- (1) The environmental impacts of this Project were previously reviewed in conjunction with File No. PGPA06-001, a General Plan Amendment for The Ontario Plan for which a Certified EIR was adopted by the City Council on January 27, 2010; and
- (2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and
- (3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and
- (4) The previous Certified EIR reflects the independent judgment of the City Council; and
- (5) The proposed Project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.
- <u>SECTION 2</u>: **Subsequent or Supplemental Environmental Review Not Required.** Based on the information presented to the Historic Preservation Commission, and the specific findings set forth in Section 1, above, the Historic Preservation Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the

Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- SECTION 3: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Historic Preservation Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.
- SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority

for the Project, the Historic Preservation Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the HISTORIC PRESERVATION COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Historic Preservation Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Historic Preservation Commission hereby concludes as follows:

- (1) The proposed demolition is necessary because all efforts to restore, rehabilitate, and/or relocate the resource have been exhausted. Restoration nor rehabilitation for adaptive reuse of the residential historic resource is feasible at the Project site due to the proposed development and location within the IL (Light Industrial) zoning district. Such preservation treatments would result in an incompatibility of land uses and building types; however, relocation of the historic resource may be possible under certain conditions. Prior to demolition, the Project conditions of approval require advertisements be placed offering the home at no cost for those who can relocate the home off-site; and
- (2) proposed demolition The is necessary because restoration/rehabilitation is not practical because the extensive alterations required would render the resource not worthy of preservation. The proposed industrial development at the Project site is consistent with existing surrounding development and land use. Continuation of the residential use, which is considered a highly sensitive land use, in conjunction with the proposed industrial development, would further intensify adverse impacts due to the incompatibility of land use. Rehabilitation of the residential building for a new industrial use is not practical because State Building Code requirements to ensure health and safety would result in extensive alterations of the residential home that has the potential to render to the resource not worthy of preservation; and
- (3) The proposed demolition is necessary because failure to demolish the resource would adversely affect or detract from the character of the District. The project site is not located in a potential, proposed, or designated historic district. The surrounding properties are developed with industrial buildings and are not worthy of preservation; and

- (4) The resource proposed to be demolished has been assigned a Tier III designation. The HPSC designated the single-family residence a Tier III historic resource on November 10, 2021.
- <u>SECTION 6</u>: *Historic Preservation Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Historic Preservation Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A" and incorporated herein by this reference.
- <u>SECTION 7</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the Applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.
- <u>SECTION 8</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.
- <u>SECTION 9</u>: **Certification to Adoption.** The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Historic Preservation Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Historic Preservation Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Historic Preservation Commission Chairman

ATTEST:

Rudy Zeledon Planning Director and Secretary of the Historic Preservation Commission

Historic Preservation Commission Resolution File No. PHP21-015 December 20, 2021 Page 9	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
I, Gwen Berendsen, Secretary Pro Tempore of the City of Ontario, DO HEREBY CERTIFY that duly passed and adopted by the Historic Preser at their special meeting held on December 20, 2	at foregoing Resolution No, was vation Commission of the City of Ontario
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A:

File No. PHP21-015 Departmental Conditions of Approval

(Departmental conditions of approval follow this page)



CERTIFICATE OF APPROPRIATENESS-DEMOLITION

The "Model Colony" CONDITIONS OF APPROVAL/TOP EIR MITIGTION MEASURES

Date: December 20, 2021

File No.: PHP21-015 (Related File Nos. PDEV20-030 & PZC-21-001)

132 feet north of Cedar Street and West of Bon View Avenue (APNs:

Location: 1050-441-05, 1050-441-09, 1050-441-11 through -62, and 1050-

441-73)

Prepared By: Robert Morales, Assistant Planner

Description:

A request for a Certificate of Appropriateness to demolish a Tier III historic resource (a 1008 square-foot Craftsman single-family residence) to allow the construction of one industrial warehouse and office building totaling 174,603 square feet on a 2.77-acre site located at 1948 South Bon View Avenue, within the IL (Light Industrial) zoning district.

Conditions:

- 1. The Certificate of Appropriateness shall become void twenty-four (24) months from the date of approval unless a building permit has been issued and work authorized by this approval has commenced prior to the expiration date and is diligently pursued to completion.
- Approval of the Certificate of Appropriateness shall not be final and complete until the
 use of the related Addendum to the Ontario Plan Environmental Impact Report and
 the related Zone Change (File no. PZC-21-001) have been approved by the City
 Council.
- Approval of the Certificate of Appropriateness shall not be final and complete until the related Development Plan (File No. PDEV21-030) has been approved by the Planning Commission.
- 4. Prior to issuance of demolition building permit, every effort shall be made to relocate the home. The home shall be offered at no cost for those who can relocate the home off site. Advertisements notifying the public of the opportunity to relocate the home shall be placed for a minimum of 60 days: on-site with temporary signage, in at least 3 local publications (newspapers, magazines, local organization newsletters), and on local bulletin boards (realtor's offices, local business). Applicant shall notify a minimum of 5 non-profit heritage organizations in writing of the home. A social media campaign including a dedicated web page with the home's information (description, square footage, photographs) and contact information should be incorporated into the home's advertisement.

Conditions of Approval/TOP EIR Mitigation Measures

File No.: PHP21-015 December 20, 2021

- 5. Full documentation, including but not limited to as built drawing, historical narrative and HABS photographs, of the historic resource pursuant to Historic American Building Survey (HABS) Level 3 standards shall be submitted to the Planning Department for subsequent release to the Ovitt Family Community Library, Model Colony History Room prior to issuance of any building permit, including demolition and grading.
- 6. A mitigation fee pursuant to Section 7.01.030 of the Ontario Development Code shall be paid to the Planning Department prior to issuance of building permit for demolition. For Tier III structures, this mitigation fee is equal to 10 percent of the price per square foot construction cost as established in the most current ICC Building Valuation Data.
- 7. A determination whether items within or on the resource should be salvaged shall be made by the Planning Department. The applicant shall be responsible for the removal, relocation and donation of such items selected for salvaging. An inventory of salvaged items shall be provided by the applicant to the Planning Department prior to be to issuance of building permit.
- 8. The applicant shall obtain a building permit prior to any demolition, relocation, or construction.
- 9. Any deviation from the approved plans shall require approval of the Planning Department and, if necessary, the Historic Preservation Commission.
- 10. Conditions of Approval table shall be reproduced onto all plans submitted for permits.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV21-030, A DEVELOPMENT PLAN TO CONSTRUCT A 174,603-SQUARE-FOOT INDUSTRIAL WAREHOUSE ON 7.47 ACRES OF LAND LOCATED ON THE WEST SIDE OF BON VIEW AVENUE, 132 FEET NORTH OF CEDAR STREET, WITHIN THE IG (GENERAL INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APNS: 1050-441-05, 1050-441-09, 1050-441-11, 1050-441-12, 1050-441-13, 1050-441-14, 1050-441-15, 1050-441-16, 1050-441-17, 1050-441-18, 1050-441-19, 1050-441-20. 1050-441-21. 1050-441-22. 1050-441-23. 1050-441-24. 1050-441-25, 1050-441-26, 1050-441-27, 1050-441-28, 1050-441-29, 1050-441-30, 1050-441-31, 1050-441-32, 1050-441-33, 1050-441-34, 1050-441-35, 1050-441-36, 1050-441-37, 1050-441-38, 1050-441-39, 1050-441-40. 1050-441-41. 1050-441-42. 1050-441-43. 1050-441-44. 1050-441-45, 1050-441-46, 1050-441-47, 1050-441-48, 1050-441-49, 1050-441-50, 1050-441-51, 1050-441-52, 1050-441-53, 1050-441-54, 1050-441-55, 1050-441-56, 1050-441-57, 1050-441-58, 1050-441-59, 1050-441-60, 1050-441-61, 1050-441-62, AND 1050-441-73.

WHEREAS, Bon View Land 10, LLC & BV Investments 10, LLC, (hereinafter referred to as "Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV21-030, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 7.47 acres of land generally located on the west side of Bon View Avenue, 132 feet north of Cedar Street, within the IG (General Industrial) zoning district, and is presently improved with residential, commercial, and industrial uses; and

WHEREAS, the property to the north and west of the Project site is within the IG (General Industrial) zoning district and is developed with industrial land uses. The property to the east and south is within the IL (Light Industrial) zoning district and is developed with one legal-nonconforming single-family residence and a contractor's storage yard. The property to the east is within the IL (Light Industrial) zoning district and is developed with an industrial building; and

WHEREAS, the Application was submitted in conjunction with three other application types that are essential to the proposed development, which include: (1) a Certificate of Appropriateness (File No. PHP21-015) to allow for the demolition of a Tier III historic resource (a 1,008 square-foot Craftsman single-family structure) that exists on the subject site; (2) a Zone Change (File No. PZC21-001) on a 2.77-acre portion of the subject site, changing the zoning designation from IL (Light Industrial) to IG (General Industrial); and a Lot Line Adjustment (File No. E202100860) consolidating three lots that

comprise the Project site, into a single lot. Additionally, the recorded condominium plan over a portion of the Project site will be quitclaimed prior to the issuance of a building permit to construct the proposed Project; and

WHEREAS, the Project will facilitate the construction of a 174,603-square-foot industrial warehouse building, having a floor area ratio (FAR) of 0.54. Passenger vehicle parking is proposed along the north and south sides of the building. The building's main entrance and visitor parking are located adjacent to Bon View Avenue; and

WHEREAS, the site will have two points of access from Bon View Avenue. The site has visitor parking located at the northeast corner of project site, gated entrance, and employee parking on the southerly side of the project site, and a screened truck parking area is located along the northerly project boundary. Pursuant to the conditions of approval, decorative pavement will be provided at all driveway approaches, extending from the back of the driveway apron to the first intersecting drive aisle or parking space; and

WHEREAS, a yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging is proposed along the north side of the building and will be screened from public view by tilt-up decorative screen walls designed to match the architecture of the proposed building. In addition, tubular steel fencing has been proposed along the north, west, and a portion of the south property lines, transitioning to a decorative wall with pilasters constructed along the south-easterly portion of the project site to provide screening for an existing single-family residence; and

WHEREAS, the Project has provided 100 off-street parking spaces. The warehouse/distribution parking standard specified in the Development Code requires 98 parking spaces; and

WHEREAS, the proposed building is of concrete-tilt up construction and includes enhanced features such as smooth paint concrete, metal siding, inset reveals, storefront windows, and anodized clear aluminum mullions and steel canopies throughout the façade of the building; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010, (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Addendum and the Project, and concluded said hearing on that date, voting to issue Decision No. DAB21-056, respectively,

recommending that the Planning Commission approve the Application, contingent upon City Council approval of the use of the related Addendum to The Ontario Plan Environmental Impact Report and City Council approval of the related Zone Change (File No. PZC21-001); and

WHEREAS, as the first actions on the Project, on December 20, 2021, the Planning Commission issued a Resolution recommending the City Council: (1) approve the use of the related Addendum to The Ontario Plan Environmental Impact Report, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference, and (2) approve the related Zone Change (File No. PZC21-001); and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: **Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the Addendum, the initial study, and the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the Addendum, the initial study, and the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of the Project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and
- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental

assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

- (4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and
- (5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.
- <u>SECTION 2</u>: **Subsequent or Supplemental Environmental Review Not Required.** Based on the EIR Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The Project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

- <u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:
- (1) The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed Project is located within the Industrial (0.55 FAR) land use district of the Policy Plan Land Use Map,

and the IG (General Industrial) zoning district, pending City Council approval of the related Zone Change, File No. PZC21-001. The development standards and conditions under which the proposed Project will be constructed and maintained is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

- (2) The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located. The Project has been designed consistent with the requirements of the Development Code and the IG (General Industrial) zoning district (pending City Council approval of the related Zone Change, File No. PZC-21-001), including standards relative to the particular land use proposed warehouse, as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.
- (3) The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project. The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan.
- (4) The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development. The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed industrial warehouse. As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

<u>SECTION 6</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

<u>SECTION 7</u>: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 8</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 9</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PDEV21-030 December 20, 2021 Page 10	
STATE OF CALIFORNIA COUNTY OF SAN BERNARDINO CITY OF ONTARIO))
City of Ontario, DO HEREBY CERTIF	Pro Tempore of the Planning Commission of the FY that foregoing Resolution No was duly Commission of the City of Ontario at their special by the following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen
	Secretary Pro Tempore

ATTACHMENT A:

File No. PDEV21-030 Departmental Conditions of Approval

(Departmental conditions of approval to follow this page)



LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

Date Prepared: 11/29/2021

File No: PDEV21-030

Related Files: PHP21-015 and PZC21-001

Project Description: A Development Plan (File No. PDEV21-030) to construct a 174,603 -square-foot industrial building on 7.47 acres of land; The project is located on the west side of Bon View Avenue, approximately 132 feet north of Cedar Street, within the IL (Light Industrial) and IG (General Industrial) zoning districts. (APNs: 1050-441-05, 1050-441-09, 1050-441-11, 1050-441-12, 1050-441-13, 1050-441-14, 1050-441-15, 1050-441-16, 1050-441-17, 1050-441-18, 1050-441-19, 1050-441-20, 1050-441-21, 1050-441-22, 1050-441-23, 1050-441-24, 1050-441-25, 1050-441-26, 1050-441-27, 1050-441-28, 1050-441-29, 1050-441-30, 1050-441-31, 1050-441-32, 1050-441-33, 1050-441-34, 1050-441-36, 1050-441-37, 1050-441-38, 1050-441-39, 1050-441-40, 1050-441-41, 1050-441-42, 1050-441-43, 1050-441-44, 1050-441-45, 1050-441-46, 1050-441-47, 1050-441-48, 1050-441-49, 1050-441-50, 1050-441-51, 1050-441-52, 1050-441-53, 1050-441-54, 1050-441-55, 1050-441-56, 1050-441-57, 1050-441-58, 1050-441-59, 1050-441-60, 1050-441-61, 1050-441-62, and 1050-441-73) **submitted by Bon View Land 10, LLC & BV Investments 10, LLC.**

Prepared By: Robert Morales, Assistant Planner

<u>Phone</u>: 909.395.2432 (direct) <u>Email</u>: Rmorales@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

- **1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions* for New Development, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions* for New Development may be obtained from the Planning Department or City Clerk/Records Management Department.
- **2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

- **2.2** <u>General Requirements.</u> The Project shall comply with the following general requirements:
- (a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.
- **(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.
- **(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 <u>Landscaping</u>.

- (a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).
- **(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.
- **(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.
- **(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.
- **2.4** <u>Walls and Fences</u>. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

- (a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).
- **(b)** All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.
- (c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the

outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

- **(d)** The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.
- **(e)** Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).
- **(f)** Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

- (a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).
- **(b)** Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.
- **(c)** Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.
- (d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:
- (i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or
- (ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.
- **(e)** The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

Screen Wall Height	Minimum Gate Height
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 <u>Site Lighting.</u>

- (a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.
- **(b)** Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 <u>Mechanical and Rooftop Equipment.</u>

- (a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.
- **(b)** All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.
- **2.9** <u>Security Standards</u>. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).
- **2.10** <u>Signs</u>. All Project signage shall comply with the requirements of Ontario Development Code Division 8.01 (Sign Regulations).
- **2.11** <u>Sound Attenuation</u>. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

- (a) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010, in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.
- **(b)** If human remains are found during project arading/excavation/construction activities, the area shall not be disturbed until any required

investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

- (c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.
- **2.13** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

- (a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.
- **(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's <u>Plan Check</u> and <u>Inspection</u> fees shall be paid at the rate established by resolution of the City Council.

2.15 <u>Additional Requirements</u>

- (a) Development Plan approval shall not be final and complete until the use of the related Addendum to the Ontario Plan Environmental Impact Report and the related Zone Change (File no. PZC-21-001) have been approved by the City Council.
- **(b)** Development Plan approval shall not be final and complete until the related Certificate of Appropriateness (File No. PHP21-015) has been approved by the Historic Preservation Commission.
- (c) The applicant shall submit a sight-line analysis/wall section plan which shows that all roll-up doors, truck trailers, and any items stored outdoors will be completely screened from view from the public street, subject to Planning Director review and approval.
- (d) The maximum wall height on the south eastern side of the Project site shall not exceed three feet, within the front setback area setback.

CITY OF ONTARIO

LANDSCAPE PLANNING DIVISION

303 East "B" Street, Ontario, CA 91764

CONDITIONS OF APPROVAL		
Sign Off		
9.7	11/17/2021	
Jamie Richardson, Sr. Landscape Planner	Date	
Phone:		

	<u> </u>	Jamie Richardson, Sr. Landscape Planner	Date
Reviewer's Name: Jamie Richardson, Sr. Landscape Planner		Phone: (909) 3	95-2615
	. File No.: V21-030, PZC-21-001, PHP-21-015	Case Plar Robert I	
	ct Name and Location: strial Building		
Nortl	nwest Corner of S. Bon View Ave. and E. Cant/Representative:	Cedar Street	
Bon	View Land 10, LLC & BV Investments msiz	zemore@panattoni.com	
	? Dupont Drive e, CA 92612		
\boxtimes	Preliminary Plans (dated 11/4/2021) me Development and have been approved below be met upon submittal of the lar	considering that the following cor	
	Development and have been approved	considering that the following condescape construction documents. en approved. Corrections noted be	nditions

Civil/ Site Plans

- 1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width, and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings, or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall equal the trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
- 2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal," approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking, and irrigating 15-gallon trees (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
- 3. Before permit issuance, stormwater infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division. Any stormwater devices in parkway areas shall not displace street trees.
- 4. Show transformers set back 5' from paving all sides. Coordinate with landscape plans.

- 5. Show backflow devices set back 4' from paving all sides. Locate on level grade.
- 6. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
- 7. Dimension all planters to have a minimum 5' wide inside dimension.
- 8. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.
- 9. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or stormwater infiltration areas are located shall be loosened by soil fracturing. For trees, a 12'x12'x18" deep area; for stormwater infiltration, the entire area shall be loosened. Add the following information on the plans: The backhoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The backhoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference, see Urban Tree Foundation Planting Soil Specifications.

Landscape Plans

- 10. Provide an arborist report and tree inventory as noted in #1.
- 11. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as masses and duplicate masses in other locations at regular intervals.
- 12. Locate light standards, fire hydrants, water, and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans
- 13. Detail irrigation dripline outside of mulched root zone of the tree; 4' from the trunk.
- 14. Call out all fences and walls, materials proposed, and heights.
- 15. Show minimum on-site tree sizes per the Landscape Development standards; see the Landscape Planning website. 5% 48" box, 10% 36 box, 30% 24" box, 55% 15 gallon.
- 16. Show 25% of trees as California native (Platanus racemosa, Quercus agrifolia, Quercus wislizenii, Quercus douglasii, Cercis occidentalis, etc.) in appropriate locations.
- 17. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See http://www.ontarioca.gov/landscape-planning/standards
- 18. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

DEVELOPMENT PLAN		L MAP	TRACT MAP
☐ OTHER	☐ FOR C	ONDOMINIUM PU	RPOSES
PR	OJECT FILE	NO. PDEV21-030	
RELATED FILE NO(S). PHP21-015			
☑ ORIGINAL ☐ REVISED:/_/_			
CITY PROJECT ENGINEER 8	PHONE NO:	Eric Woosley, P.E.	(909) 395-2134
CITY PROJECT PLANNER & PHONE NO:		Robert Morales	(909) 395-2432
DAB MEETING DATE:		December 20, 2021	
PROJECT NAME / DESCRIPTION:		A Development Plan (1) industrial building square feet on 7.47 a	g totaling 175,047
LOCATION:		Northwest corner of and Cedar Street	Bon View Avenue
APPLICANT:		Bon View Land 10, LLC & BC Investments 10, LLC	
REVIEWED BY:		Raymond Lee, P.E.	(2/8/2) Date
APPROVED BY:		Assistant City Engin Khoi Do, P.E. City Engineer	12-8-21 Date

Last Revised: 12/8/2021

Project File No. PDEV21-030 Project Engineer: Eric Woosley, P.E.

Date: December 8, 2021



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1.	PRIC	OR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check Whe Complete	n
	1.01	Dedicate to the City of Ontario, the right-of-way, described below: feet on	
		Property line corner 'cut-back' required at the intersection of	
	1.02	Dedicate to the City of Ontario, the following easement(s):	
	1.03	Restrict vehicular access to the site as follows:	
	1.04	Vacate the following street(s) and/or easement(s):	
		 All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. 	
	1.05	Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.	
	1.06	Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.	
	1.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
	1.08	File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.	
		(1)	
		(2)	
	1.09	Prepare a fully executed Subdivision Agreement (on City approved format and forms) with	

Last Revised 12/9/2021 Page 2 of 15

Date: December 8, 2021



accompanying security as required, or complete all public improvements. 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater. 1.11 Provide a preliminary title report current to within 30 days. File an application, together with an initial deposit (if required), to establish a Community Facilities 1.12 District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process. 1.13 Ontario Ranch Developments: 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents). 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability). 1.14 Other conditions: PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL: A. GENERAL (Permits includes Grading, Building, Demolition and Encroachment) pursuant to the Subdivision Map Act and in accordance Record Parcel Map/Tract Map No. 2.01 with the City of Ontario Municipal Code. 2.02 Submit a PDF of the recorded map to the City Engineer's office. 2.03 Note that parcels (APNs: 1050-441-05, 11-62, and 73) are recognized parcels in the City of Ontario Per Parcel Map 2683, Book 23, Page 31, and Tract Map 11808, Book 158, Pages 82-83. Note that parcel (APN:1050-441-09) is an 'unrecognized' parcel in the City of Ontario and shall 2.04 X require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972. Apply for a: 2.05 ☐ Certificate of Compliance with a Record of Survey; ☑ Lot Line Adjustment to merge parcels (APNs: 1050-441-05, 09, 11-62 and 73) (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);



		☐ Make a Dedication of Easement.	
	2.06	Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.	
	2.07	For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at http://tceplumecleanup.com/), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658 .	
\boxtimes	2.08	Submit a soils/geology report.	
	2.09	Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:	
		State of California Department of Transportation (Caltrans) San Bernardino County Road Department (SBCRD) San Bernardino County Flood Control District (SBCFCD) Federal Emergency Management Agency (FEMA) Cucamonga Valley Water District (CVWD) for sewer/water service United States Army Corps of Engineers (USACE) California Department of Fish & Game Inland Empire Utilities Agency (IEUA) Other:	
	2.10	Dedicate to the City of Ontario the right-of-way described below: feet on	
		Property line corner 'cut-back' required at the intersection ofand	
	2.11	Dedicate to the City of Ontario the following easement(s):	
	2.12	Vacate the following street(s) and/or easement(s):	
		 All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company. 	
	2.13	Ontario Ranch Developments:	
		☐ 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.	



		☐ 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.	
		☐ 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.	
\boxtimes	2.14	Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.	
	2.15	The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.	
\boxtimes	2.16	Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$175,397.09, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.	
\boxtimes	2.17	Other conditions: a. Pay a Storm Drain In-Lieu Fee, \$108,750.18, for the future installation of a public storm drain in Bon View Avenue along the project frontage.	
		b. Submit a Final Utilities Systems Map (USM) as part of the precise grading plan submittal that conforms to the Conceptual USM and meets all of the City's USM requirements. These requirements include to show and label all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, etc.), sizes, points of connection, and any easements. The final utility design shall comply with all Division of Drinking Water (CCR §64572) Separation Requirements which include a minimum 10' horizontal separation, outside wall to outside wall, between	

water and all other utilities.

 \boxtimes



B. PUBLIC IMPROVEMENTS (See attached Exhibit 'A' for plan check submittal requirements.)

2.17	Design and construct full public improvements in accordance with the City of Ontario Municipal
	Code, current City standards and specifications, master plans and the adopted specific plan for
	the area, if any. These public improvements shall include, but not be limited to, the following
	(checked boxes):

Improvement	Bon View Avenue	Street 2	Street 3	Street 4
Curb and Gutter	New; 32-ft. from C/L to replace drive approaches (match existing) Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace	New; ft. from C/L Replace damaged Remove and replace
AC Pavement	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions	Replacement Widen additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Drive Approach	New Remove and replace with full height curb and gutter, parkway landscape, and sidewalk	New Remove and replace	New Remove and replace	New Remove and replace
Sidewalk	☐ New ☑ Remove and replace broken panels	New Remove and replace	New Remove and replace	New Remove and replace
ADA Access Ramp	New Remove and replace	New Remove and replace	New Remove and replace	New Remove and replace

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Parkway	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)	Trees Landscaping (w/irrigation)
Raised Landscaped Median	New Remove and replace	New Remove and replace	New Remove and replace	New Remove and replace
Fire Hydrant	New (2) Relocation	New / Upgrade Relocation	New / Upgrade Relocation	New / Upgrade Relocation
Sewer (see Sec. 2.C)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Water (see Sec. 2.D)	Main Service	Main Service	Main Service	Main Service
Recycled Water (see Sec. 2.E)	Main Service	Main Service	Main Service	Main Service
Traffic Signal System (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Traffic Signing and Striping (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Street Light (see Sec. 2.F)	New / Upgrade Relocation	New / Upgrade Relocation	New / Upgrade Relocation	New / Upgrade Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	New Modify existing	New Modify existing	New Modify existing	New Modify existing
Storm Drain (see Sec. 2G)	Main Lateral	Main Lateral	Main Lateral	Main Lateral
Fiber Optics (see Sec. 2K)	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances	Conduit / Appurtenances
Overhead Utilities	Underground Relocate	Underground Relocate	Underground Relocate	Underground Relocate

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		Removal of Improvements			
		Other Improvements			
		Specific notes for improvements listed in item no. 2.17, above:			
\boxtimes	2.18	Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): Bon View Avenue, centerline to gutter.			
	2.19	Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.			
	2.20	Make arrangements with the Cucamonga Valley Water District (CVWD) to provide $\ \ \ \ \ \ \ \ \ \ \ \ \ $			
	2.21	Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.			
	2.22	Other conditions:			
	C. SE	WER			
\boxtimes	2.23	An 8-inch sewer main is available for connection by this project in Bon View Avenue. (Ref: Sewer plan bar code: S10702)			
	2.24	Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately feet away.			
	2.25	Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.			
	2.26	Other conditions: a. Construct a monitoring manhole immediately back of the project property line on private property. 10-feet of pipe leading to and 10-feet of pipe coming from the monitoring manhole must be straight. The slope must be greater 0.4%, but not greater than 2%.			
	D. WA	TER			
\boxtimes	2.27	An 8-inch water main is available for connection by this project in Bon View Avenue. (Ref: Water plan bar code: W11852) However, the main is undersized and requires removal and replacement with a 12-inch.			
\boxtimes	2.28	Design and construct a 12-inch water main to replace the existing 8-inch water main in Bon View Avenue, from the intersection of Cedar Street and Bon View Avenue, extending to the point of connection in the intersection of Francis Street and Bon View Avenue, approximately 1330LF in length. See 2.29 a.			



	2.29	 Other conditions: a. Upgrade the existing 8" domestic water main to a 12" water main along Bon View Avenue from Francis Street to Cedar Street. Applicant/Developer will be eligible for DIF reimbursement under a separate agreement. Contact the DIF group at: DIF@ontarioca.gov b. Abandon the existing irrigation service back to the public water main and construct a new stand-alone irrigation service with a back flow device to current city standard, tie to the newly installed 12" water main. c. Abandon the existing domestic water service back to the public water main and construct a new stand-alone domestic water service with a back flow device to current city standard, tie to the newly installed 12" water main. d. Construct two fire hydrants to city current standard, one 145' south of the project northerly property line and another one 350' south from it, tie to the newly installed 12" water main. 	
	E. RE	CYCLED WATER	
\boxtimes	2.30	No recycled water main is available for connection by this project in Bon View Avenue.	
	2.31	Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.	
	2.32	Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.	
	2.33	Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval. Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.	
	2.34	Other conditions:	
	F. TR	AFFIC / TRANSPORTATION	
	2.35	Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 1. On-site and off-site circulation 2. Traffic level of service (LOS) at 'build-out' and future years 3. Impact at specific intersections as selected by the City Engineer	
	2.36	New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.	
\boxtimes	2.37	Other conditions: a. Property frontage along Bon View Avenue shall be signed "No Parking Anytime".	
		 Design and construct proposed driveways in accordance with City of Ontario Standard Drawing No. 1204 for Commercial Driveway along Bon View Avenue. 	
		c. The Applicant/Developer shall install edge line striping and object markers to accommodate frontage widening improvements transition to the narrower street section on Bon View Avenue.	
		d. The Applicant/Developer shall be responsible to design and construct in-fill public street lights and potential service along its project frontage of Bon View Avenue. Street	

Date: December 8, 2021



lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.

e. The Applicant/Developer shall be responsible to replace any existing street light fixtures along its project frontage of Bon View Avenue with the current City approved LED equivalent fixture per the Traffic and Transportation Design Guidelines.

	G. DR	AINAGE / HYDROLOGY	
\boxtimes	2.38	No storm drain main is available to accept flows from this project in Bon View Avenue.	
	2.39	Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.	
	2.40	An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.	
	2.41	Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.	
	2.42	Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.	
	2.43	Other conditions:	
	H. ST (NPDE	ORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM	
	2.44	401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels. If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted. Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.	
\boxtimes	2.45	Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: http://www.sbcounty.gov/dpw/land/npdes.asp .	
	2.46	Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable	

Project File No. PDEV21-030 Project Engineer: Eric Woosley, P.E. Date: December 8, 2021



		component to facilitate maintenance and cleaning.	
	2.47	Other conditions:	
	J. SP	ECIAL DISTRICTS	
	2.48	File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.	
	2.49	Other conditions:	
	K. FIE	BER OPTIC	
\boxtimes	2.50	A fiber optic line is available for connection by this project in Bon View Avenue. See Exhibit 'B.'	
	2.51	Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall generally be located along the frontage of Bon View Avenue, with a handhole at the northerly property limit, a handhole at the southerly property limit, and a handhole on the east side of Bon View Avenue, directly across from the handhole at the northerly property limit. See Exhibit 'B.'	
	2.52	Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.	
	L. INT	EGRATED WASTE	
	2.52	Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at: https://www.ontarioca.gov/OMUC/IntegratedWaste	
	2.53	Other conditions:	

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3.	PRIO	R TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:	
\boxtimes	3.01	Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.	
	3.02	Complete all requirements for recycled water usage.	
		☐ 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.	
		☐ 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.	
		□ 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.	
	3.03	The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.	
	3.04	Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.	
\boxtimes	3.05	Confirm payment of all Development Impact Fees (DIF) to the Building Department.	
\boxtimes	3.06	Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies	
		and reports (i.e. hydrology, traffic, WQMP, etc.).	
4.	PRIO	R TO FINAL ACCEPTANCE, APPLICANT SHALL:	
\boxtimes	4.01	Complete all Conditions of Approval listed under Sections 1-3 above.	
\boxtimes	4.02	Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.	
	4.03	The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.	
\boxtimes	4.04	Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.	

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EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV21-030

The 1	<u>fol</u> l	lowing	<u>items</u>	are re	equired	<u>to be</u>	incl	<u>uded</u>	with	the	<u>first</u>	plan	check	subr	<u>nittal:</u>

1.	\boxtimes	A copy of this check list
2.	\boxtimes	Payment of fee for Plan Checking
3.	\boxtimes	One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.
4.		One (1) copy of project Conditions of Approval
5.	\boxtimes	Include a PDF (electronic submittal) of each required improvement plan at every submittal.
6.	sho	Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations wing low, average and peak water demand in GPM for the proposed development and proposed water er size).
7.		Three (3) sets of Public Street improvement plan with street cross-sections
8.		Four (4) sets of Public Water improvement plan (include water demand calculations showing low, rage and peak water demand in GPM for the proposed development and proposed water meter size)
9.	low,	Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing average and peak water demand in GPM for the proposed development and proposed water meter size an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.		Four (4) sets of Public Sewer improvement plan
11.		Five (5) sets of Public Storm Drain improvement plan
12.		Three (3) sets of Public Street Light improvement plan
13.		One (1) set of Public Street Light Improvement plan (DELTA) for the LED Upgraded light
14.		Three (3) sets of Signing and Striping improvement plan
15.		Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
16.	ultin clea	Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and nate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall trances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 6. Include Auto CAD electronic submittal)
17.	mod	Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with diffied Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal cifications.
18.		Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved liminary WQMP (PWQMP).
19.		One (1) copy of Hydrology/Drainage study
20.		One (1) copy of Soils/Geology report

Project File No. PDEV21-030 Project Engineer: Eric Woosley, P.E. Date: December 8, 2021



28.	☐ One (1) set of Final Solid Waste Handling Plan (SWHP)
	Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
	One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
25.	☐ One (1) copy of Traverse Closure Calculations
24.	☐ One (1) copy of Preliminary Title Report (current within 30 days)
23.	One (1) copy of approved Tentative Map
22.	☐ Three (3) copies of Final Map/Parcel Map
21.	☐ Payment for Final Map/Parcel Map processing fee

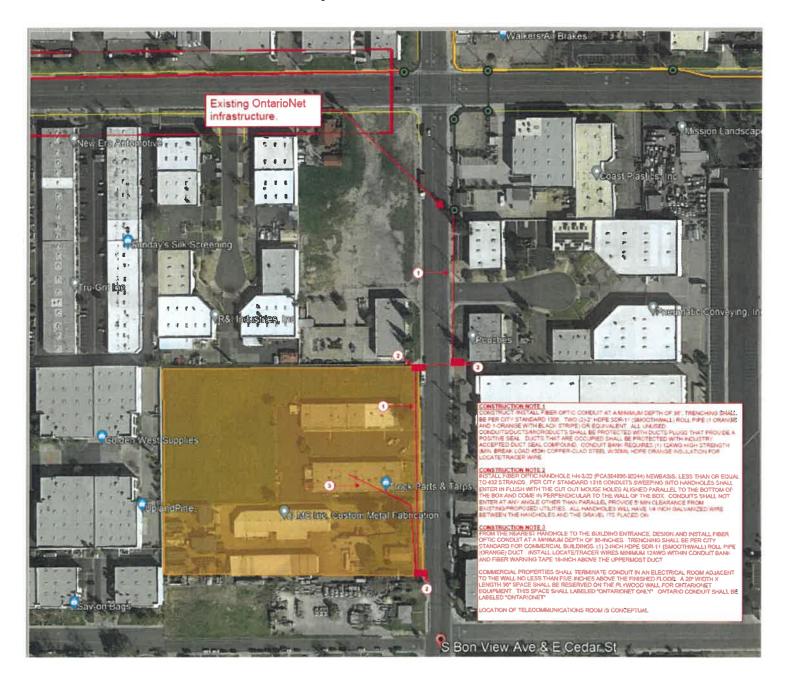
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EXHIBIT 'B'

BROADBAND DEPARTMENT Fiber Optic Exhibit

Project Number: PDEV21-030



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CITY OF ONTARIO MEMORANDUM

TO: Eric Woosley, ENGINEERING DEPARTMENT

FROM: Celia Corral

DATE: September 30, 2021 SUBJECT: File No. PDEV21-030



The PWQMP and Site Plan for this project is approved for DAB based on the following condition:

Note: This project will be conditioned in the Engineering DAB Report to prepare a WQMP based on the approved PWQMP. The WQMP template is available at: http://www.sbcounty.gov/dpw/land/npdes.asp or on the City's website under Engineering/Environmental Services.

All Priority Land Use (PLU): Land use consisting of high-density residential, defined as a land use with at least ten (10) dwelling units per acre, industrial, commercial, mixed urban, and public transportation station land uses shall comply with the statewide Trash Provisions adopted by the State Water Resources Control Board (SWRCB).

Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at

https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml



CITY OF ONTARIO MEMORANDUM

TO: Robert Morales, Assistant Planner

FROM: Officer Antonio Galban, Police Department

DATE: September 23, 2021

SUBJECT: PDEV21-030- A DEVELOPMENT PLAN TO CONSTRUCT ONE (1)

INDUSTRIAL BUILDING TOTALING 175,047 SQUARE FEET,

LOCATED AT BON VIEW AVE AND CEDAR AVE.

The "Standard Conditions of Approval" contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other
 areas used by the public shall be provided. Lights shall operate via photosensor.
 Photometrics shall be provided to the Police Department and include the types of fixtures
 proposed and demonstrate that such fixtures meet the vandal-resistant requirement.
 Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street. Associated letters shall also be included.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Antonio Galban at (909) 408-1006 with any questions or concerns regarding these conditions.



CITY OF ONTARIO MEMORANDUM

TO: Robert Morales, Assistant Planner

Planning Department

FROM: Mike Gerken, Deputy Fire Chief/Fire Marshal

Fire Department

DATE: September 29, 2021

SUBJECT: PDEV21-030 - A Development Plan to construct one (1) Industrial building

totaling 175,047 square feet on 7.47acres of land located on the southwest corner of Bon View Avenue and Cedar Avenue. APNs: 1050-441-05, 09, 11-

62, and 73. Related File(s): PZC21-001 and PHP21-015.

☐ The plan <u>does</u> adequately address Fire Department requirements at this time.

Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

A. 2019 CBC Type of Construction: III-B

B. Type of Roof Materials: Panelized

C. Ground Floor Area(s): 173,153 Sq. Ft.

D. Number of Stories: 1

E. Total Square Footage: 175,047 Sq. Ft.

F. 2019 CBC Occupancy Classification(s): S-1/B

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☑ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov/Fire/Prevention.

2.0 FIRE DEPARTMENT ACCESS

- ∑ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.

3.0 WATER SUPPLY

4.0 FIRE PROTECTION SYSTEMS

- ☑ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.

- △ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be

- submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ✓ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per <u>Standard #C-001</u>. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ∑ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ∑ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department.
 All Knox boxes shall be monitored for tamper by the building fire alarm system. See <u>Standard</u> #H-001 for specific requirements.
- ∑ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PZC-21-001, A ZONE CHANGE ON 2.77 ACRES OF LAND FROM IL (LIGHT INDUSTRIAL) TO IG (GENERAL INDUSTRIAL), LOCATED AT 1948 SOUTH BON VIEW AVENUE, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1050-441-05.

WHEREAS, Bon View Land 10, LLC & BV Investments 10, LLC (hereinafter referred to as "Applicant") has filed an Application for the approval of a Zone Change, File No. PZC-21-001, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to one 2.77-acre parcel of land generally located on the west side of Bon View Avenue, 132 feet north of Cedar Street, at 1948 South Bon View Avenue, presently within the IL (Light Industrial) zoning district. The parcel is developed with one single-family residence, one detached garage, one commercial building, and two industrial buildings; and

WHEREAS, properties to the east, north and west of the Project site are within the IG (General Industrial) zoning district and are developed with industrial land uses. The property to the south of the Project site is within the IL (Light Industrial) zoning district and is developed with a legal nonconforming single-family structure and contractor's storage yard; and

WHEREAS, the Applicant has filed a related Development Plan application (File No. PDEV21-030) to construct one 174,603-square-foot industrial building on the Project site, in conjunction with a Certificate of Appropriateness application (File No. PHP21-015) requesting to demolish a Tier III historic resource (a 1,008-square-foot Craftsman single-family structure) to facilitate the construction of the warehouse; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and

addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission approved a resolution recommending the City Council approve the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), prepared pursuant to CEQA, the State CEQA Guidelines and the City of Ontario Local CEQA Guidelines, which indicated that all potential environmental impacts from the Project were less than significant or could be mitigated to a level of less than significance; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: **Environmental Determination and Findings.** As the recommending authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the Planning Commission recommends City Council find as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City of Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001.

- (2) The Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (5) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- (6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Certified EIR, are incorporated herein by this reference.
- <u>SECTION 2</u>: **Subsequent or Supplemental Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or

- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing,

and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) The proposed Zone Change is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan as follows:

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-6</u>: <u>Complete Community</u>. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

Compliance: The proposed Zone Change will reflect the existing uses of the properties or closely coordinates with land use designations in the surrounding area and provides opportunities for choice in living environments.

- Goal LU2: Compatibility between a wide range of uses.
- ➤ <u>LU2-1: Land Use Decisions</u>. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.

Compliance: The proposed Zone Change reflects the existing uses of the properties or closely coordinates with land use designations in the surrounding area and will not create adverse impacts on adjacent properties.

- Goal LU5: Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.
- ➤ <u>LU5-7</u>: <u>ALUCP Consistency with Land Use Regulations</u>. We comply with state law that requires general plans, specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.

Compliance: The proposed Zone Change is consistent with the adopted Airport Land Use Compatibility Plan for both Ontario International Airport and Chino Airport.

Safety Element — Noise Hazards:

- Goal S4: An environment where noise does not adversely affect the public's health, safety, and welfare.
- ➤ <u>S4-6: Airport Noise Compatibility</u>. We utilize information from Airport Land Use Compatibility Plans to prevent the construction of new noise sensitive land uses within airport noise impact zones.

Compliance: The subject property is located within the 60 to 65 CNEL Noise Impact area and the proposed Industrial (0.55 FAR) land use designation is compatible with the Noise Impact area.

- (2) The proposed Zone Change would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City. The proposed zoning district is compatible with the zoning and land uses in the surrounding area.
- (3) The proposed Zone Change will not adversely affect the harmonious relationship with adjacent properties and land uses. The surrounding properties to the north, east, south, and west of the Project site are within the same Policy Plan (general plan) Industrial land use designation as the Project site, and as such, will be occupied by compatible industrial land uses of similar intensity.
- (4) The subject site is physically suitable, including, but not limited to, parcel sizes, shapes, access, and availability of utilities, for the request and anticipated development. The Project site and its surrounding properties to the north, east, south, and west are within the Policy Plan (general plan) Industrial land use district. The proposed IG (General Industrial) zoning district will implement the Policy Plan (general plan) Industrial land use district (pursuant to Development Code Section 5.01.005.D.4); therefore, the proposed Zone Change is consistent with the Policy Plan (general plan). Moreover, the proposed Zone Change will serve to promote the orderly development of the subject site through the consolidation of the two adjacent parcels that comprise the Project site, making the site physically suitable for the anticipated future industrial development and related allowed land uses. The adjoining parcel to the south of the Project site will remain with the IL (Light Industrial) zoning designation to serve as a buffer between the residential properties to the south, across Cedar Street, and the more intense industrial uses permitted to the north

<u>SECTION 6</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Application, as detailed in "Exhibit A" attached hereto and incorporated herein by this reference.

SECTION 7: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

<u>SECTION 8</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 9</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PZC-21-001 December 20, 2021 Page 8	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
I, Gwen Berendsen, Secretary Pro Tempo City of Ontario, DO HEREBY CERTIFY that foreg passed and adopted by the Planning Commissio meeting held on December 20, 2021, by the follow	oing Resolution No, was duly n of the City of Ontario at their special
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	wen Berendsen
_	ecretary Pro Tempore

Exhibit A: File No. PZC19-003 Proposed Zone Change

ZONING Legend:







PLANNING COMMISSION STAFF REPORT

December 20, 2021

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

FILE NOS: PGPA21-004, PZC-21-002, and PDCA21-001

SUBJECT: A General Plan Amendment (File No. PGPA21-004) for the Housing Element update to the Policy Plan (General Plan) component of The Ontario Plan, addressing State mandates and the 6th Cycle Regional Housing Needs Allocation (RHNA) and to modify the Policy Plan Land Use Plan (Exhibit LU-01) to establish the following: 1) a Zone Change (File No. PZC21-002) to modify the Zoning Map to establish an AH (Affordable Housing) zoning district; 2) create an Affordable Housing Overlay; and 3) a Development Code Amendment (File No. PDCA21-001) revising section 6.01.035 for the purpose of adding provisions to establish an AH (Affordable Housing) Overlay District. Staff has prepared an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by City Council on January 27, 2010. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); City Initiated. City Council action is required.

Applicant: City of Ontario

RECOMMENDED ACTION: That the Planning Commission take the following actions:

- (1) Adopt a resolution recommending the City Council approve the use of an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140);
- (2) Adopt a resolution recommending the City Council approve the General Plan Amendment, File No. PGPA21-004, pursuant to the facts and reasons contained in the staff report and attached resolution;
- (3) Adopt a resolution recommending the City Council approve the Zone Change, File No. PZC21-002, pursuant to the facts and reasons contained in the staff report and attached resolution; and

Case Planner:	Thomas Grahn
Planning Director Approval:	PZL
Submittal Date:	

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	12-20-21		Recommend
CC	2-15-22		Final

(4) Adopt a resolution recommending the City Council approve the Development Code Amendment, File No. PDCA21-001, pursuant to the facts and reasons contained in the staff report and attached resolution.

PROJECT ANALYSIS: The proposed project is an update to the Housing Element and includes related applications to establish and implement an Affordable Housing Overlay to support the necessary residential densities to achieve the City's Regional Housing Needs Assessment ("RHNA").

- (1) <u>General Plan Amendment (File No. PGPA21-004)</u> This General Plan Amendment includes the Housing Element update to the Policy Plan (general plan) component of The Ontario Plan ("TOP"), addressing State mandates, the 6th Cycle RHNA, and a modification to the Policy Plan Land Use Plan (Exhibit LU-01) to establish an Affordable Housing Overlay.
- (a) <u>Housing Element Update</u> The Housing Element is one of the seven mandated elements of the general plan, which each city and county in the State is required to periodically adopt (generally every eight years). The Housing Element is required to adequately plan to meet the existing and projected housing needs of all economic segments of the community. This Housing Element Cycle (6th Cycle) covers the RHNA period from October 15, 2021 through October 15, 2029. The Housing Element is the only element of the general plan reviewed and certified by the State to ensure compliance with State law.

The City of Ontario ("City") has initiated a process to update its Housing Element, one of the elements of the Policy Plan. The proposed 2021–2029 Housing Element Update constitutes a "project" pursuant to CEQA, and therefore, the City prepared an Addendum to The Ontario Plan Environmental Impact Report ("TOP EIR").

- (i) <u>Public Participation</u> The California Government Code requires that local governments make a diligent effort to solicit public participation from all economic segments of the community in the development of the draft Housing Element. The following provides a general timeline describing major milestones in preparing Ontario's draft 2021-2029 Housing Element Update:
 - June 14, 2021 Property Owner Open House Staff hosted a virtual open house to provide an overview of the TOP 2050 Project and land use designation changes on specific properties as part of the City's housing strategy to fulfill the 6th RHNA Cycle site inventory obligation.
 - June 28, 2021 Community Workshop #1 Staff hosted a virtual workshop to present the TOP 2050 update, including the City's housing strategy to receive comments, and answer questions from the public.

- July 29, 2021 Community Workshop #2 Staff hosted a virtual workshop to facilitate a guided discussion of the environmental justice, community health, and resiliency policies, and share the City's preferred land use plan and housing strategy. The intent of Community Workshop #2 was to further inform the community of policy updates and gather feedback regarding the preferred land use plan.
- August 31, 2021 In compliance with Senate Bill 18 notification requirements (Tribal Consultation), the City mailed a separate project notice to 16 local tribes identified by the Native American Heritage Commission (NAHC). No consultation requests were received during the requisite 90-day response period.
- October 14, 2021 City staff posted the Ontario Housing Element 2021-2029 -Public Review Draft on Ontarioplan.org/TOP2050 and submitted the draft to California Department of Housing and Community Development ("HCD") for a 60day review.
- (ii) <u>Regional Housing Needs Assessment (RHNA)</u> The Southern California Association of Governments ("SCAG") is responsible for oversight of the RHNA process for Southern California, which includes Ontario. For the 2021–2029 RHNA planning period, the City has been allocated 20,854 housing units to accommodate the estimated growth need at various income levels. As required by California law, the Housing Element must identify the City's ability to accommodate this estimated growth through available sites and appropriate zoning. Ontario's 6th RHNA Cycle allocation includes the following:

City of Ontario 6th Cycle RHNA Allocation by Income Category		
Income Category (Percent of Annual Mean Income)	RHNA Allocation	
Very Low (31 to 50)	5,640	
Low (51 to 80)	3,286	
Moderate (81 to 120)	3,329	
Above Moderate (121 or more)	8,599	
Total	20,854	

Source: SCAG 2021

The proposed 2021–2029 Housing Element must demonstrate the City's ability to accommodate the RHNA housing allocation either through production or the availability of properly zoned land that can accommodate additional growth. If adequate sites cannot be identified in the existing zoning, the City is required to rezone sites that can accommodate the remaining need at a minimum density of 30 dwelling units per acre, which is the default density set by HCD. The City is not required to build residential units in order to meet its RHNA allocation, only to identify potential sites and create the framework allowing the market the opportunity to develop these units.

The proposed 2021–2029 Housing Element Update explores opportunities to accommodate the RHNA through current production, entitled units, and the availability of properly zoned land that can accommodate additional growth. The proposed Housing Element Update will further identify and address potential and actual governmental and non-governmental constraints on the development of housing for all income levels.

- (iii) <u>State Mandates Included Within the Housing Element Update</u> The following provides a brief summary of recently adopted State laws that effect and were included in the 2021-2029 Housing Element Update. Their overarching premise is to increase the housing supply within the State by permitting more housing in more places and in greater volume.
 - SB 330 Housing Crisis Act: Prohibits the downzoning of any residential property without a corresponding increase in another location. Reduces the length of the entitlement process and provides greater certainty on development standards and fees. The offset upzoning must take place concurrently and provide no net loss in residential capacity.
 - SB 35 Streamlined Approval for Housing Projects: Expedites the construction of affordable housing in places were the permitting or construction of housing is slower than compared to the RHNA. If housing production of above moderate RHNA is below the annual target, projects with 10 percent low-income housing are eligible for streamlining. If housing production of low-income RHNA is below the annual target, projects with 50 percent low-income housing are eligible for streamlining.
 - SB 166 No Net Loss Zoning: Cities must build or maintain capacity for 100 percent of their RHNA requirement throughout the entire planning period.
 - AB 1397 Non-Vacant Sites: Provides for increased scrutiny on the development potential for underutilized sites included in the RHNA land inventory.
 - AB 72 Increased HCD Enforcement: HCD may revoke certification and report any violations to the Attorney General on a jurisdictions failure to comply.
 - AB 686 Housing Discrimination (Affirmatively Furthering Fair Housing ("AFFH")):
 Jurisdictions must facilitate deliberate action to address, combat, and relieve disparities resulting from past patterns of segregation to foster more inclusive communities.
 - AB 167 Housing Accountability Act: Prohibits reducing density, disapproving, or conditioning approvals of a lower/moderate income housing development project or emergency shelter if they comply with zoning and objective development standards.

File Nos.: PGPA21-004, PZC21-002 and PDCA21-001

December 20, 2021

(iv) <u>2021-2029 Ontario Housing Element Content</u> — California Housing Element law prescribes the scope and content of the Housing Element. Pursuant to Section 65583 of the California Government Code, the Housing Element must contain a variety of detailed analyses, listed below.

- Analysis of demographic, social, and housing characteristics; current housing needs; and future housing needs due to population and employment growth and change.
- Assessment of Fair Housing ("AFH") consistent with the core elements of the analysis required by the federal Affirmatively Furthering Fair Housing (AFFH) Final Rule of July 16, 2015.
- Analysis of governmental and nongovernmental constraints that affect the development, maintenance, and improvement of housing for all income groups and people with disabilities.
- Inventory of resources available to address the City's housing needs, including available land for housing, financial resources, and administrative capacity to manage housing programs.
- Evaluation of the accomplishments of current housing programs and specific programs to address the development, improvement, and conservation of housing to meet current and future needs.
- Documentation of public outreach for the Housing Element and the involvement of the public in shaping housing policies and programs for the 2021–2029 Housing Element.
- Housing goals, policies, and programs to address the production, maintenance, and improvement of housing for all economic segments of the community commensurate with its needs.
- The Housing Element encompasses all seven topical areas mentioned here, provides a brief synopsis of issues, and then follows with a complete set of goals, policies, and programs to be implemented over the planning period. The City also prepared a web format for ease of public distribution and use by policymakers and housing providers in implementing programs.

The Housing Element is prepared to be consistent with several policy and program plans mandated by the State. Most importantly, State law requires SCAG to determine the amount of housing needed within its six-county region and allocate a share of the regional housing need to each community. Housing Elements are required to incorporate

the estimates of housing need reflected in regional housing plans. The Housing Element is also consistent with the City's Consolidated Plan.

- (v) <u>HCD Review</u> The draft 2021-2029 Housing Element Update was submitted to HCD on October 14, 2021. HCD has 60-days to complete their review of a draft housing element and 90-days to review an adopted housing element. Comments from HCD on the draft Housing Element Update are expected to be received by December 13, 2021, which is after the date of this report being prepared; therefore, staff will provide an oral report on the status of HCD's review comments at the Planning Commission meeting.
- (b) Affordable Housing Overlay The proposed General Plan Amendment also proposes to modify the Policy Plan Land Use Plan (LU-01) to establish an Affordable Housing Overlay. The purpose of the Affordable Housing Overlay is to facilitate housing opportunities within the City through the implementation of the Housing Element. The Affordable Housing Overlay will only apply to those properties identified in Exhibits A Proposed AH, Affordable Housing Overlay and Exhibit B Revised LU-01 with AH, Affordable Housing Overlay; and those parcels listed in the Resolution of Approval for PGPA21-004, Attachment A: Housing Element Update, Appendix B: Housing Element Sites Inventory (Parcel List).

As shown in Exhibit B – Revised LU-01 with AH, Affordable Housing Overlay, the Affordable Housing Overlay will apply to those identified parcels generally located: (1) south of Riverside Drive, (2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and (3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue. As discussed in the RHNA analysis above (see Section (1)(a)(II)), the Affordable Housing Overlay is being established to ensure that there are adequate sites at sufficient residential densities to meet the City's RHNA housing obligation throughout the planning period.

Zone Change (PZC-21-002) — The proposed Zone Change will modify the Zoning Map to establish an Affordable Housing Overlay. The purpose of the Affordable Housing Overlay is to facilitate housing opportunities within the City through the implementation of Housing Element. The Affordable Housing Overlay will only apply to those properties identified in Exhibits A – Proposed AH, Affordable Housing Overlay and Exhibit C – Revised Zoning Map with AH, Affordable Housing Overlay; and those parcels listed in the Resolution of Approval for PGPA21-004, Attachment A: Housing Element Update, Appendix B: Housing Element Sites Inventory (Parcel List).

As shown in Exhibit C – Revised Zoning Map with AH, Affordable Housing Overlay, the Affordable Housing Overlay will apply to those identified parcels generally located: (1) south of Riverside Drive, (2) along East Holt Boulevard, between the Cucamonga Creek Channel and Corona Avenue, and (3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue. As discussed in the RHNA analysis above (see Section (1)(a)(II)), the Affordable Housing Overlay is being established to ensure that

there are adequate sites at sufficient residential densities to meet the City's RHNA housing obligation throughout the planning period.

(3) <u>Development Code Amendment (PDCA21-001)</u> — The Development Code (Ontario Municipal Code Title 9) provides the legislative framework for the implementation of The Ontario Plan, which states long-term principles, goals, and policies for guiding the growth and development of the City in a manner that achieves Ontario's vision, and promotes and protects the public health, safety, comfort, convenience, prosperity, and welfare of its citizens. Staff has initiated an alteration to the Development Code that is necessary to establish provisions of the Development Code, which are described below. The proposed changes, as they would appear in the Development Code, are included as Exhibit D (Proposed Development Code Amendment) of this staff report.

The proposed Development Code Amendment will modify Section 6.01.035, by adding Subparagraph C.6 to establish an AH (Affordable Housing) Overlay District. The purpose of the AH (Affordable Housing) Overlay District standards is to facilitate housing opportunities within the City through the implementation of Housing Element. The AH (Affordable Housing) Overlay District will only apply to those properties identified in Exhibit A – Proposed AH, Affordable Housing Overlay, Exhibit B – Revised LU-01 with AH, Affordable Housing Overlay, Exhibit C – Revised Zoning Map with AH, Affordable Housing Overlay; and those parcels listed in the Resolution of Approval for PGPA21-004, Attachment A: Housing Element Update, Appendix B: Housing Element Sites Inventory (Parcel List).

Generally, the AH (Affordable Housing) Overlay District will do three things:

- (a) It removes the specific plan requirement from sites where there is no existing specific plan.
- (b) It satisfies new state requirements for sites that are part of the rezoning program by:
- (i) Allowing by-right development of residential projects if 20 percent of units are affordable to lower incomes.
 - (ii) Establishing a minimum density of 20 du/ac.
- (c) It raises the maximum density on MDR zoned sites from 25 to 30 du/ac, if 25 percent of units are affordable to lower incomes.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of TOP. More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

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December 20, 2021

(1) <u>City Council Goals</u>.

- Invest in the Growth and Evolution of the City's Economy.
- Maintain the Current High Level of Public Safety.
- Operate in a Businesslike Manner.
- Pursue City's Goals and Objectives by Working with Other Governmental Agencies.
- Focus Resources in Ontario's Commercial and Residential Neighborhoods.
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities).
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities.
- Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony.

(2) <u>Vision</u>.

Distinctive Development:

- Commercial and Residential Development.
- > Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

Decision Making:

- <u>Goal G1</u>: Sustained decision-making that consistently moves Ontario towards its Vision by using TOP as a framework for assessing choices.
- ➤ <u>G1-2 Long-term Benefit</u>. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) <u>Policy Plan (General Plan)</u>.

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-1 Strategic Growth</u>. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.

File Nos.: PGPA21-004, PZC21-002 and PDCA21-001

December 20, 2021

➤ <u>LU1-6 Complete Community</u>. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).

Goal LU2: Compatibility between a wide range of uses.

Housing Element:

- <u>Goal H2</u>: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
- ➤ <u>H2-4 New Model Colony</u>. We support a premier lifestyle community in the Ontario Ranch distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
- ➤ <u>H2-5 Housing Design</u>. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.
- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.
- ➤ <u>H5-2 Family Housing</u>. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.
- ➤ <u>CE1-6 Diversity of Housing</u>. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business, and foster a balanced community.
- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
- ➤ <u>CE2-1 Development Projects</u>. We require new development and redevelopment to create unique, high-quality places that add value to the community.

File Nos.: PGPA21-004, PZC21-002 and PDCA21-001

December 20, 2021

➤ <u>CE2-2 Development Review</u>. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional, and sustainable places that will compete well with their competition within the region.

- ➤ <u>CE2-4 Protection of Investment</u>. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- ➤ <u>CE2-5 Private Maintenance</u>. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
- > <u>\$1-1</u> Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- <u>Goal CD1</u>: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.
- > <u>CD1-1 City Identity</u>. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.
- ➤ <u>CD1-2 Growth Areas</u>. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.
- ➤ <u>CD1-3 Neighborhood Improvement</u>. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.
- <u>Goal CD2</u>: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
- > <u>CD2-1 Quality Architecture</u>. We encourage all development projects to convey visual interest and character through:

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Building volume, massing, and height to provide appropriate scale and proportion;

- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- ➤ <u>CD2-2 Neighborhood Design</u>. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:
- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
 - Landscaped parkways, with sidewalks separated from the curb.
- ➤ <u>CD2-7 Sustainability</u>. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- ➤ <u>CD2-8 Safe Design</u>. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- ➤ <u>CD2-9 Landscape Design</u>. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create, and define public and private spaces, and provide shade and environmental benefits.
- ➤ <u>CD2-13 Entitlement Process</u>. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

HOUSING ELEMENT COMPLIANCE: The proposed 2021-2029 Housing Element Update is mandated by Sections 65580 to 65589 of the California Government Code. State Housing

Planning Commission Staff Report

File Nos.: PGPA21-004, PZC21-002 and PDCA21-001

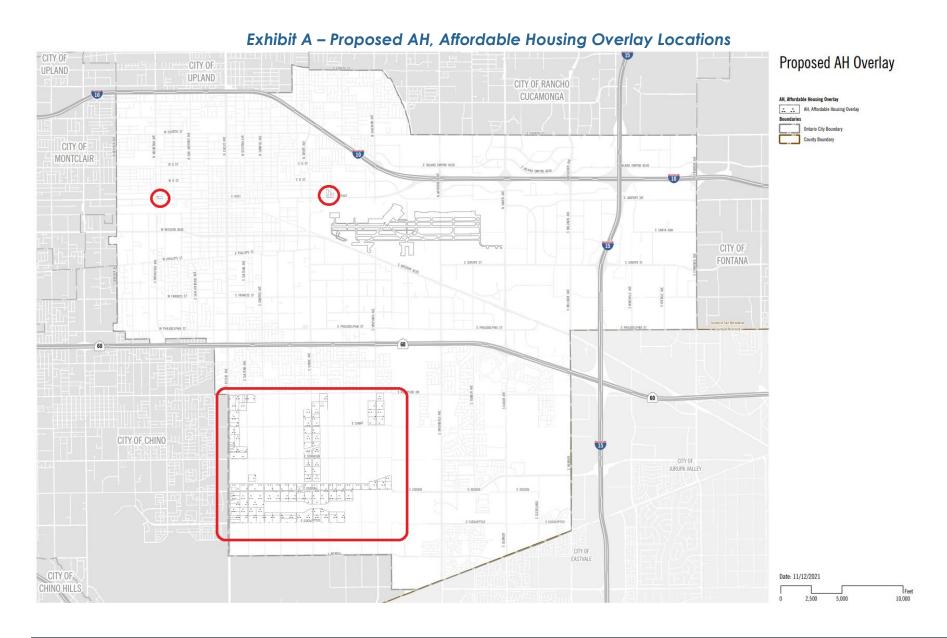
December 20, 2021

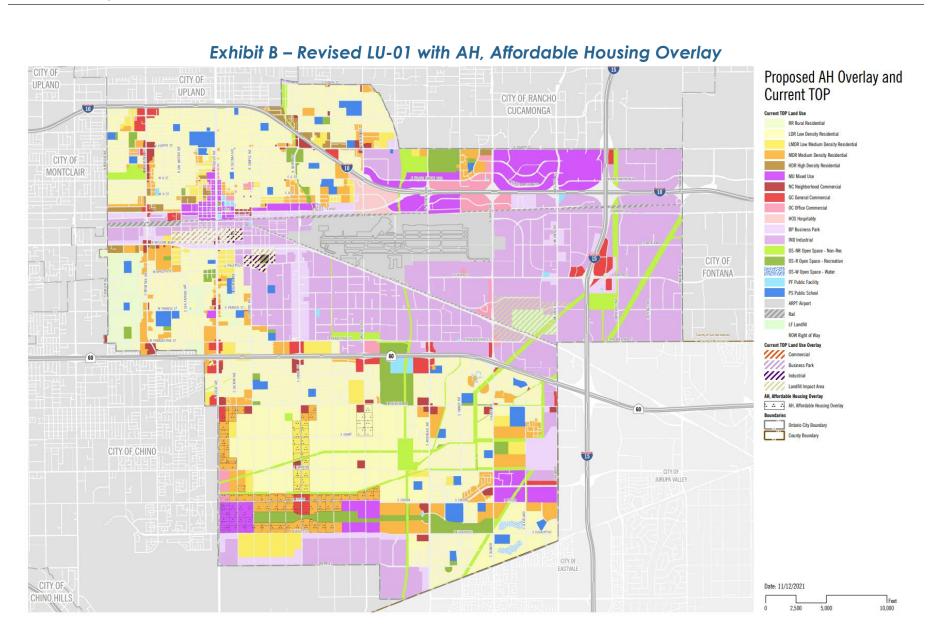
Element law requires that each city and county identify and analyze existing and projected housing needs within their jurisdiction and prepare goals, policies, and programs to further the development, improvement, and preservation of housing for all economic segments of their community commensurate with local housing needs.

AlrPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

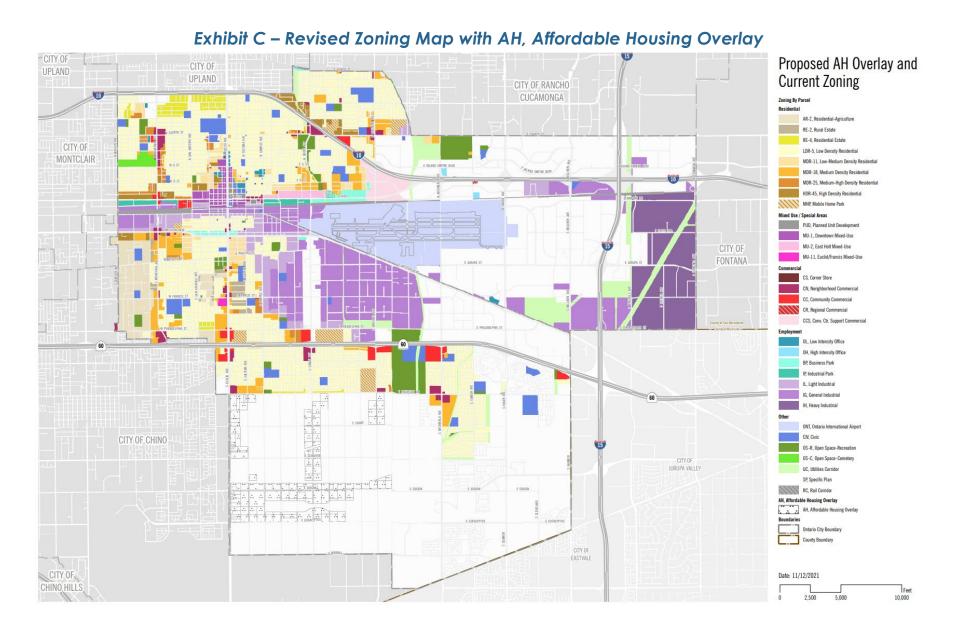
ENVIRONMENTAL REVIEW: The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a condition of project approval, as applicable, and are incorporated herein by this reference.

December 20, 2021





December 20, 2021



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Exhibit D – Proposed Development Code Amendment Draft DCA Section 6.01.035 (6. AH (Affordable Housing) Overlay District)

(Document follows this page)

Planning Commission Staff Report File Nos.: PGPA21-004, PZC21-002 and PDCA21-001 December 20, 2021

Division	4.01-	-District St	tandards	and Guidelines

Nonconforming Nonresidential Structure) of this Development Code and shall be developed consistent with the standards of the CC zoning district.

(3) New Residential Development. New residential development, which is proposed consistent with the Official Land Use Plan (Exhibit LU-01) of the Policy Plan component of The Ontario Plan, shall first require a zone change to the HDR-45 zoning district pursuant to Section 4.01.040 (Zone Changes) of this Development Code, and full compliance with the land use requirements, and development standards and guidelines of the HDR-45 zoning district.

(4) Protection of ICC Overlay District Land Use and Development Rights. All land use and development rights granted by the ICC Overlay District shall be transferable to any future owner(s) of property within the ICC Overlay District, and their assigns.

AH (Affordable Housing) Overlay District.

Q. Purpose. The purpose of the AH Overlay District is to facilitate housing opportunities within the community via implementation of required rezone programs pursuant to the City's adopted Housing Element where required for compliance with State Housing Element law.

b. Applicability. The herein established rights and responsibilities applicable to the AH Overlay District shall apply to sites identified in the housing inventory, part of the City's Housing Element, located 1) south of Riverside Drive, 2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and 3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue.

c. Definitions.

(1) Affordable Housing Project. Affordable housing projects for the purposes of the affordable housing overlay are defined as follows:

(a) Tier 1. Any projects with a residential component with at least 20% of total units are designated as affordable to lower incomes via deed restriction or another mechanism to guarantee affordability for a minimum of 30 years.

(b) Tier 2. Any projects with a residential component with at least 25% of total units are designated as affordable to lower incomes via deed restriction or another mechanism to guarantee affordability for a minimum of 30 years.

d. Development Standards. The development standards applicable to proposed development in the Affordable Housing Overlay District shall be in accordance with the applicable base zone, other overlay zones, and all applicable provisions of The Development Code, unless otherwise indicated herein, or where necessary to comply with Federal and State law.

(1) Minimum Density. Within the AH Overlay District, all new residential development, regardless of affordability, requires a minimum density of 20 dwelling units per acre. The minimum density shall be calculated as an average over the entire project area on sites south of Riverside Drive.

Ontario Development Code Page 6.01-83 (Rev. 12.01.2020)

Division 6.01—District Standards and Guidelines	S	
(2) Affordable Housi projects, as defined for the purposes of the aff within the AH Overlay <u>District</u> and are encourage	0 /-	nall be allowed by-right
(a) Specific F base zoning designation of SP may develope	Plan Base Zoning. Affordab as follows:	e housing projects with
(I) Tie	er 1 Affordable housing pro	iects:
ousing projects on properties designated a accordance with either the SP zoning district o	s MDR in the Policy Plan	
oroperties designated as MU in the Policy Plan P zoning district or a comparable existing zonin and intensity of the proposed project. The follo	may be developed in acc ng implementation tool tho	<mark>it aligns with the density</mark>
lensity allowed is equivalent to the maximum on the Land Use Designations Summary Table (Ontario Plan.	lensity identified for the app	
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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL ADOPT AN ADDENDUM TO THE ONTARIO PLAN ENVIRONMENTAL IMPACT REPORT FOR FILE NOS. PGPA21-004, PZC-21-002, AND PDCA21-001, FOR WHICH AN INITIAL STUDY WAS PREPARED, IN ACCORDANCE WITH THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, AND MAKING FINDINGS IN SUPPORT THEREOF.

WHEREAS, CITY OF ONTARIO (hereinafter referred to as "Applicant") has filed an Application for the approval of File Nos. PGPA21-004, General Plan Amendment, PZC-21-002, Zone Change, and PDCA21-001, Development Code Amendment, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, THE ONTARIO PLAN ("TOP") Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project, and no substantial changes in circumstances under CEQA Guidelines Section 15162(a)(2) have occurred since the certification of the 2010 EIR that would indicate new significant impacts or substantially increase the severity of significant impacts previously identified and background environmental conditions have not significantly changed since the certification of the 2010 Certified EIR. Additionally, no information that was not known and could not have been known at the time of the 2010 Certified EIR preparation has been revealed that shows new or substantially greater significant impacts would result under Section 15162(a)(3). There are no new or different mitigation measures that would substantially reduce one or more significant impacts of the Approved Project but that are not adopted. The Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent of supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project is on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: **Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written

and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan ("TOP") Environmental Impact Report— State Clearinghouse No. 2008101140 ("Certified EIR"), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File Nos. PGPA21-004, PZC-21-002, and PDCA21-001; and
- (2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (5) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- (6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Certified EIR, are incorporated herein by this reference.
- <u>SECTION 2</u>: **Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the

Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The Project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- <u>SECTION 3</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby finds that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.
- <u>SECTION 4</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.
- <u>SECTION 5</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

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The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File Nos. PGPA21-004, PZC-21-002, & P December 20, 2021 Page 6	DCA21-001
STATE OF CALIFORNIA COUNTY OF SAN BERNARDINO CITY OF ONTARIO	
City of Ontario, DO HEREBY CERTIFY	o Tempore of the Planning Commission of the that foregoing Resolution No was duly mmission of the City of Ontario at their special the following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A:

Addendum to The Ontario Plan Environmental Impact Report

(Addendum to follow this page)

November 2021 | Addendum to The Ontario Plan EIR State Clearinghouse No. 2008101140

2021–2029 Housing Element Update Addendum

City of Ontario

Prepared for:

City of Ontario

Contact: Thomas Grahn, Senior Planner 303 East "B" Street Ontario, California 91764 Tel: 909.395.2413

Prepared by:

PlaceWorks

Contact: Nicole Vermilion, Principal Sean Anayah, Associate II 3 MacArthur Place, Suite 1100 Santa Ana, California 92707 714.966.9220 info@placeworks.com www.placeworks.com

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1.1 PURPOSE OF AN ADDENDUM

This document is an Addendum to the previously certified Environmental Impact Report (EIR) (State Clearinghouse [SCH] No. 2008101140) for The Ontario Plan (TOP), which was certified on January 27, 2010, (referred to as the Approved Project). The purpose of this Addendum is to evaluate whether the proposed update to the Housing Element would modify the Approved Project in such a way as to result in new environmental impacts or a substantial increase in the severity of previously identified significant effects, or would otherwise trigger a need for subsequent environmental review.

TOP consists of a Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plans, and Tracking and Feedback:

- The Ontario Vision describes the future community of Ontario. Its basic purpose is to improve the quality of life for the people of Ontario. It is the rationale and motivation for everything the City does.
- The Governance Manual describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how TOP is a tool for decision making and communication.
- City Council Priorities define the short-term direction in City actions and initiatives. They are the primary
 means for exercising leadership in carrying out The Plan and realizing the Vision.
- The Policy Plan connects intent with action through the broad range of Goals and Policies that would guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan.
- **Implementation** consists of actions taken to carry out Plan policies. This includes initiatives by the City and decisions on public and private development projects.
- Tracking and Feedback allows the City to learn from experience and redirect efforts.

The primary purpose of TOP is to integrate components of City governance documents into a single guidance system that shapes the community 20 years or more into the future.

The City of Ontario released a Draft EIR in April 2009 for TOP. A Recirculated Draft EIR for TOP was released in November 2009 to update and provide additional analysis concerning greenhouse gas (GHG) emissions impacts associated with buildout of the Policy Plan. The 2009 Draft EIR, the 2009 Recirculated Draft EIR, and the 2010 Final EIR are collectively referred to as the 2010 Certified EIR.

The 2010 Certified EIR, in conjunction with this Addendum, serve as the environmental review for the proposed modifications to the Approved Project, as required by the California Environmental Quality Act (CEQA), Public Resources Code Sections 21000 et seq., and the State CEQA Guidelines (14 California Code of Regulations §§ 15000-15387). This Addendum evaluates the potential environmental impacts associated with TOP as modified by the update to the Housing Element.

1.1.1 Environmental Procedures

Pursuant to CEQA and the State CEQA Guidelines, this Addendum focuses on whether implementation of the Proposed Project would require major revisions to the Certified EIR due to the potential for new significant environmental effects or a substantial increase in the severity of previously identified significant effects, pursuant to State CEQA Guidelines Section 15162.

Pursuant to Section 21166 of CEQA and Section 15162 of the State CEQA Guidelines, when an EIR has been certified or a negative declaration adopted for a project, no subsequent or supplemental EIR or negative declaration shall be prepared for the project unless the lead agency determines that one or more of the following conditions are met:

- Substantial changes are proposed in the project which will require major revisions of the
 previous EIR or negative declaration due to the involvement of new significant
 environmental effects or a substantial increase in the severity of previously identified
 significant effects;
- (2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- (3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
 - (A) The project will have one or more significant effects not discussed in the previous EIR or negative declaration;
 - (B) Significant effects previously examined will be substantially more severe than shown in the previous EIR;
 - (C) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or
 - (D) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative. (CEQA Guidelines Section 15162[a])

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A supplement to an EIR (supplemental EIR), which is narrower in scope than a subsequent EIR, may be prepared if any of the above criteria apply, but "[o]nly minor changes or additions would be necessary to make the previous EIR adequately apply to the project in the changed situation" (CEQA Guidelines Section 15163(a)). In the absence of the need to prepare either a subsequent or supplemental EIR, an addendum to a previously certified EIR may be prepared. Section 15164 states:

- (a) The lead agency or a responsible agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary but none of the conditions described in Section 15162 calling for preparation of a subsequent EIR have occurred.
- (b) An addendum to an adopted negative declaration may be prepared if only minor technical changes or additions are necessary or none of the conditions described in Section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred.
- (c) An addendum need not be circulated for public review but can be included in or attached to the final EIR or adopted negative declaration.
- (d) The decision making body shall consider the addendum with the final EIR or adopted negative declaration prior to making a decision on the project.
- (e) A brief explanation of the decision not to prepare a subsequent EIR pursuant to Section 15162 should be included in an addendum to an EIR, the lead agency's findings on the project, or elsewhere in the record. The explanation must be supported by substantial evidence. (CEQA Guidelines Section 15164)

This Addendum to the previously certified 2010 Certified EIR for the Approved Project has been prepared because the evaluation of the Housing Element Update, also known as the Proposed Project, has not indicated any of the circumstances requiring a subsequent or supplemental EIR. As demonstrated in Section 5 of this Addendum, the Proposed Project would not result in impacts that differ from the Approved Project, and it would not trigger the need for preparation of a subsequent or supplemental EIR under the criteria in Sections 15162(a) and 15163(a). The Proposed Project is consistent with the general plan update and would not require changes to the Approved Project. This Addendum demonstrates that no substantial changes are proposed to the Approved Project or have occurred in the City that would require major revisions to the Certified EIR or substantially increase the severity of previously identified significant effects. Thus, the impacts of the Proposed Project are within the levels and types of environmental impacts disclosed in the Certified EIR.

1.1.2 Scope and Analysis for this Addendum

This Addendum reviews the changes proposed by and analyzes the potential impacts of the Proposed Project and any changes to the existing conditions that have occurred since the City certified the EIR. It also reviews any new information of substantial importance that was not known and could not have been known with exercise of reasonable diligence at the time that the City approved the certified EIR. It further examines whether, as a result of any changes or any new information, a subsequent or supplemental EIR may be required.

This examination includes an analysis of the provisions of Section 21166 of CEQA and Section 15162 of the CEQA Guidelines and their applicability to the Proposed Project. This Addendum relies on use of the Environmental Analysis, which addresses environmental checklist issues section by section.

The Proposed Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project, and no substantial changes in circumstances under Section 15162(a)(2) have occurred since the certification of the 2010 EIR that would indicate new significant impacts or substantially increase the severity of significant impacts previously identified. The background environmental conditions have not significantly changed since the certification of the 2010 Certified EIR. The City of Ontario has received no information indicating a substantial change in any circumstances that would result in a new or substantially greater significant impact.

In addition, no information that was not known and could not have been known at the time of the 2010 Certified EIR preparation has been revealed that shows new or substantially greater significant impacts would result (see CEQA Guidelines Section 15162[a][3]). There are no new or different mitigation measures that would substantially reduce one or more significant impacts of the Approved Project but that are not adopted. The Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR.

Since this Addendum does not identify new or substantially greater significant impacts, circulation for public review and comment is not necessary (CEQA Guidelines Section 15164[c]). However, the Ontario City Council will consider this Addendum at a public meeting prior to the adoption of the Housing Element (CEQA Guidelines Section 15164[d]). If the Ontario City Council approves this Addendum, it shall be required to make findings by way of a resolution, including a finding that this Addendum provides the basis and substantial evidence for the decision not to prepare a subsequent or supplemental EIR (CEQA Guidelines Section 15164[e]).

1.2 PREVIOUS ENVIRONMENTAL DOCUMENTATION

This Addendum relies on environmental analysis in the 2009 Draft EIR and 2010 Final EIR (which includes the 2009 Draft EIR and the 2009 Recirculated Draft EIR). These three documents, while discussed separately here, are collectively referred to in this Addendum as the 2010 Certified EIR. In accordance with CEQA Guidelines Sections 15148 and 15150, this Addendum incorporates the 2010 Certified EIR (and its constituent parts) by reference. A summary of the 2010 Certified EIR and how it relates to this Addendum is provided below. All documents incorporated by reference are available for review at the City of Ontario Community Development Department at 303 East B Street, Ontario, CA, 91764.

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1.2.1 2009 Draft EIR for The Ontario Plan

The City of Ontario circulated the 2009 Draft EIR for public review in April 2009, and 17 topics were evaluated in detail.

Ten impacts were considered less than significant without incorporating mitigation:

- Aesthetics
- Biological Resources
- Geology and Soils
- Hazards and Hazardous Materials
- Hydrology and Water Quality
- Land Use and Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation

One environmental category was identified as having potentially significant impacts that could be mitigated, avoided, or substantially lessened.

Utilities and Service Systems

Six environmental categories were considered to have significant and unavoidable impacts that could not be alleviated by incorporating mitigation.

- Agricultural Resources
- Air Quality
- Cultural Resources
- Global Climate Change
- Noise
- Traffic and Transportation

1.2.2 2010 Final EIR for The Ontario Plan

A Recirculated Draft EIR for TOP was released in November 2009 to update and provide additional analysis concerning GHG emissions impacts associated with buildout of the Policy Plan. This analysis was based on verbal comments made by the California Attorney General's Office after the end of the public review period and on recent rules and regulations about lowering GHG emissions.

Pursuant to Section 15088.5(c) of the CEQA Guidelines, which states that if an EIR revision is "limited to a few chapters or portions of the EIR, the lead agency need only recirculate the chapters or portions that have been modified," only the following topic areas were analyzed in the 2009 Recirculated Draft EIR:

- Global Climate Change
- Additional Project Alternative: 15 percent GHG Reduction Alternative

Remaining topics previously analyzed in the 2009 Draft EIR (see Section 1.3.1) were determined to be adequately addressed in that document. Analysis in the 2009 Recirculated Draft EIR found that significant and unavoidable impacts identified in the 2009 Draft EIR would remain significant and unavoidable for the Approved Project. These determinations were reiterated in the 2010 Final EIR when it was certified on January 27, 2010.

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Environmental Setting

2.1 PROJECT LOCATION

The City of Ontario is in the southwestern corner of San Bernardino County and is surrounded to the west by the Cities of Chino and Montclair and unincorporated areas of San Bernardino County; to the north by the Cities of Upland and Rancho Cucamonga; to the east by the City of Fontana and unincorporated land in San Bernardino County; and to the south by the Cities of Chino, Eastvale, and Jurupa Valley (see Figure 1, Regional Location). The City is in the central part of the Upper Santa Ana River Valley. This portion of the valley is bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash to the east.

The City comprises approximately 50 square miles (31,958 acres), which include the 8,200-acre Ontario Ranch in the southern portion of the City (formerly the City's sphere of influence). The northern urbanized portion of the City is known as the Original Model Colony. The City is generally bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south (see Figure 2, *Aerial Map*). Regional circulation to and through the City is provided by I10 and State Route 60 (SR60) east-west, and by I15 and SR83 (Euclid Avenue) north-south. The City is also home to the Ontario International Airport (ONT).

2.2 ENVIRONMENTAL SETTING

2.2.1 Existing Land Use

Ontario has developed from a small agricultural town centered mainly on the citrus industry to a suburban community with a large manufacturing and industrial base. The majority of the land in Ontario is developed as residential; industrial and business park; or other land uses, including the ONT and recreational and nonrecreational open space.

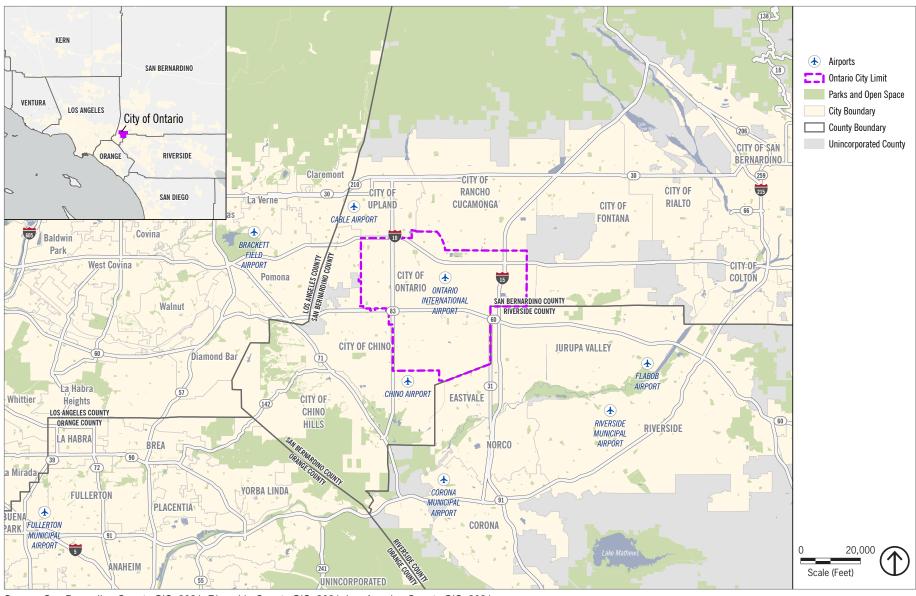
2.2.2 Surrounding Land Use

The City of Ontario is surrounded by developed urban areas and small areas of agriculture. The San Gabriel Mountains lie to the north, just beyond Upland and Rancho Cucamonga.

2. Environmental Setting

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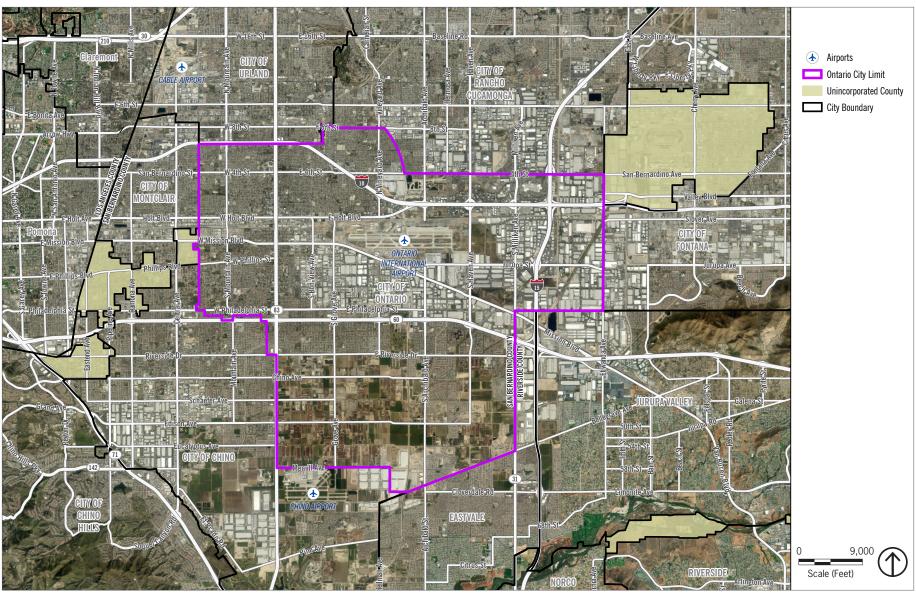
Source: San Bernadino County GIS, 2021; Riverside County GIS, 2021; Los Angeles County GIS, 2021; Orange County GIS, 2021; and PlaceWorks, 2021.

Figure 1 Regional Location

2. Environmental Setting

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Source: San Bernadino County GIS, 2021; Riverside County GIS, 2021; Los Angeles County GIS, 2021; Orange County GIS, 2021; and PlaceWorks, 2021.

Figure 2 Aerial Map

2. Environmental Setting

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3.1 BACKGROUND

The 2010 Certified EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by buildout of the Proposed Land Use Plan, within the Policy Plan, and impacts from the resultant population and employment growth in the City. The 2010 Certified EIR evaluated a buildout of 104,052 dwelling units, 358,355 people, and 330,023 jobs. Growth associated with TOP is not linked to a timeline. However, buildout of the Proposed Land Use Plan was forecast for the year 2035 in the 2010 Certified EIR. For the residential sector, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. As identified in the 2010 Certified EIR, implementation of TOP would result in a population increase of 110 percent, or a 4.3 percent annual population increase.

3.2 PROPOSED PROJECT

The City of Ontario (City) has initiated a process to update its Housing Element, one of the required elements of the City's general plan (TOP). The proposed 2021–2029 Housing Element Update constitutes a "project" pursuant to CEQA, and therefore the City is preparing an Addendum to TOP EIR for the Housing Element Update.

3.2.1 City of Ontario 6th Cycle RHNA Allocation

The Southern California Association of Governments (SCAG) is responsible for oversight of the Regional Housing Needs Assessment (RHNA) process for Southern California, including Ontario. Per the RHNA for the 2021–2029 planning period, the City has been allocated 20,854 units to accommodate the estimated growth need at various income levels. As required by California law, the Housing Element must identify the City's ability to accommodate this estimated growth through available sites and appropriate zoning. Table 1, City of Ontario 6th Cycle RHNA Allocation by Income Category, designates the housing allocation by income category in the City (SCAG 2021).

Table 1 City of Ontario 6th Cycle RHNA Allocation by Income Category

Income Category (Percent of Annual Mean Income)	RHNA Allocation
Very Low (31 to 50)	5,640
Low (51 to 80)	3,286
Moderate (81 to 120)	3,329
Above Moderate (121 or more)	8,599
Total	20,854

Source: SCAG 2021

According to the 2020 Census, the population of Ontario had grown to 185,010 with 50,621 households in July 1, 2019 (U.S. Census Bureau 2019). According to the 2010 Certified EIR, the population and household projections for 2020 were 246,304 and 65,872, respectively. The 2010 Certified EIR projected a total household of 91,936 by 2035, while SCAG's Connect SoCal (2020–2045 Regional Transportation Plan (RTP)/Sustainable Communities Strategy (SCS) projects total households at 74,500 by 2045 (SCAG 2020). The 2010 Certified EIR determined that the City of Ontario is projected to grow by 1,531 housing unit per year, which would result in an additional 22,965 housing units by 2035. Therefore, because the Proposed Project would result in a total of 20,854 residential units, it would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project, and no substantial changes in circumstances under Section 15162(a)(2) have occurred since the certification of the 2010 EIR that would indicate new significant impacts or substantially increase the severity of significant impacts previously identified. The background environmental conditions have not significantly changed since the certification of the 2010 Certified EIR. The City of Ontario has received no information indicating a substantial change in any circumstances that would result in a new or substantially greater significant impact.

The proposed 2021–2029 Housing Element must demonstrate the City's ability to accommodate the RHNA housing allocation either through production or the availability of properly zoned land that can accommodate additional growth. If adequate sites cannot be identified in the existing zoning, the City is required to rezone sites that can accommodate the remaining need at a minimum density of 30 dwelling units per acres, which is the default density set by the California Department of Housing and Community Development (HCD). The City is not required to build residential units in order to meet its RHNA allocation, only to identify potential sites and create the framework to allow the market the opportunity to develop these units. The proposed 2021–2029 Housing Element Update explores opportunities to accommodate the RHNA through current production, entitled units, or the availability of properly zoned land that can accommodate additional growth. The proposed Housing Element Update will further identify and address potential and actual governmental and non-governmental constraints on the development of housing for all income levels. Proposed zoning amendments are included in Appendix B: Housing Element Sites Inventory (Parcel List) of the Housing Element.

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Per state housing law, the City of Ontario and other jurisdictions within the SCAG region are subject to a deadline of February 11, 2022 (October 15, 2021, plus a 120-day grace period) to adopt the update. State housing law also gives HCD the authority to review housing element compliance with all relevant laws. Upon confirmation of the documents' compliance, HCD issues a letter to this effect; at this point, the document is said to be "certified." By electing to not prepare a housing element for the 2021 to 2029 cycle, the City would experience several direct consequences, including reduced time period to complete any necessary zone changes from three years to one year and reduced eligibility for certain grant funds. It would also expose the City to potential litigation and associated loss of local land use control or financial costs. Due to this deadline, the Housing Element Update will be adopted prior to the comprehensive update of the TOP. The TOP update will include Policy Plan changes necessary to maintain consistency with the proposed housing strategy. The Rezoning program will be implemented by Oct 2022 per State law, but parts of the rezoning may be done before, concurrent, or after TOP updates are adopted. Therefore, this Addendum to the 2010 Certified EIR will be adopted prior to the update of the TOP.

3.2.2 Housing Element Program Changes

CEQA requires the City to evaluate the environmental impacts associated with changes to the environment. In this instance, most of the goals, policies, and programs from the previous Housing Element and Community Safety Element are continued through to the proposed 2021–2029 Housing Element and 2021 Community Safety Element. Table 2, Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element, includes the changes in goals or policies between the existing 2013–2021 Housing Element and the proposed 2021–2029 Housing Element.

The City's proposed rezoning program is outlined in Program 13 of the Housing Element. Program 13 would create an affordable housing overlay that expands the land use categories that can accommodate at least 30 dwelling units per acre, exempt projects south of Riverside Drive with at least 25 percent of units affordable to lower incomes from specific plan requirements if no specific plan exists, and revises existing specific plans to allow at least 30 dwelling units per acre on identified sites. Each of these changes would play a key role in fulfilling the City's lower-income RHNA and would ensure adequate sites are available for higher-density development. Other changes to the development code, such as lot consolidation programs, are also outlined in Chapter 9, Programs.

Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
Neighborhoods and Housing	
1. Code Enforcement	Continue.
Code compliance is an important tool to ensure that the value, character, and quality of neighborhoods, property, and housing are well maintained. Listed below are the programs implemented by the Code Enforcement program specifically designed to improve the quality of Ontario neighborhoods and eliminate health and safety related to building conditions:	
 General Code Enforcement: The City utilizes an interdepartmental approach for inspecting properties for compliance with state and local regulations regarding the condition and maintenance of residential buildings and properties. If deficiencies are found, the property owner is notified of the code deficiency and compliance measures required, and 	

Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Element	
Program	Continue/ Modify/ Delete
the property owner is granted a period of time to correct the matter. To facilitate timely compliance, City staff direct the property owners to City–administered rehabilitation loans and/or other nonprofit housing loan programs, where available.	
 Community Improvement Team: This team has been specifically designed to proactively implement an intensive code compliance program to address serious code violations within focus areas. As part of this team approach, various City departments work together to bring a myriad of resources to the focus area to arrest neighborhood decline and improve the living conditions within the area. 	
 Systematic Health and Safety Inspection Program: The program is designed to ensure the quality of the rental stock and reduce substandard building conditions. Through this program, all rental housing units over seven years old are inspected on a four-year schedule unless it is necessary to inspect more frequently due to substandard conditions. 	
 Abandoned and Distressed Property Program and Foreclosure Opportunities Response Team (FORT) Program: These programs were established to protect Ontario neighborhoods from becoming blighted through the lack of adequate maintenance and security of abandoned and distressed properties. 	
2. Quiet Home	Delete.
Residential neighborhoods located directly west of Ontario International Airport experience high noise levels. In the early 1990s, the Federal Aviation Administration, City of Los Angeles, and City of Ontario created a program to improve the quality of life in noise-impacted neighborhoods and community/airport compatibility. Eligible homes are outfitted with sound insulation to reduce the interior noise levels to 45db CNEL. The second component consists of the voluntary acquisition of eligible properties and reuse of properties in a manner compatible with the airport.	
Eligibility is restricted to properties located within the noise contour map. Currently, the Los Angeles World Airport is updating the Part 150 Study, which may impact the eligibility area. The study is anticipated to be completed within 2014–2015.	
3. Historic Preservation	Continue.
Known as the Original Model Colony, Ontario is rich in local history. The City operates a comprehensive historic preservation program. It is a certified local government, a designation that signifies that the City's program meets state and federal historic preservation standards. The City has six historic districts and is surveying nine additional areas for the potential of historic district designation. It encourages historic preservation efforts through Mills Act contracts, surveys of potentially historic structures, and an adaptive reuse program (for the Emporia District and Downtown). The City also implements an award-winning web-based historical resource management system that catalogs local historical resources and eventually offers interface capacities for the public to search the database.	
4. Housing Rehabilitation Loan and Grants	Modify.
When funding is available, the City offers housing rehabilitation loans and grants to qualified homeowners. Due to the State elimination of redevelopment funding and recent federal funding cutbacks, the City of Ontario is not currently able to provide owner-occupied rehabilitation programs. Should funding become available, the City will re-establish this program and provide associated quantified objectives.	
5. CARES	Continue and
The City of Ontario implements the comprehensive Coronavirus Aid, Relief, and. Economic Security (CARES) Act Neighborhood Revitalization Program within selected focus neighborhoods. The components of this comprehensive, multiagency program include code enforcement, arterial street improvement, relief program, exterior improvement program, and sidewalk or safe routes to school program. The program seeks to stabilize neighborhoods through a comprehensive approach to building community. The program has several components:	modify.
• Single-Family Improvement Loans. The City offers two low-interest deferred loan programs for homeowners (with a one- to five-year deferment) to make exterior improvements to their home.	
 Neighborhood Improvements. The City improves streets (e.g., resurfacing, replacing curb and gutter, improving sidewalks and drainage), plants trees and greenways, and enforces codes. 	

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Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
6. Neighborhood Plans Ontario's neighborhoods define the sense of identity and community for residents, the quality of life experienced, and the image and role of Ontario. The City currently implements many programs to improve neighborhoods. However, the City has identified a need to foster a stronger sense of neighborhood identity in the community. While this goal is being achieved in CDBG-eligible areas (CARES program) and in historic areas, efforts need to be expanded to other neighborhoods. During the planning period, the City will begin a public outreach effort to solicit input from neighborhood leaders and residents as to particular needs and goals. This process may result in the establishment of ongoing dialog with the City, neighborhood organizations, or the preparation of neighborhood improvement plans.	Continue and modify.
7. Neighborhood Stabilization The Dodd-Frank Wall Street Reform and Consumer Protection Act of 2010 provided an additional \$1 billion for the Neighborhood Stabilization Program (NSP) that was originally established under the Housing and Economic Recovery Act of 2008. HUD awarded grants to 270 states and selected local governments to mitigate the negative impact of the nation's economic decline and housing market collapse and to stabilize and revitalize communities/areas hit the hardest. The City of Ontario was provided an allocation of \$1,872, 853 in NSP3 funds. The City will utilize these funds (1) to acquire, rehabilitate, and resell single-family homes; (2) to acquire and rehabilitate multiple-family properties; (3) to provide financial assistance; (4) to establish land banks; (5) to demolish blighted structures; (6) to redevelop demolished or vacant properties; and (7) for administration (capped at 10 percent).	Modify.
8. Community-Oriented Policing The City of Ontario Police Department uses CDBG funds to implement a community-oriented policing program in designated low- and moderate-income neighborhoods. This partnership involves working with community leaders, businesses, and property owners to address neighborhood issues including code enforcement, crime-free multi-family housing, safe and clean streets, and school interventions. With respect to housing, the Police Department implements the Crime-Free Multifamily Housing Program to control and eliminate crime in apartment buildings. Under this program, the Police Department will provide training to apartment owners, conduct a property inspection to identify and eliminate potential crime hazards, and certify properties where the owner signs a written agreement and commitment to maintain the program.	Continue.
Housing Supply and Diversity	
9. Downtown Plan Ontario's Downtown covers 12 blocks along Euclid Boulevard. In 1983, the City adopted the Center City Redevelopment Project area to encourage development of a high-intensity, multiuse central business district and surrounding neighborhoods that maximize the productivity of commercial areas and housing opportunities. The \$200 million Town Center Square project will provide a mix of housing, educational, retail, office, and government uses that will stimulate the renewal of Downtown. Although the General Plan redesignates a majority of the area for new housing and mixed uses, a comprehensive planning process is necessary to ensure the sensitive integration of new housing, commercial uses, open space, pedestrian paths, and transportation into the fabric of Downtown.	Continue.
10. Mountain and Euclid Corridors Euclid Avenue and Mountain Avenue extend the entire length of Ontario. In recent years, developers have expressed interest in building residential and commercial projects along these corridors. Mountain Avenue has had numerous senior and affordable housing projects built adjacent or near to the corridor, and developers have begun to show interest in Euclid Avenue. Both corridors have commercial property that is proposed for redesignation as residential. To facilitate corridor development, the City will redesignate properties along Euclid Avenue and Mountain Avenue for medium- and high-density residential development as shown on the Official Land Use Plan (LU-01). The City will also develop a lot-consolidation ordinance to incentivize the assemblage of parcels. Incentives may include fee modifications, flexibility in design, expedited permit processing, or others.	Modify.
11. Holt Boulevard Holt Boulevard is one of the original corridors paralleling the railroad and extending through Ontario and neighboring communities. With the development and success of commercial uses fronting the freeways, the commercial viability of Holt Boulevard has gradually eroded, leaving a significant number of underutilized uses on small parcels. The General	Modify.

Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
Plan has declared Holt Boulevard as a focus area for mixed uses, both perpendicular to Mountain Avenue, at the base of Downtown, and in the East Holt Boulevard Study Area. To stimulate investment in these areas, the City will adopt a lot consolidation ordinance and incentives to encourage the recycling of land to residential uses. The City will also explore the use of density incentives to encourage mixed-use development, offering higher densities for quality projects of a certain size.	
12. New Model Colony	Continue and
The New Model Colony covers 8,200 acres of the former San Bernardino Agricultural Preserve. This area is intended to provide a range of housing opportunities for the City's emerging regional and national employment centers. Buildout of this area is contingent on completion of infrastructure, approval of specific plans, and cancellation of Williamson Act contracts. The City has entered into an agreement with a consortium to fund \$430 million in infrastructure serving the eastern New Model Colony. Many specific plans for this area have been approved. Some of the original Williamson Act contracts will also expire during the planning period. The General Plan has designated much of the area for medium- and high-density residential and mixed use. Although development is not expected to occur during the planning period, the City will continue to process specific plan applications and work with developers to address outstanding issues, in particular the financing of infrastructure in the western New Model Colony.	modify.
13. Downtown Core Catalyst Project	Deleted.
The City of Ontario has embarked on a strategy for a large-scale undertaking that would act as the catalyst for the resurgence of Downtown Ontario. The City of Ontario was awarded one of only 13 prestigious Catalyst awards from the State of California in 2010 for efforts to revitalize downtowns through this strategy. Upon completion of all of the activities included in the Downtown Core Catalyst, 519 housing units will be developed.	
14. Design Review	Continue and
The City implements a design review program to ensure quality housing, maintain property values, stabilize neighborhoods, and improve quality of life. For standard projects, the City's Residential Design Guidelines provide objective standards and graphics to illustrate the preferred methods of planning, neighborhood design, and construction for subdivisions, open space and landscaping, lots and buildings, architecture, and other aspects. For certain infill projects in the Downtown or other focus areas of the community, the City may adopt a PUD ordinance or Planned Residential Development Overlay to provide for more flexibility in design. Specific plans provide another means to address the design of large-scale projects. The General Plan includes a Community Design Element that provides unifying and broader principles of community design.	modify.
15. Green Building	Continue.
Green building means creating structures and using materials that are environmentally responsible and resource efficient, considering a building's entire life cycle. To reduce per capita energy use, the City will promote conservation and renewable energy generation techniques in public facilities and private development. The City will require new construction to reduce energy demand by incorporating building and site design strategies. Conservation will be the priority strategy for renovation of existing facilities. The General Plan also includes land planning strategies that impact energy demand reduction, including narrowing street widths, installing broad-canopied trees for shade, and clustering compact development to reduce automobile use.	
16. Land Monitoring Program to Meet the RHNA	Continue and
The City is in the process of updating the Development Code for consistency with the Land Use designations of The Ontario Plan. This program will implement a land monitoring program to ensure that the City has enough land to meet its Regional Housing Needs Allocation, throughout the planning period. The City has identified 83 acres to be rezoned to allow development to occur at a density of 25–45 dwelling units per acre. This program will ensure that the proposed sites are rezoned to appropriate densities and identify additional sites to be rezoned if any of the proposed sites cannot be rezoned. All rezoned sites will permit owner-occupied and rental multi-family developments by right and will not require a conditional use permit, a planned unit development permit, or any other discretionary review. All sites will accommodate a	modify.
minimum of 20 units per acre and at least 16 units per site, per state law requirements. In addition, the City will ensure that at least 50% of its lower- income RHNA shortfall is accommodated on sites designated for exclusively residential uses.	

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Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
Governmental Regulations/Constraints	
17. Incentives The City of Ontario offers several different types of incentives to facilitate housing production, including:	Continue.
Financial Incentives: The City makes available financial incentives that meet certain criteria. For instance, impact fee reductions are allowed for projects built in the Downtown. The City is financially assisting a variety of nonprofit organizations to provide senior housing, housing for homeless people, and other services. Density bonuses allowed for qualified projects work as a financial incentive by increasing the revenue stream of projects. The City also has established its Community Housing Development Organization (CHDO) program to leverage the nonprofit sector resources with available HOME CHDO funding. The intent of the CHDO funding is to work with nonprofit CHDOs to help preserve, enhance, and improve existing neighborhoods through acquisition, rehabilitation, and/or new housing construction activities. Finally, the City continues to grant low-cost leases (e.g., \$1 per year leases) to qualified organizations to provide senior housing and homeless housing. These types of financial incentives will be provided to allow the City to meet its community development and housing objectives.	
Regulatory Incentives: The regulatory incentive program is intended to realize improved value, a rich palette of amenities, landmarks, and identifiable places. While the underlying land use designations still apply, the City may offer various incentives through a discretionary permit. Special incentives may be granted for mixed-use developments, residential infill projects near transit facilities, the replacement of underperforming commercial uses with new residential use, the improvement and/or intensification of existing, mid-block residential uses, or lot consolidation and development of desired projects. The menu of incentives may include density transfers, modifications in development standards, increased residential density, and other incentives to be negotiated on a case-by-case basis.	
18. Land Acquisition Land acquisition for residential development is perhaps one of the greatest challenges to creating affordable housing. Over the past five years, the City of Ontario has seen increasing land prices. To facilitate the development of affordable housing, the City has actively purchased land and made it available at a low cost (typically a \$1 per year lease) to affordable housing developers and nonprofit agencies to create affordable senior housing, emergency shelters, affordable attached ownership projects, and other affordable housing projects. As situations merit and projects are proposed that meet the City's housing goals and the public interest, the City of Ontario will continue to acquire residential land that can be leased or sold at below-market rates for the production of affordable housing.	Continue.
19. Planned Unit Development (PUD) Within an established suburban fabric, there are considerable challenges to creating affordable housing. As development standards and lot standards change over time, it is not uncommon to have irregularly shaped and nonconforming parcels that are simply not conducive to redevelopment. The City has adopted a Planned Unit Development Ordinance that permits a variety of housing types in every residential zone. The City may conditionally permit attached and detached single-family residences, town homes, patio homes, zero lot line, and any other type of housing product permitted by the regulations of the underlying zone. The PUD is a tool that has been successfully used for Town Square to encourage and facilitate innovative design, variety, and flexibility in the types of housing products, including the provision of affordable housing, that would otherwise not be allowed or possible through standards in the underlying zoning districts.	Continue.
20. Mixed-Use and High-Density Residential Zone and Standards The General Plan directs significant housing growth to mixed-use areas. These areas include the Downtown, Euclid Avenue, the I-10 Corridor, the New Model Colony, and Holt Boulevard. These mixed-use areas each have a distinct mix of land uses and density ranges (see Policy Plan Land Use Exhibit LU-11, Land Use Designation Summary Table). To facilitate the development of quality housing and exemplary design, the City will create mixed-use zoning and development standards allowing up to 125 units per acre and a high-density residential zone and standards allowing 25 to 45 units per acre. The parameters of the ordinance have yet to be designed; however, the intent of the ordinance is to facilitate high-density housing. In both these zones, high-density residential and mixed use will be allowed by right.	Continue and modify.

Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
Housing Assistance	·
21. Public Housing The Housing Authority of the County of San Bernardino administers the Housing Voucher rental program for the City of Ontario. Funded by the US Department of Housing and Urban Development, the Housing Voucher program extends rental subsidies to very low-income households by offering the tenant a voucher that pays the difference between the current fair market rent (FMR) established by the Housing Authority and 30 percent of the tenant's income. A tenant has the option to choose housing that costs more than the FMR, if the tenant pays the extra rent above the payment standard. The Housing Authority also implements the scattered site program, Family Self-Sufficiency program, Section 8 project-based assistance, and HUD-assisted multiple-family housing units. This program serves up to 600 individuals and families in the City of Ontario.	Continue and modify.
22. Homeownership	Continue and
The City has a broad-based homeownership program for residents. The City uses a combination of funds (BEGIN, HOME, CalHome, and other available funding) to provide down payment assistance to homebuyers seeking to purchase homes in Ontario. The City of Ontario also works in conjunction with Neighborhood Partnership Housing Services (NPHS), a nonprofit organization, and the Inland Fair Housing and Mediation Board (IFHMB) to further the City's homeownership goals through homebuyer education, counseling, and down payment assistance.	modify.
23. Preservation of At-Risk Housing	Continue.
The City maintains more than 1,500 units of rental housing affordable to seniors, families, and individuals earning lower incomes. The City is committed to preserving its stock of affordable housing, some of which is at risk of conversion and/or needs significant renovation and improvement. The City remains committed to preserving its affordable housing and will monitor the status of the affordable housing projects, provide technical assistance, and consider appropriate actions should these projects be at imminent risk of conversion.	
24. Jack Galvin Accord	Continue.
The City of Ontario has more than 2,100 mobile homes, which provide affordable market-rate housing for lower-income families, seniors, and individuals. In 1990, the City Council adopted an ordinance to regulate mobile home space rents but later repealed that ordinance per state law. Subsequently, in working with mobile home park owners and tenants, the City drafted the Jack Galvin Mobile Home Park Accord, which was accepted by park owners. The accord places limits on the allowable increases based on the Consumer Price Index; allows for additional adjustments for changes utilities, taxes, and capital improvements; provides a process for requesting rent reductions for service reductions; and allows for rent adjustments for resale. The term of the agreement was adopted in 1999, and per extensions continues in effect today. The City will continue to implement and enforce this ordinance.	
Special Needs Housing	
Ontario is committed to furthering fair housing opportunities so that people in all walks of life have the opportunity to find suitable housing in the community. To that end, the City contracts with a fair housing service provider to provide landlord/tenant education, conduct testing of the rental and ownership market, and investigate and mediate housing complaints where needed. The City periodically prepares the required federal planning reports, including the Analysis of Impediments to Fair Housing Choice (AI), to document the City's progress in improving and maintaining fair housing opportunities. As part of the AI update, the City will review its Municipal Code, local government regulations, and other practices such as the definition of a family. Recommendations will be made to eliminate potential constraints and further fair housing in Ontario.	Continue and modify.
26. Homeless Continuum of Care	Continue.
The City implements a Homeless Services Continuum of Care to prevent homelessness and assist people in becoming self-sufficient. Working together with homeless service providers, the City has developed a full-service homeless continuum of care consisting of a homeless outreach service center, transitional housing, permanent housing, and supportive housing services. The City funds other programs that assist homeless people utilizing Emergency Solutions Grant funds.	

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Table 2 Summary of Program Changes Between Existing Housing Element and 2021–2029 Housing Element

Program	Continue/ Modify/ Delete
The City also actively participates in regional homeless efforts, including the Interagency Council on Homelessness, which is a countywide effort of governmental and nonprofit organizations working to end homelessness within the County of San Bernardino.	
27. Senior Housing	Continue.
The City is actively working with nonprofit housing groups to build senior housing projects in the community. In addition to facilitating housing construction, the City also provides a range of supportive services for seniors. These include fair housing services, housing rehabilitation grants, preservation of subsidized senior housing, low-cost transportation services, and a range of other services tailored to meet the unique needs of Ontario's senior population.	
28. Housing for People with Disabilities	Continue and
The City enforces state and federal accessibility laws to facilitate the improvement of housing for disabled people. The City also prepares a Transition Plan to comply with state and federal accessibility laws. The City has adopted a reasonable accommodation process and administratively allows modifications to land use, building codes, and the permitting process to facilitate the reasonable accommodations without going through a standard variance process. However, given the large number of people with disabilities, the growing need for housing opportunities, and changing legal context for housing planning, additional efforts are needed. Many homes were built before the advent of modern accessibility standards and thus many homes remain inaccessible to people with disabilities and persons with developmental disabilities. To address this issue, the City will evaluate the feasibility and appropriateness of modifying building standards to encourage visitability concepts in new housing. Additionally, to ensure compliance with state law, the City will update its definition of "family" to state "One or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit."	modify.
29. Family Housing	Continue.
Ontario has a large number of family households, specifically large families with five or more members. The City has a multifaceted program for increasing and maintaining the supply of family housing. The Housing Authority of San Bernardino County allocates housing choice vouchers to lower-income families in Ontario, many of whom are large families. Another key effort is the City's program to acquire, rehabilitate, and preserve existing affordable housing units that accommodate families and large families. Over the past five years, the City and the Housing Authority have preserved the vast majority of publicly subsidized affordable units for families. Finally, the City funds through its Community Development Block Program programs such as child care, after-school programs, food programs, and other services targeted for lower-income households, including large families.	
30. Extremely Low-Income Households	Continue.
The City offers programs to address the housing needs of extremely low-income (ELI) households. As funding is available, the City provides a number of incentives to encourage the production of ELI housing. The City offers fee reductions for ELI housing, supports grant applications to increase the supply of affordable housing, works with nonprofit organizations to build affordable housing, and provides land writedowns.	
31. Special Needs Housing	Continue.
In implementing affordable housing programs, the City will work with housing providers to ensure that special housing needs are addressed for seniors, large families, female-headed households, single-parent households with children, persons with disabilities and developmental disabilities, homeless individuals and families, and farmworker families. The City will seek to meet these special housing needs through a combination of regulatory incentives, zoning standards, new housing construction programs, housing rehabilitation, homebuyer assistance programs, and supportive services programs. In addition, the City may seek funding under the federal Housing Opportunities for Persons with AIDS, California Child Care Facilities Finance Program, and other state and federal programs designated specifically for special needs groups such as seniors, persons with disabilities, and persons at risk for homelessness.	
Source: Ontario 2021a	

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4.1 BACKGROUND

1. Project Title:

Addendum to The Ontario Plan Environmental Impact Report (SCH No.: 2008101140), for approval of the City of Ontario Housing Element.

2. Lead Agency Name and Address:

City Hall, Planning Department City of Ontario 303 East B Street Ontario, CA 91764

3. Contact Person and Phone Number:

Thomas Grahn, Senior Planner (909) 395-2413

4. Project Location:

Citywide. The City of Ontario is in southwestern San Bernardino County, approximately 40 miles east of downtown Los Angeles, 20 miles west of San Bernardino, and 30 miles northeast of Orange County. The City is approximately 31,958 acres, including the 8,200-acre Ontario Ranch in the southern portion of the City (formerly the City's sphere of influence), and is surrounded to the west by the Cities of Chino and Montclair and unincorporated areas of San Bernardino County; to the north by the Cities of Upland and Rancho Cucamonga; to the east by the City of Fontana and unincorporated land in San Bernardino County; and to the south by the Cities of Chino, Eastvale, and Jurupa Valley. See Figure 1, Regional Location, and Figure 2, Aerial Map. Regional circulation to and through the City is provided by I-10 and SR-60 east-west, and by I-15 and SR-83 (Euclid Avenue) north-south.

5. Project Sponsor's Name and Address:

City Hall, Planning Department City of Ontario 303 East B Street Ontario, CA 91764

6. General Plan Designation: Not Applicable, project applies citywide.

7. Zoning: Not Applicable, project applies citywide. Zoning changes described under Project Background.

8. Description of Project:

The City of Ontario has initiated a process to update its housing element, one of the required elements of the City's general plan (TOP).

9. Surrounding Land Uses and Setting:

The City of Ontario is surrounded by developed urban areas and small areas of agriculture. The City is in the central part of the Upper Santa Ana River Valley. The San Gabriel Mountains lie to the north, just beyond the cities of Upland and Rancho Cucamonga.

10. Other Public Agencies Whose Approval Is Required (e.g., permits, financing approval, or participation agreement):

California Department of Housing and Community Development (HCD)

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4.2 ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The environmental factors checked below would be potentially affected by this project, involving at least one

impact that is a "Potentially Sign	ificant Impact," as indicated by	y the checklist on the following pages.
Aesthetics Biological Resources Geology / Soils Hydrology / Water Quality Noise Recreation Utilities / Service Systems	Agricultural and Forest Resources Cultural Resources Greenhouse Gas Emissions Land Use / Planning Population / Housing Transportation / Traffic Wildfire	Air Quality Energy Hazards & Hazardous Materials Mineral Resources Public Services Tribal Cultural Resources Mandatory Findings of Significance
4.3 DETERMINATION	N (TO BE COMPLETE	ED BY THE LEAD AGENCY)
On the basis of this initial evalua	ation:	
I find that the Proposed NEGATIVE DECLARATION	,	e a significant effect on the environment, and
	s case because revisions in the	a significant effect on the environment, there will project have been made by or agreed to by the ATION will be prepared.
I find that the Propo ENVIRONMENTAL IMPACT	,	gnificant effect on the environment, and as
unless mitigated" impact on the earlier document pursuant to ap	e environment, but at least on pplicable legal standards, and lescribed on attached sheets. A	ally significant impact" or "potentially significant ne effect 1) has been adequately analyzed in at 2) has been addressed by mitigation measure An ENVIRONMENTAL IMPACT REPORT is addressed.
all potentially significant effect DECLARATION pursuant to a	ts (a) have been analyzed acapplicable standards, and (b) he ECLARATION, including revi	a significant effect on the environment, becaus dequately in an earlier EIR or NEGATIVI have been avoided or mitigated pursuant to that isions or mitigation measures that are imposed
Signature		Date
Printed Name		For

4.4 EVALUATION OF ENVIRONMENTAL IMPACTS

- 1. A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors, as well as general standards (e.g., the project would not expose sensitive receptors to pollutants, based on a project-specific screening analysis).
- 2. All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.
- 3. Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect may be significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.
- 4. "Negative Declaration: Less Than Significant With Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less Than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level.
- 5. Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:
 - a) Earlier Analyses Used. Identify and state where they are available for review.
 - b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.
 - c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.
- 6. Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.
- 7. Supporting Information Sources: A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.

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- 8. This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.
- 9. The explanation of each issue should identify:
 - a) the significance criteria or threshold, if any, used to evaluate each question; and
 - b) the mitigation measure identified, if any, to reduce the impact to less than significance

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This chapter provides evidence to substantiate the conclusions in the environmental checklist. Each section briefly summarizes the conclusions of the Certified EIR and discusses the following three conditions pursuant to CEQA Guidelines Section 15162:

Condition 1. Whether or not the proposed project represents a substantial change that will require major revisions to the Certified EIR due to new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

Condition 2. Whether or not substantial changes in the circumstances under which the proposed project is being undertaken will require major revisions to the Certified EIR due to new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or

Condition 3. If new information shows that the proposed project would have one or more new significant effects; that significant effects would be substantially more severe than previously described; that mitigation measures or alternatives previously found not to be feasible would be feasible and substantially reduce impacts, but project proponents decline to adopt them; or that new or previously rejected mitigation measures or alternatives would be feasible and would substantially reduce one or more project impacts, but project proponents decline to adopt them.

If none of the above conditions is met, the analysis identifies where impacts of the Proposed Project would result in a less than significant impact or no impact.

5.1 AESTHETICS

5.1.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that adoption of the Approved Project would not result in significant aesthetic impacts. Visual disturbances caused by the Approved Project would include impacts from development built pursuant to the Approved Project by altering visual appearance from rural agriculture to low- and low-medium density residential land uses and to office/industrial mixed-use in some parts of the City, in addition to creating new sources of light and glare. The Approved Project would be subject to its Community Design Element and the City's Municipal Code, which would ensure that aesthetic impacts of the Approved Project were less than significant.

5.1.2 Impacts Associated with the Proposed Project

Except as provided in Public Resources Code Section 21099, would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Have a substantial adverse effect on a scenic vista?					X
b)	Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?					X
c)	In nonurbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?					X
d)	Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?					Х

a) Have a substantial adverse effect on a scenic vista?

No Impact. The 2010 Certified EIR identified that implementation of TOP could potentially degrade views of the dominant scenic resource in Ontario, the San Gabriel Mountains, which are a significant feature of panoramic views from along the northern corridors of the City. However, as northern portions of the City are developed, implementation of the Housing Element Update would not substantially alter views of this scenic resource. Furthermore, future development would not substantially alter scenic vistas in the City because the scale and design of the City, including its land uses, would not deter views of the mountain backdrop. Therefore, with adherence to the Municipal Code and review of projects with the policies of TOP, the implementation of the Housing Element Update would not impact scenic vistas in the City.

General Plan and Development Code amendments are proposed as part of the Housing Element Update to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review, pursuant to CEQA, to ensure environmental impacts are considered. Future development under the policies of the Housing Element Update would be subject to the oversight and review processes envisioned by the goals and policies stipulated in TOP 2010 and established in the City's Development Code. They would be consistent with the City's numerous policies to

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promote high quality, compatible design in order to preserve the small-town visual quality and character; protect existing scenic vistas, viewsheds, and visual resources, including views of ridgelines and other prominent features of the natural environment; and minimize potential light pollution and glare. Current zoning regulations would limit potential aesthetic impacts of new construction by applying design and development standards that require conformance with applicable zoning requirements. Similar to the analysis in the Certified EIR, adherence to existing policies and regulations would minimize potential aesthetic impacts that would be created by the proposed Project. Therefore, the proposed Project would not create new aesthetic impacts that have not been previously analyzed in the Certified EIR, and no new mitigation measures are required.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project analyzed within the 2010 Certified EIR would result in any new or substantially more severe significant impacts with respect to adverse effects on scenic vistas. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?

No Impact. The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east-west direction. The segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. Growth would result in changes to the area's existing conditions. However, TOP policies of the Community Design Element have the common goal of improving the visual quality of the area by developing guidelines to improve future development projects. In addition, Title 9, Development Code, of the City's Municipal Code requires that individual development projects submit to site-specific review pursuant to the City of Ontario processes. Therefore, the policies and development proposed under the Housing Element Update would not substantially degrade the visual character or quality of the City of Ontario.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project analyzed within the 2010 Certified EIR would result in any new or substantially more severe significant impacts with respect to adverse effects on scenic resources within a scenic highway. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) In nonurbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

No Impact. The 2010 Certified EIR identified that growth would result in changes to the area's existing conditions. However, TOP policies of the Community Design Element have the common goal of improving the visual quality of the area by developing guidelines to improve future development projects. In addition, Title 9, Development Code, of the City's Municipal Code requires that individual development projects submit to site-specific review pursuant to the City of Ontario processes. These design guidelines and standards would regulate the features of buildings and streets that affect the public realm and would guide the physical development of any development project within the City's boundaries. Therefore, the policies and development under the Housing Element Update would not substantially degrade the visual character or quality of the City of Ontario.

The Housing Element Update encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also establishes goals and policies that would enable the City to accommodate growth required by RHNA requirements. Future development could be visible to visitors, employees, and residents, but all new installations would require a permit from the City and undergo further design review on a project-by-project basis during Plan Check to ensure the visual quality of the surrounding environment is not compromised. Specifically, Title 9. Chapter 1, Part 6: General Regulations, Article 32, General Development Requirements and Exceptions, of the City's Municipal Code contains standards related to development density, screening, and setback requirements, signage, street lighting and tree planting, landscape and design, scenic resources, public art, conformity with district regulations, mixed-use requirements, fences and walls, grading, height limitations, lighting, reflective material, and subdivision design criteria for residential (Article 14) and nonresidential (Article 16) development. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Therefore, the Housing Element Update would not degrade the character or quality of existing or future development or redevelopment sites and their surroundings.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project analyzed in the 2010 Certified EIR would result in any new or substantially more severe significant impacts with respect to adverse effects on visual character. Therefore, the preparation of a supplemental or subsequent EIR is not required by CEQA.

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d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?

No Impact. The 2010 Certified EIR identified that buildout of the Approved Project would generate new sources of light and glare that could affect day or nighttime views in the City. The City is primarily built out, and a significant amount of ambient light and glare from urban uses already exists. The Downtown Ontario Design Guidelines address architectural, graphic, and lighting design principles. Adherence to the design standards of the City of Ontario Development Code (Section 9-1.3325, Light, Glare, and Heat) and Downtown Ontario Design Guidelines would ensure that light and glare from new developments associated with the Approved Project would be minimized and that significant impacts would not occur.

Implementation of the Housing Element Update would result in an increased share of housing proposed for development in Ontario. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. All new construction and development would require a permit from the City and undergo further design review on a project-by-project basis during Plan Check to ensure that the visual quality of the surrounding environment is not compromised and glare is avoided. Adherence to the design standards of the City of Ontario Development Code (Section 9-1.3325, Light, Glare, and Heat) would also ensure glare impacts are minimized. Therefore, it is not anticipated that development under the Housing Element Update would result in an increased amount of glare. Furthermore, the City of Ontario Development Code contains standards addressing lighting through its design policies. As mentioned above, adherence to the design standards of the City of Ontario Development Code (Section 9-1.3325, Light, Glare, and Heat) and Downtown Ontario Design Guidelines would ensure that light and glare from new development associated with both the Approved Project and Housing Element Update would be minimized and that significant impacts would not occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to adverse effects on light and glare. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.1.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant aesthetic impacts and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.2 AGRICULTURE AND FORESTRY RESOURCES

5.2.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would result in significant agricultural resource impacts because no feasible mitigation measures are available to reduce impacts to less than significant levels. Buildout of the Approved Project would convert the existing prime farmland, unique farmland, and farmland of statewide importance to residential, commercial, mixed-use, and industrial land uses. It would also conflict with existing Williamson Act contract lands and would impact adjacent agricultural land uses in neighboring communities and cities. Therefore, agricultural resource impacts were identified as a significant and unavoidable impact of the Approved Project.

5.2.2 Impacts Associated with the Proposed Project

In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Dept. of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board.

Would the proposed project:

		Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?					x
b)	Conflict with existing zoning for agricultural use, or a Williamson Act contract?					Х
c)	Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?					X

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		Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
d)	Result in the loss of forest land or conversion of forest land to non-forest use?					Х
e)	Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?					x

a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

No Impact. The 2010 Certified EIR identified that the Approved Project would convert existing agricultural land to nonagricultural land upon buildout. Thus, buildout in accordance with the 2010 Certified EIR would have significant and unavoidable impacts on agricultural resources within the City. New development as outlined by the Housing Element Update would not convert any additional agricultural land to nonagricultural uses.

The Housing Element Update, consistent with the Approved Project, encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also promotes transit-oriented, mixed-use development within transit centers appropriate for mixed-use development, which is consistent with the policies in the Approved Project. These areas near transit or urbanized mixed-use development do not include existing agricultural lands. The Housing Element Update would not affect the amount of farmland lost as a result of implementation of the Approved Project. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to conversion of farmland. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?

No Impact. Buildout of the Approved Project analyzed under the Certified EIR would conflict with existing Williamson Act contract lands because the Land Use Plan does not designate agricultural uses, and buildout would most likely require the cancellation or nonrenewal of existing contracts. Thus, impacts were identified as significant and unavoidable in the 2010 Certified EIR.

The Housing Element Update would not convert agricultural land or conflict with existing Williamson Act contracts. The Housing Element Update would not result in changes to agricultural land-use designations, so it would not convert agricultural zoning. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to agricultural zoning. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?

No Impact. The 2010 Certified EIR did not include this environmental issue since the 2010 Certified EIR was circulated prior to the revisions to the CEQA Guidelines, which became effective March 18, 2010; however, forest land and timberland are not present in the City, and no impacts would occur. The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to existing zoning or cause rezoning of forest land, timberland, or timberland zones Timberland Production. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Result in the loss of forest land or conversion of forest land to non-forest use?

No Impact. The 2010 Certified EIR did not include this environmental issue; however, forest land and timberland are not present in the City, and no impacts would occur. The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to loss of forest

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land or conversion of forest land to nonforest use. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?

No Impact. The 2010 Certified EIR identified that the Approved Project would impact agricultural land uses in neighboring communities and cities by encouraging conversion to nonagricultural land uses. This could lead to the cancellation or expiration of Williamson Act contracts. As a result, impacts were identified as significant and unavoidable in the 2010 Certified EIR. New development as outlined by the Housing Element Update would not covert any additional agricultural land to non-agricultural uses or result in conversion of forestland to non-forest use.

The Housing Element Update, consistent with the Approved Project, encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. However, development within the City would not result in the conversion of important forest land as no forestland or timberland is present in the City. The City's Agricultural Overlay District protects vital agricultural uses by limiting land-use activity to uses that are compatible and supportive of agricultural and related use and/or agriculture by-products.¹ Development that converts Important Farmland to nonagricultural uses would not be allowed within the City's Agricultural Overlay District. Furthermore, the Housing Element Update would not affect the amount of farmland lost as a result of implementation of TOP.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to conversion of farmland. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.2.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify any feasible mitigation measures for agricultural resources that would prevent the loss of Important Farmland within the City.

City of Ontario, 2003 Municipal Code, Section 9-1.2700, http://www.ontarioca.gov/sites/default/files/Ontario-Files/Planning/current-planning/agricultural_overlay_-_ag.pdf, accessed August 30, 2021.

5.3 AIR QUALITY

5.3.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that, even after the implementation of mitigation measures, the Approved Project would result in significant air quality impacts. Criteria air pollutant impacts were compared to the South Coast Air Quality Management District's (South Coast AQMD) significance thresholds. Construction activities related to buildout of the Approved Project would result in air pollutant levels that exceed South Coast AQMD's significance thresholds. In addition, long-term operation of the Approved Project would also result in emissions that exceed South Coast AQMD's significance thresholds. The Approved Project would cumulatively contribute to the nonattainment designations of the South Coast Air Basin (SoCAB). In addition, the 2010 Certified EIR identified that the Proposed Project may conflict with or obstruct implementation of the applicable air quality management plan (AQMP). The Approved Project would also place sensitive receptors near major sources of air pollution. Odor impacts from placement of new sensitive receptors within agricultural areas were identified as a significant impact. Carbon monoxide (CO) hotspots were found to be less than significant. Although mitigation measures included in the 2010 Certified EIR would reduce air quality impacts of the Approved Project to the extent feasible, air quality impacts were identified as a significant and unavoidable impact of the Approved Project.

5.3.2 Impacts Associated with the Proposed Project

Where available, the significance criteria established by the applicable air quality management district or air pollution control district may be relied upon to make the following determinations.

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Conflict with or obstruct implementation of the applicable air quality plan?					X
b)	Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?				x	
c)	Expose sensitive receptors to substantial pollutant concentrations?				х	
d)	Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?					Х

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a) Conflict with or obstruct implementation of the applicable air quality plan?

No Impact. The 2010 Certified EIR identified inconsistency with South Coast AQMD's AQMP as a significant unavoidable impact of TOP. Since certification of the EIR, South Coast AQMD has adopted an update to the AQMP. The current air quality plan for the SoCAB region is the 2016 AQMP, which was adopted March 2017. Regional growth projections are used by South Coast AQMD to forecast future emission levels in the SoCAB. For southern California, these regional growth projections are provided by SCAG and are partially based on land-use designations in city and county general plans. Typically, only large, regionally significant projects have the potential to affect the regional growth projections.

The Housing Element Update, consistent with the Approved Project, encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also promotes transit-oriented, mixed-use development within transit centers appropriate for mixed-use development, which is consistent with the policies in the Approved Project. This type of development would reduce traffic and improve air quality within the City by reducing vehicle-related air pollutant emissions through vehicle miles traveled (VMT) reductions. Furthermore, new development would incorporate energy efficiency measures to reduce electricity use and reduce natural gas combustion at residential, commercial, and industrial land uses within the City, which would reduce criteria air pollution locally. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Overall, development under the Housing Element Update would be consistent with and further the goals of the AQMP for the SoCAB.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to consistency with South Coast AQMD's AQMP. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that construction and operation of TOP would cumulatively contribute to the nonattainment designations of the SoCAB. Mitigation Measures 3-1 and 3-2 would reduce impacts to the extent feasible; however, air quality was identified as a significant and unavoidable impact in the 2010 Certified EIR.

Development enabled by policies in the Housing Element Update would implement measures in Ontario designed to increase energy efficiency and reduce per-capita VMT. Energy-efficiency improvements and use of alternative energy would reduce the heating and cooling requirements for buildings and would also result in a decrease in natural gas use and associated criteria air pollutants (i.e., volatile organic carbons [VOCs], nitrogen

oxides [NO_{XI}, carbon monoxide [CO], coarse inhalable particulate matter [PM₁₀], and fine inhalable particulate matter [PM_{2.5}]). The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate emissions greater than those identified in the 2010 Certified EIR because the Proposed Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project. Development under the Housing Element Update would be consistent with the anticipated criteria air pollutant emissions resulting from buildout of TOP compared to those identified in the 2010 Certified EIR.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to cumulatively contributing to the nonattainment designations of the SoCAB. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Expose sensitive receptors to substantial pollutant concentrations?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that an increase in traffic congestion in the City of Ontario at buildout of TOP would not expose sensitive receptors to substantial pollutant concentrations from CO hotpots. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate emissions greater than that identified in the 2010 Certified EIR because the Proposed Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project. Development under the Housing Element Update would not generate CO hotspots.

The 2010 Certified EIR also identified that construction activities associated with buildout of the Approved Project would generate elevated concentrations of air pollutants at sensitive receptors. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Development under the Housing Element Update would be consistent with the anticipated impact to those identified in the 2010 Certified EIR.

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The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project because the Proposed Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to exposing sensitive receptors to substantial pollutant concentrations. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

No Impact. The 2010 Certified EIR found that odorous emissions attributable to the Approved Project are not considered a significant adverse impact to air quality. The Housing Element Update, consistent with the Approved Project, encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also promotes transit-oriented, mixed-use development within transit centers appropriate for mixed-use development, which is consistent with the policies in the Approved Project. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to other emissions (such as those leading to odors) adversely affecting a substantial number of people. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.3.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measures were taken directly from the 2010 Certified EIR. Any modifications to the mitigation measures from the certified EIR are shown in strikethrough for deleted text and underline for new, inserted text. Mitigation Measure 3-1 was modified to reflect changes in technology since the 2010 Certified EIR. Mitigation Measure 3-3 is deleted because it pertains to impacts of the environment on a project, which were determined to not be subject to CEQA.

3-1 The City of Ontario building department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:

- Require fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as:
- Requiring use of nontoxic soil stabilizers to reduce wind erosion.
- Applying water every four hours to active soil disturbing activities.
- Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials.
- Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 Tier 4 interim or higher exhaust emission limits.
- Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards.
- Limiting nonessential idling of construction equipment to no more than five consecutive minutes.
- Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf.
- 3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g. sidewalks).
- The City of Ontario shall evaluate new development proposals within the City for potential incompatibilities with regard to the California Air Resources Board's Air Quality and Land Use Handbook: A Community Health Perspective (April 2005). New development that is inconsistent with the recommended buffer distances shall only be approved if feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value (MERV) filters have incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources.

5.4 BIOLOGICAL RESOURCES

5.4.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would result in potentially significant biological impacts because development associated with the project could result in habitat modification and removal, which subsequently could result in the introduction of nonnative species of weeds, insects, and domestic animals that could adversely impact sensitive species. Furthermore, buildout of the Approved Project could develop vacant land, which may affect sensitive species. Regulatory requirements and standard conditions of approval would be required upon implementation of subsequent projects, which would reduce potential impacts of the Approved Project to less than significant.

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5.4.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				X	
b)	Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?				x	
c)	Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?				x	
d)	Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?				x	
e)	Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?					х
f)	Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?					x

a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that development in accordance with the proposed Land Use Plan designations could result in the loss of sensitive species due to habitat modification and removal; however, impacts of the Approved Project would be reduced to less than significant levels after compliance with requirements of the California and federal Endangered Species Acts as well as mitigation fees that would be paid by specific projects for the acquisition and management of sensitive habitats.

The Housing Element Update would not directly result in removal of vegetation or wildlife because it does not confer entitlements for development. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The Housing Element Update would not result in new development beyond what is identified in the Land Use Plan. Future projects, such as new housing developments within potential habitat areas, may be required to conduct focused surveys and consult with the United States Fish and Wildlife Service (USFWS) and California Department of Fish and Wildlife (CDFW) to establish mitigation measures needed to minimize adverse impacts to the species. More importantly, the Housing Element Update does not result in changes to the Open Space land use designations that have the potential affect sensitive habitat. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Therefore, implementation of the Housing Element Update would not have substantial adverse impacts on sensitive plant or animal species.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to sensitive species. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would not have substantial adverse impacts to surface-water areas or to riparian or aquatic vegetation in surface-water areas or flood control channels because projects potentially affecting riparian habitat would be required through the existing permitting process to mitigate potential impacts. Furthermore, detention basins would be designated Open Space—Non-Recreation and Open Space—Parkland and would not be developed with other land uses.

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Two local habitat conservation plans are applicable to the City of Ontario: the Ontario Recovery Unit for the Delhi Sands flower-loving fly (Rhaphiomidas terminatus) and Critical Habitat Unit 4 for the San Bernardino kangaroo rat (Dipodomys merriammiparvus). The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The Housing Element Update would not result in new development beyond what is identified in the Land Use Plan. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Consequently, no impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to sensitive natural communities. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would not have substantial adverse impacts to jurisdictional waters with the incorporation of mitigation. Flood-control channels and detention basins would be designated Open Space—Non-Recreation or Open Space—Parkland and would not be developed with other land uses.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The Housing Element Update would not result in new development beyond what is identified in the Land Use Plan. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Projects that result in impacts to waters of the state would be subject to approval by CDFW through the Streambed Alteration Agreement and would require mitigation as determined by CDFW. Streambed Alteration Agreements alleviate impacts to water bodies and thus do not result in cumulative impacts. Consequently, no impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to federally protected wetlands through direct removal, filling, hydrological interruption, or other means. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would not significantly impact the movement of native resident or migratory fish or wildlife species because no regional wildlife movement corridors were identified in the City, and most of the City is ill suited for the purposes of wildlife movement. The flood-control channels and the Southern California Edison (SCE) corridors could serve as local corridors for movement within the City and between the San Gabriel Mountains to the north and the Prado Basin to the south. The segments of flood-control channels in the City would be designated Open Space—Non-Recreation under the TOP Policy Plan and would not be developed with other land uses. The SCE corridors would also be designated Open Space—Non-Recreation. Therefore, implementation of the proposed Housing Element Update is not anticipated to substantially impair the use of flood-control channels or SCE corridors in the City for wildlife movement.

There are trees and shrubs scattered throughout the City that may be used for nesting or roosting by migrating birds. The Housing Element Update would not grant specific entitlements for development; therefore, implementation of the Housing Element Update would not directly impact vegetation that could be used by migrating birds. Such projects would be required to comply with the federal Migratory Bird Treaty Act. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Therefore, the Housing Element Update is not anticipated to have substantial adverse impacts to migratory birds. TOP Policy ER51 would encourage efforts to conserve flood-control channels and transmission-line corridors as wildlife movement corridors. Consequently, no impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to wildlife corridors. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

No Impact. The 2010 Certified EIR identified that the Approved Project would not conflict with any local policies or ordinances protecting biological resources. The Housing Element Update would not grant specific entitlements for development; however, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. The Housing Element Update would be required to comply with TOP Policies ER5-1 and ER5-2 and Municipal Code Sections 10-1.25 and 10-2.05, which prohibit damage or

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destruction of trees on City property, except under conditions specified in the Municipal Code. TOP Policy ER5-1 supports the protection of biological resources through the establishment, restoration, and conservation of high-quality habitat areas. Projects consistent with the Housing Element Update would be required to comply with Municipal Code Sections 10-1.25 and 10-2.05 and restrictions in locally protected biological resource conservation areas developed under TOP Policy ER5-1.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to local policies or ordinances protecting biological resources. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

No Impact. The 2010 Certified EIR identified that the Approved Project would not conflict with the requirements of the Delhi Sands Flower-Loving Fly Ontario Recovery Unit or Critical Habitat for the San Bernardino kangaroo rat because focused surveys and USFWS consultation would be required prior to any development associated with the Approved Project. The Approved Project would also have to comply with a designated Habitat Conservation Plan area within the City. City ordinances ensure compliance with the provisions of habitat conservation plans, including the Oakmont Industrial Group Habitat Conservation Plan. TOP recognizes the development regulations of the habitat conservation plan and restricts development in other conservation areas. Thus, impacts were identified as less than significant in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Individual projects undergoing environmental review under CEQA would be required to determine whether there is potential habitat onsite for sensitive species. If sensitive species were found onsite, the project proponent would be required to consult with CDFW regarding impacts to sensitive species. As identified in the 2010 Certified EIR, existing regulations for biological resources would substantially limit degradation of habitat on a regional scale.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to provisions of an adopted Habitat Conservation Plan; Natural Community Conservation Plan;

or other approved local, regional, or state habitat conservation plan. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.4.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant biological resources impacts; therefore, no mitigation measures were identified in the 2010 Certified EIR.

5.5 CULTURAL RESOURCES

5.5.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would result in a less than significant impact to archeological resources and the potential to disturb human remains with mitigation incorporated; however, impacts to historic resources were identified as a significant and unavoidable impact of the Approved Project.

5.5.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Cause a substantial adverse change in the significance of a historical resource pursuant to § 15064.5?				x	
b)	Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?				х	
c)	Disturb any human remains, including those interred outside of dedicated cemeteries?					Х

a) Cause a substantial adverse change in the significance of a historical resource pursuant to § 15064.5?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that development in accordance with TOP could minimize impacts to historic resources classified Tier I or Tier II with implementation of policies from the Community Design Element and Ontario Municipal Code; however, impacts from buildout of the Approved Project to historic resources classified Tier III were identified as a significant and unavoidable impact in the 2010 Certified EIR.

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The Housing Element Update would allow for increased housing development, and these activities have the potential to cause changes in the significance of historical resources through alteration of physical characteristics, including resources listed in or eligible for listing in the National Register of Historic Places and/or Center for Regional Heritage Research. Housing development or redevelopment projects may have the potential to result in impacts to buildings or structures of historic age (50 years old or older), or buildings or structures that may eventually be of historic age and may qualify as historical resources pursuant to CEQA upon evaluation.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Future discretionary projects would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, Mitigation Measure 5-1 of the 2010 Certified EIR requires that historic resources be evaluated for historic significance through the City's tier system. The City of Ontario Municipal Code, Title 9, Chapter 1, Part 1, Article 2, Section 9-1.0412, Historic Preservation, requires that all proposed work to a designated cultural resource conform to the Secretary of the Interior's standards for rehabilitating historic buildings and for historic preservation projects. These ordinances establish the City's planning and zoning regulations and pertain to allowable land uses, second units, development standards, and historic preservation. Therefore, the City's ordinances on historic preservation and the historic review process would require housing development projects resulting from implementation of the Housing Element Update to be designed in a manner that would not substantially alter the historic architecture or character of buildings.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to historical resources. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project could impact archaeological resources. Ground-disturbing activities, particularly in areas that have not previously been developed with urban uses ("native soils," which include agricultural lands), have the potential to damage or destroy archaeological resources that may be present on or below the ground surface. Mitigation Measures were incorporated into the Approved Project and detail specific measures to identify, protect, and preserve archeological resources through the City planning and environmental review processes.

Residential development associated with the Housing Element could impact archeological resources; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. In the event that future housing

development projects do affect previously undiscovered archaeological resources, Mitigation Measures 5-2 through 5-4 of the 2010 Certified EIR shall apply.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to archaeological resources. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Disturb any human remains, including those interred outside of dedicated cemeteries?

No Impact. The 2010 Certified EIR identified that the Approved Project would comply with California Public Resources Code Section 5097.98 so as not to disturb human remains. Public Resources Code Section 5097.98 mandates the process to be followed in the event of a discovery of any human remains and would mitigate all potential impacts. The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to disturbance of human remains. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.5.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measures were taken directly from the 2010 Certified EIR. All of these mitigation measures apply to and would be implemented for the Housing Element. Modifications to the original mitigation measures are identified in strikeout text to indicate deletions and underlined to signify insertions.

- 5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals in the Focus Areas.
- 5-2 In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:
 - a) Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities.

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- b) Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources.
- c) Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers.
- Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment proposed project subject to CEQA and is-within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the Proposed Project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.
- Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment proposed project for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.

5.6 ENERGY

5.6.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR assessed the energy demand for electricity and gas services in Section 5.17, *Utilities and Service Systems*, and concluded that the Approved Project would result in a less than significant impact to additional demand for electrical and gas services.

5.6.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?				x	
b)	Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?					Х

a) Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would cause a higher demand for energy services; however, the demand would be accommodated by service providers within the City.

Development enabled by policies included in the Housing Element Update would implement measures within the City of Ontario designed to increase energy efficiency and reduce per capita VMT. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate more energy than identified in the 2010 Certified EIR because the Proposed Project would not propose additional growth that was not evaluated under the 2010 Certified EIR or change the buildout evaluated under the Approved Project. Development under the Housing Element Update would be consistent with the anticipated criteria air pollutant emissions resulting from buildout of TOP compared to those identified in the 2010 Certified EIR.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts

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with respect to wasteful, inefficient, or unnecessary consumption of energy resources during project construction or operation. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

No Impact. The 2010 Certified EIR did not identify impacts associated with obstruction of a plan for renewable energy or energy efficiency.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future development under the Housing Element Update would be required to adhere to all federal, State, and local requirements for energy efficiency, including the California Energy Code Building Energy Efficiency Standards (California Code of Regulations [CCR] Title 24, Part 6) and the CALGreen Code (24 CCR Part 11). Title 24 building energy efficiency standards establish minimum efficiency standards related to various building features, including appliances, water and space heating and cooling equipment, building insulation and roofing, and lighting. Compliance with Title 24 standards is required, which would significantly reduce energy usage.

Development enabled by policies in the Housing Element Update would implement measures in Ontario designed to increase energy efficiency and reduce per-capita VMT. Continued implementation of TOP promotes mixed-use development and encourages alternative modes of transportation to reduce vehicle trip lengths and reliance on the automobile, which in turn would reduce the transportation energy demand in the City. Continued implementation of the TOP also encourages development of housing near employment and transportation, which would lead to a potential decrease in VMT. Implementation of Title 24 requirements and conformity with TOP would result in no impacts to existing plans, and no mitigation is required. Therefore, the Proposed Project would not conflict with or obstruct a State or local plan for renewable energy or energy efficiency.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to conflicting with or obstructing a state or local plan for renewable energy or energy efficiency. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.6.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant energy impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.7 GEOLOGY AND SOILS

5.7.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project could subject residents, workers, and visitors in the City to seismic hazards such as ground shaking, liquefaction, and seismically induced settlement. Furthermore, people in the City could be subject to hazards arising from ground subsidence, compressible soils, expansive soils, and erosion. Development pursuant to TOP would not involve the use of septic tanks.

The 2010 Certified EIR concluded that paleontological resources would be less than significant with mitigation incorporated. Regulatory requirements and standard conditions of approval would ensure that all other impacts would be less than significant.

5.7.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:					
	i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map, issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.					x
	ii) Strong seismic ground shaking?				Х	
	iii) Seismic-related ground failure, including liquefaction?				X	
	iv) Landslides?				Х	
b)	Result in substantial soil erosion or the loss of topsoil?				X	

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
c)	Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?				x	
d)	Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?				x	
e)	Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?					Х
f)	Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?				x	

- a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:
 - i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map, issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

No Impact. As stated in the 2010 Certified EIR, there are no Alquist-Priolo Earthquake Fault Zones in Ontario. The nearest such zones are along the Chino Fault, approximately three miles southwest of the City, and along the Cucamonga Fault, approximately 4.5 miles to the north. Impacts from surface rupture were identified as less than significant in the 2010 Certified EIR.

The Housing Element Update does not confer entitlement for development, and future new housing development would comply with the seismic safety provisions of the California Building Code (CBC) (24 CCR Part 2). The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts caused by the projects are considered. Thus, the Housing Element Update would not expose people or structures to potential substantial adverse effects involving rupture of a known earthquake fault.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to rupture of a known earthquake fault as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning map. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

ii) Strong seismic ground shaking?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Upper Santa Ana River Valley and vicinity contain a number of known earthquake faults, thus potentially subjecting residents, visitors, and workers to strong seismic ground shaking.

Implementation of the Housing Element Update would not directly result in construction of new structures. Projects considered for approval would be required to comply with seismic safety provisions of the CBC. Such compliance would reduce hazards arising from ground shaking to less than significant.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to seismic ground shaking. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

iii) Seismic-related ground failure, including liquefaction?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that two of the three factors contributing to liquefaction susceptibility are present or potentially present in the City: 1) potential for strong earthquakes and 2) young, loose, unconsolidated sediments. The third factor, groundwater within approximately 50 feet of surface, is not known to occur in the Ontario area today but has in the past. Although only a small portion of the southwestern corner of the City has moderate to high liquefaction susceptibility, projects approved in accordance with TOP could subject persons or structures to potentially significant hazards arising from liquefaction.

Implementation of the Housing Element Update would not directly result in construction of new structures, however, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Housing development projects consistent with the Housing Element Update would be mandated to comply with the CBC, which would reduce hazards arising from seismic-related ground failure.

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The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to seismic-related ground failure, including liquefaction. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

iv) Landslides?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR did not identify areas with substantial unstable soils that would result in on- or off-site landslides. Despite the City being underlain by unconsolidated alluvial deposits, Ontario is relatively level and exhibits no substantial elevation changes or unusual geographic features. In the absence of significant slopes, the potential for landslides to affect any projects under the Housing Element Update is considered negligible. The City may be susceptible to seismically induced settlement as a result of the young, unconsolidated alluvial deposits underlying the City. An Approved Project would be required to meet the most current seismic safety requirements in the CBC to reduce substantial impacts. Furthermore, implementation of the Housing Element Update would not directly result in construction of new structures.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to landslides or local geologic hazards. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Result in substantial soil erosion or the loss of topsoil?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that young alluvial sediment and wind-blown sand underlying the City are generally granular, poorly consolidated, and very susceptible to erosion. Grading increases the potential for erosion when protective vegetation is removed, natural drainage patterns are altered, and slopes are constructed. As a result, development pursuant to the Approved Project could indirectly expose people and structures to hazards arising from soil erosion. Impacts of the Approved Project would be mitigated through compliance with CBC requirements.

Implementation of the Housing Element Update would not directly result in construction of new structures. Compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure that no significant impacts would occur. Additionally, construction activities to implement the RHNA requirements outlined in the Housing Element Update, would be required to prepare a Stormwater Pollution Prevention Plan (SWPPP) detailing best management practices (BMP) to reduce the potential for erosion during construction activities.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to soil erosion. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR did not identify areas in the City with substantial unstable soils that would result in on- or off-site landslides, lateral spreading, subsidence, liquefaction, or collapse; however, the central and western portions of the City are shown to have a low risk of soil subsistence (0 to 5 centimeters reduction in elevation) due to decreasing groundwater supplies to the west of the City. Compliance with the CBC and review of grading plans for individual projects by the City Engineer would ensure that no significant impacts would occur.

Implementation of the Housing Element Update would not directly result in construction of new structures. Development proposed under the Housing Element Update would be required to comply with seismic safety provisions of the CBC and would need to obtain grading permits pursuant to the City Development Code. Such compliance would reduce hazards arising from unstable geologic units and soils.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to unstable soils. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial direct or indirect risks to life or property?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that there are likely to be expansive soils in the southern parts of the City, where there are silts, sandy silts, and silty clays. Near-surface soils in the northern and central parts of the City are primarily granular, that is, silty sand, sand, and gravel; such sediments are usually nonexpansive or have very low expansion potential. Projects in the southern part of the City could expose persons or structures to potentially significant hazards from expansive soils, but compliance with the CBC and review of grading plans by the City Engineer would ensure no significant impacts would occur.

Implementation of the Housing Element Update would not directly result in construction of new structures. Individual housing development projects implemented under the Housing Element Update in the southern portion of the City could expose persons or structures to potentially significant hazards due to expansive soils.

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Compliance with the CBC and review of grading plans by the City Engineer, as required by the City Development Code and as outlined in the Approved Project, would ensure that no significant impacts would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to expansive soils. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of waste water?

No Impact. The 2010 Certified EIR identified that the use of septic tanks would not occur in the City. Development associated with the Housing Element Update would not require the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater. Future development would connect to existing and planned sewer lines in the City.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to septic tanks or alternative wastewater disposal systems. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project could impact paleontological resources. The City of Ontario is underlain by deposits of Quaternary and upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene epochs. Quaternary Older Alluvial sediments may contain significant, nonrenewable paleontological resources and are therefore considered highly sensitive. Older Pleistocene alluvial sediments can yield fossil remains, often found at depths of 10 feet or more below the modern ground surface. In addition, there has been one paleontological resource discovered within the City's boundaries, as recorded by the Archaeological Information Center at San Bernardino County Museum. As a result, there is a moderate to high possibility of finding additional paleontological resources within City boundaries at depths of 10 feet or more below the ground surface. Mitigation Measure 5-2 would reduce impacts to less than significant levels.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Additionally, Mitigation Measure 5-2 of the

2010 Certified EIR would apply, which requires implementation of specific measures to identify, protect, and preserve paleontological resources through the City planning and environmental review processes.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to paleontological resources. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.7.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measure was taken directly from the 2010 Certified EIR for paleontological resources. This mitigation measure would apply to and would be implemented for the Housing Element Update. The 2010 Certified EIR did not identify any other significant geology and soils impacts, and therefore no other mitigation measures were identified in the 2010 Certified EIR.

- In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:
 - a) Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities.
 - b) Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources.
 - c) Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers.

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5.8 GREENHOUSE GAS EMISSIONS

5.8.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR identified significant greenhouse gas (GHG) emissions impacts associated with the Approved Project as a result of the magnitude of population and employment growth projected by SCAG and TOP. Although, TOP was found to be consistent with statewide strategies adopted for the purpose of reducing GHG emissions, mitigation measures were incorporated into the 2010 Certified EIR to reduce impacts. GHG emissions were considered a significant unavoidable impact in the 2010 Certified EIR.

5.8.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?				x	
b)	Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?					Х

a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified GHG emissions impacts as a significant unavoidable impact of TOP.

Development enabled by policies included in the Housing Element Update would implement measures within the City of Ontario designed to increase energy efficiency and reduce per-capita VMT. Additionally, new projects are required to be consistent with the City of Ontario's Community Climate Action Plan (CCAP). The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate GHG emissions

greater than those identified in the 2010 Certified EIR. Development under the Housing Element Update would be consistent with the anticipated GHG emissions resulting from buildout of TOP and those identified in the 2010 Certified EIR. Additionally, Mitigation Measure 6-5 of the 2010 Certified EIR would apply.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to generating greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

No Impact. The 2010 Certified EIR did not identify impacts associated with conflicts with adopted GHG emissions plans. Since the 2010 Certified EIR was circulated, the City has prepared and implemented a CCAP. Additionally, the California Air Resources Board has adopted the 2017 Scoping Plan to achieve the GHG reduction targets of Senate Bill 32. SCAG also adopted the 2020-2045 Regional Transportation Plan/Sustainable Communities Strategy (Connect SoCal) in September 2020 to outline a path to achieve the GHG reduction targets of Senate Bill 375. The Proposed Project would be consistent with these plans through compliance with Title 24, Part 6, California Energy Efficiency Standards, which would require all residential construction to meet minimum energy conservation standards, as well as through compliance with the California Green Building Code, which would ensure energy efficiency, water conservation, material conservation and resource efficiency, and environmental quality for all new buildings. Additionally, the City of Ontario is taking other initiatives to reduce emissions of greenhouse gases. For instance, in 2018, the City received a Transformative Climate Communities grant for its Ontario Together project, which included sustainability projects such as a Rooftop Solar Project, which will install 700 kilowatts of solar power on affordable multifamily developments and single-family homes and will provide paid solar installation internships to grow the renewable energy workforce.

Implementation of the Housing Element Update would not directly or indirectly generate GHG emissions that may have a significant impact on the environment because the Housing Element Update does not confer entitlements for development. The GHG reduction measures outlined in the CCAP would reduce GHG emissions for long-term buildout of the City. The CCAP would implement measures in Ontario designed to increase energy efficiency and reduce per-capita VMT. Energy efficiency improvements and use of alternative energy would reduce the heating and cooling requirements for buildings and would also result in a decrease in electricity and natural gas use and associated GHG emissions. Likewise, land-use and transportation measures that reduce VMT would result in reduced GHG emissions from the transportation sector. Water, agricultural, and waste-reduction measures would further reduce GHG emissions in the City. Additionally, Mitigation Measure 6-5 of the 2010 Certified EIR would apply.

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The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to generating greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.8.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measures were taken directly from the 2010 Certified EIR. However, Mitigation Measure 6-1 through 6-4 and Mitigation Measure 6-6 have since been implemented to achieve the Assembly Bill 32 GHG reduction target for the City of Ontario in the Community Climate Action Plan and/or are no longer applicable to the Proposed Project. Modifications to the original mitigation measures are identified in strikeout text to indicate deletions and underlined to signify insertions. Mitigation Measure 6-5 still applies and would be implemented for the Housing Element.

- 6-1 The City of Ontario shall prepare a Climate Action Plan within 18 months after adopting The Ontario Plan. The goal of the Climate Action Plan shall be to reduce GHG emissions from all activities within the City boundaries to support the State's efforts under AB 32 and to mitigate the impact of climate change on the City, State, and world. Once completed, the City shall update The Ontario Plan and associated policies, as necessary, to be consistent with the Climate Action Plan and prepare a subsequent or supplemental Environmental Impact Report, if new significant impacts are identified. The Climate Action Plan shall include the following:
- Emission Inventories: The City shall establish GHG emissions inventories including emissions from all sectors within the City, using methods approved by, or consistent with guidance from, the CARB; the City shall update inventories every 3 years or as determined by state standards to incorporate improved methods, better data, and more accurate tools and methods, and to assess progress. If the City is not on-schedule to achieve the GHG reduction targets, additional measures shall be implemented, as identified in the CAP.
 - The City shall establish a baseline inventory of GHG emissions including municipal emissions, and emissions from all business sectors and the community.
 - The City shall define a "business as usual" scenario of municipal, economic, and community activities, and prepare a projected inventory for 2020 based on that scenario.
- **Emission Targets:** The City will develop Plans to reduce or encourage reductions in GHG emissions from all sectors within the City:
 - A Municipal Climate Action Plan which shall include measures to reduce GHG emissions from municipal activities by at least 30 percent by 2020 compared to the

- "business as usual" municipal emissions (including any reductions required by the California Air Resource Board under AB 32.
- A Business Climate Action Plan in collaboration with the business community, which shall include measures to reduce GHG emissions from business activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" business emissions.
- A Community Climate Action Plan in collaboration with the stakeholders from the
 community at large, which shall include measures reduce GHG emissions from
 community activities, and which shall seek to reduce emissions by at least 30 percent
 by 2020 compared to "business as usual" community emissions.
- 6-2 The Climate Action Plan shall include specific measures to achieve the GHG emissions reduction targets identified in Mitigation Measure 6-1. The Climate Action Plan shall quantify the approximate greenhouse gas emissions reductions of each measure and measures shall be enforceable. Measures listed below, along with others, shall be considered during the development of the Climate Action Plan (CAP):
- Require all new or renovated municipal buildings to seek Silver or higher Leadership in Energy and Environmental Design (LEED) standard, or compliance with similar green building rating criteria.
- Require all municipal fleet purchases to be fuel efficient vehicles for their intended use based on the fuel type, design, size, and cost efficiency.
- Require that new development projects in Ontario that require demolition prepare a demolition plan to reduce waste by recycling and/or salvaging a nonhazardous construction and demolition debris.
- Require that new developments design buildings to be energy efficient by siting buildings to take advantage
 of shade, prevailing winds, landscaping, and sun screening to reduce energy required for cooling.
- Require that cool roofs for non-residential development and cool pavement to be incorporated into the site/building design for new development where appropriate.
- Evaluate the feasibility of implementing a Public Transit Fee to support Omnitrans in developing additional transit service in the City.
- Require diesel emission reduction strategies to eliminate and/or reduce idling at truck stops, warehouses,
 and distribution facilities throughout the City.
- Install energy efficient lighting and lighting control systems in all municipal buildings.
- Require all new traffic lights installed be energy efficient traffic signals. Require the use of reclaimed water
 for landscape irrigation in all new development and on public property where such connections are within
 the service boundaries of the City's reclaimed water system.
- ■—Require all new landscaping irrigation systems installed within the City to be automated, high-efficient irrigation systems to reduce water use and require use of bubbler irrigation; low-angle, low-flow spray

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heads; or moisture sensors. Conduct energy efficiency audits of existing municipal buildings by checking, repairing, and readjusting heating, ventilation, and air conditioning systems, lighting, water heating equipment, insulation, and weatherization.

- Ensure that its local Climate Action, Land Use, Housing, and Transportation Plans are aligned with, support, and enhance any regional plans that have been developed consistent with state guidance to achieve reductions in GHG emissions.
- Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure.
- Reduce heat gain from pavement and other similar hardscaping.
- Work with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, carsharing, bicycling and walking.
 - Provide safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets.
- Facilitate employment opportunities that minimize the need for private vehicle trips, by:
 - Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations.
- Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate.
- Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events.
- Support and promote the use of low-and zero-emission vehicles, by:
 - Encouraging the necessary infrastructure to facilitate the use of zero-emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations.
 - Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV).
 - Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes.
 - Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles.
- Establish green building requirements and standards for new development and redevelopment projects,
 and work to provide incentives for green building practices and remove barriers that impede their use.

- Allow increased height limits and/or flexibility in other standards for projects that incorporate energy
 efficient green building practices where not prohibited by Airport Land Use Compatibility Plan
 (ALUCP)/Federal Aviation Administration (FAA).
- Identify and remove regulatory or procedural barriers to implementing green building practices within its
 jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building
 inspection staff are trained in green building materials, practices, and techniques.
- Support the use of green building practices by:
 - Providing information, marketing, training, and technical assistance about green building practices.
 - Adopting a Green Building ordinance with guidelines for green building practices in residential and commercial development.
- Adopt energy efficiency performance standards for buildings designed to achieve a greater reduction in energy and water use than currently required by state law, including:
 - Standards for the installation of "cool roofs."
 - Standards for improved overall efficiency of lighting systems.
 - Requirements for the use of Energy Star appliances and fixtures in discretionary new development.
- Encourage the performance of energy audits for residential and commercial buildings prior to completion
 of sale, and that audit results and information about opportunities for energy efficiency improvements be
 presented to the buyer.
- Establish policies and programs that facilitate the siting of new renewable energy generation.
- Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible.
- Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including:
 - Conducting energy audits.
 - Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low emissive window glass.
 - Implementing an energy tracking and management system for its municipal facilities.
 - Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations.
 - Installing energy efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations.

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- Retrofitting heating and cooling systems to optimize efficiency (e.g. replace chillers, boilers, fans, pumps, belts, etc.).
- Installing Energy Star® appliances and energy-efficient vending machines.
- Improving water use efficiency, including a schedule to replace or retrofit system components with high-efficiency units (i.e. ultra-low-flow toilets, fixtures, etc.).
- Installing irrigation control systems which maximize water use efficiency and minimize off-peak use.
- Adopting an accelerated replacement schedule for energy inefficient systems and components.
- Ensure that staff receives appropriate training and support to implement objectives and policies to reduce GHG emissions, including:
 - Providing energy efficiency training to design, engineering, building operations, and maintenance staff.
 - Providing information on energy use and management, including data from the tracking and management system, to managers and others making decisions that influence energy use.
 - Providing energy design review services to departments undertaking new construction or renovation projects, to facilitate compliance with LEED standards.
- Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off peak demand schedules for heavy commercial and industrial users.
- Establish a replacement policy and schedule to replace fleet vehicles and equipment with the most fuelefficient vehicles practical, including gasoline hybrid and alternative fuel or electric models.
- Require the installation of outdoor electrical outlets on buildings to support the use, where practical, of electric lawn and garden equipment, and other tools that would otherwise be run with small gas engines or portable generators.
- Implement measures to reduce employee vehicle trips and to mitigate emissions impacts from municipal travel.
- Conduct a comprehensive inventory and analysis of the urban forest, and coordinate tree maintenance responsibilities with all responsible departments, consistent with best management practices.
- Evaluate existing landscaping and options to convert reflective and impervious surfaces to landscaping, and will install or replace vegetation with drought tolerant, low maintenance native species or edible landscaping that can also provide shade and reduce heat-island effects.
- Implement enhanced programs to divert solid waste from landfill operations, by:
 - Establishing a diversion target which meets or exceeds AB 939 requirements.

- Promoting and expanding recycling programs, purchasing policies, and employee education to reduce the amount of waste produced.
- ■—Reduce per capita water consumption consistent with state law by 2020.
- Establish a water conservation plan that may include such policies and actions as:
 - Maintaining and refining the City's tiered rate structure for water use.
 - Establishing restrictions on time of use for landscape watering, or other demand management strategies.
 - Establishing performance standards for irrigation equipment and water fixtures, consistent with state law.
- Establish programs and policies to increase the use of recycled water, including:
 - Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation.
- Ensure that building standards and permit approval processes promote and support water conservation,
 by:
 - Establishing building design guidelines and criteria to promote water efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s).
 - Establishing menus and check-lists for developers and contractors to ensure waterefficient infrastructure and technology are used in new construction, including lowflow toilets and shower heads, moisture-sensing irrigation, and other such advances.
- Organize workshops on waste reduction activities for the home or business, such as backyard composting, or office paper recycling, and shall schedule recycling dropoff events and neighborhood chipping/mulching days.
- Organize workshops on steps to increase energy efficiency in the home or business, such as weatherizing
 the home or building envelope, installing smart lighting systems, and how to conduct a self-audit for energy
 use and efficiency.
- 6-3 The City of Ontario will amend the Municipal Code within 18 months after adopting The Ontario Plan, with provisions implementing the following GHG emission reduction concepts:
- Increase densities in urban core areas to support public transit, by, among other means:
 - Removing barriers to the development of accessory dwelling units in existing residential neighborhoods.
- Reduce required road width standards wherever feasible to calm traffic and encourage alternative modes
 of transportation.
- Add bicycle facilities to city streets and public spaces, where feasible.

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- Promote infill, mixed-use, and higher density development, and provide incentives to support the creation of affordable housing in mixed use zones.
- Plan for and create incentives for mixed-use development.
- Identify sites suitable for mixed-use development and establish appropriate site- specific standards to accommodate mixed uses which could include:
 - Increasing allowable building height or allow height limit bonuses, in appropriate areas and where safe to do so.
 - Allowing flexibility in applying development standards (such as FAR2 and lot coverage) based on the location, type, and size of the units, and the design of the development.
 - Allowing reduced and shared parking based on the use mix, and availability of and proximity to public transit stops.
 - Allowing for tandem parking, shared parking and off-site parking leases.
- Enable prototype mixed-use structures for use in neighborhood center zones that can be adapted to new
 uses over time with minimal internal remodeling.
- Identify and facilitate the inclusion of complementary land uses not already present in local zoning districts, such as supermarkets, parks and recreational fields, schools in neighborhoods, and residential uses in business districts, to reduce the vehicle miles traveled and promote bicycling and walking to these uses.
- Revise zoning ordinance(s) to allow local-serving businesses, such as childcare centers, restaurants, banks, family medical offices, drug stores, and other similar services near employment centers to minimize midday vehicle use.
- Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use.
- Implement a Housing Overlay Zone for residential properties at transit centers and along transit corridors. This may include average minimum residential densities of 25 units per acre within one quarter miles of transit centers; average minimum densities of 15 units per acre within one quarter mile of transit corridors; and minimum FAR of 0.5:1 for non-residential uses within a quarter mile of transit centers or corridors.
- Identify transit centers appropriate for mixed-use development, and promote transit-oriented, mixed-use development within these targeted areas, by:
 - Providing maximum parking standards and flexible building height limitations.
 - Providing density bonus programs.
 - Establishing guidelines for private and public spaces for transit-oriented and mixeduse development.
 - Discouraging auto-oriented development.

- Ensure new development is designed to make public transit a viable choice for residents, including:
 - Locating medium to high density development near activity centers that can be served
 efficiently by public transit and alternative transportation modes.
 - Locating medium to high density development near streets served by public transit whenever feasible.
 - Linking neighborhoods to bus stops by continuous sidewalks or pedestrian paths.
- Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use.
- Create and preserve distinct, identifiable neighborhoods whose characteristics support pedestrian travel, especially within, but not limited to, mixed-use and transit-oriented development areas, by:
 - Designing or maintaining neighborhoods where the neighborhood amenities can be reached in approximately five minutes of walking.
 - Encouraging pedestrian-only streets and/or plazas within developments, and destinations that may be reached conveniently by public transportation, walking, or bicycling.
 - Allowing flexible parking strategies in neighborhood activity centers to foster a pedestrian oriented streetscape.
 - Providing continuous sidewalks with shade trees and landscape strips to separate pedestrians from traffic.
 - Encouraging neighborhood parks and recreational centers near concentrations of residential areas (preferably within one quarter mile) and include pedestrian walkways and bicycle paths that encourage non-motorized travel.
- Ensure pedestrian access to activities and services, especially within, but not limited to, mixed-use and transit-oriented development areas, by:
 - Ensuring new development that provides pedestrian connections in as many locations
 as possible to adjacent development, arterial streets, and thoroughfares.
 - Ensuring a balanced mix of housing, workplaces, shopping, recreational opportunities, and institutional uses, including mixed-use structures.
 - Locating schools in neighborhoods, within safe and easy walking distances of residences served.
 - Encouraging new development in which primary entrances are pedestrian entrances, with automobile entrances and parking located to the rear.
 - Supporting development where automobile access to buildings does not impede pedestrian access, by consolidating driveways between buildings or developing alley access.

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- Utilizing street parking as a buffer between sidewalk pedestrian traffic and the automobile portion of the roadway.
- Prioritizing the physical development of pedestrian connectors for existing areas that do not meet established connectivity standards.
- Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure.
- Reduce heat gain from pavement and other similar hardscaping, by:
 - Including low-water landscaping in place of hardscaping around transportation infrastructure and in parking areas.
 - Establishing standards that provide for pervious pavement options.
 - Removing obstacles to natural, drought tolerant landscaping and low-water landscaping.
- Coordinate with appropriate agencies to create an interconnected transportation system that allows a shift
 in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, carsharing, bicycling and walking, including, but not limited to:
 - Providing safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets.
- ■—Upgrade and maintain the following transit system infrastructure to enhance public use, including:
 - Ensuring transit stops and bus lanes are safe, convenient, clean and efficient.
 - Ensuring transit stops have clearly marked street-level designation, and are accessible.
 - Ensuring transit stops are safe, sheltered, benches are clean, and lighting is adequate.
 - Working with transit providers to place transit stations along transit corridors within mixed-use or transit-oriented development areas at intervals appropriate for the mode of transit.
- Facilitate employment opportunities that minimize the need for private vehicle trips, by:
 - Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations.
 - Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate.
- Establish standards for new development and redevelopment projects to support bicycle use, including:
 - Amending the Development Code to include standards for pedestrian and bicyclist accommodations, including:

- Providing access for pedestrians and bicyclist to public transportation through construction of dedicated paths, where feasible.
- Requiring new development and redevelopment projects to include bicycle facilities, as appropriate with the new land use, including:
 - Where feasible, promote the construction of weatherproof bicycle facilities and at a minimum, provide bicycle racks or covered, secure parking near the building entrances.
- Establish a network of multi-use trails to facilitate direct off-street bicycle and pedestrian travel, and will provide bike racks along these trails at secure, lighted locations.
- Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events.
- Require new commercial and retail developments to provide prioritized parking for electric vehicles and vehicles using alternative fuels.
- Support and promote the use of low-and zero-emission vehicles (NEV), by:
 - Encouraging the necessary infrastructure to facilitate the use of zero emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations.
 - Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV).
 - Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes.
 - Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles.
- Establish green building requirements and standards for new development and redevelopment projects,
 and work to provide incentives for green building practices and remove barriers that impede their use.
- Allow increased height limits and/or flexibility in other standards for projects that incorporate energy
 efficient green building practices where not prohibited by ALUCP/FAA.
- Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques.
- Support the use of green building practices by:
 - Establishing guidelines for green building practices in residential and commercial development.

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- Providing incentives, which may include reduction in development fees, administrative fees, and/or expedited permit processing for projects that use green building practices.
- Adopt energy efficiency performance standards for buildings that achieve a greater reduction in energy and water use than otherwise required by current state law, including:
 - Standards for the installation of "cool roofs".
 - Standards for improved overall efficiency of lighting systems.
 - Requirements for the use of Energy Star appliances and fixtures in discretionary new development.
 - Requirements for new residential lots and/or structures to be arranged and oriented to maximize effective use of passive solar energy.
- Require that affordable housing development incorporate energy efficient design and features to the maximum extent feasible.
- Identify possible sites for production of renewable energy (such as solar, wind, small hydro, and biogas).
- Identify and remove or otherwise address barriers to renewable energy production, including:
 - Reviewing and revising building and development codes, design guidelines, and zoning ordinances to remove renewable energy production barriers.
 - Working with related agencies, such as fire, water, health and others that may have policies or requirements that adversely impact the development or use of renewable energy technologies.
 - Developing protocols for safe storage of renewable and alternative energy products
 with the potential to leak, ignite or explode, such as biodiesel, hydrogen, and/or
 compressed air.
- ■—Allow renewable energy projects in areas zoned for open space, where consistent with the Land Use element, and other uses and values.
- Promote and encourage renewable energy generation, and co-generation projects where feasible and appropriate.
- Require that, where feasible, all new buildings be constructed to allow for easy, cost-effective installation of solar energy systems in the future, using such "solar-ready" features as:
 - Optimal roof orientation (between 20 to 55 degrees from the horizontal), with sufficient south sloped roof surface, where such buildings architecture and construction are designed for sloped roofs.
 - Clear access without obstructions (chimneys, heating and plumbing vents, etc.) on the south sloped roof.

- Roof framing that will support the addition of solar panels.
- Installation of electrical conduit to accept solar electric system wiring.
- Installation of plumbing to support a solar hot water system and provision of space for a solar hot water storage tank.
- Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible.
- Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including:
 - Conducting energy audits.
 - Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass.
 - Implementing an energy tracking and management system for its municipal facilities.
 - Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations.
 - Installing energy efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations.
 - Retrofitting heating and cooling systems to optimize efficiency (e.g. replace chillers, boilers, fans, pumps, belts, etc.).
 - Installing Energy Star® appliances and energy efficient vending machines.
 - Improving water use efficiency, including a schedule to replace or retrofit system components with high-efficiency units (i.e. ultra-low-flow toilets, fixtures, etc.).
 - Installing irrigation control systems maximizing water use efficiency and minimizing off-peak use.
 - Adopting an accelerated replacement schedule for energy inefficient systems and components.
- Require that any newly constructed, purchased, or leased municipal space meet minimum standards, such as:
 - The Energy Star® New Homes Program established by US EPA.
 - The incorporation of passive solar design features in new buildings, including daylighting and passive solar heating.
- ■—Reduce per capita water consumption consistent with state law by 2020.
- Establish a water conservation plan that may include such policies and actions as:

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- Maintaining and refining the City's tiered rate structure for water use.
- Establishing restrictions on time of use for landscape watering, or other demand management strategies.
- Establishing performance standards for irrigation equipment and water fixtures, consistent with State Law.
- The City will establish programs and policies to increase the use of recycled water, including:
 - Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation.
- Ensure that building standards and permit approval processes promote and support water conservation,
 by:
 - Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s).
 - Establishing menus and check-lists for developers and contractors to ensure waterefficient infrastructure and technology are used in new construction, including lowflow toilets and shower heads, moisture sensing irrigation, and other such advances.
- Install water efficient landscapes and irrigation, including:
 - Requiring planting drought tolerant and native species, and covering exposed dirt with moisture-retaining mulch or other materials such as decomposed granite.
 - Requiring the installation of water-efficient irrigation systems and devices, including advanced technology such as moisture-sensing irrigation controls.
- Promote the planting of shade trees and establish shade tree guidelines and specifications, including:
 - Establishing guidelines for tree planting based on the land use (residential, commercial, parking lots, etc.).
 - Establishing guidelines for tree types based on species size, branching patterns, whether deciduous or evergreen, whether roots are invasive, etc.
 - Establishing tree guidelines for placement, including distance from structures, density
 of planting, and orientation relative to structures and the sun.
- Develop an Urban Forestry Program to consolidate policies and ordinances regarding tree planting, maintenance, and removal, including:
 - Establishing guidelines for tree planting, including criteria for selecting deciduous or evergreen trees low VOC producing trees, and emphasizing the use of drought tolerant native trees and vegetation.

- 6.4 Measures listed in Mitigation Measure 6.2 and 6.3 shall be considered by the City while reviewing all new development, as appropriate, between the time of adoption of The Ontario Plan and adoption of the Climate Action Plan (CAP).
- 6-5 Pursuant to a goal of overall consistency with the Sustainable Communities Strategies, the City of Ontario shall evaluate new development for consistency with the development pattern set forth in the Sustainable Communities Strategies plan, upon adoption of the plan by the Southern California Association of Governments.
- 6-6 The City of Ontario shall participate in the County of San Bernardino's Green Valley Initiative.

5.9 HAZARDS AND HAZARDOUS MATERIALS

5.9.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would involve the transport, use, and/or disposal of hazardous materials. The 2010 Certified EIR also concluded that properties in the City are included on a list of hazardous materials sites. The Approved Project would comply with federal, state, and local regulations. Furthermore, the Approved Project concluded that consistency reviews of the proposed land uses with the Airport Land Use Compatibility Plan for the Ontario International Airport (ONT) and Chino Airport would prevent significant impacts. In addition, buildout of the Approved Project would not affect the implementation of an emergency response or evacuation plan. With regard to urban and wildland fire threats, adherence to existing regulations and review of building plans by the Ontario Fire Department would reduce risks from fires to the City. Impacts resulting from the Approved Project were identified as less than significant upon implementation of regulatory requirements in the 2010 Certified EIR.

5.9.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?				x	
b)	Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?				x	

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
c)	Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?					x
d)	Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code § 65962.5 and, as a result, would it create a significant hazard to the public or the environment?				x	
e)	For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?				x	
f)	Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?				х	
g)	Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?					X

a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that TOP would increase the number of businesses and residents in the City, which would increase the amount of hazardous materials being transported, stored, and manufactured and the number of people being exposed to these materials. Buildout in accordance with the Approved Project would result in an increase in the frequency of transport, use, and disposal of hazardous materials associated with commercial and industrial growth within Ontario, especially around ONT.

Future housing projects enabled by the Housing Element Update may involve the routine transport, use, or disposal of potentially hazardous materials during construction and occupancy. During occupancy, common cleaning substances, maintenance products, and similar items would be stored and used in a residential capacity. Potentially hazardous substances such as these would not occur in sufficient quantities to pose a significant hazard to human and environmental health. Furthermore, the use of hazardous materials is regulated by federal and state regulations, City ordinances, and TOP. These regulate the handling of hazardous substances to reduce

potential releases and exposure and the risks of transporting, storing, treating, and disposing of hazardous materials and waste.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the routine transport, use, or disposal of hazardous materials. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that TOP would increase the number of businesses and residents in the City, thereby increasing the amount of hazardous materials being stored and manufactured and the amount of people being exposed to these materials. Impacts of the Approved Project were identified as less than significant due to compliance with federal, state, and City regulations, along with TOP policies.

As stated previously, future housing projects enabled by the Housing Element Update may involve the routine transport, use, or disposal of potentially hazardous materials during construction and occupancy. Potentially hazardous substances such as these would not occur in sufficient quantities to pose a significant hazard to human and environmental health. Furthermore, the use of hazardous materials is regulated by current federal and state regulations, City ordinances, and TOP. With these regulations, permits, and codes in place, the Housing Element Update would not create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the release of hazardous materials into the environment as a result of reasonably foreseeable upset and accident conditions. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

No Impact. The 2010 Certified EIR identified that pursuant to the current Development Code, Hazardous Waste Overlay Districts do not allow placement of facilities that handle, manufacture, or transport hazardous waste within 2,000 feet of a residential or institutional lot line (Ontario Development Code Sections 9-1.2815,

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9-1.2830). Thus, the Approved Project would not result in the placement of hazardous waste-generating facilities within a quarter mile of a school, and development would follow regulations set by the current Development Code, reducing impacts of the Approved Project to less than significant.

The Housing Element Update would not result in facilities that emit hazardous emissions. Furthermore, the use of hazardous materials is regulated by current federal and state regulations, City ordinances, and TOP. With these regulations, permits, and codes in place, the Housing Element Update would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the emission of hazardous emissions or handling of hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code § 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that most of the sites that use, transport, and release hazardous materials are in industrial and business park developments. Because of the high number of hazardous materials sites, there is potential for contamination of soils and groundwater, but new development in these areas associated with buildout of the Approved Project would be required to follow the regulations of the Hazardous Waste District. The majority of new development sites would be in the same general areas as existing sites and would not expand into sensitive communities. No significant impacts would occur from implementation of the Approved Project due to compliance with current City ordinances and federal, state, and local regulations.

The Housing Element Update does not identify future development at any particular location in the City. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Future housing development project proposals would be reviewed by City Planning to ensure that implementation of the Housing Element Update would not create a hazard to the public or the environment. Furthermore, the Housing Element Update would not result in new development beyond what is identified in the Land Use Plan.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require

adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the location of a site that is included on a list of hazardous materials sites and which would create a significant hazard to the public or the environment. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that ONT is in the central northern portion of Ontario. The majority of the surrounding land uses are industrial, business, and commercial. The runways of ONT run east—west, and the departure and arrival safety zones are at the ends of the runways. Portions of Mission Boulevard, Grove Avenue, Haven Avenue, Carnegie Avenue, Santa Ana Street, and Commerce Way are within the safety zones. It is the policy of the City to coordinate with the airport authorities to ensure that proposed land uses within the airport safety zones are consistent with the land-use compatibility plans for the Chino Airport and ONT. The City requires land-use compatibility reviews for designated areas near an airport in addition to compatibility with noise and safety zones in terms of land-use density and height. Furthermore, the City requires permits for development prior to construction. Impacts were identified as less than significant in the 2010 Certified EIR.

City review of future projects within the airport safety zones and more generally near the airports, including future housing development during implementation of the Housing Element Update, ensures that these uses near airports would not result in safety hazards to people in the area.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to safety hazards involving projects within an airport land-use plan or within two miles of a public-use airport. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the City manages disaster preparedness through the Technical Services Bureau of the Ontario Fire Department. This bureau is responsible for preparing the community for disasters and the organization of recovery efforts. The Approved Project would not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan because the Safety Element of TOP encourages establishment and incorporation of emergency plans. Impacts were identified as less than significant.

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The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Under the Approved Project and the Housing Element Update, future housing development would be reviewed by the City Planning Department to ensure adequate ingress and egress along roadways, as mandated by the City Municipal Code. Therefore, future development would not alter emergency response or evacuation plans.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to impairing implementation of or physically interfering with an adopted emergency response plan or emergency evacuation plan. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

No Impact. The 2010 Certified EIR identified that Ontario is within an area of moderate wildland fire threats. The San Gabriel Mountains and the Chino Hills are both designated extreme fire hazard areas. If fire fighters were to lose control of an extreme fire, portions of Ontario could be at risk. To help protect the City and its residents from fire hazards, Ontario has enacted building and fire codes. The Fire Chief may also use his or her authority to instate certain building, planning, or landscaping requirements. Building plans in Ontario must be reviewed by the Ontario Fire Department to ensure their compliance with the City's Fire Code.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not result in new impacts from wildfire hazards that were not identified in the 2010 Certified EIR. Additionally, the City of Ontario requires new development to adhere to the latest California Building Code for fire safety. New development in the City would be reviewed by the Ontario Fire Department to comply with requirements in effect at the time building permits are issued. Adherence to the City's Governance Manual process ensures that sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel. No impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to exposing people or structures to a significant risk due to wildland fires. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.9.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant hazards and hazardous materials impacts upon implementation of regulatory requirements, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.10 HYDROLOGY AND WATER QUALITY

5.10.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would increase surface-water flows into drainage systems within the watershed. However, stormwater infrastructure would be designed to accommodate flows in accordance with the San Bernardino County Hydrology Manual. The Approved Project would also increase the amount of impervious surfaces in the City, but this increase would not hinder the groundwater recharge efforts of the Chino Basin Watermaster. Portions of the City are in the 100year flood hazard zone, but the City would require structures to be above the base flood elevation. Furthermore, buildout of the Approved Project would increase pollutant concentrations during construction activities, but pollutant discharges would be minimized through implementation of BMPs specified in the water quality management plan. Portions of the City are within the inundation area for the San Antonio Dam, but the probability of catastrophic failure is low, especially when coupled with existing emergency evacuation procedures. Lastly, implementation of the Approved Project would not create hazards of inundation by seiche or mudflow. Altogether, impacts were identified as less than significant in the 2010 Certified EIR.

5.10.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?				x	
b)	Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?				x	

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
c)	Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:					
	 result in substantial erosion or siltation on- or off-site; 				X	
	ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site;				x	
	iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or				x	
	iv) impede or redirect flood flows?				X	
d)	In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?					X
e)	Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?				x	

a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or groundwater quality?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that development would increase surface-water flows into drainage systems within the watershed. Projects considered for approval under the Approved Project would be required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects would be mandated to comply with BMPs for compliance with National Pollution Discharge Elimination System (NPDES) permit requirements. No significant impacts were identified in the 2010 Certified EIR.

Water-quality degradation from erosion impacts would be specific to future project sites that could be developed and/or redeveloped as a result of implementing the Housing Element Update and depend largely on the areas affected and the length of time soils would be subject to erosion. Although implementation of the Housing Element Update may result in runoff during construction of individual projects, including high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core,

development requiring ground disturbance would be subject to regional and local regulations, including the need for a SWPPP under NPDES No. CAS000002. Furthermore, the City requires grading permits for all grading-related development, which in turn would be required to have an approved Erosion Control Plan. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to water-quality standards and waste discharge requirements. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would increase the amount of impervious surfaces in the City, but this increase would not hinder the groundwater recharge efforts of the Chino Basin Watermaster. Projects considered for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Projects must comply with BMPs consistent with NPDES requirements.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not substantially increase impermeable surface area such that groundwater recharge would be substantially affected. Additionally, the Housing Element Update must comply with BMPs identified in the project-specific hydrology studies required by the City.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to substantially decreasing groundwater supplies or interfering substantially with groundwater recharge such that sustainable groundwater management of the basin would be impeded. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

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- c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:
 - i) result in a substantial erosion or siltation on- or off-site;

Less Than Significant Impact/No Changes or New Information. Cucamonga Creek Flood Control Channel, Lower Deer Creek Channel at Chris Basin, Lower Etiwanda Creek Channel, West Cucamonga Channel, Day Creek Channel, and the Etiwanda Creek/Channel are the major water channels that flow through the City. The 2010 Certified EIR identified that development would increase surface-water flows into drainage systems within the watershed. Projects considered for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects are mandated to comply with BMPs for compliance with NPDES requirements. The City would also encourage the use of low-impact development strategies to intercept runoff, slow the discharge rate, increase infiltration, and reduce discharge volumes. No significant impacts were identified in the 2010 Certified EIR.

The Housing Element Update encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Future housing development encouraged by the Housing Element Update could alter existing drainage patters in the City due to an increase in building footprint. Nevertheless, any construction activities would be subject to regulations related to water quality, erosion, and stormwater runoff. Pursuant to the City Development Code, individual projects associated with the Housing Element Update would be subject to review by the City prior to issuance of a grading permit, which requires preparation of a drainage study and SWPPP.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to altering the existing drainage pattern in a manner that would result in substantial erosion or siltation on- or off-site. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site;

Less Than Significant Impact/No Changes or New Information. Ontario receives a significant amount of runoff from watershed areas in the San Gabriel Mountains that flows into flood-control channels and water channels in the City. Buildout in accordance with the Approved Project would alter the

existing land uses within the City, but would not result in changes to existing drainage patterns and watercourses. Increased urbanization resulting from the Approved Project may increase the amount of runoff from impervious surfaces and result in flooding. Projects considered for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects are mandated to comply with BMPs for compliance with NPDES requirements. The City would also encourage the use of low-impact development strategies to intercept runoff, slow the discharge rate, increase infiltration, and reduce discharge volumes. No significant impacts were identified in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Moreover, pursuant to the City Development Code, individual projects associated with the Housing Element Update would be subject to review by the City prior to issuance of a grading permit, which requires preparation of a drainage study and SWPPP.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to altering the existing drainage pattern in a manner that would result in flooding on- or off-site. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or

Less Than Significant Impact/No Changes or New Information. Development in accordance with the Approved Project would involve alteration and redistribution of land uses within the City. Increased urbanization may increase the amount of runoff and discharge of sediments and pollutants to stormwater drainage systems. Projects considered for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects are mandated to comply with BMPs and NPDES requirements. The City would also encourage the use of low-impact development strategies to intercept runoff, slow the discharge rate, increase infiltration, and reduce discharge volumes. No significant impacts were identified in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Moreover, pursuant to the City Development Code,

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individual projects associated with the Housing Element Update would be subject to review by the City prior to issuance of a grading permit, which requires preparation of a drainage study and SWPPP.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to creating or contributing runoff water that would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

iv) impede or redirect flood flows?

Less Than Significant Impact/No Changes or New Information. Development per the Approved Project would involve alteration of land-use designations within the City and would include a large infill development in the mostly undeveloped areas to the south. Current and future uses may result in discharge of sediment and pollutants to existing stream courses. Projects considered for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects are mandated to comply with BMPs for compliance with NPDES requirements. No significant impacts were identified in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, and thus would not otherwise substantially degrade water quality. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Nevertheless, construction activities would be subject to regulations related to water quality, erosion, and stormwater runoff. Pursuant to the City Development Code, individual projects associated with the Housing Element Update would be subject to review by the City prior to issuance of a grading permit, which requires preparation of a drainage study and SWPPP.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to impeding or redirecting flood flows. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

No Impact. The 2010 Certified EIR identified that Ontario is not on the coast and would not be affected by tsunamis. A seiche is a surface wave created when a body of water is shaken, usually by earthquake activity. Seiches could occur downstream of reservoirs due to ground shaking at the reservoirs. Also, dams near Ontario may create flooding impacts. Mudflows could occur in drainage channels in Ontario during flash floods, but

are not expected to pose a substantial hazard in the City due to the gently sloped terrain. The City's Flood Damage Prevention Program prohibits encroachments into the San Bernardino County Flood Control District's right-of-way (which includes drainage channels), with specified exceptions. The San Bernardino County Flood Control District, whose approval is required for any project that proposes alterations to a channel, maintains drainage channels in the City. TOP Policies S21 through S26 address seiche flooding from buildout, and the 2010 Certified EIR concluded that the related impacts of the Approved Project were less than significant.

The Housing Element Update encourages high-density, mixed-use, infill development and creative reuse of underutilized and/or defunct properties within the urban core. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Facilities and infrastructure built as a result of the Housing Element Update would be reviewed for adherence to TOP policies, the City's Flood Damage Prevention Program, and San Bernardino County Flood Control District encroachment permits.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to inundation by seiche, tsunami, or mudflow. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR did not identify any significant impacts associated with obstruction of a water quality control plan or sustainable groundwater management plan. Future development would be required to adhere to the State General Construction Permit, implement a project-specific SWPPP, and adhere to the City's Erosion and Sediment Control Plan requirements. These requirements would ensure that future development does not adversely impact surface and groundwater quality. In addition, implementation of low impact development and BMPs would ensure that water quality is not impacted during future development. As a result, future housing development under the Housing Element Update would not obstruct or conflict with the implementation of the Santa Ana River Basin Water Quality Control Plan. The City manages both the potable and nonpotable supplies to ensure withdrawals from the Chino Groundwater Basin do not exceed the safe yield for the Basin, as per the Chino Basin Watermaster's Optimum Basin Management Program (OBMP). Therefore, the project would not obstruct or conflict with the OBMP and impacts would be less than significant.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Facilities and infrastructure built as a result

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of the Housing Element Update would be reviewed for adherence to TOP policies, the City's Flood Damage Prevention Program, and San Bernardino County Flood Control District encroachment permits.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to conflicting with or obstructing implementation of a water quality control plan or sustainable groundwater management plan. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.10.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant hydrology and water quality impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.11 LAND USE AND PLANNING

5.11.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would not divide an established community, would not conflict with applicable plans adopted for the purpose of avoiding or mitigating an environmental effect, and would not conflict with the adopted Oakmont Industrial Group Habitat Conservation Plan. Implementation of regulatory requirements and standard conditions of approval ensured that no significant impacts would occur.

5.11.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Physically divide an established community?					Х
b)	Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?					X

a) Physically divide an established community?

No Impact. As identified in the 2010 Certified EIR, the City of Ontario is an urbanized area, and implementation of TOP would not physically divide an established community. Thus, impacts were identified as less than significant in the 2010 Certified EIR.

The Housing Element Update does not alter land use designations and does not confer entitlements for development, and thus would not physically divide an established community. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As stated in the Housing Element, neighborhood identity and preservation is encouraged. The Land Use Element has specific policies for compatibility that would reduce the amount of conflict between contradicting land uses. These include:

- LU2-1 Land Use Decisions. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.
- LU2-2 Buffers. We require new uses to provide mitigation or buffers between existing uses where potential adverse impacts could occur.
- LU2-8 Transitional Areas. We require development in transitional areas to protect the quality of life of current residents.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update

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compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to physically dividing an established community. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

No Impact. The 2010 Certified EIR identified that TOP would provide sufficient dwelling units, population, and employment capacity that would exceed SCAG's projections for 2030. However, the Approved Project was considered consistent with SCAG's Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS), which aims to improve the mobility, livability, and prosperity of a region. Implementation of the Approved Project would not result in significant land use impacts related to specific SCAG policies, goals, and principles.

Several regionally and locally adopted land-use plans, policies, and regulations would be applicable to development of the Housing Element Update. These would include the South Coast 2016 AQMD's AQMP, SCAG's Connect SoCal 2020–2045 RTP/SCS, the Airport Land Use Compatibility Plan for the Ontario International Airport (ALUCP ONT), and the Chino Airport Master Plan. The Housing Element Update would remain consistent with these plans. The Housing Element Update does not alter land use designations and does not confer entitlements for development. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future housing development encouraged by the Housing Element Update would be consistent with the Land Use plan and would not conflict with a land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect, due to an increase in building footprint. Therefore, the Housing Element Update would not conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over projects adopted for the purpose of avoiding or mitigating an environmental effect. No impacts would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to conflicting with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project adopted for the purpose of avoiding or mitigating an environmental effect. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.11.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant land-use and planning impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.12 MINERAL RESOURCES

5.12.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would not result in the loss of availability of a known mineral resource due to existing regulations and land uses. Upon implementation of regulatory requirements and standard conditions of approval, impacts of the Approved Project would be less than significant.

5.12.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Result in the loss of availability of a known mineral resource that would be a value to the region and the residents of the state?					х
b)	Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?					x

a) Result in the loss of availability of a known mineral resource that would be a value to the region and the residents of the state?

No Impact. The 2010 Certified EIR identified that active mines or quarries producing construction aggregates within the City are predominantly on the eastern side of the City. The land-use designations within TOP allow for mining operations to extract minerals in the MRZ2 areas. However, prior to permitting a use that would threaten the extraction of minerals in a MRZ2 area, the City is required under the Surface Mining and Reclamation Act (SMARA) to prepare a statement specifying its reasons for permitting the proposed use, and consider the importance of these minerals to their market region as a whole. Thus, impacts were identified as less than significant in the 2010 Certified EIR.

All of the existing active mines within the City would continue to operate upon implementation of the Housing Element Update. The Housing Element Update would not change the land-use designations or affect the ability of mining operations to extract minerals in the MRZ2 areas. Future housing development or redevelopment associated with the Housing Element Update within the MRZ2 designated areas would require City planning and environmental review to ensure that mining operations would not be affected pursuant to SMARA.

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Furthermore, the Housing Element Update would not result in new development beyond what is identified in the Land Use Plan. No impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the loss of availability of a known mineral resource that would be of value to the region and residents of the state. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

No Impact. The 2010 Certified EIR identified that the locally important mineral resource recovery sites delineated in TOP are the Deer and Day Fans Resources Sectors D-14 and D-15, and the Day Creek Fan, Mira Loma Area Resources Sectors D-2, D-3, and D-5. The land-use designations within TOP still allow for mining operations to extract minerals in the MRZ-2 areas; however, prior to permitting a use that would threaten extraction of minerals in a MRZ-2 area, the City is required under SMARA to prepare a statement specifying its reasons for permitting the proposed use, and consider the importance of these minerals to their market region as a whole. Thus, impacts were identified as less than significant in the 2010 Certified EIR.

As described above, future housing development or redevelopment in the MRZ-2 designated areas associated with the Housing Element Update would require City planning and environmental review to ensure that mining operations would not be affected. Furthermore, the Housing Element Update would not result in new development beyond what is identified in the Land Use Plan. No impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts resulting in the loss of availability of a locally important miner resource recovery site delineated in a local general plan, specific plan, or other land use plan. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.12.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant mineral resources impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.13 NOISE

5.13.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would result in an increase in traffic on local roadways in the City, which would substantially increase the existing noise environment. Sensitive land uses within the 65 dBA CNEL (community noise equivalent-level) noise contour of the ONT airport would be exposed to substantial levels of airport-related noise. Construction activities would expose sensitive uses to strong levels of groundborne vibration. In addition, construction activities would substantially elevate noise levels in the vicinity of noise-sensitive land uses. Mitigation measures would reduce other noise-related impacts to the extent feasible; however, traffic noise increase, construction noise, and construction vibration were identified as significant unavoidable impacts of the Approved Project.

5.13.2 Impacts Associated with the Proposed Project

Would the project result in:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?				x	
b)	Generation of excessive groundborne vibration or groundborne noise levels?				х	
c)	For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?				x	

a) Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that buildout of the Approved Project would result in potentially significant impacts stemming from the addition of vehicles along roadways. The greatest increases in noise are expected in areas subject to increased land-use

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intensity. Individual projects would occur over many years, and the increase in noise on an annual basis would not be readily discernable because traffic and noise would increase incrementally. Cumulative increases in the ambient noise environment along major transportation corridors as a result of substantial increase in traffic volumes were identified as significant in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate noise levels greater than that identified in the 2010 Certified EIR. Development under the Housing Element Update would be not substantially increase noise levels identified in the 2010 Certified EIR. Consequently, the Housing Element Update would not create noise that would violate the City's noise standards.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance or applicable standards of other agencies. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Generation of excessive groundborne vibration or groundborne noise levels?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that construction-related noise and vibration could expose sensitive receptors to substantial noise and vibration levels. Mitigation Measures 12-2 and 12-4 would reduce impacts associated with construction activities to the extent feasible; however, due to the proximity of construction activities to sensitive uses and potential longevity of construction activities, noise and vibration would be significant and unavoidable.

Future housing development encouraged by the Housing Element Update could cause the generation of excessive construction-related noise and ground borne vibration or ground borne noise levels; however, the Housing Element Update does not alter land use designations and does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not substantially increase construction-related noise and vibration. Additionally, Mitigation Measures 12-2 and 12-4 would reduce impacts associated with construction activities to the extent feasible.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require

adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to the generation of excessive ground borne vibration or ground borne noise levels. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that sensitive land uses within the 65 dBA CNEL (community noise equivalent-level) noise contour of the ONT would be exposed to substantial levels of airport-related noise. Mitigation Measure 12-1 was incorporated to reduce impacts. While interior noise levels are required to achieve the interior noise limits of Title 24 and Title 25, which require structures to achieve 45 dBA CNEL, exterior noise levels may continue to exceed the noise compatibility criteria for the City. Consequently, airport noise compatibility was identified as a significant unavoidable impact of the Approved Project.

The Housing Element Update does not alter land use designations and does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not substantially increase airport-noise-related hazards. Additionally, Mitigation Measure 12-1 would reduce impacts associated with airport noise compatibility to the extent feasible.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to potential projects in the vicinity of a private airstrip. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.13.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measures were taken directly from the 2010 Certified EIR. Mitigation Measure 12-3 has been removed because it pertains to impacts of the environment on a project, which were determined to not be subject to CEQA. Mitigation Measure 12-1 was retained, with modifications to address airport-specific noise only. Modifications to the original mitigation measures are identified in strikeout text to indicate deletions and underlined to signify insertions. Mitigation Measure 6-5 still applies and would be implemented for the Housing Element.

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- Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour along major roadways, freeways, railroads, or the Los Angeles/ of the Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g. setbacks, berms, or sound walls) and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Titles 24 and 21 of the California Code of Regulations).
- Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers occurring-near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g. drilled piles to eliminate use of vibration-intensive pile driver).
- Prior to the issuance of building permits for any project that involves a vibration-sensitive use directly adjacent to the Union Pacific Railroad or Southern California Regional Rail Authority main lines shall retain an acoustical engineer to evaluate potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, mitigation measures, such as use of concrete, iron, or steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB during the nighttime for residential uses, 84 VdB for office uses, and 90 VdB for workshops.
- 12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures, such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes, shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.

5.14 POPULATION AND HOUSING

5.14.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would directly result in population growth in the project area. Buildout of the Approved Project would not displace people or housing and would not necessitate the construction of replacement housing. No significant adverse impacts are anticipated upon regulatory compliance and compliance with the Approved Project policies and programs.

5.14.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?				x	
b)	Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?					X

a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that implementation of the Approved Project would accommodate population growth through land-use designations, goals, and policies that would provide a vision and guide growth in the City. The increase in population, housing, and employment would exceed SCAG's regional forecasts for the City, but the Approved Project would improve the jobs-housing balance within the San Bernardino County Transportation Authority subregion. Furthermore, TOP would accommodate future growth by providing infrastructure and associated public services to accommodate the projected growth. TOP would also be consistent with SCAG's Compass Blueprint program (now called Sustainable Communities Program).

The Housing Element Update does not alter land use designations and does not confer entitlements for development. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate additional population greater than identified in the 2010 Certified EIR. Development under the Housing Element Update would be consistent with the anticipated growth forecast.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA

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Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to inducing substantial population growth in an area, either directly or indirectly. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

No Impact. The 2010 Certified EIR identified that the Approved Project would not change land-use designations from residential to nonresidential, and thus would not result in the displacement of housing. Furthermore, TOP guides planning for new growth in the City, in part through designation of land uses that result in additional housing. The land-use plan provides land-use designations for a variety of housing types and provides for additional residential opportunities in areas that previously did not allow residential uses.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Thus, the Housing Element Update would not displace a substantial number of people, necessitating replacement housing, and there would be no impact.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to displacing substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.14.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant population and housing impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.15 PUBLIC SERVICES

5.15.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would cause both fire protection services and police services to expand due to the introduction of new structures, residents, and workers within the City's boundaries. Furthermore, construction of new schools and/or classroom facilities would also be required upon buildout. Construction of an additional 44,409 square feet of library space and 1,007.6 acres of recreational

open space would also occur upon buildout. Altogether, impacts of the Approved Project would be less than significant upon implementation of regulatory requirements and standard conditions of approval.

5.15.2 Impacts Associated with the Proposed Project

Would the project:

Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a) Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the public services:					
i) Fire protection?				Х	
ii) Police protection?				Х	
iii) Schools?				X	
iv) Parks?				X	
v) Other public facilities?				X	

a) Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the public services:

i) Fire protection?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would result in an increased number of people within the City, thereby resulting in an increase in demand for fire services and facilities. To ensure the provision of adequate fire protection services, the City of Ontario has established a Development Impact Fee (DIF) Program to provide funding for services within the City. Fees collected from developers are placed in a fire services fund that can be expended for the acquisition or construction of new fire services facilities and for the improvement or expansion of the City's existing fire service capabilities, provided that such expenditure from the fund has been authorized by the City Council. In addition, future projects are reviewed by the City of Ontario and

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the Ontario Fire Department on an individual basis and are required to comply with requirements in effect at the time building permits are issued. The 2010 Certified EIR determined that adherence to the City's Governance Manual process to achieve the City's development goals in phases, working within the budget and infrastructure constraints of the City, ensures that sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel upon buildout of TOP. Impacts to fire services were identified as less than significant.

New residential development associated with the Housing Element Update would result in an increased demand for fire services and facilities. However, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for fire protection greater than that identified in the 2010 Certified EIR. Furthermore, new development in the City would be reviewed by the Ontario Fire Department to comply with requirements in effect at the time building permits are issued. Adherence to the City's Governance Manual process ensures that sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to the number of calls and requests for fire services within the service area. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

ii) Police protection?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that buildout of the Approved Project would result in an increase in demand for police protection services, which in turn would require the hiring of new staff and building of new facilities. Future projects would also be reviewed by the City of Ontario on an individual basis and required to comply with regulations in effect at the time building permits are issued (e.g., payment of DIF). The police services would receive adequate funding through the City's general fund to cover project needs. Furthermore, as mentioned above, the Governance Section of TOP would encourage collaboration between City departments, programs, and other involved agencies to achieve the City's development goals in phases that are within the fiscal and infrastructure limitations of the City. Impacts to police services were identified as less than significant.

New residential development associated with the Housing Element Update would result in an increased demand for police services and facilities. However, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element

Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for police protection greater than identified in the 2010 Certified EIR. Furthermore, new development in the City would be reviewed by the Ontario Police Department to comply with requirements in effect at the time building permits are issued. Adherence to the City's Governance Manual process ensures that sufficient revenue would be available for necessary service improvements to provide for adequate police facilities, equipment, and personnel.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to police services. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

iii) Schools?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that development in accordance with the Approved Project would require payments to the corresponding school district for the construction of new schools. Developers would be required to pay the impact fees levied by each school district, set within the limits of California Senate Bill 50 (SB 50). The 2010 Certified EIR identified that payment of impact fees reduced impacts to a less than significant level.

New residential development associated with the Housing Element Update would result in an increase demand for school services and facilities; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for police protection greater than identified in the 2010 Certified EIR. Furthermore, payment of impact fees in compliance with SB 50 would reduce the impacts to school facilities to an acceptable level.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to schools. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

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iv) Parks?

Less Than Significant Impact/No Changes or New Information. TOP would include 1,007.6 acres of recreational open space. Population growth associated with the Approved Project buildout would increase the use of neighborhood and regional parks. New development is required to provide five acres per 1,000 residents in accordance with the City's park dedications and in-lieu fee regulations (Ontario Municipal Code Section 9-2.1515). Fees collected go toward acquiring the five acres of public parkland per 1,000 residents generated by the development. Impacts were identified as less than significant.

New residential development associated with the Housing Element Update would result in an increased demand for park services and facilities; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for parks greater than identified in the 2010 Certified EIR. Furthermore, new development in the City would be reviewed by the City to ensure compliance with the City's park dedications and in-lieu fee regulations (Ontario Municipal Code Section 9-2.1515).

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to parks. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

v) Other public facilities?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that buildout of TOP would result in an increase in demand for library services in the City. Future projects would also be reviewed by the City of Ontario on an individual basis and would be required to comply with requirements in effect at the time building permits are issued (e.g., payment of DIF). The City's general fund and payment of fees would offset the costs associated with library services; therefore, impacts to library services were identified as less than significant in the 2010 Certified EIR.

New residential development associated with the Housing Element Update would result in an increased demand for library services and facilities; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for

libraries greater than that identified in the 2010 Certified EIR. Furthermore, new development in the City would be reviewed to comply with requirements in effect at the time building permits are issued (e.g., Development Impact Fees).

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to public facilities. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.15.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant public services impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.16 RECREATION

5.16.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would generate additional residents, which would increase the use of existing park and recreational facilities. Project implementation would result in environmental impacts from the provision of new and/or expanded recreational facilities. Upon implementation of regulatory requirements and compliance with TOP policies and programs, impacts of the Approved Project would be less than significant.

5.16.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?				x	

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
b)	Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?					x

a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that population growth associated with the Approved Project buildout would increase the use of neighborhood and regional parks. New development is required to provide five acres per 1,000 residents in accordance with the City's park dedications and in-lieu fee regulations (Ontario Municipal Code Section 9-2.1515). Fees collected go toward acquiring the five acres of public parkland per 1,000 residents generated by the development. Impacts were identified as less than significant. Therefore, recreational impacts of the Approved Project were identified as less than significant.

New residential development associated with the Housing Element Update would result in an increased demand for park services and facilities; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for parks greater than identified in the 2010 Certified EIR. The Parks and Recreation Element contains relevant goals, policies, and programs that support a regular review of the City's parks and trails plans to keep pace with demographic trends and recreational needs of Ontario's residents. Furthermore, new development in the City would be reviewed by the City to ensure compliance with its park dedications and in-lieu fee regulations (Ontario Municipal Code Section 9-2.1515).

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to increasing the use of existing neighborhood and regional parks such that substantial physical

deterioration of the facility would occur or be accelerated. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment?

No Impact. The 2010 Certified EIR identified that TOP would warrant expansion of the equestrian and hiking trails and construction of improved bikeways throughout the City. TOP contains goals, policies, and actions and existing federal, state, and local regulations that would mitigate potential adverse impacts to the environment that may result from buildout of the Land Use Plan, including expansion of parks, recreational facilities, and multiuse trails. Furthermore, subsequent environmental review would be required for development of park projects under the Land Use Plan. Consequently, the 2010 Certified EIR did not identify significant impacts regarding construction or expansion of recreational facilities.

The Housing Element Update does not include recreational facilities or construction or expansion of recreational facilities in the City. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Therefore, no impacts would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to including recreational facilities or requiring construction or expansion of recreational facilities, which might have an adverse physical effect on the environment. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.16.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant recreation impacts, and therefore no mitigation measures were identified in the 2010 Certified EIR.

5.17 TRANSPORTATION/TRAFFIC

5.17.1 Summary of Impacts Identified in the Program EIR

Effective July 1, 2020, California Senate Bill 743 mandated specific types of CEQA analysis of a project's transportation impacts. Prior to implementation of SB 743, CEQA transportation analyses of individual projects typically determined impacts on the circulation system in terms of roadway delay (i.e., congestion) and/or capacity usage at specific locations, such as street intersections or freeway segments. SB 743 required changes to the guidelines for CEQA transportation analysis. The changes include the elimination of auto delay,

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LOS, and other similar measures of vehicular capacity or traffic congestion as a basis for determining transportation impacts. The purpose of SB 743 is to promote the reduction of greenhouse gas emissions, the development of multimodal transportation networks, and a diversity of land uses. Under SB 743, a project's effect on automobile delay shall not constitute a significant environmental impact under CEQA. Therefore, level of service (LOS) and similar vehicle delay or capacity metrics may no longer serve as transportation impact metrics for CEQA analysis. The California Office of Planning and Research has updated the CEQA Guidelines and provided a final technical advisory (December 2018), which recommends VMT as the most appropriate measure of transportation impacts under CEQA. The California Natural Resources Agency certified and adopted the CEQA Guidelines, including the Guidelines section implementing SB 743. The changes were approved by the Office of the Administrative Law and are in effect. The 2010 Certified EIR concluded that buildout of the Land Use Plan would cumulatively contribute to the cumulatively significant freeway LOS impacts; however, as stated previously, these are no longer considered environmental impacts.

The 2010 Certified EIR identified that circulation improvements under the recommended circulation plan would be designed to adequately address potential hazardous conditions, potential conflicting uses, and emergency access. Furthermore, the recommended circulation plan would comply with adopted policies, plans, and programs for alternative transportation.

5.17.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?					x
b)	Conflict or be inconsistent with CEQA Guidelines § 15064.3, subdivision (b)?				х	
c)	Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?					x
d)	Result in inadequate emergency access?					X

a) Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

No Impact. The 2010 Certified EIR identified that the recommended circulation plan would comply with adopted policies, plans, and programs for alternative transportation. The Housing Element Update supports various regional transportation planning efforts in the City by aligning opportunities for higher-density housing along planned bus rapid transit routes. Therefore, the Housing Element Update would conform with the goals of the applicable plans, ordinances, or policies establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation, including mass transit and nonmotorized travel. Therefore, no impacts would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to conflicting with an applicable plan, ordinance, or policy establishing measures of effectiveness for the performance of the circulation system. Future development under the Housing Element Update would conform with policies found in TOP. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Conflict or be inconsistent with CEQA Guidelines § 15064.3, subdivision (b)?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR did not assess VMT impacts because certification of the EIR predated SB 743 adoption.

Development enabled by policies in the Housing Element Update would implement measures within Ontario designed to reduce per-capita VMT. Ontario is a jobs-rich city. The introduction of additional housing brings more residents in close proximity to job opportunities within the City, reducing commute times, and improving VMT. In addition, many housing sites, such as those around the Ontario Mills Mall would bring residents closer to existing shopping destinations, providing more opportunities to walk for daily needs and resulting in fewer vehicle miles per person. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate more VMT than what would have been generated by the Approved Project. Additionally, the Housing Element Update would likely result in lower VMT per capita as a result of policies designed to encourage higher density development and affordable housing. As a result, the Housing Element Update would not conflict with CEQA Guidelines Section 15064.3, subdivision (b).

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA

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Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to CEQA Guidelines Section 15064.3, subdivision (b). Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

No Impact. The 2010 Certified EIR identified that buildout of the Land Use Plan would result in changes to the circulation network but would not increase hazards due to design features. The Housing Element Update would not include facilities that would substantially increase hazards, nor would it construct incompatible uses. Future development is subject to the Plan Check Review Process and subsequent environmental review to ensure there would be no increase in hazards to vehicles. No impacts resulting from the Housing Element Update would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to substantially increasing hazards due to a geometric design feature or incompatible use. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Result in inadequate emergency access?

No Impact. The 2010 Certified EIR identified that the roadway classification standards adopted as part of TOP would preclude the construction of unsafe features, thus ensuring adequate emergency access. Additionally, pursuant to the City's Design Review process, as mandated by the City Development Code, projects are reviewed for emergency access.

The Housing Element Update would not include facilities that would affect emergency access. Future development is subject to the Plan Check Review Process and subsequent environmental review to ensure there would be no alteration to emergency access or evacuation plans. Infrastructure along roadways would be reviewed prior to approval by the City Planning Department pursuant to the City Development Code to ensure adequate ingress and egress. No impacts resulting from the Housing Element Update would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), no changes proposed by the Housing Element Update compared to the Approved Project would result in any new or substantially more severe significant impacts with respect to inadequate emergency access. Therefore, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.17.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measure from the 2010 Certified EIR is no longer applicable because it regards level of service, which is no longer a CEQA impact under SB 743. Additionally, the lane geometry recommendations have since been made in the Mobility Element.

16-1 The Mobility Element of 'The Ontario Plan shall be consistent with the traffic study prepared by Kimley-Horn and Associates in 2009. Table 5.16-6 shows the recommended lane geometry for the Proposed Land Use Plan.

5.18 TRIBAL CULTURAL RESOUCES

5.18.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR identified impacts to prehistoric archeological resources, which include tribal cultural resources, as a potentially significant impact of the Approved Project. As part of the 2010 Certified EIR, the City of Ontario conducted tribal consultation pursuant to Senate Bill 18 (SB 18) to identify resources that may be of cultural value to California Native American tribes. Mitigation Measures 5-3 and 5-4 were incorporated to reduce impacts to tribal cultural resources to less than significant levels.

5.18.2 Impacts Associated with the Proposed Project

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code § 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:					
	i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or					х

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
ii)	A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code § 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code § 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.					X

- a) Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code § 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:
 - i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k),
 - ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code § 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code § 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified impacts to tribal cultural resources as a potentially significant impact of the Approved Project. Mitigation Measures 5-3 and 5-4 were incorporated to reduce impacts to tribal cultural resources to less than significant levels.

The City requested a local government tribal consultation list from the California Native American Heritage Commission (NAHC) on June 9, 2021. The tribal consultation list was requested in accordance with SB 18 requirements for a housing element. The NAHC responded on June 22, 2021, and provided a list of tribes for the City to contact regarding potential consultation. The City sent initial notification letters to California Native American tribes and tribal contacts on August 31, 2021. Responses were received from the Agua Caliente Band of Cahuilla Indians, Gabrieleño Band of Mission Indians–Kizh Nation, Quechan Tribe of the Fort Yuma Reservation, and San Manual Band of Mission Indians. The tribes confirmed that there are no conflicts with the proposed Housing Element Update.

Residential development in the City could impact tribal cultural resources. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. In the event that future housing development projects do affect previously undiscovered archaeological resources, Mitigation Measures 5-3 through 5-4 of the 2010 Certified EIR shall apply. As requested by the tribes, at the time of future discretionary development, the City will consult with tribes and discuss the City's standard conditions of approval that may be applicable to future residential development in accordance with Assembly Bill 52 and SB 18, as applicable.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to tribal cultural resources. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.18.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following tribal cultural resources mitigation measures from the Cultural Resources section were taken directly from the 2010 Certified EIR. All of these mitigation measures apply to and would be implemented for the Housing Element Update. Modifications to the original mitigation measures are identified in strikeout text to indicate deletions and <u>underlined</u> to signify insertions.

- Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment proposed project subject to CEQA and is within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.
- Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment proposed project for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and

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human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.

5.19 UTILITIES AND SERVICE SYSTEMS

5.19.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR concluded that the Approved Project would require additional water storage and/or supply to accommodate water demand associated with buildout. Buildout would also generate additional wastewater, which would be adequately treated in accordance with the Santa Ana Regional Water Quality Control Board and California Department of Public Health requirements. Furthermore, storm drainage systems would be expanded to accommodate growth. Also, buildout of the Approved Project would be served by landfills with sufficient permitted capacities to accommodate the project's solid-waste disposal needs. Existing and/or proposed facilities would also be able to accommodate project-generated utility demands (i.e., electricity and natural gas). Impacts to utilities would be less than significant upon implementation of regulatory requirements and standard conditions of approval.

5.19.2 Impacts Associated with the Proposed Project

Would the project:

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?				x	
b)	Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?				х	
c)	Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?				x	

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
d)	Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?				x	
e)	Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?					X

a) Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR did not identify significant impacts to wet and dry infrastructure, such as water, wastewater, stormwater, electricity, natural gas, and telecommunication facilities. Development associated with the Approved Project would result in an increase demand for wet and dry infrastructure. However, review of the infrastructure facilities by the City of Ontario, payment of development impact fees, and existing regulatory requirements would ensure that infrastructure would be expanded to keep pace with urban development associated with the Approved Project.

Residential development associated with the Housing Element Update would also result in additional demand for new/expanded wet and dry infrastructure in the City; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate demand for fire protection greater than that identified in the 2010 Certified EIR. As a result, demand for new/expanded wet and dry infrastructure would be similar to that identified for the Approved Project. Additionally, like the Approved Project, review of the infrastructure facilities by the City of Ontario, payment of development impact fees, and existing regulatory requirements would ensure that infrastructure would be expanded to keep pace with urban development associated with the Approved Project.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with

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respect to utilities and service systems. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would result in an increase in development that could create a potentially significant impact on sources of potable and nonpotable water in the City. Compliance with the goals and policies of TOP as well as existing regulatory requirements would ensure less than significant impacts.

Residential development associated with the Housing Element Update would result in an increase in potable and nonpotable water demand within the City. Development enabled by policies in the Housing Element Update would implement measures in Ontario designed to reduce per-capita water use. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate water demand greater than that identified in the 2010 Certified EIR. As a result, water demand associated with the Housing Element Update would not exceed that identified in the City's 2020 Urban Water Management Plan for normal, dry, and multiple dry years. Future development proposed under the Housing Element Update would also minimize water use through use of recycled water, water-efficient appliances and landscaping required by the California Building Code, and compliance with the City's Water Efficient Landscaping Ordinance.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years. Furthermore, future environmental review of development projects would ensure minimal impacts to water supplies. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that future growth in accordance with TOP would result in an increase in wastewater flow to the Inland Empire Utilities Agency's (IEUA) regional water reclamation plants. To ensure that the regional water reclamation plants

have sufficient capacity to serve new development in addition to existing development, the IEUA prepares a wastewater treatment master plan annually with flow projections for all of its contracting agencies. IEUA has a capital improvement plan to develop needed capacity and charges a capacity fee to new development to fund the needed capacity. The IEUA improvement plan is sequenced based on the rate of development to ensure adequate treatment capacity exists at the time building permits are issued without premature construction of unneeded capacity.

Residential development associated with the Housing Element Update would result in an increase in wastewater flows in the City that would require treatment at IEUA's regional water reclamation plants. The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Additionally, the 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772 housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate wastewater flows greater than those identified in the 2010 Certified EIR.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments. Furthermore, future environmental review of development projects would ensure minimal impacts to wastewater treatment. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Less Than Significant Impact/No Changes or New Information. The 2010 Certified EIR identified that the Approved Project would result in an increase in solid-waste disposal in the City. New development would require the need for additional service trucks, collection bins, and personnel, which would be offset by payment of development impact fees. Additionally, landfills have sufficient capacity to accept municipal solid waste associated with the Approved Project in addition to cumulative demands for solid waste disposal.

Residential development associated with the Housing Element Update would result in an increase in municipal solid waste disposal to landfills; however, the Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. Rather, the Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. The 2010 Certified EIR evaluated environmental impacts associated with an increase of 57,772

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housing units and 187,404 people. Residential growth associated with the Housing Element Update (see Table 1) would generate 20,854 residential units and would not generate solid waste disposal greater than identified in the 2010 Certified EIR.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to being served by a landfill with sufficient permitted capacity to accommodate the project's solid-waste disposal needs. Furthermore, future environmental review of development projects would ensure adequate capacity for solid waste. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

No Impact. The 2010 Certified EIR did not identify significant impacts for compliance with regulations regarding solid waste disposal. In an effort to reduce waste disposal, AB 939 required every California city and county to divert 50 percent of its waste from landfills by the year 2000. The City of Ontario has met this waste diversion requirement through local recycling programs and participation in regional recycling programs. Thus, the City of Ontario has met the state requirements for waste diversion and complies with regulations related to solid waste. The Housing Element Update would not result in additional solid waste demand above that identified for the Approved Project and would continue to comply with existing regulations regarding diversion and disposal of municipal solid waste. Therefore, there would be no impacts.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to compliance with federal, state, and local statutes and regulations related to solid waste. Furthermore, future environmental review of development projects would ensure that solid waste reduction goals would be met. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.19.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The following mitigation measures were taken directly from the 2010 Certified EIR. However, these mitigation measures have since been incorporated into the Policy Plan. Modifications to the original mitigation measures are identified in strikeout text to indicate deletions and underlined to signify insertions.

- The City shall include a policy in the Policy Plan that requires water conservation measures for development projects to improve water use efficiency and reduce overall water demand. Reduce potable water demand, through conservation measures, including but not limited to:
 - a) Work cooperatively with all developers to incorporate conservation measures into project designs (such as those recommended by the California Urban Water Conservation Council).
 - b) Continue to develop and implement drought contingency plans to assist citizens and businesses reduce water use during water shortages and emergencies.
 - c) Revise the City Code to include a Water-Efficient Landscape Ordinance to encourage or, as appropriate, require the use of water-efficient landscaping consistent with AB 1881.
- The City shall include a policy in the Policy Plan that maximizes the use of recycled water as an irrigation (nonpotable) source for landscaping, parks, and other irrigation opportunities in all areas of the City and requires use of recycled water in dual-system office and industrial uses in selected urban areas of the City, where available and feasible.
- The City shall include a policy in the Policy Plan that the City participate through the Chino Basin Water Master and the Inland Empire Utilities Agency in regional efforts to develop finding additional sources of water for groundwater recharge, such as capture of stormwater runoff, recycled water, or other sources to ensure that the Chino Basin stays in long term hydraulic balance and sustainability and that adequate additional local water sources would be available to increase the flexibility of the City's water supply.

5.20 WILDFIRE

5.20.1 Summary of Impacts Identified in the Program EIR

The 2010 Certified EIR did not identify impacts associated with wildfire hazards. The 2010 Certified EIR identified that the City had been mapped as having a moderate wildland fire threat. The San Gabriel Mountains and the Chino Hills are both designated extreme fire hazard areas. If firefighters were to lose control of an extreme fire, portions of Ontario could be at risk. Measures to confine or avoid fires—such as conforming to Ontario's Building and Fire Codes—would be implemented for all future development. Adherence to existing regulations and review of building plans by the Ontario Fire Department would reduce risks from fires in the City. In addition, buildout of the Approved Project would not affect the implementation of an emergency response or evacuation plan.

5.20.2 Impacts Associated with the Proposed Project

If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

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	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Substantially impair an adopted emergency response plan or emergency evacuation plan?				x	
b)	Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?					X
c)	Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?					x
d)	Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?				x	

a) Substantially impair an adopted emergency response plan or emergency evacuation plan?

Less Than Significant Impact/No Changes or New Information. Section 5.8, *Hazards and Hazardous Materials*, of the 2010 Certified EIR identified that the City of Ontario manages disaster preparedness through the Technical Services Bureau of the Ontario Fire Department. This bureau is responsible for the preparation of the community for disasters and the organization of recovery efforts. The key principles of the bureau are preparedness, mitigation, response, and recovery, as adopted by the Federal Emergency Management Agency. The Ontario Fire Department works with other local public departments (police, library, public works, community services, and finance departments) to enact these principles and to protect the community in the event of a disaster. The City also has various programs or plans for the purpose of reducing and/or eliminating the loss of life and property due to hazards, as well as for preparation and readiness in responding to those hazards. These include a Local Hazard Mitigation Plan (LHMP), updated in 2018, and Ready Ontario, an online tool with information on how to engage with the Office of Emergency Management (OEM) and with training on emergency preparedness including the Community Emergency Response Team (CERT) Basic Academy (Ontario 2018, Ontario 2021b). These resources would be utilized by Ontario in an emergency event, and the impact to emergency response was identified as less than significant in the 2010 Certified EIR.

Although the Housing Element Update itself would not impair an adopted emergency response plan or emergency evacuation plan, it is a policy document intended to guide future housing growth within the City. Therefore, implementation of the goals, programs, and policies in the Housing Element Update would result

in an increased share of housing proposed for development in Ontario and potential need for future emergency response. The Housing Element Update also proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. New development in the City would be reviewed by the Ontario Fire Department to comply with requirements in effect at the time building permits are issued. Additionally, under the Approved Project and the Housing Element Update, future housing development would be reviewed by the City Planning Department to ensure adequate ingress and egress along roadways, as mandated by the City Municipal Code. Therefore, future development would not alter emergency response or evacuation plans.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to compliance with federal, state, and local statutes and regulations related to impairing an adopted emergency response plan or emergency evacuation plan. Furthermore, future environmental review of development projects would ensure that emergency response plans are adequate. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

No Impact. The 2010 Certified EIR identified that Ontario is within an area of moderate wildland fire threats. The San Gabriel Mountains and the Chino Hills are both designated extreme fire hazard areas. If firefighters were to lose control of an extreme fire, portions of Ontario could be at risk. To help protect the City and its residents from fire hazards, Ontario has enacted building and fire codes. The fire chief may also use his or her authority to instate certain building, planning, or landscaping requirements. Building plans in Ontario must be reviewed by the Ontario Fire Department to ensure their compliance with the City's Fire Code.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not result in new impacts from wildfire hazards that were not identified in the 2010 Certified EIR. Additionally, the City of Ontario requires new development to adhere to the latest California Building Code for fire safety, and new development in the City would be reviewed by the Ontario Fire Department to comply with requirements in effect at the time building permits are issued. Adherence to the City's Governance Manual process ensures that sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel. No impact would occur.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update

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compared to the Approved Project would not result in any new impacts or increase the severity of impacts with respect to slope, prevailing winds, and other factors exacerbating wildfire risks and thereby exposing project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire. Furthermore, future environmental review of development projects would ensure that wildfire impacts are avoided. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

No Impact. The 2010 Certified EIR identified that Ontario is in an area of moderate wildland fire threats. To help protect the City and its residents from fire hazards, Ontario has enacted building and fire codes. The fire chief may also use his or her authority to instate certain building, planning, or landscaping requirements. Building plans in Ontario must be reviewed by the Ontario Fire Department to ensure their compliance with the City's Fire Code.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. As a result, the Housing Element Update would not result in new impacts from wildfire hazards that were not identified in the 2010 Certified EIR. Additionally, the City of Ontario requires new development to adhere to the latest California Building Code for fire safety, and new development in the City would be reviewed by the Ontario Fire Department to comply with requirements in effect at the time building permits are issued. Adherence to the City's Governance Manual process ensures that sufficient revenue would be available for necessary service improvements to provide for adequate fire facilities, equipment, and personnel. The Housing Element Update would not result in installation of new infrastructure that would exacerbate fire risk in the City.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to the installation or maintenance of associated infrastructure. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

d) Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Less Than Significant Impact/No Changes or New Information. As discussed in Section 5.10, Hydrology and Water Quality, Ontario receives a significant amount of runoff from watershed areas in the San Gabriel Mountains that flow into flood-control channels and water channels in the City. Buildout in accordance with the Approved Project would alter the existing land uses within the City but would not result in changes to existing drainage patterns and watercourses. However, increased urbanization resulting from the Approved Project may increase the amount of runoff from impervious surfaces and result in flooding. Projects considered

for approval are required to prepare project-specific hydrology studies as prescribed in the San Bernardino County Hydrology Manual. Furthermore, projects are mandated to comply with BMPs for compliance with NPDES requirements. The City would also encourage the use of low-impact development strategies to intercept runoff, slow the discharge rate, increase infiltration, and reduce discharge volumes. No significant impacts were identified in the 2010 Certified EIR.

The Housing Element Update does not confer entitlements for development, so increased urbanization is not anticipated to occur. The Housing Element Update proposes General Plan and Development Code amendments to ensure adequate sites are available for higher-density development. Future discretionary projects proposed in these areas would undergo subsequent environmental review pursuant to CEQA to ensure environmental impacts are considered. Moreover, pursuant to the City Development Code, individual projects associated with the Housing Element Update would be subject to review by the City prior to issuance of a grading permit, which requires preparation of a drainage study and SWPPP. Therefore, exposure of people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes would be less than significant.

The Housing Element Update would not include new or substantially greater significant impacts than those analyzed in the Certified EIR for the Approved Project, and the Proposed Project does not identify or require adoption of any further mitigation measures beyond those provided in the Certified EIR. With regard to CEQA Section 21166 and CEQA Guidelines Section 15162(a), the changes proposed by the Housing Element Update compared to the Approved Project would not result in any new impacts or increase the severity of impacts with regard to exposing people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes. Thus, preparation of a supplemental or subsequent EIR is not required by CEQA.

5.20.3 Mitigation Measures Identified in the EIR and Applicable to the Proposed Project

The 2010 Certified EIR did not identify significant wildfire impacts under the hazards and hazardous materials analysis upon implementation of regulatory requirements.

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5.21 MANDATORY FINDINGS OF SIGNIFICANCE

	Environmental Issues	Condition 1: Substantial Change in Project Requiring Major Revisions	Condition 2: Substantial Change in Circum- stances Requiring Major Revisions	Condition 3: New Information Showing New or Increased Significant Effects	Less Than Significant Impact/No Changes or New Information Requiring Preparation of an EIR/MND	No Impact
a)	Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?				X	
b)	Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?				х	
c)	Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)				X	
d)	Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?				X	

a) Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Less Than Significant Impact/No Changes or New Information. The Approved Project and the Housing Element Update have similar development footprints, with the exception of several zoning amendments, included in Appendix B: Housing Element Sites Inventory (Parcel List) of the Housing Element. Therefore, the geographic range and severity of impacts to biological resources resulting from the Housing Element Update would be similar to those that would result from implementation of the Approved Project. The Housing Element Update would not result in any new impacts or substantially increase the severity of previously

disclosed impacts related to biological or cultural resources. Thus, preparation of supplemental or subsequent EIR is not required by CEQA.

b) Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Less Than Significant Impact/No Changes or New Information. The Housing Element Update would not result in the achievement of short-term environmental goals to the disadvantage of long-term environmental goals. The Approved Project and the Housing Element Update have similar development footprints, with the exception of several zoning amendments. Therefore, the geographic range and severity of impacts to environmental resources resulting from the Housing Element Update would be similar to those that would result from implementation of the Approved Project. Future development under the Housing Element Update would be required to implement mitigation measures to reduce impacts to less than significant levels; be required to comply with all applicable regulatory requirements; and would require a conditional use permit and other entitlements for approval. Implementation of the Housing Element Update would not preclude the State from meeting its long-term environmental goals. Rather, since the Housing Element Update is addressing a need in anticipation of population growth, it would assist the state in meeting long-term environmental goals. The Housing Element Update would not result in any new impacts or substantially increase the severity of previously disclosed impacts related to short-term or long-term environmental goals. Thus, preparation of supplemental or subsequent EIR is not required by CEQA.

c) Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.)

Less Than Significant Impact/No Changes or New Information. Changes proposed by the Housing Element Update compared to the Approved Project would not result in any new cumulatively considerable impacts or substantially increase the severity of previously disclosed cumulatively considerable impacts. TOP addresses cumulative impacts in accordance with CEQA Guidelines Section 15130. Pursuant to CEQA Guidelines Section 15130 (b)(1), the information used in an analysis of cumulative impacts should come from one of two sources:

- 1) A list of past, present, and probable future projects producing related cumulative impacts, including, if necessary, those projects outside the control of the agency; or
- A summary of projections contained in an adopted general plan or related planning document designed to evaluate regional or area-wide conditions.

The cumulative impacts identified in the 2010 Certified EIR used method No. 2. As a result, the 2010 Certified EIR and this Addendum address the cumulative impacts of development in Ontario. Thus, preparation of supplemental or subsequent EIR is not required by CEQA.

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d) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?

Less Than Significant Impact/No Changes or New Information. Changes proposed by the Housing Element Update compared to the Approved Project would not result in any new substantial adverse impacts or substantially increase the severity of previously disclosed adverse impacts. Thus, preparation of supplemental or subsequent EIR is not required by CEQA.

5. Environmental Analysis

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6. List of Preparers

6.1 PLACEWORKS

Nicole Vermilion, Principal

Sean Anayah, Associate II

6.2 CITY OF ONTARIO

Kimberly Ruddiness, Principal Planner

Thomas Grahn, Senior Planner

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6. List of Preparers

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7. References

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7. References

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Appendix

Appendix A Tribal Consultation Responses

Appendix

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From: Thomas Grahn

To: <u>Nicole Vermilion</u>; <u>Sean Anayah</u>

Cc: Kimberly Ruddins

Subject: FW: City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

Date: Monday, September 13, 2021 3:13:44 PM

Attachments: <u>image002.png</u>

Response from Agua Caliente Band of Cahuilla Indians.

From: Gonzalez Romero, Arysa (TRBL) <aromero@aguacaliente.net>

Sent: Monday, September 13, 2021 11:01 AM **To:** Thomas Grahn <TGrahn@ontarioca.gov>

Subject: City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

Greetings,

A records check of the Tribal Historic preservation office's cultural registry revealed that this project is not located within the Tribe's Traditional Use Area. Therefore, we defer to the other tribes in the area. This letter shall conclude our consultation efforts.

Thank you,

Arysa Gonzalez Romero, M.S., RPA.

Historic Preservation Technician Agua Caliente Band of Cahuilla Indians Tribal Historic Preservation Office Main (760)-883-1327 | Cell (760)-831-2484





GABRIELENO BAND OF MISSION INDIANS - KIZH NATION

Historically known as The Gabrielino Tribal Council - San Gabriel Band of Mission Indians recognized by the State of California as the aboriginal tribe of the Los Angeles basin

September 13, 2021

Project Name: 6th Cycle Housing Element Update, City of Ontario

Dear Thomas Grahn,

Thank you for your letter dated August 31,2021 regarding the project above. This is to concur that we are in agreement with the Housing Element Update. However, our Tribal government would like to request consultation for any and all future projects within this location.

Sincerely,

Andrew Salas, Chairman

Gabrieleno Band of Mission Indians - Kizh Nation

1(844)390-0787

Andrew Salas, Chairman Albert Perez, treasurer I

Nadine Salas, Vice-Chairman Martha Gonzalez Lemos, treasurer II Dr. Christina Swindall Martinez, secretary Richard Gradias, Chairman of the council of Elders

PO Box 393 Covina, CA 91723 www.gabrielenoindians.org

admin@gabrielenoindians.org

From: Thomas Grahn

To: <u>Nicole Vermilion</u>; <u>Sean Anayah</u>

Cc: <u>Kimberly Ruddins</u>

Subject: FW: City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

Date: Tuesday, September 7, 2021 4:52:44 PM

Response from the Quechan Tribe of the Fort Yuma Reservation.

From: Quechan Historic Preservation Officer <historicpreservation@quechantribe.com>

Sent: Tuesday, September 7, 2021 10:31 AM **To:** Thomas Grahn <TGrahn@ontarioca.gov>

Subject: City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

This email is to inform you that we have no comments on this project. We defer to the more local Tribes and support their decisions on the projects.

Thank you, H. Jill McCormick, M.A.

Quechan Indian Tribe Historic Preservation Officer P.O. Box 1899

Yuma, AZ 85366-1899 Office: 760-572-2423 Cell: 928-261-0254

E-mail: <u>historicpreservation@quechantribe.com</u>



[avast.com] Virus-free. www.avast.com [avast.com]

From: Thomas Grahn

To: <u>Nicole Vermilion</u>; <u>Sean Anayah</u>

Cc: <u>Kimberly Ruddins</u>

Subject: FW: SB18 City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

Date: Tuesday, September 7, 2021 4:58:20 PM

Attachments: <u>image002.png</u>

Response from the San Manuel Band of Mission Indians.

From: Ryan Nordness < Ryan. Nordness@sanmanuel-nsn.gov>

Sent: Tuesday, September 7, 2021 3:16 PM **To:** Thomas Grahn < TGrahn@ontarioca.gov>

Subject: RE: SB18 City of Ontario Housing Element, City of Ontario, San Bernardino County, CA

Hello Thomas,

Thank you for contacting the San Manuel Band of Mission Indians (SMBMI) regarding the above referenced project. SMBMI appreciates the opportunity to review the project documentation, which was received by our Cultural Resources Management Department on August 31st. The project is located within Serrano ancestral territory, and the area for the project is of interest, but Tribe sees no conflicts with the zoning changes at this time. However, when specific projects are planned and implemented, SMBMI might have comments and/or request formal consultation with the Lead Agency pursuant to CEQA (as amended, 2015) and CA PRC 21080.3.1.

This communication concludes SMBMI's input on this project, at this time, and no additional consultation under SB18 is required. If you should have any further questions with regard to this matter, please do not hesitate to contact me at your convenience, as I will be your Point of Contact (POC) for SMBMI with respect to this project.

Respectfully, Ryan Nordness

Ryan Nordness

CULTURAL RESOURCE ANALYST

Email: Ryan.Nordness@sanmanuel-nsn.gov

O: (909) 864-8933 Ext 50-2022

Internal: 50-2022 M: (909) 838-4053

26569 Community Center Dr Highland California 92346

SANMANUEL

BAND OF MISSION INDIANS[sanmanuel-nsn.gov]

THIS MESSAGE IS INTENDED ONLY FOR THE USE OF THE INDIVIDUAL OR ENTITY TO WHICH IT IS ADDRESSED AND MAY CONTAIN INFORMATION THAT IS PRIVILEGED, CONFIDENTIAL AND EXEMPT FROM DISCLOSURE UNDER APPLICABLE LAW. If the reader of this message is not the intended recipient or agent responsible for delivering the message to the intended recipient, you are hereby notified that any dissemination or copying of this communication is strictly prohibited. If you have received this electronic transmission in error, please delete it from your system without copying it

and notify the sender by reply e-mail so that the email address record can be corrected. Thank You

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PGPA21-004, A GENERAL PLAN AMENDMENT TO UPDATE THE HOUSING ELEMENT OF THE POLICY PLAN (ATTACHMENT A), AND AN AMENDMENT TO THE LAND USE ELEMENT OF THE POLICY PLAN, REVISING EXHIBIT LU-01 (OFFICIAL LAND USE PLAN), ESTABLISHING AN AFFORDABLE HOUSING OVERLAY, AND MAKING FINDINGS IN SUPPORT THEREOF (ATTACHMENTS B AND C) (PART OF CYCLE 1 FOR THE 2022 CALENDAR YEAR).

WHEREAS, CITY OF ONTARIO has filed an Application for the approval of a General Plan Amendment, File No. PGPA21-004, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, State law requires all local governments within the Southern California Association of Government area to prepare and adopt their 6th Cycle Housing Element by October 15, 2021 (or within 120 days); and

WHEREAS, the City of Ontario conducted a virtual open house on June 14, 2021 to gain input from impacted property owners; and

WHEREAS, the City of Ontario conducted a virtual community meeting on June 28, 2021 to gain input from members of the public; and

WHEREAS, the City of Ontario conducted a follow virtual open house on July 29, 2021 to gain input from members of the public; and

WHEREAS, the CITY OF ONTARIO adopted the Policy Plan (General Plan) as part of The Ontario Plan in January 2010. Since the adoption of The Ontario Plan, the City has evaluated Exhibit LU-01: Official Land Use Plan further and is proposing modifications; and

WHEREAS, the proposed changes to Exhibit LU-01 Official Land Use Plan include changes to land use designations of certain properties shown on Attachments B and C to establish an Affordable Housing Overlay; and

WHEREAS, the purpose of the Affordable Housing Overlay is to facilitate housing opportunities within the City through the implementation of required rezone programs pursuant to the City's adopted Housing Element where required for compliance with State Housing Element law; and

WHEREAS, the Affordable Housing Overlay applies to sites identified in the housing inventory, part of the City's Housing Element, located 1) south of Riverside Drive, 2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and 3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, a related Zone Change (File No. PZC-21-002) is being processed concurrently with this application to establish an Affordable Housing Overlay District; and

WHEREAS, a related Development Code Amendment (File No. PDCA21-001) is being processed concurrently with this application to establish density standards applicable to the Affordable Housing Overlay District; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021 the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission recommended approval of a Resolution recommending City Council adopt an Addendum to the Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010 for File No. PGPA06-001. The Addendum finds that the proposed project introduces no new significant

environmental impacts, and all previously adopted mitigation measures are incorporated into the Project by reference; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: **Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan ("TOP") Environmental Impact Report State Clearinghouse No. 2008101140 ("Certified EIR"), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and
- (2) The Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (5) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- (6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Certified EIR, are incorporated herein by this reference.

<u>SECTION 2</u>: Additional Environmental Review Not Required. Based on the Addendum, all related information presented to the Planning Commission, and the

specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental environmental impact report is not required for the Project, as the Project:

- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- SECTION 3: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts

of current and future airport activity. As the recommending body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 4</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 3, above, the Planning Commission hereby concludes as follows:

(1) The proposed General Plan Amendment is consistent with the goals and policies of The Ontario Plan; and

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- LU1-6: Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

Compliance: The proposed General Plan Amendment reflects the existing uses of the properties or closely coordinates with land use designations in the surrounding area and provides opportunities for choice in living environments.

- Goal LU2: Compatibility between a wide range of uses.
- LU2-1: Land Use Decisions. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.

Compliance: The proposed General Plan Amendment reflects the existing uses of the properties or closely coordinates with land use designations in the surrounding area and will not create adverse impacts on adjacent properties.

• Goal LU5: Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.

▶ <u>LU5-7: ALUCP Consistency with Land Use Regulations</u>. We comply with state law that requires general plans, specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.

Compliance: The proposed General Plan Amendment is consistent with the adopted Airport Land Use Compatibility Plan for both Ontario International Airport and Chino Airport.

Housing Element:

- <u>Goal H2</u>: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
- ➤ <u>H2-1: Corridor Housing</u>. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally, and aesthetically suited to corridors.

Compliance: The proposed General Plan Amendment supports higher density residential and mixed-use development that is functionally and aesthetically suited along transportation corridors.

➤ <u>H2-4: New Model Colony</u>. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

Compliance: The proposed General Plan Amendment is consistent with the policy to support a diverse housing type throughout Ontario.

➤ <u>H2-6: Infill Development</u>. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Compliance: The proposed General Plan Amendment supports the revitalization of neighborhoods through the infill development of higher-density residential and mixed-use developments on underutilized properties.

• <u>Goal H3</u>: A City regulatory environment that balances the need for creativity and excellence in residential design, flexibility and predictability in the project approval process, and the provision of an adequate supply and prices of housing.

➤ <u>H3-2: Flexible Standards</u>. We allow flexibility in the application of residential and mixed-use development standards in order to gain benefits such as exceptional design quality, economic advantages, sustainability, or other benefits that would otherwise be unrealized.

Compliance: The proposed General Plan Amendment supports flexibility in the application of development standards to achieve a variety of residential development on higher density residential and mixed-use properties.

- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.
- ➤ <u>H5-2: Family Housing</u>. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Compliance: The proposed General Plan Amendment supports the development of larger rental apartments on higher density residential and mixed-use properties to meet the housing needs of all individuals and families in Ontario.

- (2) The proposed General Plan Amendment would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City; and
- (3) The Land Use Element is a mandatory element allowed four general plan amendments per calendar year and this general plan amendment is the first amendment to the Land Use Element of the 2022 calendar year consistent with Government Code Section 65358; and
- (4) The proposed 2021-2029 Housing Element Update is mandated by Sections 65580 to 65589 of the California Government Code. State Housing Element law requires that each city and county identify and analyze existing and projected housing needs within their jurisdiction and prepare goals, policies, and programs to further the development, improvement, and preservation of housing for all economic segments of their community commensurate with local housing needs; and
- (5) During the amendment of the general plan, opportunities for the involvement of citizens, California Native American Indian tribes (Government Code Section 65352.3.), public agencies, public utility companies, and civic, education, and other community groups, through public hearings or other means were implemented consistent with Government Code Section 65351.

<u>SECTION 5</u>: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby

RECOMMENDS THE CITY COUNCIL APPROVE the proposed General Plan Amendment, as depicted in Attachment A (Ontario Housing Element Update Public Review Draft October 2021), Attachment B (Affordable Housing Overlay), and Attachment C (Policy Plan Land Use Plan (Exhibit LU-01) Revision – Affordable Housing Overlay) of this Resolution.

<u>SECTION 6</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 7</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Planning Commission Chairman

ATTEST:

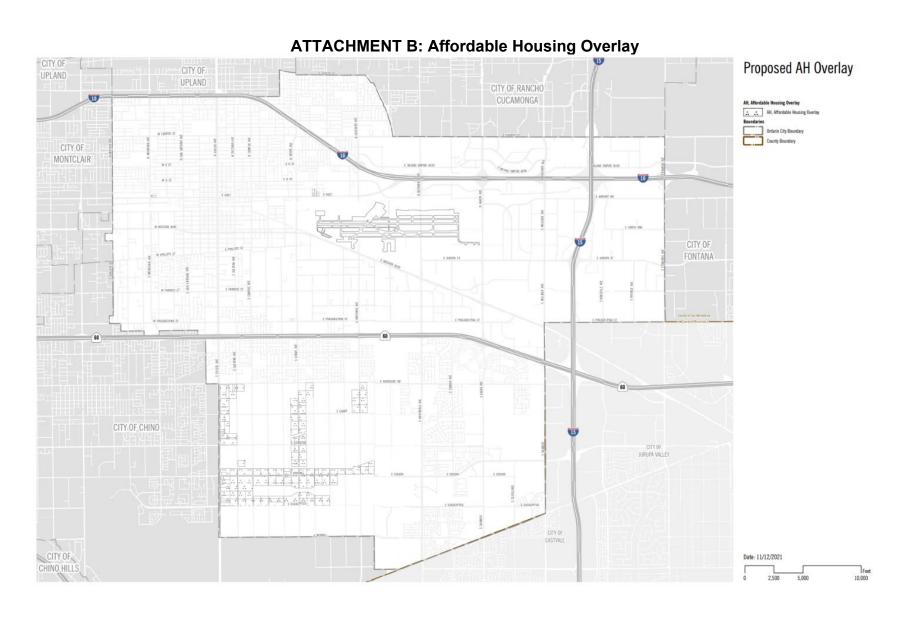
Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

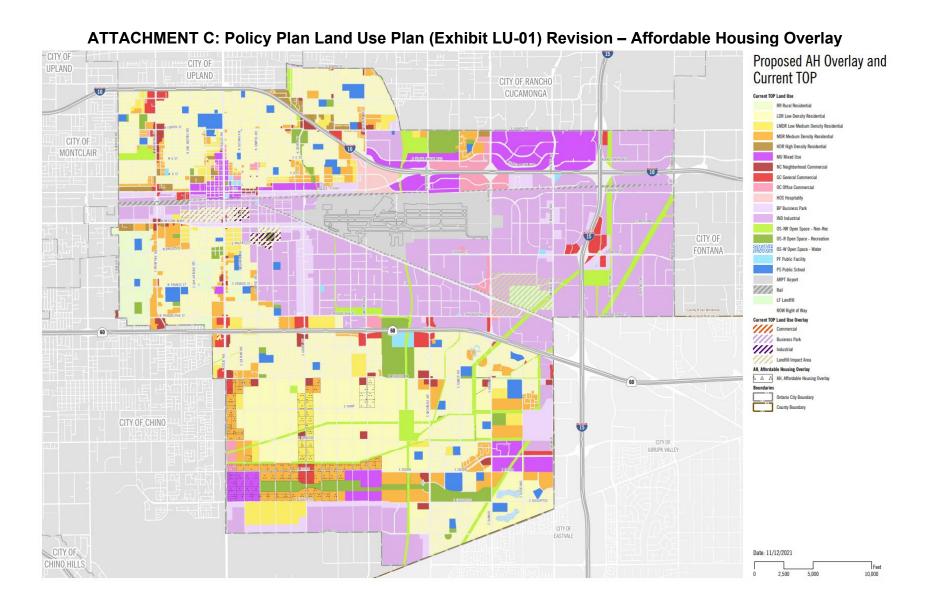
Planning Commission Resolution File No. PGPA21-004 December 20, 2021 Page 10	
STATE OF CALIFORNIA) COUNTY OF SAN BERNARDINO) CITY OF ONTARIO)	
City of Ontario, DO HEREBY CERTIFY	Tempore of the Planning Commission of the that foregoing Resolution No was duly nmission of the City of Ontario at their special e following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore
	decietary fro rempore

ATTACHMENT A:

Ontario Housing Element Public Review Draft October 2021

(Draft Housing Element Update to follow this page)







City of Ontario

Ontario Housing Element

Public Review Draft October 2021







City of Ontario Ontario Housing Element

Public Review Draft | October 2021



Prepared By: PlaceWorks

3 MacArthur Place, Suite 1100 Santa Ana, CA 92707 714.966.9220

City of Ontario Policy Plan Housing Element Technical Report



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HOUSING ELEMENT

1.1 Planning Context

State Law Requirements

The State of California recognizes the importance of housing and therefore legislates requirements for local jurisdictions to contribute to solutions to meeting their local and regional housing needs. All communities across California are required to prepare a Housing Element every eight years to address their local housing needs and a share of the region's need for housing.

The Housing Element is mandated by Sections 65580 to 65589 of the California Government Code. State Housing Element law requires that each city and county identify and analyze existing and projected housing needs within their jurisdiction and prepare goals, policies, and programs to further the development, improvement, and preservation of housing for all economic segments of their community commensurate with local housing needs.

To that end, the California Government Code requires that Housing Elements achieve legislative goals through the following actions:

- Identify adequate sites to facilitate and encourage the development, maintenance, and improvement of housing for households of all economic levels, including persons with disabilities.
- Remove, as feasible and appropriate, governmental constraints to the production, maintenance, and improvement of housing for persons of all incomes, including those with disabilities.
- Assist in the development of adequate housing to meet the needs of low- and moderate-income households.
- Conserve and improve the condition of housing and neighborhoods, including existing affordable housing.
- Promote housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry, national origin, color, familial status, or disability.
- · Preserve for lower-income households the publicly assisted multifamily housing developments within each community.

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The Housing Element must be updated every eight years. The City of Ontario Housing Element covers the period from October 15, 2021, to October 15, 2029.

The Ontario Plan Consistency

State law requires that "the general plan and elements and parts thereof comprise an integrated, internally consistent, and compatible statement of policies." The purpose of requiring internal consistency is to avoid policy conflict and provide a clear policy guide for the future maintenance, improvement, and development of housing within the city. All elements of The Ontario Plan have been reviewed for consistency in coordination with the update to the Housing Element. The City will continue to maintain Policy Plan consistency.

The City is completing an update to The Ontario Plan concurrently with the Housing Element, including updates to the Circulation Element that will address Assembly Bill (AB) 1358 complete streets requirements, drafting and incorporating Environmental Justice policies and actions, and updating the Safety Element. In addition, per Senate Bill (SB) 379 (California Government Code Section 65302), the City will evaluate and amend as appropriate the Safety and Conservation Elements of The Ontario Plan to include analysis and policies regarding flood hazard and management information.

Purpose

An adequate supply of quality and affordable housing is fundamental to the economic and social well-being of the residents of Ontario. The Housing Element is required to address the production, preservation, and improvement of housing in the community. Among its most important functions, the Housing Element analyzes existing and future housing needs; addresses constraints to meeting local housing needs; identifies land, financial, and administrative resources for housing; sets forth goals and policies to meet community housing needs; and establishes housing programs and an implementation plan.

Principles

The City believes:

- A range of housing for all income levels is essential to a complete community.
- The housing stock should match the type and price needed by current and future residents and the workforce, including those with special needs.

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- Preserving, maintaining, improving, and creating distinct neighborhoods and the housing stock protects property values and provides a desirable place to live.
- Affordable, quality housing helps attract and retain a qualified workforce and supports a prosperous local economy.

Content of Housing Element

California Housing Element law prescribes the scope and content of the Housing Element. Pursuant to Section 65583 of the California Government Code, the Housing Element must contain a variety of detailed analyses, listed below.

- Analysis of demographic, social, and housing characteristics; current housing needs; and future housing needs due to population and employment growth and change.
- Assessment of Fair Housing (AFH) consistent with the core elements of the analysis required by the federal Affirmatively Furthering Fair Housing (AFFH) Final Rule of July 16, 2015.
- Analysis of governmental and nongovernmental constraints that affect the development, maintenance, and improvement of housing for all income groups and people with disabilities.
- · Inventory of resources available to address the city's housing needs, including available land for housing, financial resources, and administrative capacity to manage housing programs.
- Evaluation of the accomplishments of current housing programs and specific programs to address the development, improvement, and conservation of housing to meet current and future needs.
- Documentation of public outreach for the Housing Element and the involvement of the public in shaping housing policies and programs for the 2021–2029 Housing Element.
- Housing goals, policies, and programs to address the production, maintenance, and improvement of housing for all economic segments of the community commensurate with its needs.

The Housing Element Technical Report encompasses all seven topical areas mentioned here, provides a brief synopsis of issues, and then follows with a complete set of goals, policies, and programs to be implemented over the planning period. The City also prepared a web format for ease of public distribution and use by policymakers and housing providers in implementing programs.

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The Housing Element is prepared to be consistent with several policy and program plans mandated by the State of California. Most importantly, state law requires the Southern California Association of Governments (SCAG) to determine the amount of housing needed within its six-county region and allocate a share of the regional housing need to each community. Housing Elements are required to incorporate the estimates of housing need reflected in regional housing plans. The Housing Element is also consistent with the City's Consolidated Plan.

Housing Planning Context

Ontario's housing planning context, like that of many urbanized and growing communities, is influenced by many regional forces. Traditionally, the high cost of housing in the Los Angeles metropolitan region has served as an impetus for housing growth in the Inland Empire. With businesses now moving inland to follow the workforce, the city, like other inland communities, is emerging as a center of economic activity; as such, housing prices are also increasing with economic growth.

The demographics of Ontario have evolved over time, reflecting changes in its industrial base and broad demographic changes reflective of the region. Originally an agricultural community settled by Canadians and Europeans who established the citrus and dairy industries, the city's population gradually became home to a younger Hispanic population. Ontario's demographics are again changing and diversifying, in part due to trends reflective of the Inland Empire and unique to Ontario.

Ontario has also experienced commercial and industrial growth that has transformed the city into the economic engine of the Inland Empire. The development of the Ontario Airport Metro Center and Ontario Ranch will play significant economic roles in reshaping the city's future. Housing development in the Ontario Airport Metro Center area is progressing. Housing development in the Ontario Ranch has been predominantly single-family housing to date, as was expected. Intensification of the mixed-use areas along Interstate 10 and on the east side of the city will also drive economic development while also providing opportunities for housing in close proximity to employment opportunities.

Growth areas for future development include intensified development in the downtown and Holt Boulevard areas, including some affordable housing. Recent development has tended to move from the east to west in the southern half of the city. On the west side of the Cucamonga Creek Channel, future development is expected to occur starting in the south and moving north. Development around the Ontario International Airport will continue to allow a mix of uses including hospitality, entertainment, and housing. Future development of the Ontario Mills mall area will allow for a mixture of residential and commercial uses.

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City of Ontario Policy Plan Housing Element Technical Report



The area adjacent to the Chino Airport includes a mixture of warehousing, industrial, and adjacent residential uses. This area will require future infrastructure development to support future land uses and expected to occur within the next three to five years. Additionally, community members have expressed a desire for linear park and mixed-use development in this area.

Housing Challenges

Although the housing market has experienced significant changes in recent years and will continue to change, it is an appropriate time to plan for the city's future. How we house Ontario's present and future residents and its workforce remains the key challenge to creating the type and quality of community and securing Ontario's future. In this context, Ontario's 2021–2029 Housing Element must address several challenges:

- 1. Addressing the needs of existing Ontario residents for decent, quality, and affordable housing for residents of all incomes.
- 2. Ensuring that the city's housing stock matches the type, price, and tenure needed by Ontario's residents and workforce.
- 3. Creating, preserving, and (where needed) improving the quality and identity of Ontario's distinct neighborhoods.
- 4. Assisting residents of all ages and backgrounds to allow them to live, work, and enjoy themselves and their families in Ontario.
- 5. Obtaining financing for affordable housing following the dissolution of the Redevelopment Agency and as tax credits become more competitive make it more difficult to obtain financing for affordable housing.

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HOUSING NEEDS ASSESSMENT

2.1 Demographic Profile

A variety of demographic factors influence existing and future housing needs in Ontario. This section describes and analyzes the primary demographic characteristics of population growth and change, household characteristics, special housing needs, and economic trends to provide insight into the type and magnitude of housing needs in the city.

Population Growth

According to the US Census Bureau's American Community Survey (ACS) 5-Year Estimates, Ontario's 2019 population was 176,760. This represents an increase of approximately 7 percent from the 2010 population of 165,215. Compared to surrounding cities, Ontario's population growth has been minor. Other neighboring cities, such as Eastvale and Chino, have grown more during the same period, with Eastvale growing by approximately 26 percent and Chino growing by 15 percent. These cities, however, have much smaller overall populations and other large cities with population sizes similar to Ontario, such as Rancho Cucamonga, have grown at a rate (10 percent) closer to Ontario's. Table 2-1 shows the population growth rates for Ontario and its neighboring cities.

Table 2-1
Ontario and Neighboring Jurisdictions Population Growth,
2010 to 2019

Year	2010 Population	2019 Population	Numeric Change	Percentage Change
Ontario	165,215	176,760	11,545	7%
Chino	77,729	89,631	11,902	15%
Eastvale	49,131	62,046	12,915	26%
Fontana	189,466	210,759	21,293	11%
Jurupa Valley	*	105,653	N/A	N/A
Montclair	36,704	39,155	2,451	7%
Rancho Cucamonga	160,780	176,379	15,599	10%
Upland	73,887	76,596	2,709	4%

Source: US Census Bureau ACS 2006-2010, 2015-2019. *2010 data not available for Jurupa Valley

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Race and Ethnicity

The County of San Bernardino, much like California as a whole, is experiencing racial and ethnic diversification. Table 2-2 displays changes in the race and ethnicity of Ontario residents from 2010 to 2019.

According to the 2015-2019 ACS, Hispanic residents experienced a 9 percent increase and are the largest ethnic group in Ontario, at 70 percent of the population. White and Black/African American residents experienced the most significant decrease, each declining 6 percent. The Asian ethnic group grew by approximately 46 percent, the fastest growth rate of any ethnic group, yet accounts for only 7 percent of the total population. Native Americans/American Indians also grew significantly, by approximately 58 percent, while also comprising less than 1 percent of Ontario's population. All other racial/ethnic groups and multiracial residents grew, as a group, by approximately 21 percent.

Table 2-2 Changes in Race and Ethnicity

Race and	2010		20	119	Percentage
Ethnicity	Number	Percentage	Number	Percentage	Change
Hispanic	113,085	69%	123,668	70%	9%
Asian ¹	8,078	5%	11,817	7%	46%
White	29,898	18%	28,167	16%	-6%
Black/African American	9,598	6%	9,013	5%	-6%
Native American/ American Indian ²	361	0%	571	<1%	58%
All Others ³	2,904	2%	3,524	2%	21%
Total	163,924	100%	176,760	100%	8%

Sources: US Census Bureau 2010; ACS 2015-2019.

- 1. Asian category includes Asian, Hawaiian, and Pacific Islander.
- 2. American Indian category includes American Indian, Eskimo, and Aleut.
- 3. "All others" includes multiracial categories.

Age Characteristics

The age characteristics of Ontario's residents are related to differences in the type of housing needed. Younger households and seniors typically prefer smaller housing units, with the former preferring rental accommodations and the latter ownership units. Middle-aged adults typically prefer to move up to larger homes that can accommodate families with children. Ontario is unique in that its future demand will be driven not only by changes in age characteristics but by the type of housing built in strategic focus areas.



Ontario's population is one of the youngest in the County of San Bernardino with a median age of 32.4, versus a median age of 33.8 years countywide. Approximately 68 percent of the city's adult residents were below age 44 as of 2019, and nearly 37 percent of all residents were below age 24. As shown in Table 2-3, the city's largest age group occurs among those between the ages of 25 and 44. Ontario is experiencing population aging, with the fastest-growing population group being seniors aged 65 years and older. Between 2010 and 2019, the senior age group expanded by approximately 47 percent, while minors less than 18 years declined by approximately 6 percent.

Table 2-3 Changes in Age Characteristics

Ago Crouno	2010		2019		Percentage	
Age Groups	Number	Percentage	Number	Percentage	Change	
Less than 18	49,443	30%	46,430	26%	-6%	
18-24 (college age)	19,296	12%	19,225	11%	0%	
25-44 (young adults)	49,428	30%	54,928	31%	11%	
45-64 (middle age)	34,703	21%	39,876	23%	15%	
65+ (seniors)	11,054	7%	16,301	9%	47%	
Total	163,924	100%	176,760	100%	8%	

Source: US Census Bureau 2010; ACS 2015-2019.

The age characteristics of Ontario's existing residents suggest a greater need for family and senior housing. A large presence of families and middle-aged persons also implies a high demand for single-family housing that is large enough to accommodate children. As is shown in Table 2-4, the majority of family households in 2019 did not have children; however, younger households, such as those in the 25-44 years age group, may be looking for housing that can accommodate future children. Increases in the middle-age population, should they remain in Ontario over the next decade, should materialize in an increasing demand for senior housing, such as condominiums, that require less maintenance than a single-family home.

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Household Type and Size

Household type and size influence housing demand. For example, families with young children frequently seek the living space and the financial investment that single-family homeownership has to offer. In contrast, single-person households tend to desire apartments, condominiums, and townhomes or other smaller housing options. These preferences underscore the importance of providing a diversity of housing types and prices suitable to residents in all household types.

Ontario is known as a predominantly family-oriented community; 78 percent of households are families. The most significant increase in household types between 2010 and 2019 occurred in the married families, no children category, rising by an approximate 169 percent, implying an increase in the occurrence of children moving out or new couples cohabitating and either delaying or electing not to have children. Married families with children also increased though less dramatically, at a rate of 68 percent. Meanwhile, the number of single-person households grew also, from 6,741 in 2010 to 8,299 in 2019 (23 percent).

At 60 percent of total households, the most prominent household size in the city has between 2 and 4 members. The prominent household types in Ontario suggest a higher demand for family housing with enough bedrooms for 2 to 4 people per household. Table 2-4 shows changes in household types from 2010 to 2019. The number of large households with five or more members declined while the number of single person and 2-to 4-person households grew during the same period. As a result, the overall, average household size only increased slightly between 2010 and 2019 from 3.6 to 3.7 persons per household.

Table 2-4 Changes in Household Type

Hayaahald Tima	2010		2019		Percentage	
Household Type	Number	Percentage	Number	Percentage	Change	
Family Households	35,595	79%	39,495	78%	11%	
Married families with children	13,205	29%	22,189	44%	68%	
Married families, no children	10,584	24%	28,432	56%	169%	
Other families	11,806	26%	13,430	27%	14%	
Nonfamily Households	9,336	21%	11,126	22%	19%	
Single persons	6,741	15%	8,299	16%	23%	
All other households	2,595	5%	2,827	6%	9%	
Total	44,931	100%	50,621	100%	13%	

Household - Comprises all the people who occupy a housing unit. Can include the related family members and all the unrelated people, if any, such as lodgers, foster children, wards, or employees who share the housing unit. A person living alone in a housing unit, or a group of unrelated people sharing a housing unit, is also counted as a household.

Family Household -

Comprises a group of two or more persons related through birth, marriage, or adoption and residing together and any unrelated people residing there.

Nonfamily Household -Comprises unrelated persons living together or one person living alone.



Table 2-4 Changes in Household Type

Household Tune	2010		2019		Percentage	
Household Type	Number	Percentage	Number	Percentage	Change	
Household Size						
Single person	6,741	15%	8,299	16%	23%	
2 to 4 persons	24,936	56%	30,620	60%	23%	
5 persons or more	13,254	29%	11,702	23%	-12%	
Average Size	3	3.6	3.	7*	3%	

Source: US Census Bureau 2010; ACS 2015-2019; SCAG 2020.

*Average size data for 2019 comes from the 2020 SCAG Local Housing Data report for Ontario

Employment

Housing demand is also driven by the wages earned by households, affecting the types of housing that can be afforded. Moreover, overall employment in Ontario also affects housing demand, because as employment levels increase in Ontario, some of these future workers will desire to live in Ontario. This section describes current patterns in employment levels by industry and occupation.

Employment and Occupations

Table 2-5 details the types of occupations held by residents in 2020 in the Riverside-San Bernardino-Ontario Metropolitan Statistical Area (MSA), based on the 2020 Occupational Employment and Wage Statistics reported by the California Economic Development Department (EDD). Transportation and material moving occupations and office and administrative support occupations make up the largest proportions of the MSA's workforce, representing and 15 percent respectively. These occupations earn an average income of \$44,925 and \$40,914, respectively, which is less than EDD's reported mean annual wage for the Riverside-San Bernardino-Ontario (\$55,049). Management occupations and legal occupations earned the highest mean annual wage, each exceeding \$117,000. Together, management occupations and legal occupations represent less than 5 percent of the MSA workforce. Food service and serving related occupations and farming, fishing, and forestry occupations each earned the lowest average annual salary (slightly greater than \$32,000). While farming, fishing, and forestry occupations comprise a relatively small proportion of the MSA population (0.5 percent), food service and serving related occupations comprises the fourth largest proportion of the MSA's employed residents. Food service and serving related occupations comprise 9.0 percent of employed

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persons, following sales and related occupations which comprises 9.1 percent of employed persons in the MSA.

Table 2-5 Occupations by Type for Riverside-San Bernardino-Ontario Metropolitan Statistical Area

	20	020	Average		
Occupations	Number	Percentage	Annual Salary		
Management occupations	64,650	4.3%	\$117,862		
Business and Financial Operations Occupations	58,060	3.9%	\$73,959		
Computer and Mathematical Occupations	18,390	1.2%	\$89,837		
Architecture and Engineering Occupations	16,560	1.1%	\$91,836		
Life, Physical, and Social Science Occupations	10,160	0.7%	\$85,766		
Community and Social Service Occupations	27,930	1.9%	\$62,523		
Legal Occupations	5,780	0.4%	\$117,415		
Educational Instruction and Library Occupations	99,940	6.7%	\$71,328		
Arts, Design, Entertainment, Sports, and Media Occupations	10,120	0.7%	\$61,636		
Healthcare Practitioners and Technical Occupations	82,190	5.5%	\$102,182		
Healthcare Support Occupations	85,470	5.7%	\$35,293		
Protective Service Occupations	41,470	2.8%	\$59,895		
Food Preparation and Serving Related Occupations	134,450	9.0%	\$32,268		
Building and Grounds Cleaning and Maintenance Occupations	45,150	3.0%	\$38,856		
Personal Care and Service Occupations	29,600	2.0%	\$35,614		
Sales and Related Occupations	135,630	9.1%	\$45,301		
Office and Administrative Support Occupations	177,130	11.9%	\$44,925		
Farming, Fishing, and Forestry Occupations	7,610	0.5%	\$32,135		
Construction and Extraction Occupations	83,650	5.6%	\$58,145		
Installation, Maintenance, and Repair Occupations	57,360	3.9%	\$56,287		
Production Occupations	75,250	5.1%	\$42,134		
Transportation and Material Moving Occupations	223,180	15.0%	\$40,914		
Total all occupations	1,489,730	100%			
Source: EDD Occupation & Wage Statistics, 2021					



In terms of commute patterns, more people travel into Ontario for work than leave the city to work. Table 2-6 shows the jobs-to-housing ratio and job inflow for Ontario. According to the US Census Bureau and the California Department of Finance, there is a jobs-to-housing ratio of 2.48, indicating that there are twice as many jobs in Ontario as there are housing units. Although current Ontario residents may not need to travel far for employment if they work within the city, the jobs-to-housing ratio indicates that there are not enough housing units for all of the workers employed within Ontario.

Table 2-6 Job-to-Housing Ratio

Metric	Number		
Jobs	128,637		
Housing Units	51,814		
Job-to-Housing Ratio	2.48		
Net Job Inflow	54,023		
Source: US Census Bureau OnThe Map 2018 – Ontario; CA DOF 2021			

Household Income

Along with housing costs, household income is the most fundamental factor affecting housing opportunity. According to the 2015–2019 ACS, Ontario's median household income was \$65,000, slightly higher than the San Bernardino County median of \$63,362. Median family income in Ontario was lower at \$58,400, with married families earning much higher income (\$79,100) than other types of families, such as female-headed households (\$44,300) and male-headed households (\$55,200). Single male-headed households earn a comparable income to the median for the entire county while female-headed households earn the lowest median income in the city (see Table 2-7).

Table 2-7 illustrates the household income distribution for different types of households in Ontario. The median income represents the point where 50 percent of all households earn less than that point. Married families tend to earn the highest income, presumably because both adults are working. Other families and nonfamilies typically earn the lowest incomes because these households often consist of single persons or seniors living on fixed incomes.

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Table 2-7 Household Income by Type, 2010

Hayaahald Tyma	20	2019			
Household Type	Percentage of Households	Median Income*			
All Households	100%	\$65,000			
Family Households	78%	\$58,400			
Married families	51%	\$79,100			
Married families w/children	24%	\$73,100			
Other Families	27%	\$44,300 (Female Householder) \$55,200 (Male Householder)			
Nonfamily Households	22%	\$48,200			

Source: US Census Bureau; 2015-2019 American Community Survey.

Although difficult to forecast, the city's median household income is expected to significantly increase over the planning period of the 2050 Policy Plan. Residential development in the Ontario Ranch and Ontario Airport Metro Center, the increasing relocation of corporation headquarters to Ontario, and significant revitalization efforts ongoing throughout the community are anticipated to broaden the income makeup of Ontario's future population.

Household Income Distribution

The California Department of Housing and Community Development (HCD) analyzes the distribution of income among households in a community relative to the area median income (AMI), as adjusted for households of different sizes. Households are grouped into five income classifications for purposes of determining the need for assistance. Each year, HCD analyzes the distribution of income by county and develops maximum income limits for each income classification. The 2021 income limits set by HCD for San Bernardino County are:

- *Extremely low*: households earning 30 percent or less of AMI, or a maximum income of \$26,500 for a four-person household
- *Very low:* households earning 31 to 50 percent of AMI, or a maximum income of \$39,500 for a four-person household
- Low: households earning 51 to 80 percent of AMI, or a maximum of \$63,200 for a four-person household
- *Moderate:* households earning 81 to 120 percent of AMI, or a maximum income of \$93,000 for a four-person household

^{*} Median income rounded to nearest \$100.



Above moderate: households earning above 120 percent of AMI for a four-person household, or an annual income greater than \$93,000 for a four-person household.

State income guidelines also often combine extremely low and very low income into one category, called "very low income." The extremely low-, very low-, and low-income categories are also often combined into a larger "lower" income category, a term used throughout this Housing Element. This is because lower-income households as a whole have markedly different housing needs than moderate- and above moderate-income households.

Table 2-8 describes the income distribution of households by tenure. As shown in Table 2-8, 41 percent of all households earn lower incomes, with 11 percent of total households categorized as extremely low income. Homeowners have a much higher proportion of moderate- or above moderate income-households, while renters have a higher share of lower-income households.

Table 2-8
Household Income Distribution

		Tenure of H				
Income Categories	Owners	Percentage of Households	Renters	Percentage of Households	Total of Households	Percentage of Total
Extremely Low	1,600	6%	3,855	17%	5,455	11%
Very Low	2,120	8%	3,520	15%	5,640	12%
Low	3,500	13%	5,380	24%	8,880	18%
Moderate or Above Moderate	19,090	73%	10,110	44%	29,200	59%
Total	26,310	100%	22,865	100%	49,170	100%

Source: Comprehensive Housing Affordability Strategy (CHAS), 2013-2017.

Note: Numbers differ from the Regional Housing Needs Assessment (RHNA) because the CHAS household income levels are adjusted for household size, whereas the RHNA distribution is not adjusted for household size.

2.2 Housing Profile

This section describes and analyzes various housing trends and housing characteristics to provide a basis for assessing the demand and supply of available housing for the community. They include housing growth trends, housing characteristics, age and condition of housing, housing prices and rents, and homeownership trends.

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Housing Growth

Between 2010 and 2021, the housing stock in the city increased 9 percent and SCAG predicts that the housing stock could increase by an additional 44 percent by 2045 (Table 2-9).

Table 2-9 Housing Projections 2000–2045

Year	Households	Change		
real	real riouseriolus	Numeric	Percentage	
2000	44,912			
2010	47,449	2,537	6%	
2021	51,814	4,365	9%	
2045	74,500	22,686	44%	

Source: California Department of Finance 2000, 2021; US Census Bureau 2010; SCAG 2020 RTP/SCS

Housing Characteristics

Housing Type

As shown in Table 2-10, the majority of Ontario's existing housing is single-family detached units. Ontario's overall housing production activity over the past decade has trended notably towards multifamily construction of developments with five or more housing units, increasing 23 percent between 2010 and 2021. Single-family detached housing also increased during the same period but at a far less significant rate of approximately 8 percent. The number of single-family attached and multifamily of two to four units stayed relatively stable over this time period, while the number of mobile homes declined by approximately 15 percent. Overall, however, more housing was built between 2010 and 2021 than demolished, resulting in a net growth in housing stock of approximately 9 percent. Most of this growth was due to the ongoing development of the former dairy farms south of Riverside Drive and east of the Cucamonga Channel as well as new multifamily projects with five or more units throughout the more established portions of the city.



Table 2-10 Housing Type 2010–2021

Unit Tuno	Number o	of Units	2010–2021 Change		
Unit Type	2010	2021	Number	Percentage	
Single-family detached	28,007	30,244	2,237	8%	
Single-family attached	3,114	3,114	0	0%	
Multiple-family (2–4 units)	5,078	5,110	32	1%	
Multiple-family (5+ units)	9,087	11,169	2082	23%	
Mobile homes and other	2,163	1,846	-317	-15%	
Total	47,449	51,483	4,034	9%	

Source: California Department of Finance 2021; City of Ontario 2021

Unit Size

Housing size is an important factor in housing availability. There must be an adequate supply of different sized housing that matches family needs, particularly large housing units affordable to lower-income families likely seeking rental housing options. While the number of large families (five or more persons) decreased by 12 percent since 2010, the number of renter households of at least four persons (9,704 households) exceeds the number of renting households with three or more bedrooms (7,282 units), as shown in Table 2-11. Meanwhile, the number of owner-occupied units with three or more bedrooms (21,380) exceeds the number of owner-households with four or more persons (10,992 households, see Table 2-18). Increased competition for limited number of rental units appropriately sized for large families can lead to higher overcrowding. Overcrowding often occurs because of two factors: (1) the cost of housing relative to income that causes families to double up, and (2) the fact that the building industry typically does not produce large apartment units.

Table 2-11 Housing Size by Tenure

<u> </u>						
Bedrooms	Owner	Renter	Total			
Studio or 1 bedroom	849	5,835	6,684			
2 bedrooms	4,885	10,390	15,275			
3 or more bedrooms	21,380	7,282	28,662			
Total	27,114	23,507	50,621			

Source: US Census ACS 2015-2019.

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Housing Tenure

Homeownership

The American dream is intertwined with the goal of homeownership, which is often associated with independence, economic success, safety, and family. Historically, one of the most efficient and effective ways to build wealth over time is with the purchase of a home.

As of the 2015-2019 ACS, the city has a homeownership rate of 53.6 percent, with 27,114 homeowners and 23,507 renter households. This percentage decreased from the 59 percent homeownership rate in 2010. Table 2-12 shows the total number of occupied housing units by tenure.

Table 2-12 Occupied Housing Units by Tenure

Tenure	Number	Percentage
Owner	27,114	53.6%
Renter	23,507	46.4%
Total	50,621	100.0%

Source: US Census ACS 2015-2019.

Vacancy Rates

The housing vacancy rate is a key indicator of the housing market and how well housing supply matches the demand. Typically, vacancy rates of 5 to 6 percent for rental units and 1 to 2 percent for ownership housing are needed to offer a variety of choice for residents, incentive for developers, and sufficient price options for consumers. Vacancies in excess of these norms are usually considered to be excessive and lead to price depreciation. Lower vacancy rates are deemed to indicate a tight market, where housing rents and prices are expected to increase.

In 2019, Ontario's housing vacancy rate for rental units was 2.9 percent, a low rate for rental vacancies. From 2010 to 2019, the city experienced a decrease in rental vacancy rates, falling from 5.8 percent in 2010.

The 2015-2019 ACS indicated a 1 percent vacancy rate for ownership units, a decline from 2 percent in 2010. The decrease in both the owner-occupied and renter vacancy rates for housing units in Ontario suggests that housing demand is exceeding the ability of the market to build and deliver new housing stock.



Housing Prices and Rents

According to a survey of home sales listings on Zillow in late May 2021, the median resale price in Ontario was \$544,990 for a three-bedroom house. The highest sales price surveyed was \$657,990 for a four-bedroom house while the lowest sales price was \$365,000 for a two-bedroom house. Most homes available were three-bedroom houses; only one one-bedroom house was listed as of the date of the survey. The lack of smaller, lower-cost homes on the market may present a challenge for smaller households, particularly younger households or seniors that may have limited incomes when trying to downsize their homes.

In 2021, 3.6 percent of housing units in Ontario were mobile home units, a decrease in recent years, as discussed previously. Mobile homes provide an additional opportunity for lower-income households to own a home. On May 24,2021 there were 22 mobile homes in Ontario listed for sale on Zillow.com. List prices ranged from \$45,000 to \$199,000, with a median price of \$93,000 and an average price of \$100,464. Typically, owners of mobile homes must pay rent to the mobile home parks where they are located. In exchange, the mobile home parks typically provide landscaping and infrastructure maintenance, easing the maintenance burden on park residents. This is particularly helpful for those with limited mobility, such as seniors and disabled persons.

Apartments and rental single-family homes are a key housing option for young adults and young families so they may dedicate their limited funds to other needs. Senior housing also provides a rental opportunity for seniors with limited incomes or mobility, who can benefit from the greater affordability, compact nature, and lower maintenance needs of apartments.

According to a survey of rental listings on Zillow in late May 2021, the median rent in Ontario was \$1,974, while the minimum and maximum prices were \$1,425 and \$3,000, respectively. The most common rental unit on the market during the survey had two bedrooms and was approximately 800 square feet in size.

Housing Affordability

Housing affordability is a critical issue. The inability to afford housing leads to a number of situations, including the doubling up of families in a single home, low homeownership rates, illegal units, overextension of a household's financial resources, premature deterioration of units from the inability to afford maintenance, and situations where young families and seniors cannot afford to live near other family members. Table 2-13 and the following discussion describe housing affordability in Ontario.

Affordable Housing - The U.S. Census and

numerous housing programs consider an affordable housing payment to be no more than 30 percent of a household's gross income. However, many different standards exist for housing affordability and the standard used depends on the agency consulted, funding source used, and whether household size is considered.

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Homeownership

According to a survey of home sale listings on Zillow conducted in May 2021, the median home price in Ontario was \$544,990. As shown in Table 2-13, this price exceeds the maximum affordable price of \$410,000 for households of four members with a moderate- or lower-income household income. Although some homes surveyed were within the affordability range of moderate-income households, only 1 of the 25 units surveyed was below the limit of \$410,000 and while affordable to moderate-income households, this home was unaffordable to lower-income households. This suggests that overwhelming majority of market-rate homes for sale in Ontario are mostly unaffordable to any household earning a moderate income and are entirely unaffordable to any household earning a lower income.

Rental Housing

Rental housing provides an important source of affordable housing for young adults, families with children, and seniors who earn low and moderate incomes. Since approximately 41 percent of Ontario households earn lower incomes, providing a sufficient quantity of decent and affordable rental housing for the workforce, young adults, families with children, and seniors is an important goal. Table 2-13 summarizes the affordability of rental housing in Ontario.

Based on a market survey conducted in May 2021 of rental listings posted on Zillow, the median rent price in Ontario is \$1,974. Table 2-13 indicates that only a moderate-income household could afford the median rent price, being able to pay no more than \$2,325 a month on rent and expenses. Low-income households can afford to pay no more than \$1,580 a month on rent. The minimum rent surveyed was \$1,425 and thus some rental units are affordable to low-income households, but most rental units are unaffordable to most low-income households. Very low and extremely low-income four-person households can afford no more than \$988 and \$663 per month on rent expenses, respectively. Thus, marketrate rental units at the median rental rate as of May 2021 are unaffordable to any household earning a very low or extremely low income in Ontario. Lower-income households have greater difficulty affording housing. Both very low- and extremely low-income households could not afford to rent a home without doubling up and significantly overpaying for housing. Low-income households could afford a limited number of rentals, but most likely face overpayment, overcrowding, or both.



Table 2-13 Housing Affordability Summary

Income Levels	Definition (Percentage of County AMI)	Maximum Household Income ¹	Maximum Affordable Price²	Maximum Affordable Rent³
Extremely Low	Less than 30%	\$26,500	\$111,200	\$663
Very Low	31% to 50%	\$39,500	\$170,200	\$988
Low	51% to 80%	\$63,200	\$275,600	\$1,580
Moderate	81% to 120%	\$93,000	\$410,000	\$2,325

Assumptions:

- 1 Household size of four persons. Maximum income limits are established by the Department of Housing and Community Development according to median family income (AMI) for 2021.
- 2 Rounded to nearest \$100. Assumes 10% down payment, 30-year loan at an interest rate of 3%, and standard housing costs for San Bernardino County. Housing affordability is calculated at 30% of income, assuming mortgage costs are tax deductible. Affordability estimates created June 11, 2021, using: https://www.wellsfargo.com/mortgage/planning/comfort-zone/afford.
- 3 Rental payment is assumed at no more than 30% of income.

Housing Problems

In today's housing market, where prices and rents have increased faster than personal income over the past decade, Ontario households are paying increasingly more of their income for housing and have less discretionary income to afford other necessities. Overcrowding is also becoming more prevalent as residents choose to live in smaller housing units. The following discussion focuses on both issues in Ontario.

Overpayment

Housing overpayment is an increasing problem in many cities, particularly among lower-income households. The federal and state governments define housing overpayment as when a household spends more than 30 percent of their income toward rental costs or toward a monthly mortgage payment. Overpaying is a housing problem because it leaves a household with limited financial resources for other expenses.

As of 2017, housing overpayment in Ontario affected approximately 60 percent of renters (13,215 households) and approximately 40 percent of homeowners (8,655 households) (see Table 2-14). Overpayment is traditionally more prevalent among renters than owners. While fixed-rate mortgages are the norm, it is still worth noting that adjustable rate mortgages have the potential to lead to overpayment. In any case, housing overpayment tends to be most severe for lower-income households, regardless of tenure.

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Table 2-14
Overpayment by Household Type and Tenure

Overpayment	Low- Income Renter Households	All Renter Households	Low- Income Owner Households	Owner Households	Total Overpaying Households
30%–50% of Household Income	4,085	6,400	1,770	5,295	11,705
More than 50% of Household Income	6,610	6,815	2,960	3,360	10,175
Total more than 30% of Household Income	10,695	13,215	4,730	8,655	21,880
Percentage of Households Overpaying (> 30%)	49%	60%	22%	40%	100%

Source: CHAS 2013-2017

Note: Total numbers of units in the Comprehensive Housing Affordability Strategy (CHAS) may differ slightly as compared to ACS totals due to sampling differences.

Overcrowding

In response to higher housing costs, residents may accept smaller-sized housing or double up in the same house, which leads to overcrowding. Overcrowding strains physical facilities and the delivery of public services, contributes to a shortage of parking, and accelerates the deterioration of housing. Housing overcrowding is also considered one of several substandard housing conditions according to the Uniform Housing Code.

Many different definitions of housing overcrowding exist (see side bar). The US Census considers a situation when a household has more members than habitable rooms in a home overcrowded. For example, a two-bedroom apartment with a living room and kitchen (a total of four rooms excluding bathrooms and hallways) would be considered overcrowded if more than four occupants lived in the home. Overcrowding can be moderate (1.0 to 1.5 persons per room) or severe (more than 1.5 persons per room).



Overcrowding is caused by a range of situations and complex factors, including a mismatch between household income and the cost of housing, and differences regarding preferences for adequate living space. Regardless of these factors, overcrowding typically occurs in a number of situations, such as (1) a family lives in a small unit; (2) a family provides accommodations for extended family; (3) a family rents space to nonfamily members; or (4) students double up to afford housing.

As of 2019, there were 6,159 households, or approximately 12 percent of all households, in Ontario experiencing some degree of overcrowding. Approximately 71 percent of overcrowded households were moderately overcrowded while 29 percent were extremely overcrowded. Although homeowners made up the majority (54 percent) of all households in the city, renters made up a supermajority (69 percent) of Ontario's overcrowded households, whereas only 31 percent of households were owner-occupied. This trend also occurred among the moderately and severely overcrowded households, where renters made up 70 and 68 percent of these categories, respectively.

Table 2-15 provides data on household overcrowding in Ontario according to the tenure of the household.

Table 2-15 Overcrowding by Tenure

Overcrowding Level	Homeowners		Renters		Total	
Overcrowding Level	Number	Percentage	Number	Percentage	Total	
No Overcrowding	25,211	57%	19,251	43%	44,462	
Moderate Overcrowding	1,320	30%	3,033	70%	4,353	
Severe Overcrowding	583	32%	1,223	68%	1,806	
Total Households	27,114	54%	23,507	46%	50,621	
Total Overcrowding	1,903	31%	4,256	69%	6,159	

Source: ACS 2015-2019

2.3 Special Housing Needs

Certain individuals and families in Ontario encounter greater difficulty in finding decent, affordable housing because of their special circumstances. Special circumstances may be related to income, family characteristics, medical condition or disability, or household characteristics. A major emphasis of the Housing Element is to ensure that persons from all walks of life have the opportunity to find suitable and affordable housing in Ontario.

Overcrowding -

Many different standards exist for overcrowding, and the standard used depends on the agency and the area of authority. The California Building Code uses the most permissive definition based on strict health and safety reasons. The California Department of Fair Employment and Housing uses another standard for fair housing. Because of its wide application, the Housing Element uses the Census Bureau definition to determine what constitutes overcrowding, with moderate overcrowding defined as 1.0 to 1.5 persons per room, and severe overcrowding defined as more than 1.5 persons per room.

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State Housing Element law identifies the following special-needs groups: senior households, people with disabilities (physical, developmental, mental, substance abuse, etc.), female-headed households (single parent), large households, persons and families in need of emergency shelter, and farmworkers. This section provides a discussion of housing needs for each particular group and identifies the major programs and services available to address their housing and support needs.

Table 2-16 shows the number of special housing needs groups residing in Ontario based on the 2010 Census and the 2015-2019 ACS unless otherwise noted.

Table 2-16 Special-Needs Groups

Special-Needs Group		Persons or eholds	Percentage of Persons or Households ⁹		
	2010	2019	2010	2019	
Large Families ¹	13,254	20,696	28%	41%	
Female-Headed Households ²	10,568	9,358	26%	18%	
Single-Parent Families ³	6,012	13,430	13%	27%	
Senior Households ⁴	8,349	8,039	18%	16%	
Total Disabilities ⁵		28,252			
Homeless Persons ⁶	452	102	<1%	<1%	
Farmworkers ⁷	617	505	<1%	<1%	
Lower-Income Households ⁸	17,185	19,975	36%	39%	

Source: CHAS, 2009; US Census ACS 2015-2019; SCAG Local Housing Profile Data; San Bernardino County Homeless Partnership - 2020 PiTC

Notes:

- Large families are defined as households with five or more members. Percentage refers to the percentage of all households in Ontario comprising large families.
- Female-headed households refer to single-person and family households with a female listed as the head of household. Percentages represent the share of all households that are headed by a female.
- Single-parent families refer to households with children that are headed by one parent. Percentages represent the share of all households with children that are headed by a single parent.
- 4. Senior households refer to households where a member is 65 years of age or older. Percentages represent the share of all households that are headed by a senior.
- 5. Total Disabilities refers to the total number of all disabilities tallied. No valid percentages can be displayed since disabilities are not equal to people as one person may have multiple disabilities. 2010 data are not displayed since the 2010 Census counted the total population living with disabilities rather than total disabilities.
- 6. Homeless people refer to the number of people counted as homeless in Ontario according to the 2020 San Bernardino County Homeless Count. Percentages refer to the share of the total Ontario population.
- Farmworkers refer to the number of farmworkers working in Ontario according to the SCAG 2021 Local Profile Data for Ontario.
- Lower-income households refer to the number of households who earn 80 percent or less of the median family income according to the 2015-2019 ACS.
- 9. Percentages refer to the share of all households.



Family Households

Ontario is a family-oriented community, with approximately 8 out of every 10 households composed of related family members. In recent years, housing market conditions have led to increasing home prices, a higher prevalence of overpayment and overcrowding, and in some cases, substandard living conditions for families. The burden of higher housing costs typically is most severe for large families and female-headed families, making them special-needs households under state law.

In today's housing market, single-parent families are increasingly at-risk because they must balance work and their families. According to the 2015-2019 ACS, Ontario has a total of 13,430 households headed by a single parent.

Large households with five or more members also constitute a special-needs group because of their unique housing needs. Of the 20,696 large families, 9,704 rent and 10,992 own homes. Large households earning lower incomes also have a high prevalence of housing overpayment, defined as paying more than 30 percent of income toward housing. As shown in Table 2-17, approximately 43 percent of all large families (renters and owners) overpay for housing.

Table 2-17
Large-Family Housing Overpayment

Income I evel	Number of Households Overpaying				
income Levei	Renters	Percentage	Owners	Percentage	
Extremely Low (up to 30% AMI)	590	24%	115	11%	
Very Low (30% - 50% AMI)	845	35%	310	30%	
Low (50% - 80% AMI)	980	41%	600	59%	
Total Low-Income Households Overpaying	2,415	100%	1,025	100%	
Total Large-Family Households Overpaying	2,750	58%	1,620	30%	
All Large-Family Households	4,755		5,450		
Source: CHAS 2013-2017					

Ontario single-parent families can access resources, including childcare opportunities, through the County's Preschool Services Department, which administers the Federal Head Start, Early Head Start, and Early Head Start – Child Care Partnership, as well as state preschool programs. The Preschool Services Department provides free services to qualifying low-income families and provides wrap-around services to ensure to support child wellness, including health, nutritional, and dental services.

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Support services are also available to parents, including free online high school diploma program, employment training, and job placement services. For families at risk of homelessness, the Preschool Services Department provides emergency and crisis assistance in the form of food, housing, transportation, and clothing. Counseling is offered to combat issues of substance abuse and domestic violence. In the County's community assessment prepared by Head Start, the lack of affordable opportunities for childcare was identified as a pressing unmet need to support low-income families, including large families and single-parent families.

Housing Supply

To avoid housing overcrowding and overpayment, large families require affordable homes with three and preferably four or more bedrooms to accommodate children. As shown in Table 2-18, Ontario has about 10,992 large families who own homes compared to the nearly 21,380 owner-occupied units with three or more bedrooms. The city has about 9,704 large renter families, yet only 7,282 rental units with three or more bedrooms. Thus, many large renter families are crowded into smaller rental units.

Table 2-18 Household Size by Tenure in Ontario

Household Size	Number of Owner Households	Percentage of Owner Households	Number of Renter Households	Percentage of Renter Households	Total Households	Percentage of Total Households
Single Person	4,032	15%	4,267	18%	8,299	16%
2 to 3 persons	12,090	45%	9,536	41%	21,626	43%
Large families (4 or more)	10,992	40%	9,704	41%	20,696	41%
Total	27,114	100%	23,507	100%	50,621	100%

Source: US Census 2015-2019 ACS

As shown in Table 2-19, Ontario provides a variety of housing opportunities for lower-income families. In 2021, the city had 39 publicly assisted multiple-family, senior, and transitional housing projects that provided 1,942 deed-restricted units affordable to moderate- and lower-income families. Ontario also has 1,846 mobile homes in parks that provide very low-cost family housing at current market sales prices.



Table 2-19
Affordable Family Housing in Ontario

Housing Types	Number of Units	Affordability of Units
Affordable Housing Units (deed-restricted)	1,942	Very low, low, and moderate income
Mobile Home Parks	1,846	Low-moderate income
Source: City of Ontario, 2021.		

Seniors

Senior households have special housing needs for three primary reasons: income, health care costs, and disabilities. Because of these needs, seniors have more difficulty finding suitable and affordable housing. According to 2021 SCAG Pre-Certified Local Housing Data for Ontario, there were 8,039 households with a householder aged 65 or older, or approximately 16 percent of all households.

Although often viewed in a more homogenous fashion, Ontario's senior population is quite diverse. This diversity is reflected not only in age but in income and housing needs as well. Of the total 16,301 seniors, 62 percent are ages 65 to 74 and 39 percent are older than 75. Each of these groups has different health, transportation, and housing needs that require different strategies and plans. Seniors often have greater difficulty finding and maintaining affordable housing because of their fixed retirement incomes.

In terms of tenure, an approximate 72 percent of senior-held households in Ontario are owner-occupied, whereas only an approximate 28 percent of senior-held households are rented. As shown in Table 2-20, the largest group (approximately 15 percent of all householders in Ontario) of senior householders occurs among those aged 65 to 74 years who own their home. Those who pay a monthly rent among this same age bracket comprise a much smaller portion of the city's householders, at approximately 6 percent.

Table 2-20 Senior Housing Needs

Age of	ge of Renters		Owners	
Householder	Number	Percentage	Number	Percentage
15-24 years	1,405	6%	135	1%
25-34 years	6,070	26%	2,598	10%
35-44 years	5,897	25%	5,007	19%
45-54 years	4,797	21%	6,895	26%
55-59 years	1,565	7%	3,149	12%

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Table 2-20 Senior Housing Needs

Age of	ge of Renters		Owners	
Householder	Number	Percentage	Number	Percentage
60-64 years	1,319	6%	2,748	10%
65-74 years	1,368	6%	3,826	15%
75-84 years	637	3%	1,417	5%
85+ years	254	1%	537	2%
Total	23,312	100%	26,312	100%

Source: SCAG Pre-Certified Local Housing Data - Ontario 2021

Nearly half (approximately 46 percent) of senior households in Ontario earn an annual income of at least \$75,000 or more indicating a substantial number of seniors have higher incomes than Ontario households overall. Table 2-21 shows the senior households in Ontario by their annual household income.

Table 2-21 Senior Households by Income

Income	Number	Percentage
Less than \$10,000	376	5%
\$10,000 to \$14,999	263	3%
\$15,000 to \$19,999	395	5%
\$20,000 to \$24,999	426	5%
\$25,000 to \$29,999	282	4%
\$30,000 to \$34,999	731	9%
\$35,000 to \$39,999	274	3%
\$40,000 to \$44,999	156	2%
\$45,000 to \$49,999	277	3%
\$50,000 to \$59,999	549	7%
\$60,000 to \$74,999	581	7%
\$75,000 to \$99,999	1592	20%
\$100,000 to \$124,999	595	8%
\$125,000 to \$149,999	558	7%
\$150,000 to \$199,999	612	8%
\$200,000 or more	253	3%
Total	7,920	100%



The needs of Ontario's senior residents involve more than just the limited retirement incomes of some seniors. Seniors typically have much higher health costs, which stretch their incomes. Seniors also have a greater percentage of disabilities, as discussed later in this report. This makes it more difficult for seniors to stay in their current home. Limited incomes make it harder to maintain housing, particularly as homes age and require rehabilitation. Access to transportation also becomes important as seniors age and choose transportation alternatives to driving cars.

Housing Supply

With respect to housing choices and opportunities, seniors typically have greater difficulty finding suitable housing. As Ontario's population ages, it has become important to provide more of a "continuum of care" to allow seniors to remain in Ontario. As discussed later, the city offers the following types of senior housing.

- Senior Citizen Housing Development. Senior citizen housing developments are designed to meet the physical and social needs of seniors consistent with the California Fair Employment and Housing Act.
- Convalescent Homes. Nursing Care Facilities (Convalescent Homes (Hospital), Rest Home, or Rehabilitation Facility).
 Nursing care facilities are lodging and care facilities for those who are convalescing, invalids, or aged persons requiring specialized health care services, but primary treatment is given in hospitals
- Community Care Facilities. Residential care facilities for the elderly or other State-licensed care facilities located in residential neighborhoods.

The City recognizes the goal of providing supportive services to enable seniors to "age in place," which is the ability to maintain one's residence and not need to move in order to secure support services in response to life's changing needs. To help seniors, the City offers grants and loans to pay for accessibility improvements, emergency repairs, home renovations, and other services that improve the homes and lives of senior and disabled Ontario residents (Program 3). The City also operates a Senior Center, where a wide variety of supportive services are provided to Ontario's senior residents.

Not all seniors will be able, due to financial constraints or health issues, to age in place and remain in their home. As shown in Table 2-22, residential care facilities in Ontario have a total facility capacity to house 614 seniors. Almost 50 percent of the capacity is accommodated by Inland Christian Home, INC, which houses 297 seniors. A list of residential care facilities that serve seniors is shown in Table 2-22.

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The City partners with service provider organizations to provide services directly to seniors. The Ontario Senior Center provides a senior meal program, including meal delivery, Silver Stars senior transportation program, and a variety of resources to assist seniors with meeting their daily needs. Additionally, the Ontario Senior Center offers opportunities for socialization and community through shared meals, clubs and classes, and a newsletter specifically for seniors.

The City funds the Senior Support Services operated by Inland Fair Housing and Mediation Board (IFHMB). IFHMB has provided application-based services to seniors in Ontario for the State of California's Home Energy Assistance Program (HEAP). This program provides senior citizens with a rebate of up to \$248 in utility rebates. IFHMB supports seniors through the application process.

Table 2-22 Residential Care Facilities in Ontario

Facility Name	Capacity	Address
Adult Res	sidential Facility	
Applegate Home	6 (2019)	5495 Applegate St Ontario, CA 91762
Benson Home	9 (2021)	1941 S. Benson Ave Ontario, CA 91762
Blue Jay Home	4 (2020)	414e. Blue Jay Way Ontario, CA 91761
Bonnie Brae ARF	6 (2018)	1656 E. Bonnie Brae Ontario, CA 91764
Contempo Home	4 (2021)	1127 Contempo Ct Ontario, CA 91762
Gala Home	5 (2020)	10986 Gala Lane Ontario, CA 91762
Galongo Michael Home	6 (2020)	1452 W 5 th St Ontario, CA 91762
Gemma's Care Center	6 (2021)	2950 Roan St Ontario, CA 91761
Holly Land Care Home	6 (2021)	2044 Holly Ave Ontario CA, 91762
House of Generosity	4 (2019)	724 N Greenwood Ave Ontario, CA 91764
Josephine's Care Home	4 (2021)	1566 E Hazeltine St Ontario, CA 91761
LMB Care Home	6 (2019)	1813 N Calaveras Ave Ontario, CA 91764
LMB Care Home	6 (2019)	1125 West J St Ontario, CA 91762
Manzanita Home	6 (2020)	720 Manzanita Ct Ontario, CA 91761



Table 2-22 Residential Care Facilities in Ontario

Facility Name	Capacity	Address	
Monte Vista Family Home	5 (2019)	1922 Brookeside Dr Ontario CA, 91761	
Myers Home	4 (2021)	4799 Grand Ave Ontario, CA 91762	
Nabih's Care Home	4 (2021)	407 W. Spruce St Ontario, CA 91762	
Oak Hill Home	4 (2021)	2420 S. Oak Hill Dr Ontario, CA 91761	
Plainfield SO. #17	4 (2021)	2617 S. Plainfield Dr Ontario, CA 91761	
Pleasant Board and Care	6 (2021)	1559 SO. Pleasant Ave	
Salem Christian Homes INC – " Casa Puente"	6 (2021)	2904 Del Norte Pl Ontario CA, 91761	
Salem South Home	12 (2020)	2326 S. Cucamonga Ave Ontario CA, 91761	
Schoneveld Home	4 (2021)	3457 South Wrangler Place Ontario CA, 91761	
ST. Anthony Family Home	5 (2019)	2744 S Cucamonga Ave Ontario, CA 91761	
Sterling Home	5 (2021)	2431 S Seagull Ave Ontario CA, 91761	
Sunrise Home	6 (2019)	1435 W Rosewood Ct. Ontario, CA 91762	
Susong Home	4 (2020)	1046 Sunsong Ct Ontario, CA 91762	
Adult Residential Facility Subtotal	147		
Assisted	d Living Facility		
Arcadian Shores Manor	6 (2019)	2620 Arcadian Shores Rd Ontario, CA 91761	
Best Care Guest Home	14 (2020)	817 S Oaks Ave Ontario, CA 91762	
Brookdale North Euclid	140 (2021)	1021 N Euclid Ave Ontario, CA 91762	
Salvery Care	6 (2018)	939 E Banyan St Ontario, CA 91761	
Thelma G. Smith Family Care	4 (2020)	632 E Maitland Ave Ontario CA, 91761	
Assisted Living Subtotal	170		
Residential Care for the Elderly Continuing Care Contracts			
Inland Christian Home, INC	297 (2019)	1950 S. Mountain Ave Ontario, CA 91762	
Residential Care for the Elderly Continuing Care Contracts Subtotal	297		
Total Facility Capacity	614		
Source: California Department of Social Services, 2021			

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People with Disabilities

As an established community, the City of Ontario is home to many permanent residents with physical, developmental, or other disabilities that may require different independent living arrangements and services. A disability is a physical or mental impairment that substantially limits one of more major life activities. These disabilities and their severity may require specialized housing arrangements to allow persons with disabilities to live full and independent or semi-independent lives.

SCAG data indicates that Ontario's population living with disabilities has a total of 28,252 disabilities divided into six types, as shown in Table 2-23. Ambulatory difficulties represent the largest share, at an approximate 29 percent of the city's tallied disabilities, followed by cognitive disabilities at approximately 19 percent, and then by independent living disabilities at approximately 18 percent. The least common disability consists of self-care disabilities at approximately 10 percent.

While many disabled people live in independent housing or with family members, many require supportive or institutionalized settings. For instance, disabled people may suffer from serious mental illnesses, drug and alcohol problems, physical disabilities, or other conditions that require short- or long-term residency in an institutional setting. There is no available data documenting the actual incidence of such conditions or the demand for semi-independent residential settings.

Table 2-23
Disability Types in Ontario

Disability Type	Number	Percentage	
Independent Living	4,963	18%	
Self-care	2,957	10%	
Ambulatory	8,097	29%	
Cognitive	5,240	19%	
Vision	3,244	11%	
Hearing	3,751	13%	
Total	28,252	100%	
Source: SCAG Pre-Certified Local Housing Data - Ontario 2021, ACS 2014-2018			

People with disabilities may have difficulty finding employment opportunities, which could pose challenges with being able to afford housing costs. In Ontario, there is a total of 112,008 people of working age. Approximately 23 percent of the working age population is not in the labor force, approximately 6 percent are unemployed, and approximately 72 percent are employed. Of Ontario's working age



population with a disability, there are more people with disabilities who are currently not in the labor force (4 percent) or who are unemployed (less than 1 percent) than those who are employed (3 percent). This shows that some people with disabilities living in Ontario may require additional assistance to afford costs of living. Table 2-24 shows the proportions of Ontario's working age population with disabilities by employment status.

Table 2-24
Disabled Residents in Ontario by Employment Status

Disability Type	Number	Percent
Employed - Total	80,429	72%
with a disability	3,412	3%
no disability	77,017	69%
Unemployed - Total	6,256	6%
with a disability	509	<1%
no disability	5,747	5%
Not in Labor Force - Total	25,323	23%
with a disability	4,020	4%
no disability	21,303	19%
Total	112,008	100%

Source: SCAG Pre-Certified Local Housing Data - Ontario 2021.

Persons with Developmental Disabilities

Senate Bill (SB) 812 requires the City to include in the special housing needs analysis needs of individuals with a developmental disability within the community. According to Section 4512 of the Welfare and Institutions Code, a "developmental disability" means a disability that originates before an individual attains 18 years of age, continues, or can be expected to continue, indefinitely, and constitutes a substantial disability for that individual, which includes mental retardation, cerebral palsy, epilepsy, and autism.

Many developmentally disabled persons can live and work independently within a conventional housing environment. More severely disabled individuals require a group living environment where supervision is provided. The most severely affected individuals may require an institutional environment where medical attention and physical therapy are provided. Because developmental disabilities exist before adulthood, the first issue in supportive housing for the developmentally disabled is the transition from the person's living situation as a child to an appropriate level of independence as an adult.

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The California Department of Developmental Services (DDS) currently provides community-based services to approximately 329,600 persons with developmental disabilities and their families through a statewide system of 21 regional centers and 2 developmental centers. The Inland Regional Center, serving San Bernardino and Riverside Counties, is 1 of 21 regional centers in California that provide point of entry to services for people with developmental disabilities. The center is a private, nonprofit community agency that contracts with businesses to offer services to individuals with developmental disabilities and their families.

The following information from California Department of Development Services (DDS), provided to the City by SCAG, provides a closer look at the disabled population (see Table 2-25).

Table 2-25
Developmentally Disabled Residents by Age

Location	0–17 Years	18+ Years	Total
Ontario Total	1,479	737	2,216

Source: SCAG Pre-Certified Local Housing Data - Ontario 2021 (based on CA DDS consumer count by CA ZIP, age group and residence type for the end of June 2019

A number of housing types are appropriate for people living with a development disability: rent-subsidized homes, licensed and unlicensed single-family homes, inclusionary housing, obtaining rental housing using Section 8 vouchers, special programs for home purchase, HUD housing, and SB 962 homes. The design of housing-accessibility modifications, the proximity to services and transit, and the availability of group living opportunities represent some of the types of considerations that are important in serving this need group. Incorporating "barrier-free" design in all new multifamily housing (as required by California and federal fair housing laws) is especially important to provide the widest range of choices for disabled residents. Special consideration should also be given to the affordability of housing, as people with disabilities may be living on a fixed income.

Service providers that participated in stakeholder interviews identified a serious lack of affordable housing and limited opportunities for rental assistance, which can particularly impact persons with disabilities.

Rolling Start Inc. is a nonprofit organization that promotes independent living for persons with disabilities. Its mission is to empower and educate people with disabilities to achieve the independent life of their choice. To accomplish this mission, Rolling Start offers a variety of services in San Bernardino, Inyo, and Mono Counties and serves approximately 1,500 clients with disabilities annually. Rolling Start provides wrap-around services to create independence for its clients, including housing





assistance to secure appropriate housing, personal assistant referrals and training, transportation assistance, and teaching independent living skills, such as budgeting, cooking, cleaning, and laundry. Assisting clients to find employment is a core service for Rolling Start. Clients receive job-seeking skills to secure employment through skills trainings and local networking. Rolling Start also offers clients access to assistive technology to support with everyday tasks, including devices ranging from jar openers and magnifiers to wheelchairs, grab bars, and assistive computer applications.

The San Bernardino County Department of Behavioral Health provides mental health programs to individuals and families who are experiencing serious or ongoing mental health and/or substance abuse disorders in San Bernardino County. In addition to crisis response teams, the Department of Behavioral Health provides outpatient mental health clinics include intervention, assessment/referral, that crisis individual/group therapy, medication support, case management, drug/alcohol, and educational workshops. The Department of Behavioral Health partners with service providers for specific communities, including military families, veterans, American Indians, Latinx population, African American population, LGBTQ+ community, preschool and school-aged children, and community health workers. The community-focused groups offer mental health resources, referral services, education, awareness and advocacy, and prevention resources.

To assist in the housing needs for persons with developmental disabilities, the City will implement programs to coordinate housing activities and outreach with the Inland Regional Center and encourage housing providers to designate a portion of new affordable housing developments for persons with disabilities, especially persons with developmental disabilities, and pursue funding sources designated for persons with special needs and disabilities (Program 30).

Housing Design and Availability

The needs of people with disabilities and available program responses vary considerably, as these individuals do not live in institutionalized settings. Whereas many live in independent living arrangements, others require more supportive settings. Therefore, typically, people with disabilities have three primary needs with respect to suitable housing: (1) affordable and accessible housing, both new and rehabilitated; (2) an adequate supply of institutional settings for those requiring more specialized care; and (3) a system of supportive services that allow for a full life.

Universal Design -

Universal Design is the design of products and environments to be usable by all people, to the greatest extent possible, without the need for adaptation or specialized design.

Visitability-

Visitability is the design approach for new housing such that anyone who uses a wheelchair or other mobility device should be able to visit. A social visit requires the ability to get into the house, to pass through interior doorways, and enter a bathroom to use the toilet.

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Cities that use federal housing funds must meet federal accessibility guidelines. For new construction and substantial rehabilitation, at least 5 percent of the units must be accessible to persons with mobility impairments, and an additional 2 percent of the units must be accessible to persons with hearing or visual impairments. New multiple-family housing must be built so that (1) public and common-use areas are readily accessible and usable by disabled people; (2) doors into and within units can accommodate wheelchairs; and (3) units contain adaptive design features, such as universal design.

HUD also recommends, but does not require, that all design, construction, and alterations incorporate, wherever practical, the concept of accessibility. This recommendation is in addition to requirements of Section 504 of the Fair Housing Act. Recommended construction practices include wide openings for bathrooms and interior doorways and at least one accessible means of egress and ingress for each unit. The City enforces all federal and state accessibility laws but does not require or mandate that new units meet more stringent universal design or visitability (see sidebar) standards.

At some point, people with disabilities may require an institutional setting. State law requires communities to allow people with disabilities to live in normal residential neighborhoods and therefore preempts many local laws and regulations for residential care facilities. The City allows for a range of residential care facilities in its neighborhoods, as summarized in Table 2-26. Ontario also has 28 residential-care facilities (also known as assisted living, retirement homes, etc.), providing accommodations for 158 disabled clients.

Table 2-26
Housing for People with Disabilities

Housing Types	Facility Capacity	
Adult Daycare facilities	42	
Adult Residential Care facilities	106	
Social Rehabilitation facilities	10	
Total	158	
Source: California Department of Developmental Services - Community Licensing Care Division 2021		

Source: California Department of Developmental Services - Community Licensing Care Divisi

Homeless People

Homeless persons are those who have a primary nighttime residence that is a supervised shelter designed to provide temporary living accommodations or a public or private space not designed for regular sleeping accommodation. The 2020 San Bernardino County Homeless Count identified 102 homeless persons residing in Ontario, including 74



persons unsheltered, and 28 homeless individuals living in emergency shelters or transitional housing.

Homeless populations have a complex range of housing and supportive service needs. The housing needs of homeless individuals cannot be met without a service system with a strong outreach component that engages homeless people and encourages them to enter the shelter system. A variety of housing types and supportive programs are needed to serve the homeless, depending on whether it is a homeless individual or family, if there is substance abuse involved, and if the person is disabled.

Continuum of Care Program

The City contracts with Mercy House to implement a Homeless Services Continuum of Care (CoC) to prevent homelessness and assist individuals and families in becoming self-sufficient. The City's CoC offers the following services and programs:

- Homeless Outreach Service Center. The Ontario Access Center is the first step in the CoC and is designed to get people off the street and into an environment where services can be provided. The center offers showers, laundry facilities, lockers, restrooms, and case management offices. Ontario also funds an emergency shelter for victims of domestic violence (House of Ruth).
- Transitional Housing. Transitional housing is designed to provide accommodations for up to two years, during which the homeless individual or family prepares for independent living. In conjunction with the City of Ontario and the Ontario Housing Authority, as of 2021, Mercy House continued to provide a 34-bed transitional living facility, Assisi House, located on Virginia Avenue. Foothill Family Shelter, located in Upland, also has transitional housing units serving the West End of San Bernardino County, including Ontario.
- Permanent Supportive Housing. The City provides permanent supportive housing in the form of vouchers and direct assistance to renters. As described in the 2020-2024 Consolidated Plan for the Ontario CoC, the affordable permanent housing program consists of 76 units of affordable housing that offers optional aftercare supportive services. Tenancy is ongoing provided the tenant adheres to polices outlined in the tenant lease. In addition, the City has worked in cooperation with Mercy House Living Centers, the County of San Bernardino Department of Behavioral Health, and the Housing Authority of the County of San Bernardino to develop Project Gateway which provides up to 12 units of Permanent Supportive Housing within the inventory of the 76 permanent housing units. These units are available to mentally ill,

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chronically homeless individuals with supportive housing services. In addition, the City has implemented a HOME Tenant Based Rental Assistance (TBRA) program targeted to providing permanent housing through a 12-month voucher to chronically homeless individuals and families. A total of 168 units are provided between these three projects that specifically serve homeless individuals and families: Affordable Permanent Housing Program (76 units), Project Gateway (12 units), and Tenant-Based Rental Assistance (80 units).

- SOVA Program Center. The SOVA Program Center, located at 904 East California Street, is operated by the Inland Valley Hope Partners. The center provides clients with emergency food, utility, and rental assistance. SOVA provides a 15-meal supply of nutritional food for each member of a family, every 30 days. The agency also offers classes in nutrition education, assistance for utilities and rent, motel vouchers, and access to job listings, bilingual health and safety information, and referrals.
- Other Partnerships. The City of Ontario also works with other nonprofit partners to address the complex individual and interjurisdictional issue of homelessness, both locally and regionally. Partners include the Interagency Council on Homelessness, HMIS Advisory Committee, Foothill Family Shelter, House of Ruth, Inland Valley Hope Partners, Mercy House, Transitional Assistance Department (motel vouchers), the Salvation Army, Housing Authority of the County of San Bernardino, San Bernardino County Department of Behavioral Health, Step-Up on Second, West Valley Regional Steering Committee, and surrounding jurisdictions.

Since the establishment of the CoC, Ontario has expended over \$15 million in capital investment and operating subsidies for various programs designed to end homelessness. The major expenditures were in the acquisition and substantial rehabilitation of permanent housing units and creation of the Ontario Access Center. Ontario's CoC supports:

- The Ontario Access Center, which continues to provide basic needs and services.
- Assisi House and Aftercare Services Program, which provides transitional housing and aftercare services.
- The HOME Tenant-Based Rental Assistance (TBRA), which provides tenant--based rental assistance.



- HUD's Supportive Housing Program, Project Gateway, which helps secure permanent housing with wrap--around services for chronically homeless individuals with disabilities and their families.
- In cooperation with Ontario Housing Authority, Mercy House, and Mercy House Community Housing Development Organization (CHDO) a total of 76 permanent housing units, continue to be provided for priority occupancy to participants in the CoC.

Ontario has created new programs to assist in the delivery of services designed to house persons experiencing homelessness within the city:

- During Fiscal Year 2019-20, funding for the street outreach team was increased to increase outreach efforts from 10 hours per month to 40 hours per week.
- The Extreme Weather Motel Voucher Program assisted 11 persons with a total of 58 bed nights in 2019, and 14 persons in 2020. The program was adapted to the COVID-19 motel voucher program in March 2020 and served 57 households between March and September 2020. In November 2020, the City launched the Emergency Motel Voucher Program that has served 92 households during Fiscal Year 2020-21. All individuals assisted are provided with the opportunity for case management focused on connecting the individuals to housing.
- The LMIHF Utility Assistance Program assists persons experiencing homelessness with \$0 income to participate in the existing HOME TBRA Program operated as part of the CoC. This program was canceled in March 2020 to focus resources on sheltering homeless persons during the COVID-19 pandemic.
- The City partnered with a local school district to identify homeless families and assist these families with rental subsidies through the HOME TBRA program.
- The City facilitated monthly meetings with Ontario focused homeless providers and governmental agencies to coordinate services to transition individuals/families from homelessness into a stable housing program.

The COVID-19 Rapid Re-Housing Program finds housing solutions for persons at risk of homelessness during the pandemic. During Fiscal Year 2020-21, two households received assistance.

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As indicated earlier, the City continues to make ongoing subsidies available to various homeless service providers to provide for public service programs serving homeless individuals and families, such as the Family Stabilization Program at SOVA Program Center, services for victims of domestic violence and their children provided by House of Ruth, and the Ontario Access Center and Assisi House and Aftercare Services Program provided by Mercy House CoC (Program 32). Table 2-27 shows the city's current supply of housing for homeless persons. As shown in Table 2-27, the total beds available (at least 155) can accommodate the city's homeless persons (102 persons) based on the 2020 San Bernardino County Homeless Count.

Table 2-27 Housing for Homeless People

Housing Types	Type of Housing	Clients	Number of Beds
Mercy House Ontario Access Center	Intake Center	Homeless people	N/A
Assisi House	Transitional housing	Single men, women, and women with children	9 units 34 beds
Foothill Family Shelter	Transitional housing	Homeless families with children	26 units All 2- bedroom units (56 beds or more)
House of Ruth	Emergency shelter, transitional housing, and permanent housing	Battered women and children	20 emergency beds; 35 transitional beds, and 2- bedroom units for permanent housing or rapid re-housing (up to 10 beds)
Affordable Permanent Housing Program	Permanent Housing	Priority access for homeless individuals and families	76 units
Project Gateway	Shelter Plus Care	Mentally ill, chronically homeless	12 units
Tenant-Based Rental Assistance	Rental Subsidies for Permanent Housing	Chronically Homeless individuals	80 units
Veterans Affairs Supportive Housing	Voucher	Veterans	352 vouchers
Family Unification Program	Voucher	Families	1,109 vouchers
SOVA Food Security Center	Voucher	Families	As available
		Total	155 beds or more 1,666 units or more

Source: City of Ontario, 2020-2024 Consolidated Plan; Communication with homelessness services agencies.

Farmworkers

Ontario first developed as an agricultural community, devoted primarily to the citrus industry. A reminder of the heyday of orange groves, the Sunkist plant, has now closed operations. Dairies later replaced the citrus industry. In the mid-1980s, in fact, the Chino-Ontario area was renowned



for the highest concentration of dairy cows per acre in the world. Twenty years later, however, only about 50 dairy farms were still located in the Ontario-Chino area. Many moved to Fresno, Kern, and San Joaquin Counties or to other states.

In 2021, SCAG reported that 505 farmworkers had jobs in Ontario, 380 were full-time, year-round jobs. There was also a total of 586 workers employed in the agricultural industry in general, 461 of which were full-time, year-round jobs. Table 2-28 shows the amounts of farmworkers and agricultural industry workers in Ontario. In the past decades, the dairy industry has dramatically changed. Ontario's dairy industry today is highly automated and generally family-owned and operated. Some dairy farms employ farmworkers to assist with the daily operations, but the use of technology, automation, and family labor has minimized the need for farmworkers.

The housing needed for dairy workers is different from that of traditional seasonal/migratory farm laborers. Traditional migrant laborers move from place to place to harvest crops on a seasonal basis and live in migrant farmworker housing, such as dormitories. In contrast, dairy work is relatively constant, and employees, who are often family members, live on-site. Today, many dairy farms have two or more dwellings to accommodate the owner/operator and several key employees.

The City has an Agricultural Overlay District to allow existing agricultural uses to continue until a development is approved for urban uses. The City's zoning allows single-family homes by-right, agricultural caretaker units as an accessory use, and manufactured housing by-right.

Conservative estimates are that each farm residence is occupied by a farm owner/operator and one family member working on-site at the dairy. The other homes in the Ontario Ranch agricultural areas are assumed to have one to two residents working in the agricultural business. With these assumptions, existing housing in Ontario Ranch accommodates between 500 and 800 agricultural workers. Additional agricultural laborers work in Ontario, but many are employed in the food processing, horticultural, or other agricultural industries.

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Table 2-28 Farmworkers in Ontario

Farmworkers Occupation and Employment	Number in Ontario	Percentage of Ontario Workers
Total jobs: Farming, fishing, and forestry occupations	505	0.61%
Full-time jobs only: Farming, fishing, and forestry occupations	380	0.66%
Total employment in agriculture, forestry, fishing, and hunting	586	0.70%
Full-time employment only in agriculture, forestry, fishing, and hunting	461	0.80%
Source: SCAG Pre-Certified Local Housing Data - Ontario 2021		

Extremely Low Income

Extremely low-income households are defined as households earning annual incomes that are 30 percent or less of the AMI. Based on state income limits for 2021, a four-person, extremely low-income household earns no more than \$26,500 and can afford approximately \$663 per month for rent. Homeownership for extremely low-income households is considered financially infeasible throughout much of California because of the levels of subsidies required for a single unit.

According to the 2013-2017 CHAS, approximately 5,455 households (11 percent) earn extremely low income in Ontario. Of the extremely low-income households, it is estimated that 3,855 rent and 1,600 own the home they live in. The average income of a wide range of service and retail occupations falls into this category, at approximately 18 percent of Ontario's workforce. As businesses cope with the economic recession, many are converting jobs into part-time employment, further increasing the number of individuals earning extremely low incomes.

Extremely low-income households experience a broader range and severity of housing problems (overcrowding and overpayment) than other households because of their income level. For instance, the majority of extremely low-income households are renter households (3,855) and 3,145 (81 percent) of extremely low-income renter households overpay for housing. Of the 1,600 extremely low-income households who own a home, 1,215 (76 percent) overpay for housing. Overcrowding is also predominantly concentrated among very low- and extremely low-income households.

According to SCAG, the City of Ontario has a construction goal of 5,640 very low-income units from 2021 through 2029. Of that total, the City estimates that the construction need for extremely low-income units is 50 percent of that number, or 2,820 units. This estimate is based on a methodology approved by HCD for estimating the need for extremely



low-income housing. Providing housing affordable to extremely low-income households is challenging because of the significant financial subsidies required to make rental housing projects financially feasible.

The City's strategy to house extremely low-income households is focused on rental assistance and housing preservation. The Housing Authority of San Bernardino County (HASCB) has issued 773 housing vouchers to Ontario residents, predominantly those with extremely low incomes. The vouchers are primarily in traditional voucher programs (735) with a small amount (38) used in special voucher programs such as Veterans Affairs Supportive Housing. Of the total number of vouchers, a significant portion is assumed to be for families.

2.4 Neighborhood Conditions

Ontario's history is rooted in agriculture, and many of the city's homes, lot patterns, and other neighborhood features reflect that history. In other instances, the city's neighborhood fabric is defined by recent patterns of development. Today, Ontario's neighborhoods are the building blocks of the community. Neighborhoods profoundly define the sense of identity and community for residents, the quality of life experienced, and the image and role of Ontario in the Inland Empire. Therefore, the design of neighborhoods, the maintenance of housing, and historic preservation are all critical aspects of building Ontario's future.

Historic Neighborhoods

The City has developed historic contexts to describe and explain the circumstances and period within which historic resources were built. Contexts provide an understanding of the importance of resources and features. Contexts also provide insight as to the location of neighborhoods.

To date, the City has identified the following historic contexts:

- Ontario Irrigation Colony, which includes the Chaffey Brothers, the Ontario Land and Improvement Company, and the Citrus Industry.
- Wine Industry, which is in the eastern part of Ontario and was exemplified by Hofer Ranch and the Guasti Winery.
- Citrus Industry, which is in the central portion of Ontario and symbolized by the Sunkist Plant.
- Dairy Industry, which is in the southern portion of Ontario, mostly in what is known as Ontario Ranch.

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 Aviation Industry, located at the Ontario International Airport, which identifies aviation themes in commercial, civil, military, and architecture.

Historic surveys are a fundamental part of this effort. The City's first survey of historic properties was completed in 1983. The survey identified almost 3,000 properties as eligible for designation as Historic Landmarks or as part of Historic Districts. Of the 3,000 listed properties, approximately 300 properties were nominated for designation. Currently, Ontario has designated 99 properties designated as Local Historic Landmarks and eight Historic Districts. Nine additional areas have been identified as potential districts. These districts are illustrated on Figure 2-1.





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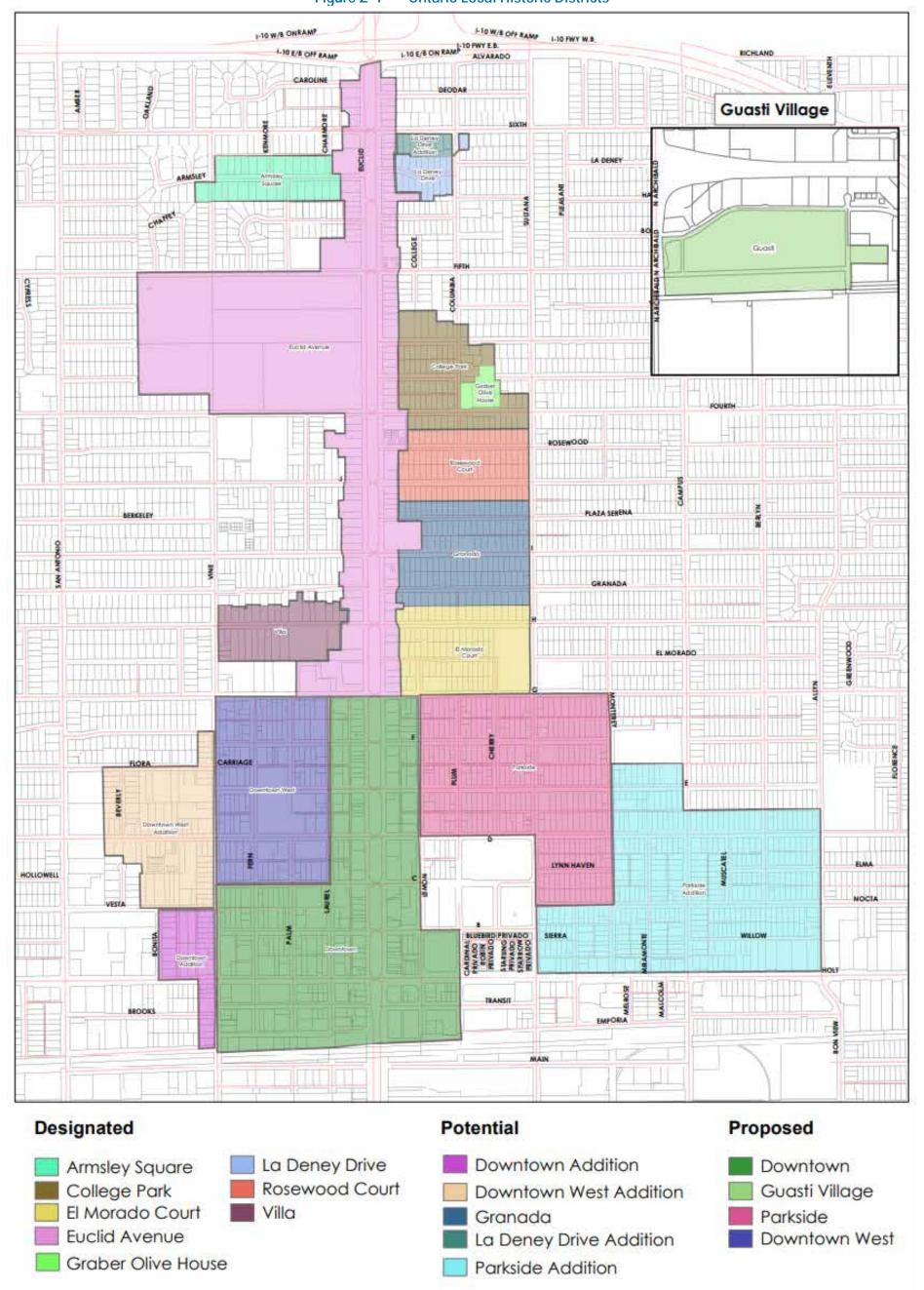


Figure 2-1 Ontario Local Historic Districts



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Age and Condition of Housing Stock

Ensuring decent and well-maintained housing helps provide safe housing for families, improves property values and the image of Ontario, and contributes to higher levels of neighborhood investment. Like any physical asset, housing requires regular maintenance and repair to extend its life. The age of the existing housing stock is one way of measuring housing conditions and is a factor in determining the need for home rehabilitation.

Housing age is correlated with rehabilitation needs. Homes built between 30 and 50 years ago are more likely to need rehabilitation or substantial repairs. Homes built before 1971 are less likely to meet seismic standards enacted following the Sylmar Earthquake of 1971. Homes older than 50 years often need new electrical, plumbing, roofing, and other subsystems. Older homes may also have been altered without building permits, and the alterations do not meet current health and safety standards. As shown in Table 2-29, 39 percent (22,270) of the homes in Ontario were built prior to 1970. Program 3 is included to assist with home rehabilitation.

Housing deterioration is associated with several other conditions, such as overcrowding and small rental projects, as well as investor-owned homes. Accelerated home deterioration is caused by overcrowding, which places additional wear and tear on housing designed for fewer occupants. Smaller rental projects often appear to need major rehabilitation because they are often owned by inexperienced investors. Finally, investors tend not to maintain single-family homes as well as resident owners.

Table 2-29 Age of Housing Stock

Year Built	Housi	Housing Units				
reai buiit	Number	Percentage				
Before 1940	5,093	9%				
1940–1949	2,648	5%				
1950–1959	9,142	16%				
1960–1969	5,387	9%				
1970–1979	9,882	17%				
1980–1989	10,326	18%				
1990–1999	4,972	9%				
2000-2009	3,497	6%				
2010 or later	7,096	12%				
Total	58,043	100%				

Source: US Census ACS 2015-2019; City of Ontario APRs 2019, 2020.



As part of periodic windshield surveys undertaken over the past few years, City staff has identified several residential areas with significant rehabilitation needs that may provide opportunities for improvement and new programs. The following discussion describes general areas, provides a map illustrating their locations, and concludes with an estimate of housing rehabilitation and repair needs.

CDBG Conservation Home Improvement Program (CHIP Loan)

This new program launched in June 2020, provides low-income homeowners with a loan to make energy efficiency or water conservation improvements to the exterior of their homes. Eligible improvements include roof repairs/replacement, window replacement, exterior painting, landscaping improvements, irrigation systems and other improvements deemed necessary to provide energy/water conservation benefits.

Distressed Multifamily Development

The City of Ontario was incorporated in 1891. Like most cities of this age, there are areas within the community that a need substantial reinvestment to eliminate the deteriorated and blighted conditions that occur when properties are not adequately maintained. Most of these areas are in portions of the city that were formally designated as Redevelopment Project Areas. Most of the deteriorated residential properties are in the city's former Central City and Cimarron Project Areas. These areas contain some of the oldest multifamily housing in the city. In 2007, a survey of 2,400 homes was conducted in the Cimarron Project Area and found 22 percent of the units needed repair and maintenance and 28 percent were deteriorated or dilapidated. Prior to the dissolution of redevelopment by the State, hundreds of these multifamily housing units had been rehabilitated using a variety of funding sources (including Redevelopment Low- and Moderate-Income Housing Funds (LMIHF) and federal HOME funds). Most of the funding was provided through LMIHF funding. The City has worked to develop innovative programs to address the rehabilitation needs of multifamily units. Funding for this type of reinvestment is limited.

The City recently added a Systematic Health and Safety Inspection requirement for all rental units over seven years old to be inspected by Community Improvement staff every four years (Program 1). Any units not in compliance must make necessary improvements to the property to ensure the units meet all applicable codes. These efforts have resulted in the improvement of many properties to meet these minimum standards and improve the quality and safety of Ontario's housing stock.

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Housing Construction Needs

Every eight years, California law requires cities to plan to accommodate population and employment growth in their community through the implementation of responsive housing policies and programs. To assist in that effort, SCAG prepares housing construction needs goals for each city in Southern California as part of the Regional Housing Needs Assessment (RHNA). All local governments, including Ontario, are required to set aside sufficient land, adopt programs, and provide funding, to the extent feasible, to facilitate and encourage housing production commensurate with that need.

Total "housing construction need" includes three components: (1) the number of housing units needed to accommodate future population and employment growth; (2) an additional allowance to replace demolished units and restore normal vacancy rates; and (3) a fair adjustment that determines housing need by different affordability levels. The following discusses the specifics of each factor in Ontario.

Population and Employment Growth

The first component of construction need represents the number of units needed to accommodate new households forming as a result of population and employment growth. Ontario's housing need is based on SCAG's regional growth forecast, adopted as part of the 2020 Regional Transportation Plan (RTP) and revised to reflect further local comments. Table 2-30 compares projected population, employment, and household growth in Ontario from 2016 through 2045.

Table 2-30 Households, Employment, and Population Projections 2016–2045

Catagory	2016	2045	Cha	nge
Category	2010	2043	Numeric	Percentage
Households	46,000	74,500	28,500	62%
Employment	113,900	169,300	55,400	49%
Population	172,200	269,100	96,900	56%

Source: SCAG 2020 RTP/SCS

Housing Factors

The RHNA goal for new construction incorporates additional units to accommodate two factors in the housing market. First, the housing market requires a certain number of vacant units to allow for sufficient choice for consumers, maintain rents and prices at adequate levels, and encourage normal housing maintenance and repair.



Over time, it is expected that a certain number of housing units will be lost to residential uses from demolition, fire, conversion to nonresidential uses, recycling to other uses, or a variety of other reasons. In other cases, the City's redevelopment activities throughout the community will also result in the demolition and replacement of certain uses. Therefore, SCAG adjusts the City's housing production goals by a standard "replacement factor" based on the historical rate of units lost to demolition or conversion to nonresidential uses in each community.

Fair-Share Allocation

Ontario's housing construction need represents the total construction needed to accommodate expected population and employment growth while accommodating vacancies and replacement units. This need is further divided into five household income categories defined by state law. The income limits defined by HCD for San Bernardino County in 2021 are:

- Extremely low: households earning 30 percent or less of AMI, or a maximum income of \$26,500 for a four-person household
- Very low: households earning 31 to 50 percent of AMI, or a maximum income of \$39,500 for a four-person household
- Low: households earning 51 to 80 percent of AMI, which translates into a maximum of \$63,200 for a four-person household
- Moderate: households earning 81 to 120 percent of AMI, or a maximum income of \$93,000 for a four-person household
- Above moderate: households earning above 120 percent of AMI, or a minimum of \$93,001 for a four-person household

California law states that the RHNA is required to avoid or mitigate the overconcentration of income groups in a jurisdiction to achieve its objective of increasing supply and mix of housing types, tenure, and affordability in an equitable manner. In practice, jurisdictions with a smaller proportion of lower-income units are required to provide a larger share of those units as part of their construction need to compensate for jurisdictions that already accommodate more than their fair share. Table 2-31 shows the City's RHNA by affordability level.

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Table 2-31
Regional Housing Needs Goals, 2021–2029

Household Income Levels for the RHNA	Number of Housing Units	Percentage of Units by Affordability level
Extremely Low Income	2,820	14%
Very Low Income	2,820	14%
Low Income	3,286	16%
Moderate Income	3,329	16%
Above Moderate Income	8,599	41%
Total	20,854	100%

Source: Southern California Association of Governments 2021.

Housing Preservation Needs

Subsidized housing provides the largest amount of affordable housing to persons and families earning extremely low, very low, and low income. As shown in Table 2-32, as of September 2021, Ontario has an identified 33 developments with 1,959 units of housing built with various local, state, and federal subsidies that are deed-restricted as affordable for lower-income households and persons with special housing needs. California law requires that all housing elements include an analysis of "assisted multiple-family housing" projects as to their eligibility to change from low-income housing to market rates within 10 years of the beginning of the eight-year planning period that will begin on October 15, 2021. This at-risk analysis section thus addresses any affordable assisted units that are at-risk of market-rate conversion as late as October 15, 2031.

Assisted housing developments or at-risk units are multifamily rental housing complexes that receive government assistance under federal, state, and local programs within the current and subsequent eight-year planning period of the housing element. It there are units at-risk, the element must include a detailed inventory and analysis. The inventory must list:

- · Each development by project name and address;
- Type of governmental assistance received;
- Earliest possible date of change from low-income use;
- Total elderly and nonelderly units that could be converted;
- An analysis of the costs of preserving and replacing these units;
- Resources for preservation of at-risk units; and





Program for preservation of at-risk units and quantified objectives.

Affordable housing periodically converts to market rents, particularly during inflationary times when market rents escalate and create a financial incentive.

The City of Ontario made significant progress in preserving many affordable housing projects at-risk of conversion to market rents. During the past housing planning period, the City actively preserved the Ontario Townhomes project, an 85-unit project-based Section 8 property, for an additional 20 years. Table 2-32 provides an inventory of all publicly subsidized affordable housing projects in Ontario and their status.

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Table 2-32 Publicly Subsidized Multiple-Family Housing

				•	, 3			
Project/Address	Unit Type	Total Units	Total Assisted Units	Assisted Units at Risk	Funding Source	Earliest Expiration		
			Units	at Risk of Converting	J			
WOODSIDE II (SENIOR) 302 West G Street	Senior	60	60	12	Amended and Restated Regulatory and Declaration of Restrictive Covenants First Supplemental Regulatory Agreement and Declaration of Restrictive Covenants	12/1/2021		
ENCORE TOWNHOMES (F/K/A WAVERLY PLACE) 1725 East G Street	Multifamily	155	62	31	MFHB	12/1/2021		
RANCHO VISTA TOWNHOMES (F/K/A CAMBRIDGE SQUARE) 1037 N. Archibald Avenue	Multifamily	125	50	25	MFHB	12/1/2022		
CEDAR VILLAS (SENIOR) 301 East Cedar	Senior	136	123	123	Housing Revenue Bond	3/25/2024		
MISSION OAKS 1427 West Mission Boulevard	Multifamily	80	80	80	RDA Housing Set-Aside	5/30/2025		
CICHON 225 East D Street and 415 North Plum A & B	Multifamily	3	3	3	LMIHF	7/15/2025		
ESTANCIA 1720 East D Street	Multifamily	152	85	85	ORA Agreement with Owner	8/6/2026		
CINNAMON RIDGE (SENIOR) 1051 East Fourth Street	Senior	101	101	101	Housing Revenue Bond	8/6/2026		
SUBTOTAL	SUBTOTAL		564	460				
			Units No	ot at Risk of Converti	ing			
AVANTE (F/K/A WATERFORD COURT) 1675 East G Street	Multifamily	165	17	-	МҒНВ	2/9/2059		
WOODSIDE III (SENIOR) 408 West G Street	Senior	84	67	-	MFHB	2/9/2059		
ONTARIO TOWNHOUSES 1360 East D Street	Multifamily	86	85	-	HUD Assisted Project Section 236(J)(1)	3/26/2074		
SEASONS (SENIOR) 955 North Palmetto	Senior	80	80	-	Housing Revenue Bond; LIHTC	12/31/2072		

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Table 2-32 Publicly Subsidized Multiple-Family Housing

				•			
Project/Address	Unit Type	Total Units	Total Assisted Units	Assisted Units at Risk	Funding Source	Earliest Expiration	
VESTA (HOGI) 520-526 1/2 West Vest Street	Multifamily	6	6	-	HOME	6/6/2057	
MT. VIEW (SENIOR) Phase I 511 North Palmetto Avenue	Senior	86	86	-	HOME; RDA Set-Aside: LIHTC	2/13/2058	
PALM TERRACE II 1449 East D Street	Senior	48	47	-	Section 202	6/30/2059	
PARK CENTRE 850 North Center Street	Multifamily	404	101	-	Housing Revenue Bonds	12/1/2060	
CASITAS 1900 South Campus	Multifamily	253	48	-	Parc Vista/Terrace View deal	1/11/2061	
SUMMIT PLACE 1130 West Fourth Street	Multifamily	75	75	-	MFHB, RDA Set-Aside	1/11/2061	
SUMMIT WALK 1206 West Fourth Street	Multifamily	78	78	-	MFHB, RDA Set-Aside	1/11/2061	
LANDMARK @ONTARIO 950 North Dussenberg Drive	Multifamily	469	71	-	City DDA with property owner	11/20/2061	
VINTAGE APARTMENTS 955 North Dussenberg	Multifamily	300	45	-	DDA (Developer Agreement)	4/17/2062	
Mt. VIEW (SENIOR) Phase II 511 North Palmetto Avenue	Senior	20	20	-	LIHTC	7/15/2062	
FRANCIS APARTMENTS 307-311 West Francis	Multifamily	15	15	-	HOME, LMIHF	7/1/2114	
CITY CENTER SENIOR APARTMENTS 280 North Lemon	Senior	76	75	-	HOME, LIHTC	9/21/2065	
METRO 102 (F/K/A COLONY APARTMENTS) 102 North Lemon Avenue	Multifamily	160	160	-	LMIHF	9/21/2064	
PALM TERRACE I 1433 East D Street	Senior	91	90	-	HOME; Section 202	8/12/2060	

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Table 2-32 Publicly Subsidized Multiple-Family Housing

5 1 1/6 1 1			Total Assisted		- " -	Earliest	
Project/Address	Unit Type	Total Units	Units	Assisted Units at Risk	Funding Source	Expiration	
BEGONIA AVENUE APARTMENTS 209, 216, 217, 222, 223, 228, and 231N. Begonia Ave.	Multifamily	28	28	-	NSP1, LMIHF, NSP3, HOME	1/24/2066	
305 NORTH BEGONIA AVENUE	Multifamily	4	4	-	LMIHF, HOME	7/1/2114	
1164 WEST VESTA STREET and 1165 WEST HOLLOWELL STREET	Multifamily	8	8	-	NSP3, HOME	7/5/2067	
EMPORIA PLACE 220 South Fern Avenue	Multifamily	75	74	-	LIHTC, OHA Funds	8/27/2075	
VISTA VERDE APARTMENTS 110 North Virginia Avenue	Multifamily	101	101	-	TCC, OHA Funds, MF Housing Revenue Bonds	4/25/2074	
MERCY HOUSE Guadalupe 411 & 412 North Parkside Drive	Multifamily	15	14	-	RDA Set Aside	"411 – 02/14/2073	
MERCY HOUSE Assisi House (Transitional Housing) 517,521 & 525 Virginia Street	Transitional Housing) Transitional Housing) Transitional		34 beds	-	НОМЕ	412 – 02/28/2069"	
SUBTOTAL (Does not include the 34 beds at Mecy House Transitional Housing)		2,727	1,395				
GRAND TOTAL (Does not include the 34 beds at Mecy House Transitional Housing)		3,539	1,959	460			

Source: City of Ontario. 2021 F/K/A = Formerly Known As

Funding Sources

DDA = Disposition and Development Agreement
HOME = HOME Investment Partnerships Program
UHTC = Low-Income Housing Tax Credit

NSP = Neighborhood Stabilization Program
OHA = Ontario Housing Authority
ORA = Ontario Redevelopment Agency

LMIHF = Low and Moderate Income Housing Fund

RDA Set-Aside = Redevelopment Housing Set-Aside funds

MFHB = Multiple-Family Housing Revenue Bonds

TCC = Transformative Climate Communities



Potential At-Risk Projects

City records identified eight affordable housing projects totaling 460 units for lower-income and moderate-income households where the owner's obligation to retain the units as affordable will expire in the next 10 years if there is no intervention from the City or other entity. The potential of conversion is greater in an escalating rental market, where owners have a greater financial incentive to convert the projects.

The following describes the at-risk properties in detail.

- Avante Townhomes. This project provided 50 affordable units to very low-, low-, and moderate-income households. The affordability restrictions for 33 of the 50 assisted units expired on July 15, 2021. Seventeen (17) units remain restricted for moderateincome households until February 9, 2059.
- Woodside III. This project provided 84 affordable units to very low-, low-, or moderate-income senior households. The project is financed through multifamily Housing Revenue Bonds. The affordability restrictions for 17 of the 84 assisted units expired on July 15, 2021. Sixty-seven (67) units remain restricted for moderate-income households until February 9, 2059.
- Woodside II. This project provides 60 affordable units to very low-, low-, or moderate-income senior households. The affordable units in this project were secured through an Amended and Restated Regulatory and Declaration of Restrictive Covenants First Supplemental Regulatory Agreement and Declaration of Restrictive Covenants. The affordability restrictions for 12 of the 60 assisted units expire as soon as December 1, 2021.
- Encore Townhomes. This project provides 62 affordable units to low- and moderate-income households. The project is financed through multifamily Housing Revenue Bonds. The affordability restrictions for 31 of the 62 affordable units expire as soon as December 1, 2021.
- Rancho Vista Townhomes. This project provides 50 affordable units to very low-, low-, and moderate-income households. This project is financed through multifamily Housing Revenue Bonds. The affordability restrictions for 25 of the 50 assisted units expire as soon as December 1, 2021.

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- Cedar Villas. This project provides 123 affordable units to very low- and moderate-income seniors. The project is financed through Housing Revenue Bonds. The affordability restrictions expire as soon as March 25, 2024.
- Mission Oaks. This project provides 80 affordable units to very low- and moderate-income families. The project is financed through an RDA Housing Set-Aside. The affordability restrictions expire as soon as May 30, 2025.
- · Cichon. This project provides three affordable units to very low-, low-, and moderate-income families. The project is financed through the Low- and Moderate-Income Housing Fund. The affordability restrictions expire as soon as July 15, 2025.
- Estancia. This project provides 85 affordable units to low- and moderate-income families. The affordability of units in this project is secured through an Occupational Rights Agreement with its owner. The affordability restrictions expire as soon as August 6, 2026.
- **Cinnamon Ridge.** This project provides 101 affordable units to very low-, low-, and moderate-income seniors. The project is financed through Housing Revenue Bonds. The affordability restrictions expire as soon as August 6, 2026.

Preservation Options

Typically, local governments have a wide range of options to replace affordable housing units lost through conversion to market rents. However, the four primary ways are to replace the expired rental subsidies, construct new affordable housing units, offer incentives to rehabilitate the units in return for extended affordability controls, or facilitate the transfer of the project to another entity.

Replacement of Rent Subsidies

Rental subsidies using non-federal (state, local, or other) funding sources can be used to maintain affordability of the at-risk affordable units. These rent subsidies can be structured to mirror the federal Housing Choice Voucher (Section 8) program. Under Section 8, HUD pays the difference between what tenants can pay (defined as 30 percent of household income) and what HUD estimates as the fair-market rent on the unit. The feasibility of this alternative is highly dependent on the availability of other funding sources necessary to make rent subsidies available and the willingness of property owners to accept rental vouchers if they can be provided.





Table 2-33 calculates the annual subsidy needed to replace HUD subsidies at fair-market rents based on 2021 prices. As an example, the annual cost to subsidize the difference between affordable rent and fair-market rent for an existing very low-income unit for a family of four in a three-bedroom unit in Ontario that is at-risk of market-rate conversion would be approximately \$11,154, in 2021 dollars. Low-income households occupying one- and two-bedroom units as well as moderate-income households of any size currently do not require subsidies as the affordable rents for these types of housing situations are estimated to be higher than the fair-market rent costs. Very low-income households of any size as well as low-income households occupying three- or four-bedroom units will likely require subsidies to pay rent as the fair-market rent estimates are higher than the affordable rent estimates.

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Table 2-33 Cost to Replace Rent Subsidies

Unit Size	Fair-Mar	ket Rent	Household Size		ry Low-Income MI) Rent		Low- Income MI) Rent	Affordable Mod (120% Al		Mon	thly per-Unit Subs	idy	Annı	ıal per-Unit Subsi	dy
Unit Size	Monthly	Annual	Household Size	Monthly	Annual	Monthly	Annual	Monthly	Annual	Very Low- Income	Low Income	Moderate Income	Very Low- Income	Low Income	Moderate Income
1-bedroom	\$1,106	\$13,272	2	\$790	\$9,480	\$1,265	\$15,180	\$1,860	\$22,320	\$316	\$0	\$0	\$3,792	\$0	\$0
2-bedroom	\$1,390	\$16,680	3	\$889	\$10,665	\$1,423	\$17,070	\$2,093	\$25,110	\$501	\$0	\$0	\$6,015	\$0	\$0
3-bedroom	\$1,917	\$23,004	4	\$988	\$11,850	\$1,580	\$18,960	\$2,325	\$27,900	\$930	\$337	\$0	\$11,154	\$4,044	\$0
4-bedroom	\$2,369	\$28,428	5	\$1,068	\$12,810	\$1,708	\$20,490	\$2,511	\$30,135	\$1,302	\$662	\$0	\$15,618	\$7,938	\$0

Sources: HUD Fair Market Rents 2021; San Bernardino County; 2021 California HCD Income Limits

Note: Affordability based on 30% of monthly income for each category as outlined in the 2021 California HCD Income Limits. Subsidy costs of \$0 indicate that the affordable rent estimate is higher than the estimated fair market rent price from HUD so no subsidy is needed.





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Based on the data and cost estimates presented in Table 2-34, Table 2-34 presents the estimated costs to preserve the identified at-risk housing units. Currently, the estimated annual cost to preserve all at-risk housing in Ontario using rent subsidies would likely be \$470,093. The median cost to preserve all the at-risk units within one housing project is an estimated \$65,511. The most expensive project to preserve via rent subsidies would be 123 units at Cedar Villas at an estimated \$162,405 in 2021 dollars, whereas the least costly project in the city to subsidize would be the 3 units at Cichon, at an estimated \$3,792 in 2021 dollars. Encore Townhomes and Estancia likely have units whose affordable rents are currently higher than the estimated fair-market rent cost and thus they likely would be ineligible to receive subsidies. These estimates may vary depending on data limitations relating to the bedroom mix and income category since these factors together determine the cost of the rent subsidy.



Table 2-34 Cost to Subsidize At-Risk Units in Ontario, 2021

	l lmit	Total	Assisted		At-Risk	Units			Annual	Fauliant
Project/Address	Unit Type	Total Units	Units Total	Total	Very Low Income	Low Income	Moderate Income	Total Unit Bedroom Mix	Subsidy Cost	Earliest Expiration
WOODSIDE II (SENIOR) 302 West G Street	Senior	84	84	12	6	6	0	68 1-bedroom, 15 2-bedroom, and 13-bedroom units	\$58,842	12/1/2021
ENCORE TOWNHOMES (F/K/A WAVERLY PLACE) 1725 East G Street	Multifamily	155	62	31	0	31	0	152 2-bedroom and 3 4-bedroom units	\$0	12/1/2021
RANCHO VISTA TOWNHOMES (F/K/A CAMBRIDGE SQUARE) 1037 N. Archibald Avenue	Multifamily	125	50	25	12	13	0	116 2-bedroom and 9 3-bedroom units	\$72,180	12/1/2022
CEDAR VILLAS (SENIOR) 301 East Cedar	Senior	136	123	123	27	0	96	104 1-bedroom and 32 2-bedroom units	\$162,405	3/25/2024
MISSION OAKS 1427 West Mission Boulevard	Multifamily	80	80	80	16	0	64	8 1-bedroom, 64 2-bedroom, and 8 3-bedroom units	\$93,242	5/30/2025
CICHON 225 East D Street and 415 North Plum A & B	Multifamily	3	3	3	1	1	1	1 1-bedroom, 1 2-bedroom, and 1 3-bedroom units	\$3,792	7/15/2025
ESTANCIA 1720 East D Street	Multifamily	152	85	85	0	31	54	50 1-bedroom and 102 2-bedroom units	\$0	8/6/2026
CINNAMON RIDGE (SENIOR) 1051 East Fourth Street	Senior	101	101	101	21	60	20	38 1-bedroom and 63 2-bedroom units	\$79,632	8/6/2026
Total		836	588	460	83	142	235		\$470,093	

Source: City of Ontario, 2021.

Note: Costs to preserve at-risk units are estimates only based on available data. Cost estimates rely on 2021 Fair Market Rent data from HUD to determine costs of subsidizing at-risk units, which varies by income category and bedroom count. Data on each housing project does not indicate the bedroom count or income category of each at-risk unit concurrently so cost estimates assume the bedroom count and income category of each unit at-risk. These assumptions are as follows:

Woodside II: Estimate assumes 6 Very Low Income 1-bedroom and 6 Low Income 2-bedroom units

Encore: Estimate assumes 31 Low Income 2-bedroom units

Rancho Vista: Estimate assumes 12 Very Low Income and 4 Low Income 2-bedroom and 9 Low Income 3-bedroom units

Cedar Villas: Estimate assumes 96 Moderate Income 1-bedroom and 27 Very Low Income 2-bedroom units

Mission Oaks: Estimate assumes 8 Very Low Income 1-bedroom, 64 Moderate Income 2-bedroom, and 8 Very Low Income 3-bedroom units

Cichon: Estimate assumes 1 Very Low Income 1-bedroom, 1 Low Income 2-bedroom, and 1 Moderate Income 3-bedroom units

Estancia: Estimate assumes 31 Low Income 1-bedroom and 54 Moderate Income 2-bedroom units

Cinnamon Ridge: Estimate assumes 21 Very Low Income and 17 Low Income 1-bedroom and 43 Low Income and 20 Moderate Income 2-bedroom units

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Construction of New Units

The second option is to replace the actual affordable units through new construction. This alternative entails finding suitable sites, purchasing land, negotiating with a developer, funding the project, and the other costs associated with building new housing. The final cost of constructing deed-restricted affordable housing units depends on whether the developer needs to purchase land (or whether the City can transfer the land at a subsidized price) and whether the City or private developer's initial financial contribution can be leveraged with other funding sources.

No recent examples of a non-subsidized affordable multifamily project have been developed. Construction costs are higher than normal because of the nature of the projects and the desire for quality housing. Based on construction cost estimates derived from R.S. Means Construction Cost data, a five-story, 68-unit building built with precast concrete panels and a steel frame would cost approximately \$140.84 per square foot. Using an average of 1,000 square feet per unit, this would equate to \$140,840 per unit. At this per-unit cost, it would cost \$64,786,400 to construct 460 new units to replace the 460 at-risk units.

The final cost to the City could be lowered through access to affordable housing funds from the state, federal government, or private funding sources.

Purchase of At-Risk Units

The City could purchase the units and facilitate transfer to a nonprofit organization dedicated to providing affordable housing. Under the right transfer provisions, this option would be an effective way to preserve the units because the new owner would have a vested interest in maintaining the affordability of the units and have access to funding sources not necessarily available to private for-profit companies. A nonprofit housing corporation could also rehabilitate it using low-income housing tax credits and extend affordability controls.

To facilitate the transfer to a nonprofit, the City could purchase the building outright at market prices and transfer it to the new owner. The market price could be determined in many different ways. The valuation of apartments is often done by examining the sales price of similarly situated properties. When this is not possible, apartments are often valued based on a combination of gross income, vacancy rate, operating and maintenance costs, condition of the property, and the capitalization rate.

The current market value of the projects was estimated using information from multifamily sales listings within Ontario's boundaries. The average cost to purchase a multifamily development was \$289,700 per unit. There are 460 units at-risk of converting to market rate within the current



planning cycle. Using the previously established average cost per unit, if these were purchased, the estimated cost of acquiring these for the City would be \$133,262,000.

Rehabilitation of At-Risk Units

Apartment projects often need rehabilitation, and the property owner may have insufficient funds to complete periodic repairs and renovations. In these situations, the City may find it advantageous to work with the property owner and offer a flexible number of financial incentives (e.g., low-interest loans, renegotiation of current loan packages, cash incentives) in return for extending the length of the affordability covenants on the affordable units. In fact, the City of Ontario has successfully used this approach for the vast majority of affordable housing units.

Rehabilitation and preservation costs depend on a number of factors, most notably the condition of the property, the amount of deferred maintenance, the financial viability of the project, and the length of affordability term. Projects requiring structural improvements may be more expensive, particularly if lead-based paint hazards must be abated.

Qualified Entities

Nonprofit entities serving the larger Greater Los Angeles region, including San Bernardino County and Ontario, can be contacted to gauge their interest and ability in acquiring and/or managing units at-risk of conversion. Table 2-35 shows a partial listing of entities with resources in the San Bernardino County and Greater Los Angeles area. In addition, the full list of quantified entities is included as Appendix A.

Table 2-35
Qualified Entities Near Ontario

Entity Name	Address	City
Neighborhood Housing Services of the Inland Empire Inc.	1390 North D Street	San Bernardino
National Community Renaissance	9421 Haven Avenue	Rancho Cucamonga
Nexus for Affordable Housing	1572 N. Main Street	Orange
Orange Housing Development Corporation	414 E. Chapman Avenue	Orange
Richman Group of California LLC	21520 Yorba Linda Blvd, Suite G-548	Yorba Linda
CSI Support & Development Services	201 E. Huntington Drive	Monrovia
Highland Property Development LLC	250 W. Colorado Boulevard. Suite 210	Arcadia
Source: California Department of Housing ar	nd Community Development, 2021.	

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Summary of Options

As described previously, estimates to preserve the 460 at-risk units in Ontario are:

Annual cost of rent subsidies: \$470,093

Construction of new units: \$64,786,400

Purchase of existing multifamily units: \$133,262,000.

Determining the most cost-effective approach to preserving affordable housing at-risk of conversion to market rents must consider a number of cost factors and market contingencies. Important cost considerations include the achievable rents under current market conditions, the condition of the property and need for rehabilitation, the income levels of the occupants, and the willingness of property owners to accept one or more of the available options. Moreover, one option may be more effective than another, depending on the timing of the decision.

Under the first scenario, City replacement of rent subsidies would easily be the most cost-effective approach in the present market since there is little difference between fair-market rents and affordable rents. But this could quickly change if the occupants had very low or extremely low incomes or rents increased. For preservation options with a longer guarantee of affordability, when funding is available, the City of Ontario could offer rehabilitation loans. The City has successfully used this option to preserve the affordability of many projects.

If the City wishes to preserve the building for as long as possible, potentially in perpetuity, transfer of ownership is the best route. Qualified entities in the business of affordable housing are looking for opportunities to purchase at-risk projects. However, they may lack the financing to make such a purchase. In these cases, if funding is available, the City could offer low-interest loans or gap financing that would allow a nonprofit entity to purchase the property. This strategy would allow the City to ensure the long-term affordability of the project while minimizing the amount of direct public investment.

Program 25 is the City's program to assist with at-risk housing projects.



ASSESSMENT OF FAIR HOUSING

Assembly Bill (AB) 686 requires that all housing elements due on or after January 1, 2021, contain an Assessment of Fair Housing (AFH) consistent with the core elements of the analysis required by the federal Affirmatively Furthering Fair Housing (AFFH) Final Rule of July 16, 2015.

Under state law, AFFH means "taking meaningful actions, in addition to combatting discrimination, that overcome patterns of segregation and foster inclusive communities free from barriers that restrict access to opportunity based on protected characteristics."

AB 686 requires the City of Ontario (City), and all jurisdictions in the state, to complete three major requirements as part of the housing element update:

- 1. Conduct an AFH that includes a summary of fair housing issues; an analysis of available federal, state, and local data knowledge to identify patterns of segregation or other barriers to fair housing; and prioritization of contributing factors to fair housing issues.
- 2. Prepare the Housing Element Land Inventory and identification of sites through the lens of AFFH.
- 3. Include a program in the Housing Element that affirmatively furthers fair housing and promotes housing opportunities throughout the community for protected classes and addresses contributing factors identified in the AFH (applies to housing elements beginning January 1, 2019).

To comply with AB 686, the City has completed the following outreach and analysis. Some of the information is based on the City of Ontario's 2020 AFH adopted in June 2020, and the San Bernardino County Regional Analysis of Impediments (Regional AI), completed in April 2020.

3.1 Outreach

As discussed in the *Housing Element Outreach* section, the City has used a variety of outreach methods, in addition to the standard public hearing process, to reach stakeholders and members of all socioeconomic segments of the city.

To prepare the Ontario 2020 AFH, the City and the Lawyers' Committee for Civil Rights Under Law conducted a broad array of outreach to ensure active community involvement, including groups typically underrepresented in the planning process. Through a combination of community meetings, focus groups, community surveys, and public

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hearings, the City and the Lawyers' Committee reached out to Ontario residents, including Hispanic groups, tenants, homeowners, fair housing organizations, advocacy groups, social service providers for persons with disabilities, low-income households, and persons experiencing homelessness, among others.

Beginning in February 2020, the Lawyers' Committee held meetings with individual stakeholders throughout the region. In addition, the City of Ontario organized a series of meetings in predominantly Hispanic communities. On Saturday, February 29, 2020, an all-day community fair was held that attracted hundreds of residents. The City also held an evening meeting with a wide array of organizations to discuss the AFH. All community meetings had translation services available in Spanish. In addition, all meetings were held in locations accessible to people with mobility issues.

Through the outreach process, the City and the Lawyers' Committee identified the following salient Fair Housing Goals and Policies, which informed the Ontario 2020 AFH. The City's commitment to these goals and priority actions is included as Program 27.

Goal #1: Increase the supply of affordable housing in high-opportunity areas.

Ontario has a significant portion of its residents who are rent-burdened and facing severe housing problems. Additionally, publicly supported affordable housing accounts for slightly under 3 percent of the total housing stock, and Ontario and its environs are experiencing rapidly rising housing costs. Members of protected classes, particularly Hispanic and Black residents, experience these problems most acutely. These findings indicate a need to expand the supply of affordable housing. The Ontario 2020 AFH proposes the following priorities to increase the supply of affordable housing:

- 1. Explore the creation of new funding sources of affordable housing.
- Using best practices from other jurisdictions, explore policies and programs that increase the supply of affordable housing, such as linkage fees, inclusionary housing, public land set-aside, community land trusts, transit-oriented development, and expedited permitting and review.
- 3. Explore opportunities to provide low-interest loans to single-family homeowners and grants to homeowners with household incomes of up to 120 percent of the Area Median Income to develop accessory dwelling units with affordability restriction on their property.



4. Align development codes to conform to recent California affordable housing legislation.

Goal #2: Increase community integration for persons with disabilities.

There is a lack of permanent supportive housing for non-elderly persons with disabilities in Ontario. The Ontario 2020 AFH identifies the following priority to expand housing opportunity for non-elderly persons with disabilities.

1. Prioritizing HOME funding for such projects, which should ideally set aside 10 to 25 percent of units for persons with disabilities who need supportive services, the City can help make development proposals more competitive for low-income housing tax credits (LIHTC) and Mental Health Services Act assistance.

Goal #3: Ensure equal access to housing for persons with protected characteristics, who are disproportionately likely to be lower-income and to experience homelessness.

As community stakeholder meeting attendees were unaware that landlords are required to accept housing vouchers and third-party checks and would benefit from fair housing education, targeted education efforts would help to reduce the incidence of unlawful source of income discrimination. Therefore, the Ontario 2020 AFH proposes the following priority to ensure equal access to housing.

1. Conduct fair housing training for landlords and tenants on California's Source of Income Discrimination protections to reduce the number of voucher holders turned away.

The San Bernardino County Regional AI used a variety of approaches to achieve meaningful public engagement with residents and stakeholders, including 20 public meetings, 20 stakeholder interviews, and a communitywide survey. Respondents to the community survey reported overwhelmingly that there is a lack of affordable housing in San Bernardino County for both individual and families (selected by 71 percent of respondents) and additionally identified displacement risk due to rising housing costs as a barrier to fair housing (selected by 67 percent of respondents). Stakeholder interviews were conducted with 20 local providers of fair housing/legal advocacy, affordable housing, persons with disabilities, seniors, low-income households, and others. Stakeholders spoke about housing conditions and fair housing issues regionally but offered insights specific to Ontario. In response to the question "Are public resources (e.g., parks, schools, roads, police & fire services, etc.) available evenly throughout all neighborhoods in your

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community?" stakeholders found that Ontario has "a good distribution of parks, schools, and community centers."

3.2 Description of Ontario by Area

The AFH relies on comparing the history of development and land use practices across neighborhoods in Ontario and resulting differences in demographics and access to opportunity that have potential to affect the quality of life and achievement for residents in differing areas of the city. For the purposes of the AFH, the areas of the city used for comparison are based on the 2021 California Housing and Community Development (HCD) and California Tax Credit Allocation Committee (TCAC) Opportunity Area Map (Figure 3-2). As will be described in more detail, the HCD/TCAC Opportunity Map shows broadly that residents may experience differing access to opportunity depending on what area of Ontario they live in and, therefore, defining these areas of the city is meaningful to understand land use patterns that result in fair housing issues. Unless otherwise specified, these areas of Ontario identified in the HCD/TCAC Map that are used in this assessment for the purposes of comparison are defined by streets as follows (see Figure 3-1, Areas of Ontario, for visual representation).

Northwest area/Northwest Ontario: West of Archibald Avenue, inclusive of Ontario International Airport to the city's western limits and north of Riverside Drive to the city's northern limits.

Southeast area/Southeastern Ontario: East of the Ontario International Airport and Archibald Avenue to the eastern city limits and south of Riverside Drive to the city's southern limits.

South and east Ontario is further delineated by:

Eastern area/Eastern Ontario: East of the Ontario International Airport and Archibald Avenue to the city limits and north of Riverside Drive.

Southern area/South Ontario: South of Riverside Drive, bounded by the city's southern limits.



UPLAND CITY OF RANCHO CUCAMONGA . 718 TORS MONTCL FONTANA. 1 0000 E EDISON. CITY, OF JURUPA VALLEY COCREENS CITY OF CHINO Source: PlaceWorks 2021 Areas of Ontario Northwest area/Northwest Ontario **Boundaries** Northwest area/Northwest Ontario Ontario City Boundary County Boundary Southeast area/Southeastern Ontario Eastern area/Eastern Ontario Southern area/South Ontario

Figure 3-1 Areas of Ontario

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3.3 City of Ontario History

The City of Ontario was established in the 1880s with the founders' vision of a planned community created on guiding principles including a mutual water company, prohibition of liquor, a grand thoroughfare through the city (Euclid Avenue), and an agricultural college for general education. Ontario was incorporated in 1891, and in 1903, Ontario had been declared the "Model Colony" by an Act of Congress for its establishment of a new standard for urban living.

The City of Ontario first developed as an agricultural community, primarily producing citrus. The town expanded around Euclid Avenue, which established two of Ontario's historic districts, the Historic Downtown and the College Park Historic District. Ontario's population grew in the 1950s as the city shifted from an agricultural-based economy to an industry-based economy and manufacturing jobs became increasingly available. Mass production housing was constructed, reflecting a national beginning of suburban sprawl and the rise of the middle class. This growth radiated from the city's historic cores, establishing most of the residential neighborhoods that dominate northwestern Ontario as it exists today. With the exception of the College Park Historic District, housing in the northwestern area is primarily renter-occupied, and as will be discussed further, households are projected to have poor economic and educational outcomes.

Ontario's rapid job growth continued through the late twentieth century (1980s and 1990s), especially in industrial expansion of automotive plants, air cargo, commerce centers, and housing to match the growing job opportunities. During the 1980s, Ontario was ranked the seventh-fastest growing city in California. Much of the housing growth was focused in master-planned communities on either side of Riverside Drive in the southern and eastern areas of Ontario, including the communities of Archibald Ranch and Creekside. Currently, this area has higher rates of owner-occupied housing and generally higher median incomes than northwestern Ontario.

In 1999, Ontario annexed an 8,200-acre sphere of influence south of Ontario's "Model Colony" southern border, referred to as the "Ag Preserve" and dedicated as the "New Model Colony," and later renamed to "Ontario Ranch." The annexation included the last significant remaining agricultural areas in San Bernardino Valley and provided land development opportunity. Ontario Ranch is the largest master-planned community in Southern California and includes residential neighborhoods, commercial facilities, and public open space, parks, and schools. Ontario's 2020 AFH identified a "lack of affordable housing in south Ontario, in general, and Ontario Ranch in particular" as an



impediment to fair housing. The western half of Ontario Ranch is still largely undeveloped and used for agriculture.

Growth areas for future development include intensified development in the downtown and Holt Boulevard areas, including affordable housing. Development around the Ontario International Airport will continue to include a mix of uses, including hospitality, entertainment, and housing. Future development of the Ontario Mills mall area will not include a reduction in commercial uses, but instead will focus on redevelopment that allows housing opportunities within mixed-use areas.

Recent affordable housing developments have been completed in downtown and along Holt Boulevard. The developments include Emporia Place (75 units of family housing for households with incomes ranging up to 60 percent of area median income) and Vista Verde Apartments (101 units of family housing for households with incomes ranging up to 60 percent of area median income). Additional affordable housing developments are planned in these areas, as well as other housing developments, including new rental and owner-occupied housing.

3.4 Assessment of Fair Housing Issues

California Government Code Section 65583 (10)(A)(ii) requires the City to analyze areas of segregation, racially or ethnically concentrated areas of poverty, disparities in access to opportunity, and disproportionate housing needs, including displacement risk. According to the 2021 HCD/TCAC Opportunity Area Maps (Figure 3-2), Ontario contains a mixture of high-resource, moderate-resource, and low-resource areas. The HCD/TCAC Opportunity Area Maps identify areas in every region of the state whose characteristics have been shown by research to support positive economic, educational, and health outcomes for low-income families—particularly long-term outcomes for children. Specifically, the HCD/TCAC Opportunity Area Map uses a composite score based on education, economic, and environmental indicators to categorize areas as "high resource," "medium resource," and "low resource." Some of the indicators identified by TCAC and HCD to determine the access to opportunity include high levels of employment and close proximity to jobs, access to effective educational opportunities for both children and adults, low concentration of poverty, and low levels of environmental pollutants, among others. For purposes of evaluating fair housing, resource levels refer to the geographic proximity and ease of access to resources, such as low-cost transportation, jobs, and high-quality schools, with low-resource areas having the most limited access.

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Ontario's high-resource areas are concentrated in the southeastern areas of the city, east of the Ontario International Airport and Archibald Avenue to the city limits and south of Highway 60 to S Archibald Avenue and E Riverside Drive, bounded by the city's southern limits. Ontario's high-resource areas are characterized by the city's recent and future development. Business parks and industrial commerce centers dominate east Ontario north of Highway 60, while housing (primarily single-family residential) is south of Highway 60 in eastern Ontario Ranch (east of Archibald Avenue to Hamer Avenue). Western Ontario Ranch (west of Archibald Avenue to Euclid Avenue), largely dairy and other agricultural farms, will continue to be developed into a mixed-use area of residential homes, commercial centers, and industrial and business parks.

More than half of Ontario's northwestern area (west of Archibald Avenue, inclusive of Ontario International Airport and north of Riverside Avenue to Euclid Avenue and Highway 60 west of Euclid Avenue) is considered low-resource and has relatively low access to education and employment opportunities. In comparison to Ontario's high-resource areas, Ontario's low-resource areas score poorly for economic and educational indicators. This implies low economic mobility from high rates of poverty and unemployment, low rates of post-secondary school attainment and proximity to jobs, and low upward mobility because of the impact of neighborhood conditions on students' academic proficiency measured by math and reading proficiency, high school graduate rates, and student poverty rates. The remainder of Ontario's northwestern area is made up of moderate-resource areas. These areas have scored much higher for economic outcomes than the low-resource areas but only slightly higher for educational outcomes, indicating Ontario's moderate-resource areas received this categorization based on ability to achieve higher economic mobility than the low-resource areas. All of Ontario, including highresource areas, scored poorly for environmental outcomes, suggesting Ontario has high exposure to pollution that could result in significant health issues.

Three census tracts in the city are designated as an area of high segregation and poverty (Figure 3-3). The TCAC/HCD created the high segregation and poverty designation to identify census tracts where at least 30 percent of the population is below the federal poverty level (\$26,500 annually for a family of four in 2021) coincides with an overrepresentation of people of color relative to the county. Two of the census tracts are adjacent and are bounded by Holt Boulevard to the south, N Vineyard Avenue to the east, E G Street to the north, and N Allyn Avenue to E D Street and Florence Avenue to the west. N Grove Avenue divides the two census tracts. The third census tract is bounded by Interstate 10 to the south, N Baker Avenue to E 6th Street and N Grove Avenue to the east, Ontario city boundaries to the north and west to



Interstate 10. All three census tracts are primarily characterized by small-lot single-family residential and multifamily development.

The City has conducted the following analysis of available data to assess local access to opportunities and indicators of fair housing issues, in addition to the designations provided by the TCAC/HCD Opportunity Areas Map. Data for racial/ethnic concentrations of poverty, median income, poverty status, predominant population (Hispanic), familial status, overpayment, and overcrowding was available at the census-tract level, and data for overpayment and diversity were available at the blockgroup level. The City has used the most localized level of data available for the analysis.

Patterns of Integration and Segregation

To assess patterns of segregation and integration, the City analyzed six Racially/Ethnic Concentrations characteristics: or Ethnically Concentrated Areas of Poverty (R/ECAP), Hispanic majority, diversity index, income and poverty, familial status, and population with a disability as of 2019 (2018 for Diversity Index and 2010 for Hispanic Majority). Ontario has two census tracts that are considered a R/ECAP, as defined by the United States Department of Housing and Urban Development (HUD) (Figure 3-3). HUD identifies an R/ECAP as any area with a non-white population of more than 50 percent and either a poverty rate of 40 percent or more or a poverty rate of more than three times the average poverty rate for the county. The R/ECAPs are adjacent to one another and located in central Ontario within the low-resource area, and border two of the areas of high segregation and poverty identified in the TCAC /HCD map. One R/ECAP includes Ontario International Airport to the east and is bounded by Mission Boulevard to the south, the city limits along Benson Avenue to the west, and Holt Boulevard to Main Street and Holt Boulevard to N Vineyard Avenue, Interstate 10 to N Archibald Avenue, and E Airport Drive to S Haven Avenue to the north. The second R/ECAP is located north of the first, bounded by Holt Boulevard to the south, N Sultana Avenue to the west, E G Street to the north, and Florence Avenue to ED Street and N Allyn Avenue to the east. According to the analysis provided in the Ontario 2020 AFH, the presence of R/ECAPs has arisen in the last 10 years; from 1990 to 2010, there were no R/ECAPs present in the city. This indicates poverty has become increasingly concentrated in Ontario and may correlate with issues such as increasing economic pressure, lack of affordable housing choice, and lack of educational or economic mobility, and this has disproportionately impacted communities of color. As of the 2015-2019 American Community Survey (ACS), the population of the R/ECAP tracts were estimated to be 4,218 people for the northern R/ECAP (census tract 15.01) and 5,363 people for the southern R/ECAP (census tract 16.0). While there

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are few Housing Choice Vouchers (HCVs) in use in either R/ECAP, the south R/ECAP has 11 vouchers in use (1.2 percent of renting households) and the north R/ECAP does not have any HCVs in use, both areas are predominantly renting households, suggesting that low-income housing is concentrated in this area and is a contributing factor to mobility to medium- and high-resources areas in the city. While the majority of Ontario's population identifies as Hispanic, there is a predominant Hispanic majority in both R/ECAPs (gap greater than 50 percent of the population), as well as in the TCAC-identified areas of high segregation and poverty, whereas much of the rest of the city only has a gap between 10 and 50 percent Hispanic (Figure 3-4). The R/ECAPs correlate with the highest levels of linguistic isolation, which can limit residents' access to resources, essential services, and mobility to moderate- and high-resource areas because of language barriers outside of the R/ECAP neighborhoods. Much of San Bernardino Valley's population, including the cities of Fontana, Bloomington, Rialto, and San Bernardino, predominantly identify as Hispanic. These areas similarly coincide with R/ECAPs within those cities, higher levels of linguistic isolation, and predominantly low-resource areas and areas of high poverty and segregation. This indicates that San Bernardino Valley's Hispanic population is more likely to reside in neighborhoods with limited upward mobility due to poor economic outcomes, educational outcomes, and linguistic barriers. Conversely, the cities of Upland and Rancho Cucamonga are predominantly White by a gap ranging from 10 to 50 percent. Northern Upland and Rancho Cucamonga are categorized as highest resource on the TCAC /HCD map, suggesting the residents of these neighborhoods (predominantly White) will have the best economic, educational, and health outcomes. The City has included Program 24 to promote its first-time homebuyer program and other means of connecting residents with housing opportunities in the city, and Programs 6, 24, and 27 to reduce barriers to mobility from language barriers, particularly Spanish, to promote an inclusive community for all families, individuals, and households.

Overall, Ontario exhibits high diversity based on the Diversity Index, with nearly all of the city receiving a diversity index greater than or equal to 70, with 100 being perfect diversity and 0 being no diversity (Figure 3-5). With exception to the R/ECAP south of Holt Boulevard, the city's other R/ECAPs and TCAC/HCD-identified areas of high segregation and poverty coincide with very high levels of diversity. While predominantly Hispanic, the second most prevalent population in the block groups that form the census tracts are either White or Black/African American. Much of southeastern Ontario exhibit the very high levels of diversity, receiving a diversity index of 85 or higher. Hispanics comprise the majority of the population in these block groups; however, Whites are the second-most prevalent population, indicating the diversity in



Ontario's high resource areas are primarily Hispanic and White residents and that other communities of color are not highly represented in Ontario's high resource areas. Regionally, similar patterns arise; overall, nearly all of the San Bernardino Valley received a diversity index greater than or equal to 70, an indicator of integrated communities. As seen in Ontario, regionally TCAC/HCD-identified areas of high segregation and poverty and low-resource areas correspond with high levels of diversity, particularly in the cities of San Bernardino, Bloomington, Rialto, and Fontana. The population for the block groups that comprise the areas of high segregation and poverty and low-resource areas are predominantly Hispanic with Black/African Americans often the second-most prevalent community. Conversely, San Bernardino Valley's high-resource areas have relatively low diversity with a predominantly White population. Regionally, Ontario's diversity reflects the highly urbanized areas of Los Angeles County, Orange County, and Riverside County, often corresponding with the low-resource areas. Rural areas to the east and coastal areas to the south have lower diversity and are predominantly White. The coastal areas are largely categorized as high and highest resource.

The City will address concentrations of communities of color in low-resource areas, TCAC/HCD-identified areas of high segregation and poverty, and R/ECAPs through implementing Program 11 and Program 27, improving housing choice citywide by providing opportunities for affordable housing in high-resource areas.

The areas with the highest median income in Ontario are in south Ontario, which includes newer, high-end single-family homes in eastern Ontario Ranch (east of S Archibald Avenue) and the future Ontario Ranch (Figure 3-6). Currently, western Ontario Ranch (west of S Archibald Avenue) is largely agriculture and industrial uses, particularly truck parking and related small-scale transportation services. However, single-family development east of S Archibald Avenue is under construction and will be affordable to above-moderate income households. Few neighborhoods north of Highway 60 exceed a median income greater than the HCD 2020 Median Income (\$87,100). Further. State Ontario's R/ECAP neighborhoods and areas of high segregation and poverty primarily are low income, with median income not exceeding \$55,000. These areas similarly show high rates of poverty with at least 20 to 30 percent of the population whose income is below the poverty level and two neighborhoods with 30 to 40 percent living in poverty (Figure 3-7). Ontario's distribution of income and poverty mirrors similar cities in the San Bernardino Valley, with patterns of R/ECAPs and TCAC/HCDidentified areas of high segregation and poverty reinforced by low median income and high rates of poverty. Throughout the Southern California Association of Governments (SCAG) region, there tends to be

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an increase in median income and decrease in poverty levels outside of large city centers.

Ontario has historically been a family-oriented community, influencing the dominance of single-family housing in many parts of the city, and results in patterns that still exist today. In most areas of the city, including all areas south of Highway 60 and east of Archibald Avenue, 40 percent of households consist of married couples with children (Figure 3-8); however, neighborhoods with lower percentages of children in marriedcouple households (20 to 40 percent) coincide with the city's low-resource areas in northwest Ontario, including the R/ECAPs and areas of high segregation and poverty north of Holt Boulevard. This suggests Ontario's low-resource areas have a higher percentage of single-parent households. Single-parent households, reliant on one income, tend to have a lower rate of home ownership and face additional difficulties securing housing. Affordable housing options available to single-parent households may be limited to the low-resource areas in northwest Ontario, resulting in the concentration of single-parent households in these areas. The concentration of single-parent households could also indicate discriminatory practices or an uneven distribution of housing types throughout Ontario that would support single-parent households. Similar trends persist regionally within the San Bernardino Valley and countywide, implying that single-parent households have limited housing options in moderate- and high-resource areas of the county. To address the discrepancy in access, the City will implement Program 10 and 11 to ensure the development of a variety of affordable housing options, including in high-resource areas.

Ontario also does not feature any areas with high levels of individuals living with disabilities, which would therefore be especially vulnerable from a fair housing perspective due to accessibility concerns or risk of discriminatory actions. In San Bernardino County, Upland, the City of San Bernardino downtown, northeastern Victorville, the rural High Desert, and much of the Coachella Valley, have the highest concentrations of persons with disabilities. These areas largely do not coincide with R/ECAPs, except for downtown San Bernardino and the portion of Victorville that has a concentration of disabled persons. Within Ontario, disability was the most common alleged basis for discrimination cases received by HUD, with over one-third of cases identifying this protected class. While Ontario features a lower proportion of disabled residents than other areas of San Bernardino County, resulting in fewer access concerns for current residents, it may be worth considering whether there are factors, such as transit access, cost, or Americans with Disabilities Act (ADA)-accessible units, that are tending to preclude disabled individuals from residing in Ontario. In particular, the Ontario 2020 AFH identified "there is a significant shortage of affordable, accessible housing in a range of unit sizes in both Ontario and in the broader region." Ontario's



available affordable, accessible housing are predominantly senior housing, limiting opportunity for non-elderly persons with disabilities to secure affordable housing within Ontario. To support the findings of the Ontario 2020 AFH, this Housing Element includes Program 30 and 32 to prioritize funding for developments that include permanent supportive housing for non-elderly persons with disabilities and ensure that existing housing may be retrofitted for ADA accessibility. In addition, while not indicated as an area of discrimination in data provided by HUD, current policies regarding restrictions on the criminal histories of residents in boarding, lodging, or rooming houses may be considered discriminatory under California Code of Regulations, Title 2, Section 12264-12271. As part of Program 20, the City commits to reviewing and removing this restriction to prevent future discrimination against federal, state, or youth authority parolees.

Access to Opportunity

As shown in Figure 3-9, access to job centers in Ontario falls along the east-west divide. Much of northwest Ontario, particularly areas west of S Euclid Avenue, received the lowest scores within the city based on HUD's job proximity index for 2014-2017, which calculates scores based on the number of jobs filled by workers with less than a bachelor's degree that fall within a typical commute distance for low-wage workers in the region for each block group. Eastern Ontario, and to a lesser degree southern Ontario, primarily received a score of greater than 80, the highest score on the job proximity index. This suggests that while job opportunities exist in high- and moderate-resource areas, the low-resource areas in northwest Ontario may offer fewer job opportunities and further commutes for low-wage workers. The mean commute time for Ontario residents in 2019 was 32.8 minutes, which exceeds the national average (26.9 minutes) and the San Bernardino County average (31.6 minutes), supporting that Ontario's low job proximity may be a result of long commute times. The TCAC map categorized Ontario's low resource areas due in part to less positive economic outcomes for households within those neighborhoods, based on low access to jobs and wages offered at available jobs, as well as low median household incomes and home values. To increase job opportunity and improve opportunity for economic mobility, Ontario will partner with San Bernardino County to promote the CalWorks program to assist eligible low-income families with children to meet basic needs and enter, or re-enter, the workforce (Program 27).

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In February 2021, the California Office for Environmental Health Hazard Assessment (COEHHA) released the fourth version of CalEnviroScreen, a tool that uses environmental, health, and socioeconomic indicators to map and compare community environmental scores. A community with a high score has higher levels of pollution and other negative environmental indicators. While all of Ontario received scores above the 50th percentile, the highest scores are concentrated in the city's lowresource areas, including the R/ECAPs and areas of high segregation and poverty, showing residents of these areas are disproportionately burdened by multiple sources of pollution. Ontario's environmental conditions are similar to the dense urban communities in the San Bernardino Valley, which are highest in the low-resource areas of San Bernardino, Rialto, Bloomington, and Colton and drastically decrease in the high-resource areas of Upland and Rancho Cucamonga. To address environmental justice concerns in Ontario, the City is concurrently updating the Safety Element and incorporating Environmental Justice policies and actions, which include an assessment to identify neighborhoods that are disproportionately affected by pollution and other hazards that contribute to negative health effects, exposure, and environmental degradation as well as access to parks, grocery stores, and bicycle routes to inform policies to be included in the Policy Plan.

Each year, the California Department of Education publishes performance metrics for each school in the state, including student assessment results for English Language Arts and Mathematics as they compare to the state on meeting grade-level standards. Reporting of educational indicators was suspended in 2020 due to the COVID-19 pandemic: therefore, 2019 is the most recent data available. There are 33 schools in Ontario, including 23 elementary schools, 6 middle schools, and 4 high schools. In 2019, the state-wide performance for English Language Arts was 2.5 points below standard and 33.5 points below standard for Mathematics. These scores measure how far students are from meeting the lowest possible score for their grade level standard, on average. A complete list of 2019 performance metrics is shown in Table 3-1. Of the elementary schools, only Edison Elementary was higher than the standard for both English Language and Mathematics; all other elementary schools fell below the standard in either Mathematics or English Language. Of the 23 elementary schools, 4 exceeded the California statewide average for both English Language and Mathematics. While none of Ontario's middle schools or high schools exceeded the statewide average for both English Language and Mathematics, all four high schools and Grace Yokley Middle School exceeded the statewide average for English Language. Colony High School and Ontario High School both exceeded the statewide average for college/career preparedness.



Table 3-1 School Performance Metrics, 2019

School	English Language Arts Score	Mathematics Score	College/Career Preparedness Score
California Statewide Average	2.5 points below standard	33.5 points below standard	44.1% prepared
	Elementar	y Schools	
Edison Elementary	38.5 points above standard	19.3 points above standard	N/A
Liberty Elementary	4.3 points above standard	5.7 points below standard	N/A
Ranch View Elementary	20.3 points above standard	0.2 points below standard	N/A
Creek View Elementary	16.1 points below standard	34.5 points below standard	N/A
Mountain View Elementary	1.5 points below standard	23.3 points below standard	N/A
Vineyard Elementary	3.8 points below standard	33.2 points below standard	N/A
Richard Haynes Elementary	25.8 points below standard	38.6 points below standard	N/A
Levi H. Dickey Elementary	30.8 points below standard	48.2 points below standard	N/A
Vista Grande Elementary	11.2 points above standard	14.4 points below standard	N/A
Elderberry Elementary	4.5 points below standard	7.1 points below standard	N/A
El Camino Elementary	22.8 points below standard	43.3 points below standard	N/A
Sultana Elementary	6 points below standard	22.3 points below standard	N/A
Corona Elementary	21.6 points below standard	27.4 points below standard	N/A
The Ontario Center	19.3 points below standard	38.3 points below standard	N/A
Central Language Center	5.1 points below standard	42.8 points below standard	N/A
Hawthorne Elementary	24.4 points below standard	43.5 points below standard	N/A
Del Norte Elementary	26.8 points below standard	48.8 points below standard	N/A
Arroyo Elementary	28.8 points below standard	47.3 points below standard	N/A
Mission Elementary	23.4 points below standard	34.2 points below standard	N/A
Euclid Elementary	16.7 points below standard	34.3 points below standard	N/A
Berlyn Elementary	46.6 points below standard	66.8 points below standard	N/A

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Table 3-1 School Performance Metrics, 2019

School	English Language Arts Score	Mathematics Score	College/Career Preparedness Score
Bon View Elementary	30 points below standard	50.9 points below standard	N/A
Mariposa Elementary	39.4 points below standard	55.2 points below standard	N/A
	Middle S	chools	
Grace Yokley Middle School	7.1 points above standard	46.1 points below standard	N/A
Woodcrest Junior High	4.4 points below standard	39.8 points below standard	N/A
Oaks Middle School	13.3 points below standard	53.1 points below standard	N/A
De Anza Middle School	46 points below standard	63.9 points below standard	N/A
Vina Danks Middle School	28.6 points below standard	83.8 points below standard	N/A
Ray Wiltsey Middle School	36.2 points below standard	76.7 points below standard	N/A
	High Sc	hools	
Colony High School	34.3 points above standard	37.6 points below standard	53.3% prepared
Ontario High School	1.8 points above standard	65.7 points below standard	48.9% prepared
Chaffey High School	1 point below standard	89.3 points below standard	37.5% prepared
Chaffey District Online High School	70.6 points above standard	80.1 points below standard	57.1% prepared
	School D	vistricts	
Chaffey Joint Union High District	38.4 points above standard	38.1 points below standard	53.1% prepared
Mountain View Elementary District	3.6 points above standard	33.8 points below standard	N/A
Ontario-Montclair District	16.9 points below standard	39.2 points below standard	N/A
Chino Valley Unified District	Not Available	Not Available	N/A
Cucamonga Elementary District	Not Available	Not Available	N/A
Source: California School Dashboard, 2019.			•



Schools are fairly well distributed throughout the city, with no areas with dramatically less access or proximity to schools. The highest and lowest ranked schools (per California Student Dashboard) are not clustered in any particular area or neighborhood of the city. Ontario's highestperforming schools (Edison Elementary, followed by Liberty Elementary) are located in moderate-resource areas with low rates of poverty, higher rates of home ownership, and higher median incomes, suggesting residents attending these schools may have more economic and housing stability than residents of low-resource areas. Ontario's lowest performing schools (Berlyn Elementary and Ray Wiltsey Middle School) are located in a low-resource area and a TCAC/HCD-identified area of high segregation and poverty, respectively. For both schools, English learners, socioeconomically disadvantaged students, and homeless students have the highest rate of chronic absenteeism (missing more than 10 percent of instructional days) and suspension rate, further indicating the role of housing security in student performance and engagement at school. While most schools in the district perform similarly, there is not a significant difference in access to schools based on proficiency outside of access to Edison Elementary. Overall, addressing housing security and availability of multilingual support services may improve educational opportunities for all students, and particularly students residing in lowresource areas, areas of high segregation and poverty, and R/ECAPs. This Housing Element includes a set of programs to increase housing opportunity for extremely low-income households, including Programs 23, 24, 32, and 33.

Ontario residents are served by OmniTrans, which provides bus routes connecting cities within San Bernardino Valley. There are six OmniTrans routes with transit stops within Ontario, most providing north-south service connecting Ontario to Upland, Montclair, and Rancho Cucamonga to the north and Chino and Eastvale to the south. Route 61, Route 82, and Route 290 provide east-west service through Ontario, connecting Pomona and Montclair to Fontana and San Bernardino via Ontario. Ontario bus routes typically arrive at stops at 60-minute intervals during morning and evening peak hours; however, Route 61 provides transit approximately every 20 to 30 minutes. Exception for express routes, OmniTrans bus routes operate seven days a week, typically with reduced schedules on weekends. Given that several routes are available to Ontario residents, transit is not considered a barrier to fair housing in the city; however, the City will meet biannually with Orange County Transportation Authority (OCTA) to assess if any new unmet transit needs have developed and, if so, will provide technical assistance in applying for state and federal funding for expansions (Program 27).

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To meet the needs of persons with disabilities in Ontario, there are 6 licensed residential care facilities for the elderly and 27 licensed adult residential care facilities. Additionally, residents that qualify under the ADA can use OmniAccess, a curb-to-curb shared ride service that complements the OmniTrans fixed-route system. The OmniAccess service area is up to three-quarter mile on either side of an existing bus route. OmniAccess riders make reservations for trips or arrange a subscription service for recurring trips. OmniTrans offers a Travel Training Program, providing one-on-one or group assistance to seniors and individuals with disabilities to learn to use the bus system. The City also requires new developments to comply with Title 24 of the 2019 California Building Code to ensure that all new construction meets accessible design standards, thus ensuring that all new housing is accessible for all residents regardless of disability. Additionally, the City ensures that older housing that may not meet the same accessibility requirements can be adapted as needed by seeking funding to assist with rehabilitations (Program 30).

Disproportionate Housing Need and Displacement Risk

As discussed previously in the Needs Assessment, overcrowding is an issue in the City of Ontario and significantly impacts renter households. According to California Health and Human Services (CHHS), the rate of overcrowding is higher in the northwest area of the city and highest in areas designated as R/ECAPs and TCAC/HCD-identified areas of high segregation and poverty, where households are primarily renting (Figure 3-10). Overall, approximately 12 percent of households are experiencing overcrowding, which breaks out to 18 percent of renter-occupied households and 0.7 percent of owner-occupied households. Within the areas in Figure 3-10 showing the highest rate of overcrowding, approximately 22 to 32 percent of households are overcrowded. South of Highway 60, tracts experiencing overcrowding do not exceed 12 percent of households. Overcrowding typically implies that either appropriately sized housing is unaffordable or unavailable to current residents, and results in an increased risk of displacement for households living in unit types that do not meet their needs. The area of the city with the highest rate of overcrowding has older, smaller housing units compared to the newer development in southeast Ontario. To address overcrowding as a result of doubling up, unaffordable housing options, or multigenerational households, among other reasons, the City will continue promoting the construction of ADUs (Program 20 and 27), expand housing opportunities for extremely low income households (Program 33) to encourage an increase in housing supply and reduce risk of displacement for residents of these neighborhoods.



As with overcrowding, overpayment is a widespread issue in Ontario, impacting over a third of owner-occupied housing (33 percent) and a majority of renter-occupied housing (58 percent of households). This trend reflects patterns of overpayment throughout the SCAG region, and in much of California. As seen in Figure 3-11 the highest concentration of cost-burdened owner-occupied households are located in the city's northwest areas, corresponding with the location of low-resource areas, R/ECAPs, and TCAC/HCD-identified areas of high segregation and poverty. Overpayment by renter-occupied households is experienced citywide, with a majority of the city's census tracts made up of at least 40 percent of renter-occupied households experiencing cost burden (Figure 3-12). In the low resource areas in northwest Ontario, including the R/ECAP south of Holt Boulevard and the area of high segregation and poverty bounded Holt Boulevard, N Grove Avenue, E G Street, and N Allyn Avenue, the percentage of renter households experiencing cost burden increases to between 60 and 80 percent. In south Ontario, the area bounded by Edison Avenue, Hammer Avenue, E Riverside Avenue, and S Archibald Avenue also increases to between 60 and 80 percent costburdened, indicating the new development in south Ontario is unaffordable to renter households. Overpayment increases the risk of displacing residents who are no longer able to afford their housing costs. To address displacement risks from overpayment, the City will provide incentives to encourage affordable development and will develop a targeted program to connect lower-income residents with affordable home ownership within the city (Programs 6 and 24) and preserve at-risk affordable housing units to maintain affordability (Program 25).

Generally, Ontario's recent development has focused in southeast Ontario while the city's older housing is located in northwest Ontario. While some of northwest Ontario's neighborhoods have maintained housing stock in good condition, such as the College Park Historic District, the burden of rehabilitation disproportionately impacts Ontario's low-resource areas in northwest Ontario. As the housing stock ages, need for repair and rehabilitation may become more common. In some cases, the cost of repairs can be prohibitive, resulting in the owner or renter living in substandard housing or increasing the risk of displacement for occupants of those units. In July 2019, the City developed a Neighborhood Preservation Strategy Plan concentrated on four target neighborhoods: Downtown, Nocta, Mission-Mountain, and Fourth Grove neighborhoods. All of the target neighborhoods are identified as low-resource areas on the TCAC/HCD opportunity map and the Nocta and Mission-Mountain neighborhoods include areas designated as R/ECAPs. neighborhoods were identified due to predominance of lower-income households, renter-occupied households, households experiencing a housing cost-burden at a higher rate than the city as whole, and calls for police services or other city services, such as debris removal from private

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property or public rights-of-way. The majority of housing stock in the four target neighborhoods exceeds 40 years of age and has a higher proportion of housing units greater than 80 years old (12.5 percent) than the city (5.6 percent) as a whole. A key strategy identified in the Neighborhood Preservation Strategy Plan is increasing opportunities homeownership with the intention to increase housing stability, educational achievement, property maintenance, and reduce crime in these neighborhoods. This Housing Element includes programs that will increase opportunities for homeownership through identifying funding mechanisms for homeowner assistance programs (Program 6 and Program 24). To address substandard or older housing, the City will continue to use its code enforcement program to bring substandard units into compliance with city codes and improve overall housing conditions in Ontario (Program 1). Additionally, the City will continue to provide rehabilitation loans and grants for low- and moderate-income homeowners and rental property owners who need assistance to rehabilitate or repair their homes (Program 3).

Lending Discrimination

The City of Ontario's AFH identified lending discrimination as a potential contributing factor to fair housing issues in Ontario. Based on Home Mortgage Disclosure Act (HDMA) Data, "White residents are most likely to have their loan applications results in originated loans, Blacks are least likely, and Hispanics fall between the two groups. Hispanic borrowers are most likely to receive a high-priced loan followed by Black borrowers, while White and Asian borrowers are least likely to be given a high-cost loan. Data is similar for the region." As new housing in southeast Ontario becomes available, past lending practices in Ontario imply White households will have the highest accessibility to the new development and, therefore, high resource areas. The analysis of HDMA data in Ontario found Black households and Hispanic households have less success originating loans that are low or moderate in cost, which can deny access to high resource areas. Additionally, home ownership is a primary means of building equity and intergenerational wealth. White households that have easier access to home ownership can benefit from the economic mobility home ownership provides, while Black and Hispanic households have limited access to economic mobility through home ownership. The City will expand opportunities for homeownership through identifying funding mechanisms for homeowner assistance programs (Program 6 and 24).



Enforcement and Outreach Capacity

The City enforces fair housing and complies with fair housing laws and regulations through a twofold process: review of city policies and code for compliance with State law and referring fair housing complaints to appropriate agencies.

Ontario refers fair housing complaints to IFHMB. IFHMB serves as an intermediary to assist individuals in resolving issues related to housing discrimination, homeownership sustainability, rental complaints, and disputes in court through the provision of resource recommendations, education, and mediation. In addition, the Fair Housing Council provides fair housing education, landlord/tenant counseling, and homebuyer HUD counseling, which includes first-time homebuyer education and mortgage default counseling. Services are available in English, and Spanish and are provided free of charge to clients. The City disseminates information about fair housing laws, resident rights, and remedies for fair housing complaints.

During the outreach process for the Ontario 2020 AFH, fair housing surveys were conducted in person both in English and Spanish at the community meetings and community fair held in February 2020. The majority of respondents were members of protected classes. Of the 73 respondents, 21 (29 percent) found housing discrimination to be an issue in Ontario, and 14 (19 percent) directly experienced discrimination. Survey respondents cited race as the reason for discrimination, followed by color, familial status, national origin, and disability.

As part of the Fair Housing Assistance Program (FHAP), the California Department of Fair Employment and Housing (DFEH) dual-files fair housing cases with HUD's Region IX Office of Fair Housing and Equal Opportunity (FHEO); HUD FHEO reported that 32 cases were filed by residents of Ontario between January 1, 2013, and March 23, 2021 (see Table 3-2). The most common alleged basis of discrimination was Disability (inclusive of Disability and Rehabilitation) with over one-third of cases identifying this protected class. While a majority of cases were found to have no cause determination (53.1 percent), six cases were closed due to successful conciliation/settlement. The Fair Housing Foundation and DFEH were unable to provide specific location information for cases either because they do not track the geographic origin of complaints or because of confidentiality concerns. Therefore, the City was unable to conduct a complete spatial analysis of fair housing issues within the city. Program 30 has been included to work with fair housing enforcement organizations and agencies to track issues and identify patterns in the city.

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Table 3-2 Discrimination Cases, 2013-2021

Bases	Number of Cases	Percentage of Total Cases	Closure Reason	Number	Percentage
Disability and			Conciliation/settlement successful	4	33.3%
Disability & Rehabilitation	12	37.5%	Dismissed for lack of jurisdiction	1	8.3%
			No cause determination	7	58.4%
Familial Status			Conciliation/settlement successful	1	20%
and Familial	5	15.6%	No cause determination	1	20%
Status & Rehabilitation			Complaint withdrawn by complainant after resolution	3	60%
			No cause determination	3	42.9%
National Origin	National Origin 7	21.9%	Complaint withdrawn by complainant after resolution	1	14.3%
-			DOJ Settlement	2	28.5%
			Unable to locate complainant	1	14.3%
Race, Race &			Conciliation/settlement successful	1	14.3%
Color, and Race &	7	21.9%	No cause determination	5	71.4%
Retaliation			Unable to locate complainant	1	14.3%
Sex & Race	1	3.1%	No cause determination	1	100%
			Conciliation/settlement successful	6	18.8%
			Dismissed for lack of jurisdiction	1	3.1%
Total 32		No cause determination	17	53.1%	
	32	32 100%	Complaint withdrawn by complainant after resolution	4	12.5%
			DOJ Settlement	2	6.2%
			Unable to locate complainant	2	6.2%

Source: HUD, 2021.



Site Inventory Analysis

Using the statewide opportunity area map and indicators of concentrated poverty, displacement risk, and access to opportunity as overlays to the City's sites inventory (Figure 3-13), the City was able to identify if the sites identified in the inventory to accommodate the lower-income RHNA disproportionately concentrate these units or increase patterns of segregation.

As shown in Figure 3-13, the City primarily identified candidate sites to accommodate low- and very low-income households in south Ontario bounded by Eucalyptus Avenue to the south and E Riverside Drive to the north, within the Ontario Ranch master plan area and a TCAC-designated high-resource area. Candidate sites for moderate and above moderate sites are also primarily located in Ontario Ranch. As shown in Table 5-4 8,746 low- and very low-income units (79 percent of the total very lowand low-income units), 3,286 moderate-income units (69 percent of moderate-income units), and 9,555 percent of above moderate-income units (92 percent of above moderate-income units) have been identified within Ontario Ranch, allowing integration of income levels in future development. Development within Ontario Ranch will be determined through specific plans, with the proposed Policy Plan designations laying groundwork to support mixed-use development, including mixeddensity residential neighborhoods, commercial uses, and open space and park area. The candidate sites for low- and very low-income units have proposed Policy Plan designations for Medium-Density Residential and Mixed-Use, adjacent to areas designated for recreational open space, general commercial uses, and business parks. As discussed, Ontario's lower-income households are primarily concentrated in northwest Ontario, which is the location of the city's TCAC/HCD-identified areas of high segregation and poverty and R/ECAPs and correlate with factors that limit economic mobility and perpetuate poverty. As a key area for growth and investment for the city, locating the majority of candidate sites intended for low- and very low-income in Ontario Ranch disrupts patterns of concentrated poverty in Ontario. Moreover, populations currently concentrated in northwest Ontario, including single-parent households, lower-income Hispanic households, and Black/African American households, will have access to positive education and economic outcomes from expanding affordable housing opportunities to south Ontario. The City will implement Program 11 to ensure that in addition to affordable housing opportunities to high-resource areas, such as Ontario Ranch, the City will reduce barriers to mobility from language barriers, particularly Spanish, to promote an inclusive community for all families, individuals, and households.

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The City will additionally locate low- and very low-income sites along Holt Boulevard. Holt Boulevard serves as a boundary for the city's R/ECAPs and TCAC/HCD-identified areas of high segregation and poverty and is designated as a low-resource area on the TCAC map. As shown in Table 5-4, a total of 623 low- and very low-income units (about 5 percent of the total very low- and low-income units) are in Downtown (20) and along West Holt (227 units) and East Holt (284 units), which transverse R/ECAPs or areas of high segregation and poverty. Locating additional affordable housing along Holt Boulevard, particularly within R/ECAPs and areas of high segregation and poverty, supports placedbased solutions to alleviating fair housing issues and disproportionate housing need issues. As shown in Figure 3-10 and Figure 3-11, northwest Ontario and particularly areas of high segregation and poverty and R/ECAPs have the highest rates of overcrowding and overpayment in the city. Expanding affordable housing options will reduce competition for existing units. The Downtown District Plan (Plan), which encompasses portions of Holt Boulevard and seven of the low- and very low-income candidate sites, creates an opportunity for reinvestment in northwest Ontario. Downtown will also accommodate moderate housing units, with a total of 20 units located in the Downtown area. The Plan includes goals to expand housing choice and "ensure access to diverse range of quality housing options, encourage density, and variety of affordable price Along Holt Boulevard specifically, the plan identifies opportunities to create market-rate and affordable housing, develop vertical and horizontal mixed-use, and provide residential access to downtown amenities, shopping, public services, open space, and transportation. The City will ensure existing residents are protected from displacement risk through implementing Program 6. Through focused community reinvestment along Holt Boulevard, the City will reduce fair housing issues and expand economic opportunities for new and future residents in northwest Ontario.

Contributing Factors

Through discussions with stakeholders, fair housing advocates, and the Assessment of Fair Housing, the City identified factors that contribute to fair housing issues in Ontario, as shown in Table 3-3.



Table 3-3 Contributing Factors to Fair Housing

AFH Identified Fair Housing Issues	Contributing Factors	Meaningful Actions
	Lack of meaningful language access for individuals with limited English proficiency	Program 24 (Homeownership) and Program 27 (Fair Housing Implementation) will provide resources in multiple languages to reduce language barriers.
Presence of R/ECAPs and	Lack of affordable housing in moderate and high-resource areas of the city Lack of investment/community revitalization strategies in low-resource areas of	Program 11 (Ontario Ranch) encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city.
Areas of High Segregation and Poverty	the city	Program 6 (Neighborhood Stabilization) implements the Neighborhood Preservation
and Poverty	Availability of rentals that accept HCV in moderate and high resource areas of the city	Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario.
	Displacement of residents in moderate and/or high resource areas of the city due to economic pressure	Program 23 (Public Housing) commits to expanding HCV use across the city, including high and moderate resource areas.
Concentration of single-	Concentration of affordable housing in low resource areas of the city	Program 11 (Ontario Ranch) encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city.
parent households Lack of allordable housing in moderate and high-	Lack of affordable housing in moderate and high-resource areas of the city Lack of access to opportunity due to high housing cost	Program 23 (Public Housing) commits to expanding HCV use across the city, including high and moderate resource areas.
Discriminatory actions	Instances of private discrimination	Program 3 (Housing Rehabilitation Loans & Grants) will continue providing rehabilitation programs, with the goal of rehabilitating at least 30 units.
against persons with disabilities	Lack of accessible affordable housing appropriate for persons with disabilities Cost of home repairs	Program 30 (Housing for People with Disabilities) commits the City to assist with the development of affordable housing for persons with disabilities across the city.
		Program 31 (Family Housing) implements programs through CDBG funding to support large families, including HCV use for units appropriate for large families.
Displacement risk from overcrowding	Availability of affordable housing units in a range of sizes Availability of affordable housing in the form of ADUs and JADUs	Program 20 (Development Code Amendments) implements code updates to comply with recent state law for ADUs, supporting the development of ADUs citywide. Program 27 (Fair Housing) explores initiatives to promote ADU development in high resource areas, including reviewing impact fees, actively marketing ADU materials, and implementing a monitoring program.
	Age of housing stock in north west area of the city	Program 3 (Housing Rehabilitation Loans & Grants) will continue providing
Displacement risk due to housing condition	Cost of home repairs	rehabilitation programs, with the goal of rehabilitating at least 30 units.
nousing condition	Lack of meaningful language access for individuals with limited English proficiency to learn about rehabilitation options	Program 24 (Homeownership) and Program 27 (Fair Housing Implementation) will provide resources in multiple languages to reduce language barriers.

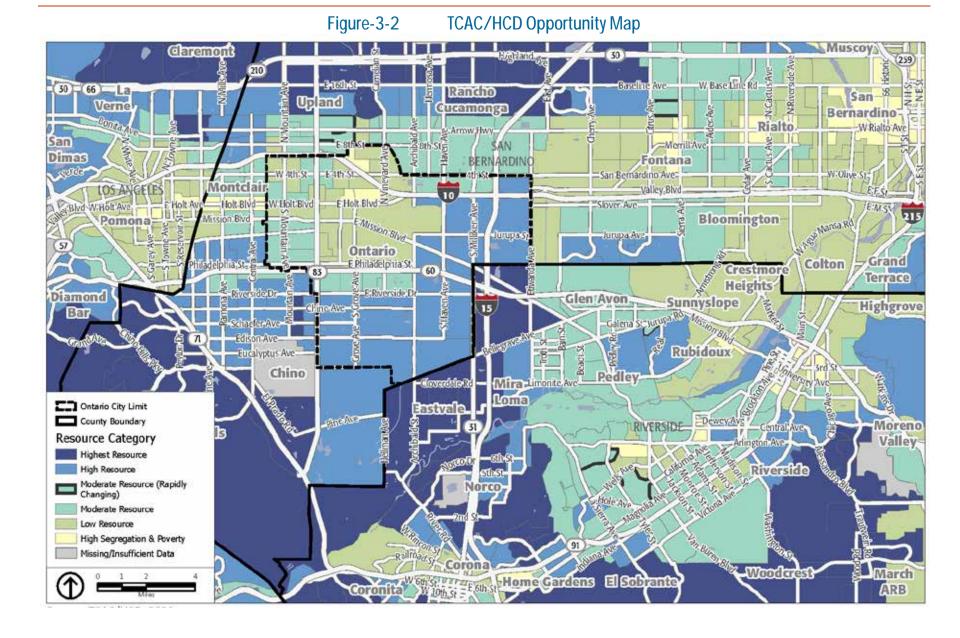
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Table 3-3 Contributing Factors to Fair Housing

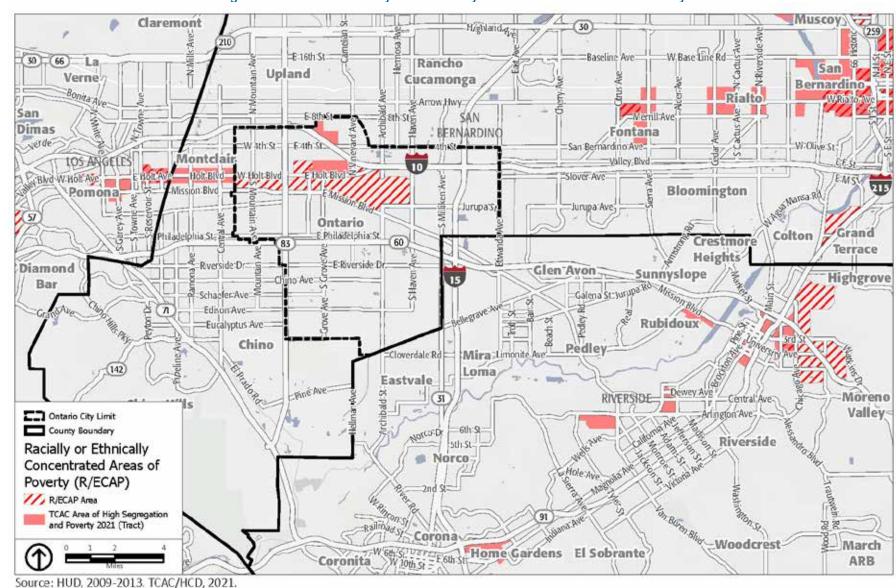
AFH Identified Fair Housing Issues	Contributing Factors	Meaningful Actions
Displacement due to overpayment	Lack of affordable housing in moderate and high-resource areas of the city Displacement of residents in moderate and/or high resource areas of the city due to economic pressure	Program 11 (Ontario Ranch) encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city. Program 23 (Public Housing) commits to expanding HCV use across the city, including high and moderate resource areas.
Location of environmental health hazards	Lack of investment/community revitalization strategies in low-resource areas of the city that would improve health outcomes for residents	Program 6 (Neighborhood Stabilization) implements the Neighborhood Preservation Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario.
Access to proficient schools and low student performance	Lack of access to opportunity due to high housing cost Lack of investment/community revitalization strategies in low-resource areas of the city to improve economic outcomes for residents Availability of rentals that accept HCV in moderate and high resource areas of the city	Program 11 (Ontario Ranch) encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city. Program 23 (Public Housing) commits to expanding HCV use across the city, including high and moderate resource areas. Program 6 (Neighborhood Stabilization) implements the Neighborhood Preservation Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario.
Location of employers	Lack of investment/community revitalization strategies in low-resource areas of the city that would improve health outcomes for residents Lack of affordable housing in moderate and high-resource areas of the city	Program 11 (Ontario Ranch) encourages development of affordable housing in future developments in Ontario Ranch, the high resource area in the city. Program 23 (Public Housing) commits to expanding HCV use across the city, including high and moderate resource areas. Program 6 (Neighborhood Stabilization) implements the Neighborhood Preservation Strategy Plan, which identifies revitalization strategies in key neighborhoods in northwest Ontario.



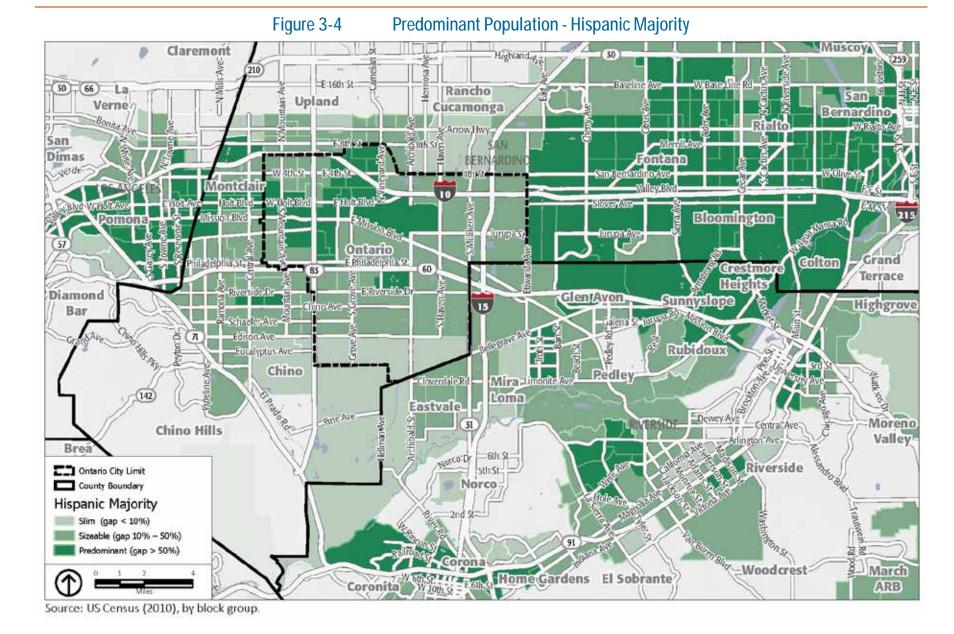


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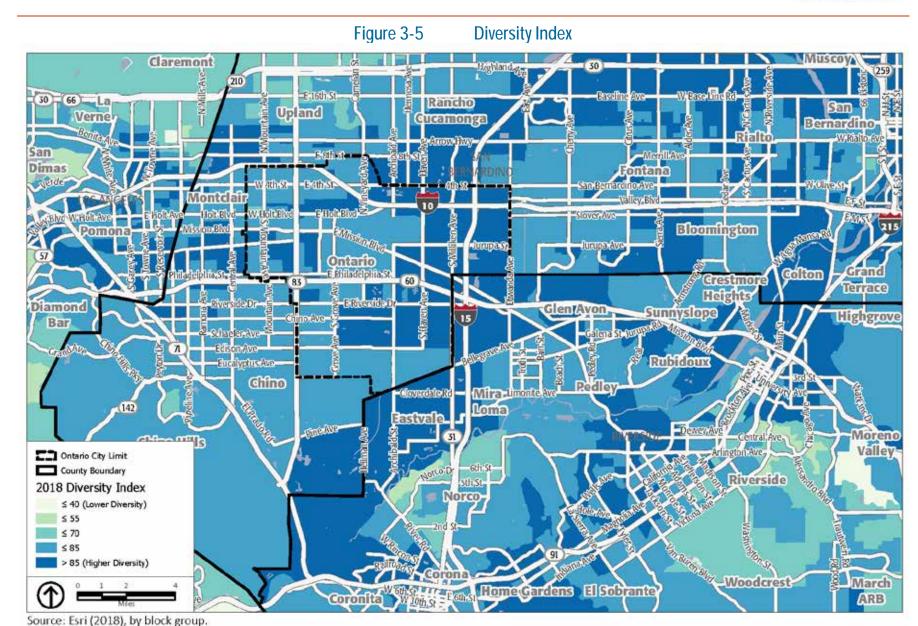






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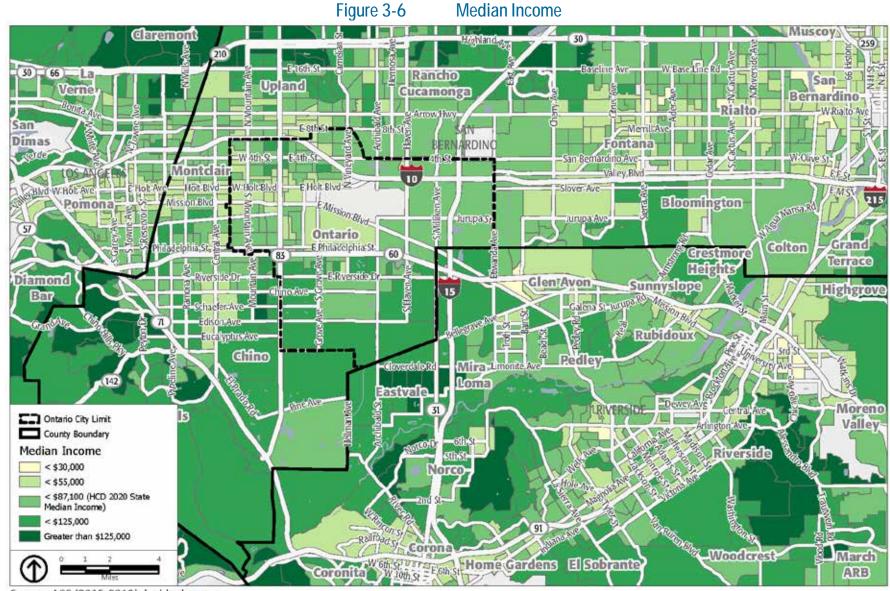




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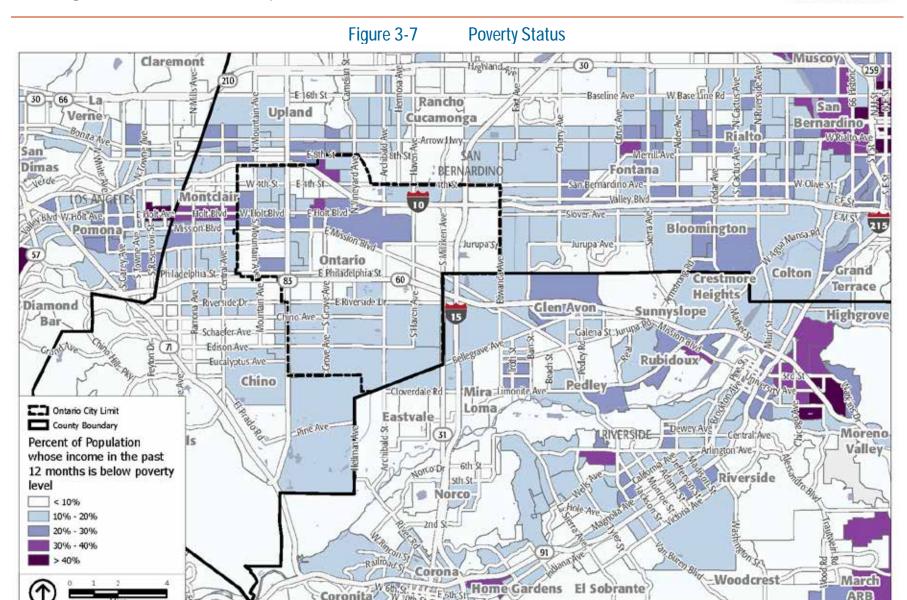
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Source: ACS (2015-2019), by block group.





Coronita Wood S F Sth St Home Gardens El Sobrante

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Source: ACS (2015-2019), by tract.

March

ARB



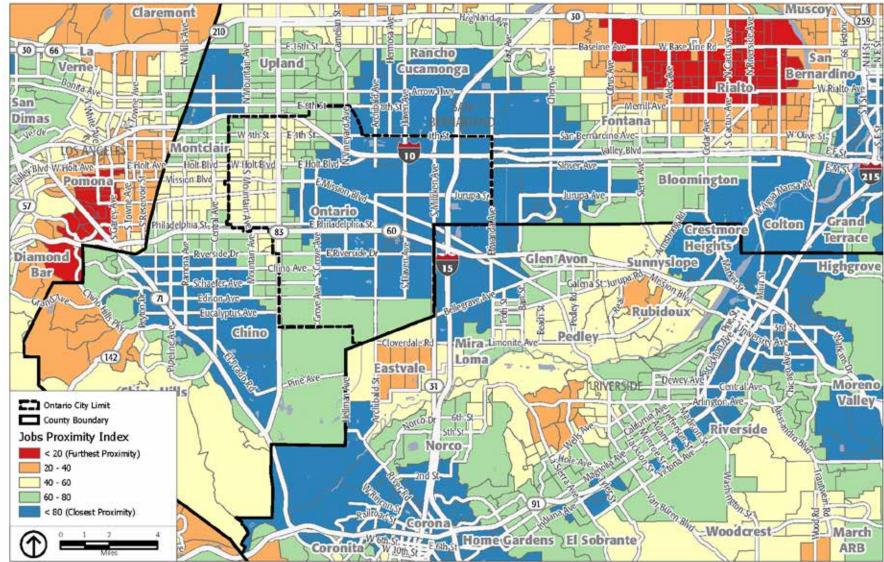


Figure 3-8 Children in Married Couple Households

Source: ACS (2015-2019), by tract.

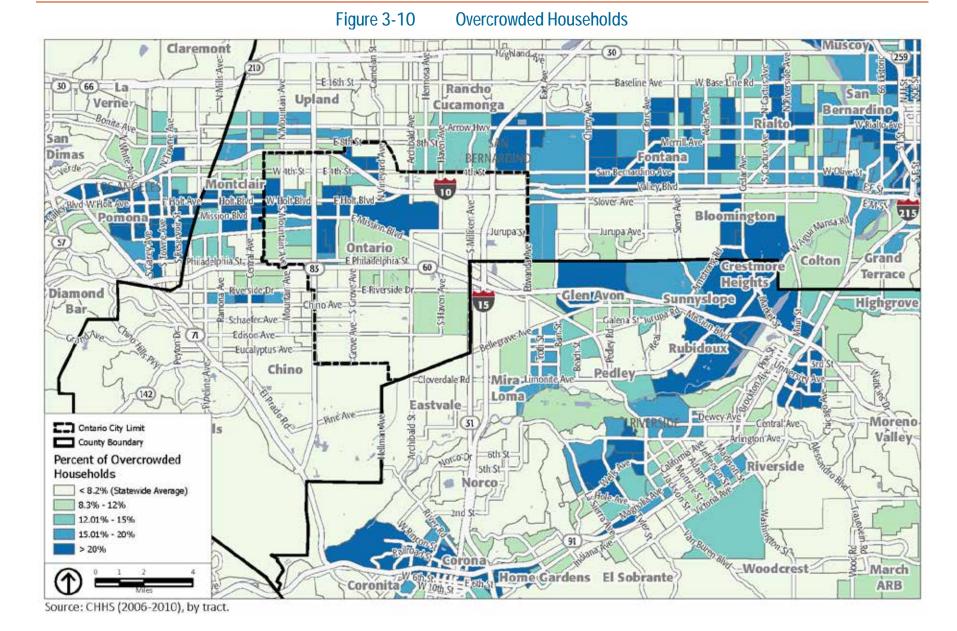






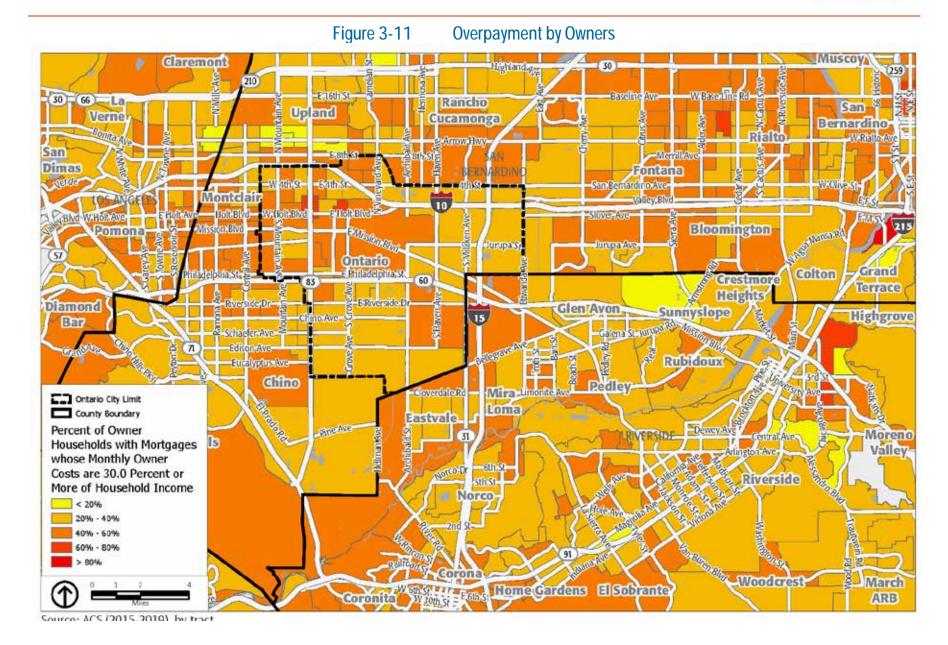
Source: HUD (2014-2017), by block group.



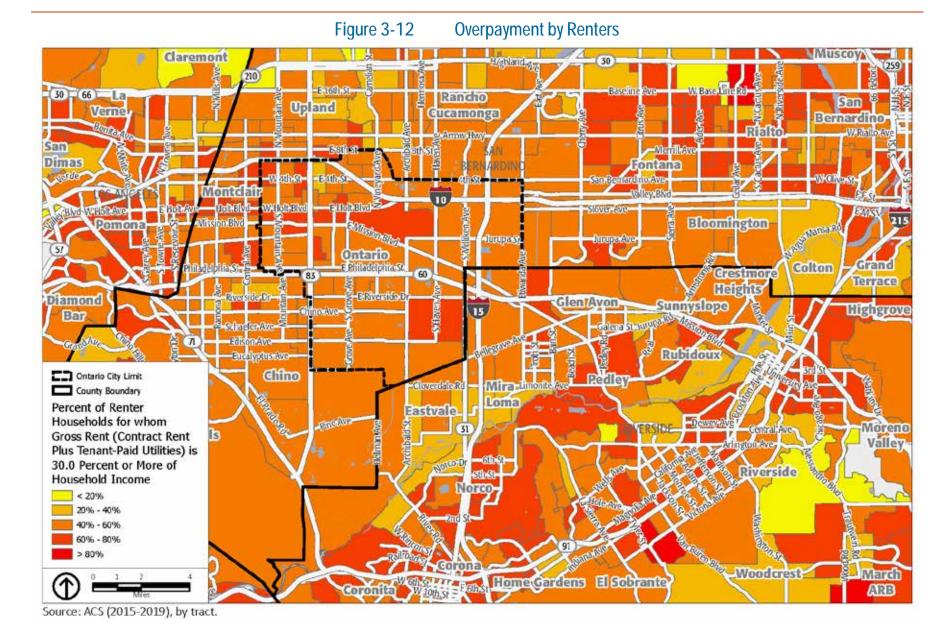


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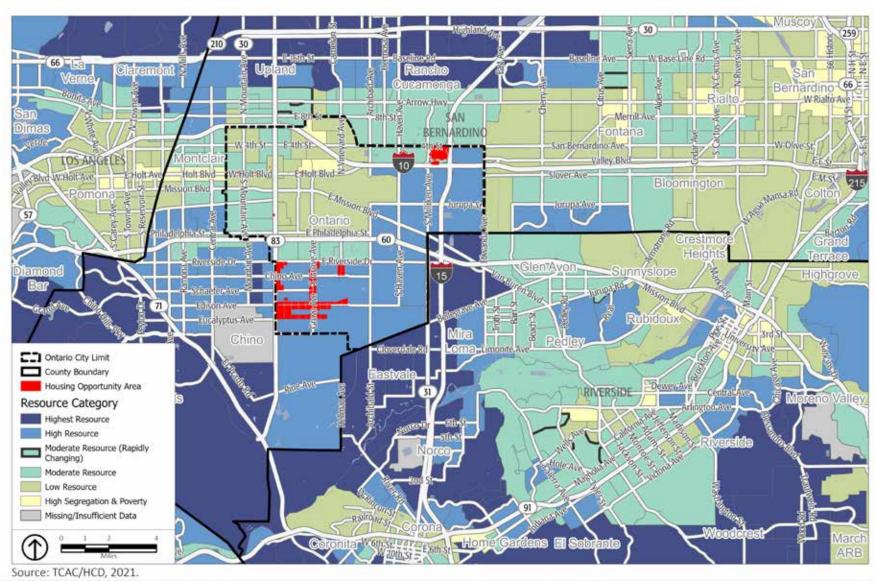




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Figure 3-13 Sites Inventory Analysis





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4. HOUSING CONSTRAINTS

Various factors may constrain or limit the City's ability to address its housing production needs, such as governmental regulations or environmental considerations. Market factors, including a change in interest rates or construction costs, may affect the feasibility of building housing or the affordability of housing to the community. Moreover, housing goals may at times conflict with the need to promote other important City goals, including open space or economic development.

These and other governmental constraints may affect the development, improvement, and maintenance of housing for all economic and social groups in the city. State law requires the housing element to analyze potential and actual governmental and nongovernmental constraints to the production, maintenance, and improvement of housing for all persons of all income levels, including persons with disabilities.

This chapter analyzes the following three potential constraints to the production, maintenance, and improvement of housing in Ontario:

- Market factors. Including the demand for housing, development costs, availability of financing, the price of land, and other factors affecting supply, cost, and affordability of housing.
- Governmental factors. Including land use regulations, residential development standards, building codes, local fees and taxes, permit procedures, and other local policies.
- *Environmental factors.* Including the adequacy of infrastructure, public services, and water supply to support new development within the older and newer portions of the community.

The constraints analysis must also demonstrate local efforts of the City to remove governmental constraints that hinder achievement of its various housing goals. Should actual constraints preclude the achievement of state and local housing goals, a jurisdiction is required to address and, where appropriate and legally possible, remove governmental constraints to the maintenance, improvement, and development of housing.

This section reviews the City's Policy Plan, Development Code, and other housing and planning documents to analyze public policies and governmental regulations that may limit housing opportunities in Ontario. Also presented are ways in which the City has acted to remove or mitigate potential constraints to the production of housing.



Market Factors

The feasibility of building new single-family and multiple-family housing depends on a number of market factors: land costs, the availability or lack of infrastructure and services for the site, the cost of site improvements, construction costs, the availability of financing, and the achievable sales price or rent structure. Fees charged for housing also play into the overall financial pro forma for new housing. This section details these market factors and its overall impact on housing costs.

Land Costs

Land costs typically represent one of the largest components of the total cost of new housing. Because the availability of land has dwindled over the past years, land costs have increased, as have housing prices. Land costs vary throughout the community and depend on the underlying zoning for the site (single- or multiple-family), whether infrastructure is needed, the surrounding area, and location. Because the sphere of influence area surrounding the city is entirely incorporated, there has not been an opportunity during the last planning period to annex new vacant land into the city limits, nor will there be during the current planning period.

In Ontario, land costs range significantly, depending on whether the site is vacant, improved, and has infrastructure in place to support immediate development. Available properties for sale on Zillow.com, Redfin.com, and LandandFarm.com indicate vacant land in northwest Ontario (north of Riverside Drive and west of Ontario Airport) ranges from \$24 to \$83 per square foot. In south Ontario (south of Riverside Drive), the only available land at the time of the search (September 2021), was about \$8 per square foot. Table 4-1 illustrates the cost of vacant land that could support residential use in Ontario.

Table 4-1
Typical Vacant Land Costs in Ontario

Location	Residential	Commercial (Mixed-Use)
Northwest Ontario	\$24 to \$83/square foot (sf)	\$16 to \$38/sf
South Ontario	\$8/sf	N/A

Source: Zillow.com; redfin.com; landandfarm.com, September 2021.

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Construction Costs

Construction costs are the largest component of housing. Construction costs include labor and materials. Backbone infrastructure costs in Ontario Ranch will also increase the cost of development and lower land costs. Like all cost components, the cost of constructing housing can vary significantly by project type (e.g., apartments, townhomes, single-family homes), the quality of construction materials, the location of new housing, the number of stories of the project, whether underground or subsurface parking is required, labor costs, and profit margin. Currently, growth areas for future development include intensified development in the downtown and Holt Boulevard areas, including some affordable housing. Recent development has tended to move from the east to west in the southern half of the city. On the west side of the channel, future development is expected to occur starting in the south and moving north. Development around the airport will continue to include a mix of uses, including hospitality, entertainment, and housing. Future development of the mall area will not include a reduction in commercial uses, but instead will focus on redevelopment of outbuildings and parking.

R. S. Means Construction Cost data (2021) provides manuals for calculating the average cost per square foot for residential construction. According to standard estimates, the cost for good housing in the five-county Southern California Association of Governments (SCAG) region could be around \$203 per square foot for a 2,000-square-foot, two-story, single-family dwelling. Projects with lower construction costs can be expected to contain limited site work, while the higher-cost projects could be inclusive of site work.

Based on recent residential projects built in Ontario, the construction cost was approximately \$123,000 per apartment unit and approximately \$405,500 for single-family units. The city's higher construction costs reflect the standards for quality construction and amenities that contribute to higher home values over time. These requirements are intended to address the lack of quality construction in past years, which today requires the City to implement extensive and costly housing rehabilitation programs.

Financing Costs

The cost and availability of financing can impact a household's ability to purchase a home or to perform necessary maintenance and repairs. As shown in Table 4-2, conventional mortgage loans for homes range between 2 and 4 percent for a standard fixed-rate loan with a 30-year term. In recent years, interest rates have decreased, reaching historic lows, but are starting to increase. Increases in interest rates can have a dramatic impact on housing affordability. For example, for a home loan for



\$200,000 and a 20-percent down payment (\$40,000), the difference in the monthly payment between a 3.5-percent interest rate (\$718) and a 4.5percent interest rate (\$811) is nearly \$100. The difference paid over the life of the loan (assuming a 30-year, fixed-rate loan) exceeds \$33,000.

As prices for market-rate housing increase, the subsidies to bridge the amount a household can afford to pay and the market price of the unit have become very high. As a result, substantial financial subsidies, often from multiple funding sources, are required to finance the construction of affordable housing; however, only a few affordable housing developers can assemble multiple funding sources and have experience in complying with the complex regulatory requirements governing the use of various funding programs.

Table	4-2
Interest	Rates

interest rates				
	Interest	Annual Percentage Rate		
30-year fixed	2.875%	2.996%		
15-year fixed	2.250%	2.398%		
5-year Adjustable Rate Mortgage	2.000%	2.537%		
Federal Housing Administration Rates				
30-year fixed	3.125%	4.184%		
Veterans Loans				
30-year fixed	2.250%	2.484%		
Source: http://www.wellsfargo.com_2021: h	ittn://www.ushank.com			

Program Response

Although state housing element law does not require the City to mitigate the impact of market factors on the feasibility of constructing affordable housing, the City does implement many programs to help facilitate the construction of affordable housing and assist renters and homeowners. A commercial linkage or affordable housing in-lieu fee may further support the development of affordable housing and mitigate displacement of lower-income households. Programs 6, 8, 9, 10, 11, 24, and 27 will help mitigate the impact of market factors and achieve the City's affordable housing goals. In some cases, the market downturn also provides the City with a greater ability to influence land costs, such as through land writedowns.

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Land Writedowns

Because of the high cost of residential land and its impact on the feasibility of constructing affordable housing, the City has a program (Program 18) to help developers purchase or lease land. For the Mercy House Continuum of Care (CoC) Program, the City and/or the Ontario Housing Authority is leasing some properties to Mercy House for a minimum of \$1 per year to help support the operation of the homeless CoC (Program 28).

Working with Nonprofits

The City's affordable housing program works with developers, both nonprofit and for-profit, to facilitate the packaging of financial deals to allow for the construction of affordable housing. All of the recent affordable housing projects built in the city have a range of public and private funding sources that have been leveraged together.

Developer Concessions

The City of Ontario implements various housing programs to reduce or modify development standards that add costs to constructing affordable housing. These may include modification of parking, open space, and other standards through administrative exceptions. Moreover, considerable fee reductions are offered in return for affordability agreements. Finally, developers of affordable housing are also able to secure density bonuses that work to increase the cash flow of a project and indirectly mitigate the cost of construction, land costs, and financing constraints. Each of these incentive programs is described later.

Development Impact Fees and Taxes

The City charges a range of development impact fees and exactions to recover the costs of providing services to new development. Fees are designed to ensure that developers pay a fair pro-rata share of the costs of providing infrastructure and compensate the City for processing the application and fund the construction of future infrastructure necessary to sustain the growth of the city. The types of fees and their amounts are regulated by the California Government Code.

- Planning and Building Fees. The City charges fees to recover the cost for processing applications, building permits, and services.
- Local Impact Fees. Ontario charges fees to construct infrastructure (water, sewer, library, etc.) required to serve new development, including housing.



- Regional Impact Fees. Regional or government entities charge fees to provide infrastructure and services for each new development project, such as schools and regional wastewater entities.
- Ontario Ranch Fees. Developers pay fees to construct improvements in accordance with City master plans, specific plans, subdivision requirements, and development agreements.

Table 4-3 itemizes fees charged for prototypical projects in Ontario. Development Impact Fees are available on the City's website. Generally, fees range from approximately \$26,000 to \$42,200 in the general city. Fees in Ontario Ranch range from approximately \$20,000 to \$52,000 per unit due to the lack of infrastructure in that area.

Table 4-3 Residential Development Fees

General City Ontario Ranch					_	
5 0 1 1		General City	T		Untario Ranci	1
Fee Category ¹	Detached Dwellings	Attached Dwellings	High-Density Dwellings	Detached Dwellings	Attached Dwellings	High-Density Dwellings
City Building Permits ²	9,614.74	11,309.16	8,606.65	11,290.02	10,049.65	8,748.91
Public Safety (Police/Fire)	\$722	\$627	\$627	\$1,151	\$989	\$989
Streets (Regional/ Local)	\$2,439	\$1,629	\$1,008	\$4,847	\$3,237	\$2,002
Storm Drainage (Regional/Local)	\$3,404	\$1,094	\$508	\$5,335	\$1,211	\$988
Water Distribution (Regional/Local)	\$7,473	\$5,109	\$3,447	\$8,997	\$4,939	\$2,621
Parks, Library, and Aquatics	\$14,506	\$12,858	\$10,174	\$14,506	\$12,858	\$10,174
Sewer (Regional/Local)	\$1,384	\$1,211	\$1,038	\$902	\$684	\$413
Solid Waste	\$699	\$509	\$255	\$699	\$509	\$255
General Facilities	\$610	\$127	\$93	\$610	\$127	\$93
Public Meetings	\$1,386	\$1,228	\$972	\$1,386	\$1,228	\$972
Fiber Optics (Regional/Local)				\$1,943	\$1,943	\$1,943
		School Distric	ct (per sq. ft.)			
Chaffey Joint Union High School District	\$4.08	\$4.08	\$4.08	\$4.08	\$4.08	\$4.08
Chino Valley Unified School District	\$4.08	\$4.08	\$4.08			
Cucamonga School District	\$2.82	\$2.82	\$2.82			
Mountain View School District	\$2.82	\$2.82	\$2.82			
Ontario-Montclair School District	\$4.95	\$4.95	\$4.95	\$4.95	\$4.95	\$4.95
Total Fees per Unit						
Building	\$9,614.74	\$11,309.16	\$8,606.65	\$11,290.02	\$10,049.65	\$8,748.91
Impact	\$32,623	\$24,392	\$18,122	\$40,376	\$27,725	\$20,450
Total	\$42,237.74	\$35,701.16	\$26,728.65	\$51,666.02	\$37,774.65	\$29,198.91

Source: City of Ontario, 2021.

Attached Dwelling Unit – Apartments, townhomes, condominiums, or any other residential unit that is attached to any other residential unit; usually corresponding to an allowable land use designation of Low-Medium-Density Residential (LMDR) and Medium-Density Residential (MDR) or Mixed-Use (MU).

High-Density Residential – Any residential units with density ranges of more than 25 units per acre; usually corresponding to an allowable land use designation of High-Density Residential (HDR) or Mixed-Use (MU).

2. Building Permit Fees are based on total project valuation and will vary depending on project type, including detached dwelling, attached dwelling, or high-density dwellings.

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^{1.} Detached Dwelling Unit – Any residential building containing one dwelling unit on one parcel of land, including a single-family residence, single-family residential condominium or detached townhome, and a manufactured unit on an individual lot.



Affordable Housing Reductions

Although development impact fees add to the cost of residential construction, they are not considered a constraint to the production of affordable housing. In compliance with California Government Code Section 66005, a local government is required to ensure that fees do not exceed the estimated reasonable cost of providing the service. California Government Code Section 66001 requires that impact fees have a reasonable nexus to the project and the fee amount be reasonably related to the cost of providing services and capital facilities. Moreover, the City offers significant fee reductions for qualified projects.

With the adoption of Resolution No. 2007-023, the City Council determined that the development and redevelopment of affordable housing is of utmost importance to promote the objectives of the Policy Plan, the Housing Element, revitalization objectives, and the overall supply of decent and affordable housing. Therefore, the City Council approved the reduction of development impact fees for projects covered by an Affordable Housing Agreement with the City.

The ordinance sets a sliding scale of fee reductions for qualified residential projects, with potential fee reductions shown in Table 4-4. All qualified projects must make available a minimum of 20 percent of affordable units for very low-income households and the remaining units affordable to low-income households. To assist Ontario Ranch developers and their substantial commitment to fund infrastructure improvements, the City issues reimbursements or credits to the developer for the eligible costs of public infrastructure based on the estimated and/or actual eligible construction costs identified in the Development Impact Fee Nexus Report and Master Facilities Plan that will serve their project.

Table 4-4
Residential Development Fee Credits

	General City		
Project	Percentage of Maximum Fee	Dollar Amount of Possible Reduction	
Where 10% of units are affordable	65%	\$15,000 to \$17,000/du	
Where 15% of units are affordable	35%	\$28,000 to \$33,000/du	
Where 15% of units are affordable	15%	\$37,000 to \$43,000/du	
Multistory Building with Mixed-Uses	50%	\$21,000 to \$25,000/du	
- with Structured Parking	10%	\$39,000 to \$45,000/du	

Source: City of Ontario, 2007.

Notes:

For these projects, a minimum of 20 percent of the affordable units must be affordable to very low-income households and the remainder must be affordable to low-income households.

Fee reductions do not apply to the Streets, Signals, and Bridges Fee category attributable to the 36 regional projects constructed by SANBAG under the Measure I program.



Land Use Controls

The Land Use Element prescribes the allowable uses of land in Ontario. Land use categories are provided to guide the type of development, intensity or density of development, and the permitted uses of land. The City's Development Code implements the Policy Plan by providing specific direction and development standards within each of the general land use categories through zoning. Previously, the City had separate categories for its Ontario Ranch area. Recent projects in the city have reached the City's target densities, and projects tend toward the higher ends of permitted densities. Other areas of the city have been rezoned to a more intensive land use but are largely built out.

As part of the 2050 Policy Plan update, the City revised its Policy Plan land use designations, most notably expanding the Mixed-Use category to include area-specific Mixed-Use designations to create focal points for community activity and identity and facilitate the use of transit. These designations facilitate the development of high-density residential projects, principally with the Ontario Center Mixed-Use designation allowing up to 125.0 dwelling units per acre (du/ac). Some parts of the city permit blended densities. The new Policy Plan land use designations apply to Ontario Ranch.

Table 4-5 presents the 2050 Policy Plan land use designations, corresponding zoning districts, and the permitted densities for residential development.

Table 4-5
Primary Policy Plan Land Uses Allowing Housing

2050 Pc	olicy Plan
Policy Plan Land Use and Allowable Density	Zoning District and Allowable Density
Rural 0.0–2.0 du/ac	AR-2 and RE-2 Districts 0.0–2.0 du/ac
Low Density 2.1–5.0 du/ac	RE-4 and District 2.1–4.0 du/ac LDR-5 District 2.1 – 5.0 du/ac
Low Medium Density 5.1-11.0 du/ac	MHP District 5.1 – 8.0 du/ac MDR-11 District 5.1–11.0 du/ac
Medium Density 11.1-25.0 du/ac	MDR-18 District 11.1–18.0 du/ac MDR-25 District ¹ 18.1 – 25.0 du/ac

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Table 4-5
Primary Policy Plan Land Uses Allowing Housing

2050 Policy Plan				
Policy Plan	Zoning District and			
Land Use and Allowable Density	Allowable Density			
High Density	HDR-45			
25.1 – 45.0 du/ac	25.1 – 45.0 du/ac			
Downtown Mixed-Use Area 25.0 – 75.0 du/ac	MU-1 District and LUA-1, LUA-2N, LUA-3, LUA-4 Sub-Districts 25.0 – 75.0 du/ac			
East Holt Mixed-Use Area	MU-2 District ¹			
14.0 – 40.0 du/ac	14.0 – 40.0 du/ac			
Meredith Mixed-Use Area	Existing Specific Plan			
14.0 – 125.0 du/ac	14.0 – 25.0 du/ac			
Multimodal Mixed-Use Area	Existing Specific Plan			
20.0 – 80.0 du/ac	20.0 – 80.0 du/ac			
Inland Empire Corridor Mixed-Use Area	Existing Specific Plan			
14.0 – 30.0 du/ac	14.0 – 30.0 du/ac			
Guasti Mixed-Use Area	Existing Specific Plan			
25.0 – 65.0 du/ac	25.0 – 65.0 du/ac			
Ontario Center Mixed-Use Area	Existing Specific Plan ¹			
20.0 – 125.0 du/ac	20.0 – 125.0 du/ac			
Ontario Mills Mixed-Use Area	Existing Specific Plan ¹			
25.0 – 85.0 du/ac	25.0 – 85.0 du/ac			
NMC East Mixed-Use Area	Existing Specific Plan			
14.0 – 50.0 du/ac	14.0 – 50.0 du/ac			
NMC West Mixed-Use Area	Specific Plan Required ¹			
14.0 – 65.0 du/ac	14.0 – 65.0 du/ac			
Euclid/Francis Mixed-Use Area	MU-11 District			
14.0 – 25.0 du/ac	14.0 – 25.0 du/ac			

Source: City of Ontario, 2021.

To provide for greater land use controls and guidance, Ontario has 49 different Specific Plans, 18 of which contain significant residential uses. Pursuant to the annexation of the dairy lands south of the city in 1998, the City is processing Specific Plans for Ontario Ranch as well. Table 4-6 displays the Specific Plan areas that are primarily residential.

City is proposing changes to increase the minimum and/or maximum density for sites for lower-income units subject to requirements of Section 65583.2(h) in the MDR-25 land-use designation, Mixed-Use Districts, and Ontario Mills Specific Plan, Ontario Center Specific Plan (and Piedmonte Overlay), and Armstrong Ranch Specific Plan. See Program 13 for details.



Table 4-6 Existing Specific Plans with Residential Uses

No.	Specific Plan	Description	Development Status
1	Ontario Center (1987)	701-acre residential, commercial, industrial, and office development plan	Partially developed
2	Ontario Festival (2003)	37.6-acre commercial and residential development	Residential portion built out
3	Meredith Center (1981)	258-acre multiple-use commercial, office, hotel, and residential development	Partially developed
4	Guasti Plaza Specific Plan (2011)	78.4-acre historic preservation of Guasti Winery and office, hotel, and commercial development with a possible residential component. Residential uses are allowed at a density of 25-60 du/ac on 7.76 acres within the plan.	Approved
5	Mountain Village	Pedestrian-oriented commercial/retail/residential district; entertainment destination with "round-the-clock" district	Built out
6	Borba Village	32-acre residential, neighborhood commercial, and open space linked by a pedestrian corridor	Built out
7	Creekside (1994)	410-acre planned residential community with 9 activity centers, with lake and school site	Built out
8	Wagner Specific Plan (1992)	Now converted from commercial to residential specific plan proposing 275 units on 45 acres of land, 11 of which are residential	Built out
No.	Ontario Ranch	Description	Development Status
9	Countryside (2006)	178-acre master-planned residential with 819 single- family homes	Partially developed
10	Edenglen (2005)	160-acre master-planned community with 277 single-family and 307 multiple-family residences	Residential portion built out
11	Rich-Haven	510.6-acre traditional neighborhood design, residential, and regional commercial/mixed-use with 2,732 single-family and 1,524 multifamily units	Partially developed
12	Esperanza	223-acre residential planned community with 914 single-family and 496 multiple-family homes	Partially developed
13	Sub-Area 29	532-acre planned residential, commercial, and recreational uses with 2,418 single-family units	Partially developed
14	The Avenue	560-acre specific plan with 2,875 single-family and multiple-family residences with parks	Partially developed
15	West Haven Specific Plan	200-acre residential development with 753 single-family residences	Partially developed
16	Parkside	250-acre planned community with 437 single-family and 1,510 multiple-family homes and 50 acres of parks	Partially developed
17	Armstrong Ranch	176-acre specific plan with 891 single-family units	Approved
18	Grand Park	106-acre specific plan with 1,327 housing units, including 587 units of high-density housing	Partially developed

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Environmental Factors

Water and wastewater services are provided by the Ontario Municipal Utilities Company (OMUC), which is a department of the City. Environmental and infrastructure issues affect the amount, location, and timing of new residential development. New housing opportunities create challenges regarding public infrastructure extensions and expansions, and encroachment into agricultural land. In addition, the availability of adequate water, public infrastructure such as wells and wastewater treatment facilities, and other public services and facilities can impact the feasibility of new residential development. The City will examine potential alternative infrastructure funding sources to evaluate opportunities to provide fee reductions or offer fee waivers for affordable housing (Program 34).

A lack of water and wastewater capacity or infrastructure can present a barrier to the development of affordable housing in many jurisdictions. The status of current infrastructure capabilities and capacities by planning area are presented below.

- Campus Site. The site has no development, infrastructure or environmental constraints, and is ready to be developed immediately.
- Downtown. The City installed sewer infrastructure along East Holt Boulevard to accommodate development projected under the Policy Plan. The capacity is adequate to serve the projected new residential and commercial development in the Downtown and Emporia District. There is adequate water for the sites and no known environmental constraints. Roadway improvements have also been completed along Holt Boulevard. While storm drain capacity upgrades have been recommended, this is not anticipated to be a constraint to development. In the southwest corner of the planning area, 12.3 acres are designated as a special flood hazard area, which will require additional assessment prior to development. No properties within this special flood hazard area are included in the City's housing sites.
- East Holt. As East Holt Boulevard serves the East Holt commercial area as well as the Downtown areas and Emporia Districts. The City recently installed sewer infrastructure along East Holt Boulevard to accommodate development projected under the Policy Plan. Sewer capacity is now adequate to accommodate projected new residential and commercial development in the East Holt areas, though infrastructure improvements are recommended. There are no known water or stormwater constraints that would preclude or delay the



development of housing in any of these three areas, though infrastructure improvements are recommended.

- Holt. While storm drain capacity upgrades have been recommended, this is not anticipated to be a constraint to development. Approximately 4.7 acres of this area are designated as a special flood hazard area, which will require additional assessment prior to development. No properties within this special flood hazard area are included in the City's housing sites.
- Mountain Corridor. The corridor is ripe for conversion due to its underutilized nature, new Policy Plan land use designation that doubles or triples the allowable density, and the construction of capital improvement projects along the corridor that address water and sewer needs.
- Euclid Corridor. Water and sewer infrastructure is currently in place to support residential development. However, the properties on Fern Avenue, north of Philadelphia Street, and on Philadelphia Street, between Fern Avenue and Euclid Avenue, are on septic systems and will require sewer facilities. In these cases, developers will be required to make on-site improvements. Given the project size possible on these sites, the cost of these types of improvements is not anticipated to preclude or delay the construction of housing.
- Grove Corridor. The sites are predominantly vacant and have no infrastructure or environmental constraints that would preclude or delay development. Adequate water and sewer capacity is available.
- Mission Corridor. Currently, there are no known constraints that
 would preclude or delay development of these sites. Water
 infrastructure and sewer infrastructure is in place and adequate to
 accommodate the development. The sites do not contain any
 environmental hazards, as they are predominantly residential and
 commercial in nature.
- Ontario Airport Metro Center. Master plans for infrastructure will need to be prepared as will appropriate environmental clearance for these projects. There is adequate sewer and water capacity for each of these sites proposed to be developed during the planning period. Limited areas are within a special flood hazard area, but this only represents 5.3 acres of the planning area, and no properties within the special flood hazard area are included in the City's housing sites. Stormwater improvements are anticipated to alleviate local flooding in this area.

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 Ontario Ranch. The City entered into an agreement with a consortium of 14 developers to fund \$430 million in infrastructure (streets, drainage, water, parks, etc.) that will serve the eastern portion of Ontario Ranch. While areas adjacent to Cucamonga Creek are considered to be in a flood zone, this is not anticipated to limit proposed development.

Housing Opportunities

California law requires that all local governments adopt and administer programs to facilitate and encourage the provision of a range of types and prices of housing for all income levels. The City's zoning implements the intent of the Policy Plan by specifying the type of housing allowed, the location of residential uses, the permitted density, and the permitting processes involved for different types of housing.

Table 4-7 summarizes the types of conventional housing allowed in each zoning district and whether the use is permitted by right or conditionally permitted. Where no notation is provided, the use is prohibited. Following is an explanation of the housing types and their legal or planning context. Table 4-8 addresses how special-needs housing types are allowed.





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Housing Element Technical Report



Table 4-7 Conventional Housing Permitted by Zoning District

	R		tial Zon tricts	ing		Сог	nmercia	al Zonii	ng Dist	ricts			Mix	red-Use	Zonin	g Distri	icts		Indus	strial 2	Zonin	g Dis	tricts		alized U Zoning	se and Districts	
			25											MU-1													
Residential Uses	AR-2 & RE-2	RE-4 & LDR-5	MDR-11, MDR-18, MDR-25	HDR-45	SO	CN	22	CR	SOO	70	Ю	LUA-1	LUA-2N	LUA-2S	LUA-3	LUA-4	MU-2	MU-11	ВР	d	1	91	王	AG	CIV	МНР	Additional Regulations (Development Code References)
Accessory Structures, including guesthouses	Р	Р	Р	Р								Р	Р		Р									Р			Section 5.03.010 (B))
Accessory Dwelling Units	Р	Р	Р	Р								Р	Р		Р		Р	Р						Р			Section 5.03.010 (A)
Caretaker Quarters						С	С	С	С		С				1				С		С	С	С	Р			Excludes Caretaker Quarters established in conjunction with Self-Storage Facilities
Residential Mixed-Use Developments (Development projects containing a mix or commercial and residential on the same site)						С	С					Р	Р		Р		Р	Р									Section 5.03.285
Work/Live Units												С	С	С			С										Section 5.03.425
Mobile Home Parks															- 1											Р	Section 5.03.295
Mobile Home or Manufactured Home ¹	Р	Р	Р	Р								Р	Р		Р		Р							Р			
Multiple-Family Dwellings			Р	Р								Р	Р		Р	Р	Р	Р									Table 6.01-3
Single-Family Dwellings (Traditional Residential Subdivisions)	Р	Р	Р	Р								Р			Р									Р			Section 5.03.365; Table 6.01-1.
Single-Family Dwellings (Small Lot Traditional Residential Subdivisions)		Р	Р	Р								Р			Р		Р										Section 5.03.365; Table 6.01-2A
Single-Family Dwellings (Small Lot Alley-Loaded Residential Subdivisions)			Р	Р								Р			Р		Р										Section 5.03.365; Table 6.01-2B
Single-Family Dwellings (Cluster Residential Subdivisions)			Р	Р								Р			Р		Р										Section 5.03.365; Table 6.01-2C
Small-Lot Infill Subdivisions			Р	Р								Р	Р		Р		Р										Section 6.01.010.F

P=Permitted Use C=Conditionally Permitted Use -- = Prohibited

Source: City of Ontario, 2021.

Residential Zoning Districts:

AR-2 = Residential-Agricultural 0-2; RE-2 = Rural Estate 0-2; RE-4 = Residential Estate 0-2; RE-4 = Residential 18.1-25; HDR-45 = Low-Medium-Density Residential 25.1-45 Commercial Zoning Districts:

CS = Corner Store; CN = Commercial Neighborhood; CC = Community Commercial; CR = Regional Commercial; CCS = Convention Center Support Commercial; OL = Low Intensity Office; OH = High Intensity Office Mixed-Use Zoning Districts:

MU-1= Downtown Mixed-Use; LUA-1 = Euclid Avenue Entertainment; LUA-2N = Arts; LUA-3 = Holt Boulevard; LUA-4 = Civic Center; MU-2 = East Holt Mixed-Use, MU-11 = Euclid/Francis Mixed-Use Industrial Zoning Districts:

BP = Business Park; IL = Light Industrial; IG = General Industrial; IH = Heavy Industrial

Overlays and Specialized Use Zoning Districts

AG = Agriculture Overlay; CIV = Civic; MHP = Mobile Home Park

¹Mobile Homes/Manufactured Homes are treated as single-family homes and permitted by right anywhere single-family homes are permitted.





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Single- and Multiple-Family Housing

The City permits single-family and multiple-family housing types as by-right uses in their respective zoning districts. The City allows a multitude of single-family housing land uses to encourage a variety of design and allow for small-lot and/or infill development. Traditional single-family housing is permitted by right in the most zoning districts (AR-2, RE-2, RE-4, LDR-5, MDR-11, MDR-18, MDR-25, HDR-45, LUA-1, LUA-3, and AG), followed by single-family dwellings on small lots (RE-4, LDR-5, MDR-11, MDR-18, MDR-25, HDR-45, LUA-1, LUA-3, and MU-2). Single-family dwellings, alley-loaded, and single-family dwellings, clustered, are allowed by right in the same zoning districts: MDR-11, MDR-18, MDR-25, HDR-45, LUA-1, LUA-3, and MU-2. Small-lot infill subdivisions are allowed by right in the MDR-11, MDR-18, MDR-25, HDR-45, LUA-1, LUA-2N, LUA-3, and MU-2 zoning districts. Flexibility in residential subdivision design can also be achieved through a Planned Unit Development (PUD) Overlay District that conditionally permits a range of housing types in every residential zone. The PUD is a tool to encourage and facilitate innovative design, variety, and flexibility in housing products that would otherwise not be allowed in other zoning districts. Under a PUD, the City may permit attached and detached single-family residences, townhomes, and zero lot line and any other type of housing product permitted by the regulations of the underlying zone. Multiple-family housing is permitted by right in medium- and highdensity residential zones (MDR-11, MDR-18, MDR-25, and HDR-45) and mixed-use zones (LUA-1, LUA-2, LUA-3, LUA-4, MU-2, and MU-11).

Mixed-Use

Residential mixed-use projects are projects containing single-family and/or multiple-family dwellings constructed in conjunction with a variety of complementary commercial land uses—such as office, retail, public, or entertainment—in the same building or site as an integrated development that has both significant functional interrelationships and a coherent physical design. Mixed-use can be vertically integrated or horizontally placed (side by side). Mixed-use developments are permitted by right in the LUA-1, LUA-2N, LUA-3, LUA-4, MU-2, and MU-11 zoning districts and conditionally permitted in the CN and CC zoning districts. The 2050 Policy Plan established 12 Mixed-Use land use designations to support the development vision in important corridors within the city, creating focal points for community activity and identity and to integrate transit. While not all 12 mixed-use land use designations have a corresponding zoning district, the Downtown Mixed-Use zoning districts (MU-1 and subdistricts LUA-1, LUA-2N, LUA-2S, LUA-3, and LUA-4) support the Downtown Mixed-Use designation, the MU-2 supports the East Holt Mixed-Use designation, and MU-11 supports the



Euclid/Francis Mixed-Use designation, integrating more opportunities for housing in these neighborhoods.

Mobile Homes and Manufactured Housing

Mobile homes or manufactured housing offer an affordable housing option to many low- and moderate-income households. California Government Code Section 65852.3 requires cities to treat certified mobile homes (manufactured homes) on a permanent foundation for permanent occupancy the same as single-family dwellings. They may not be excluded from lots zoned for single-family dwellings and are subject to the same rules as site-built homes, except for certain architectural requirements. Further, a city may not require an administrative permit, planning or development process, or requirement that is not imposed on a conventional single-family dwelling.

The City allows, by right, factory-built housing in all zones permitting single-family dwellings. Factory-built housing on residential lots not constructed within a mobile home park must conform to the same development standards applied to site-built homes with regards to setbacks, parking, placement, and other standards, but have additional specific architectural requirements related to exterior finish and roofing material to blend factory-built housing with site-built housing. Mobile home units may also be used as rental accessory dwelling units (ADUs) subject to certain construction standards (e.g., National Mobile Home Construction and Safety Standards of 1974), and architectural requirements. These standards do not impose a constraint on the placement of mobile homes or unreasonable cost burdens on mobile homeowners since new factory-built homes normally comply with the City's requirements with little or no modification.

Mobile homes are allowed by right in the Mobile Home Park (MHP) zone constructed as mobile home parks, permitting 5.1 to 8.0 du/ac. According to the 2021 Department of Finance numbers, an estimated 2,175 mobile homes are located in the city.

Accessory Dwelling Units

ADUs (second units) are defined in the Development Code (see Section 5.03.010) as an ancillary dwelling unit providing complete independent living facilities for one household located on a parcel with the primary single-unit dwelling that houses a separate household. An ADU may be within the same structure as the primary unit, in an attached structure, or in a separate structure on the same parcel. State legislation requires jurisdictions to allow ADUs that meet certain standards by right anywhere that single-family or multifamily uses are allowed. Junior accessory dwelling units (JADUs), that is, smaller units located entirely

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within an existing single-family primary unit, are also allowed under state law.

Ontario permits ADUs by right in all zoning districts where single-family and multiple-family residential is permitted, including mixed-use zoning districts, subject to the provisions of the Development Code Section 5.03.010. Ontario allows detached and attached ADUs up to 800 square feet for a studio or one bedroom and 1,000 square feet for a unit with two or more bedrooms. ADUs are permitted through an approved ministerial ADU permit, although some conversions or detached ADUs only require an approved building permit.

The City adopted updates to the ADU ordinance as a part of the development Code update in 2020 to comply with State law. During the planning period, the City will implement the ADU ordinance and update it to comply with any new State requirements (Program 20). Additionally, the City will explore initiatives to promote ADU development as an affordable housing option, including considering establishing a loan program for homeowners for ADUs, market ADU guidance materials, and create frequently-asked-questions webpage for ADUs on the City's website (Program 27).

Special-Needs Housing

State law requires that housing elements analyze the needs of certain groups of households that have special housing needs. Furthermore, state and federal fair housing laws are designed to ensure that persons and families with special housing needs (e.g., disabled people [including those with developmental disabilities], homeless people, etc.) have adequate access to a full range of housing opportunities. An important component of meeting this challenge is to ensure that adequate housing opportunities are permitted in the community.

Table 4-8 summarizes the types of special-needs housing allowed in each zoning district in Ontario and whether the type of housing is permitted by right or conditionally permitted. Where a land use is not expressly permitted, the use is considered prohibited by the Municipal Code.



Table 4-8 Special-Needs Housing Permitted by Zoning District

Special-Needs Uses			lentia Distr		Pro			l and ng Di			cial	М	lixed-	-Use	Zoni	ng Di	istric	ts	Inc		trial i istric		ng	and	ialized d Over ng Dis	rlay	
			MDR-25											MU-1													Additional Regulations
	AR-2 & RE-2	RE-4 & LDR-5	MDR-11, MDR-18, MD	HDR-45	SO	CN	22	CR	SOO	OL	НО	LUA-1	LUA-2N	LUA-2S	LUA-3	LUA-4	MU-2	MU-11	ВР	dI	=	91	Ξ	AG	CIV	MHP	(Development Code References)
Senior Citizen Housing			Р	Р			С					Р			Р	Р											Section 5.03.360
Continuing Care Retirement Communities 6 or fewer clients	*P	*P	*P	*P							-1	*P	*P	*P	*P		*P	*P						*P		1	
Continuing Care Retirement Communities More than 6 clients			С	С			С											С								С	
Assisted Living Facilities for the Elderly 6 or fewer clients	*P	*P	*P	*P								*P	*P	*P	*P		*P	*P						*P			Section 5.03.110
Assisted Living Facilities for the Elderly More than 6 clients			С	С			С				1				- 1			С		- 1				1		С	Section 5.03.105
Nursing Care Facilities							С	С			С				1					-				-		С	
Residential Intellectual and Development Disability, Mental Health, and Substance Abuse Facilities 6 or fewer clients	*P	*P	*P	*P							-	*P	*P	*P	*P		*P	*P		1				*P		1	
Residential Intellectual and Development Disability, Mental Health, and Substance Abuse Facilities More than 6 clients			С	С			С							-				С								С	
Other Residential Care Facilities 6 or fewer clients	*P	*P	*P	*P							-	*P	*P	*P	*P		*P	*P						*P		1	Section 5.03.345

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City of Ontario Policy Plan

Housing Element Technical Report



Special-Needs Uses	_		lentia Distr		Pro			l and ng Di			cial	М	lixed-	-Use	Zoni	ng Di	istric	ts	Inc		rial i istric		ny	and	ialized d Over ng Dis	rlay	
	AR-2 & RE-2	RE-4 & LDR-5	MDR-11, MDR-18, MDR-25	HDR-45	SO	CN	22	CR	SOO	70	НО	LUA-1	LUA-2N	LUA-2S GM	LUA-3	LUA-4	MU-2	MU-11	ВР	Ы	1	91	포	AG	CIV	МНР	Additional Regulations (Development Code References)
Other Residential Care Facilities More than 6 clients			С	С			С											С								С	
Boarding, Lodging, and Rooming Houses	Α	Α	Α	С																				Α		А	Section 5.03.080
Single-Room Occupancy (SRO) Facilities				С			С		С																		Section 5.03.360
Emergency Shelters			С				С								С						Р	С	С		С	С	Section 5.03.405
Supportive Housing	Р	Р	Р				С					С			С						Р	С			С		Section 5.03.405
Transitional Housing	Р	Р	Р	Р			С													-	Р	С	С		С	Р	Section 5.03.405
Transitional Living Centers															С						С	С	С				Section 5.03.405
Low-Barrier Navigation Centers															1					- 1						1	
Employee (Farmworker) Housing																								Р			Section 5.03.177

Source: City of Ontario, 2021.

P=Permitted Use C=Conditionally Permitted Use A=Administratively Permitted Use -- = Prohibited

Residential Zoning Districts:

AR-2 = Residential-Agricultural 0-2; RE-2 = Rural Estate 0-2; RE-4 = Residential Estate 2-4; LDR-5 = Low-Density Residential 2-5; MDR-11 = Low-Medium-Density Residential 5-11; MDR-11 = Low-Medium-Density Residential 11-18; MDR-25 = Low-Medium-Density Residential 18-25; HDR-45 = High-Density Residential 25-45

Commercial Zoning Districts:

CS = Corner Store; CN = Commercial; OL = Low Intensity Office; OH = High Intensity Office Mixed-Use Zoning Districts:

MU-1= Downtown Mixed-Use; LUA-1 = Euclid Avenue Entertainment; LUA-2N = Arts; LUA-3 = Holt Boulevard; LUA-4 = Civic Center; MU-2 = East Holt Mixed-Use, MU-11 = Euclid/Francis Mixed-Use Industrial Zoning Districts:

BP = Business Park; IL = Light Industrial; IG = General Industrial; IH = Heavy Industrial

Overlays and Specialized Use Zoning Districts

AG = Agriculture Overlay; CIV = Civic; MHP = Mobile Home Park

^{*}Allowed only in conjunction with an existing single-family residence.



Senior Housing

The Development Code contains regulations that encourage the production or location of a continuum of housing suitable for seniors. The intent of these ordinances is to ensure that seniors have the ability to remain in Ontario throughout their lives regardless of medical condition.

The major types of senior housing facilities are summarized below.

- Senior Citizen Housing Development. Senior citizen housing developments are designed to meet the physical and social needs of seniors consistent with the California Fair Employment and Housing Act. The City permits by right senior citizen housing developments in the MDR-11, MDR-18, MDR-25, HDR-45, LUA-1, LUA-3, and LUA-4 zones and conditionally permits senior housing in the CC zone and offers significant incentives for new senior housing. Applications for senior citizen housing developments are reviewed based on their proximity to frequently-serviced public transit, parks and open space, medical facilities, libraries, and pharmacies. Additionally, senior citizen housing developments must provide high-speed internet, a service coordinator to assist with activities of daily living, and an exercise facility.
- Nursing Care Facilities (Convalescent Homes [Hospital], Rest Home, or Rehabilitation Facility). Nursing care facilities are lodging and care facilities for those who are convalescing, invalids, or aged persons, in which surgery is not performed and primary treatment given in hospitals is not provided. These uses are permitted conditionally in the CC, CR, OH, and MHP zones.
- Residential Care Facilities. As discussed in later sections, the City also allows state-licensed community care facilities and residential care facilities for the elderly, further categorized in the Development Code Table 5.02-1 (Land Use Matrix) as Continuing Care Retirement Communities and Assisted Living Facilities for the Elderly. Community Care Facilities, including those that serve seniors, comply with the Community Care Facilities Act and are discussed below within the Community Care Facilities section.

The City has excellent examples of facilities offering CoC options for seniors. Inland Christian Home, a nonprofit provider of health and retirement care services for the elderly, has four facilities that provide accommodations for seniors. These include independent living, memory care, assisted living, and skilled nursing facilities.

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Community Care Facilities

The Welfare and Institutions Code (Lanterman-Petris Act) and the Health and Safety Code (Community Care Facilities Act) declare that it is the policy of the state that people with a wide variety of disabilities are entitled to live in normal residential settings. The Health and Safety Code (California Residential Care Facilities for the Elderly Act) also extends this protection to elderly persons. State law sets forth regulations and guidelines for care facilities that preempt or limit many local regulations.

Facilities covered under these acts include:

- Residential care facility
- Adult day program
- Therapeutic day services facility
- Foster family agency or home
- Small family home
- Social rehabilitation facility
- Community treatment facility
- Full-service adoption agency
- Noncustodial adoption agency
- Transitional shelter care facility
- Transitional housing placement facility
- Residential care facility for the elderly (Continuing Care Retirement Communities and Assisted Living Facilities for the Elderly)
- · Alcoholism or drug abuse recovery or treatment facility (Residential Mental Health and Substance Abuse Facilities)
- Congregate care facility

The Health and Safety Code (Section 1500 et seq.) requires that licensed community care facilities serving six or fewer persons be (1) treated the same as a residential use; (2) allowed by right in all residential zones; and (3) treated the same with respect to regulations, fees, taxes, and permit processes as other residential uses in the same zone. The Health and Safety Code extends this protection to residential care facilities for the elderly (Section 1569.84 et seq.), to alcoholism or drug abuse recovery or



treatment facilities (Section 11834.22 et seq.), and to congregate care facilities (Section 1267.16 et seq.), all of which serve no more than six clients.

As required by Health and Safety Code Section 1569.84 et seq., community care facilities serving six or fewer people are allowed by right in the residential zoning districts and the LUA-1, LUA-2N, LUA-2S, LUA-3, MU-2, MU-11, and the AG zoning districts. Licensed community care facilities are also subject to the same development standards, fees, taxes, and permitting processes as other similar residential uses in the same zone. Large facilities (seven or more persons) are conditionally permitted in the MDR-11, MDR-18, MDR-25, HDR-45, MU-11, and MHP zoning districts. To expand the use of care facilities for seven or more persons, the City has included Program 20 to explore amending provisions in the Municipal Code to allow state-licensed residential care facilities for seven or more persons only subject to those restrictions that apply to residential uses in the same zone or otherwise amending the Municipal Code to make it easier to locate a state-licensed residential care facility for seven or more persons in the city. Residential care facilities would still be subject to state licensing.

Boarding, Lodging, or Rooming House

In contrast to community care facilities licensed by the State of California, boarding, lodging, and rooming houses are non-licensed facilities. This category refers to a residence or dwelling other than a hotel wherein one or more rooms with or without individual or group cooking facilities are rented, leased, or subleased to individuals under separate agreements, either written or oral.

Unlike licensed community care facilities, cities can regulate such boarding, lodging, and rooming houses that are not used as transitional or supportive housing. "A city may prohibit, limit or regulate the operation of a boarding house or rooming house business in a single-family home located in a low-density residential (R-1) zone, where boarding house is defined as a residence or dwelling, other than a hotel, wherein three or more rooms, with or without individual or group cooking facilities are: rented to individuals under separate rental agreements or lease in order to preserve the residential character of the neighborhood" (86 Ops. Cal. Atty. Gen. 30 (2003)). The City permits boarding, lodging, or rooming houses in the AR-2, RE-2, RE-4, LDR-5, MDR-11, MDR-18, MDR-25, AG, and MHP zoning districts with an approved administrative use permit and the HDR-45 zoning district with a conditional use permit.

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Boarding, lodging, or rooming houses have, at times, been a source of concern that they be operated in a manner compatible with residential neighborhoods. To that end, the City Municipal Code requires that such homes cannot be occupied by more than one federal, state, or youth authority parolee. Moreover, all such homes shall require boarders to sign a "Crime-Free Lease Addendum" to their rental or lease agreement. In Program 20, the City commits to reviewing and removing this restriction to prevent discrimination based on criminal history (see California Code of Regulations, Title 2, Section 12264-12271). The Municipal Code limits providing accommodations to a maximum occupancy of six individuals, excluding a resident owner, agent, or manager. The operator may seek relief from the strict application of this provision by submitting a request for reasonable accommodation pursuant to Section 4.02.035 (Fair Housing and Reasonable Accommodation).

Single-Room Occupancy

The City permits single-room occupancy (SRO) uses within the community. The Development Code defines SRO uses as a cluster of five or more dwelling units on one property for weekly or longer tenancy and providing sleeping and living facilities for one or two persons within the unit, in which sanitary facilities are also normally provided and cooking facilities may be provided within each unit or shared by multiple units. SROs are conditionally permitted in three zones (HDR-45, CC, and CCS).

To secure a conditional use permit, a comprehensive management plan must be submitted with the application. The operator must submit a plan that includes the company or agency responsible for resident selection, day-to-day maintenance of the facility, proposed security arrangements, and background information and references about the proposed management company or agency. Moreover, SROs may not be located within 500 feet of any school for children, church, daycare facility, or other existing SRO facility. SRO units are an important source of affordable housing for extremely low-income households. In Programs 20 and 32, the City commits to addressing the needs of extremely lowhouseholds, including expanding affordable opportunities. As a part of this effort, the City will explore amending the Development Code to reduce location constraints for SRO facilities and permitting SROs through a ministerial process rather than requiring a conditional use permit or administrative use permit.

Housing for Homeless People

In recognition of the homeless population in Ontario, and with the desire to act affirmatively to address the issue, the City entered into an agreement with Mercy House to implement a CoC. Under the CoC, Mercy House operates a homeless intake center, transitional housing, and



permanent affordable housing, including housing with supportive services. Regulations were subsequently adopted to facilitate the completion and implementation of the CoC.

Emergency Shelters

The California Health and Safety Code (Section 50801) defines an emergency shelter as "housing with minimal supportive services for homeless persons that is limited to occupancy of six months or less by a homeless person. No individual or households may be denied emergency shelter because of an inability to pay."

California Government Code Section 65583(a)(4) states that every jurisdiction must identify a zone or zones where emergency shelters are allowed as a permitted use without a conditional use or other discretionary permit. The identified zone or zones must include sufficient capacity to accommodate the need for an emergency shelter as identified in the housing element, and each jurisdiction must identify a zone or zones to accommodate at least one year-round shelter. Adequate sites can include sites with existing buildings that can be converted to emergency shelters to accommodate the need for emergency shelters.

The Development Code permits an emergency shelter by right in the IL zone and conditionally permits an emergency shelter in the MDR-11, MDR-18, MDR-25, CC, LUA-3, IG, and IH zones. Emergency shelters are also permitted by right in the Emergency Shelter Overlay, subject to the base zone standards and consistent with California Government Code Section 65583(4)(A).

The overlay zone is an approximately 500-foot-deep area on the north side of Mission Boulevard and bounded by Benson Avenue on the west and Magnolia Avenue on the east. The overlay area is suitable for emergency shelters since it is near two transit routes (Mission Boulevard and Mountain Avenue) and services, such as a grocery store. The overlay zone comprises 36 acres of land, of which, 0.4 acres are vacant (additionally, the area has 38 parcels, 4 of which are vacant). Many of the parcels in the proposed overlay district are underutilized, providing many opportunities for developing new facilities or reusing or converting underutilized buildings into one or more shelters. The overlay zone contains five properties that have transient lodging that might be suitable for conversion to an emergency shelter, should one be warranted in the community. Figure 4-1 provides a map for the location of the Emergency Shelter Overlay.

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Figure 4-1 Emergency Shelter Overlay





The City has objective development and management standards that are designed to encourage and facilitate the development of emergency shelters:

- The maximum length of stay for an Emergency Shelter client shall be six months.
- On-site management shall be provided during the hours that the Emergency Shelter is in operation.
- · On-site security shall be provided during the hours that the Emergency Shelter is in operation.
- No more than 20 client/tenant beds shall be allowed within any Emergency Shelter.
- An intake waiting area equal to a minimum of 10 square feet for each client/tenant bed shall be provided.
- The exterior of the intake waiting areas shall be screened from public view by a six-foot-high decorative masonry block wall and appropriate landscaping.
- A storage area for use by clients/tenants shall be provided at a rate of seven square feet for each client/tenant bed.
- A storage area is not required to be provided adjacent to the respective client/tenant bed.
- An emergency shelter shall provide lavatory, toilet, and shower facilities adequate for the number of clients/tenants served; however, a minimum of one such facility shall be provided for each 15 client/tenant beds.

Program 20 commits the City to reviewing these standards and revising as needed to be consistent with California Government Code Section 65583(a)(4), including establishing sufficient parking requirements to accommodate all staff working in the emergency shelter.

Transitional and Supportive Housing

Consistent with Health and Safety Code Section 50675.14(b)(2), the City defines supportive housing as "housing with no limit on length of stay, that is occupied by the target population, and that is linked to onsite or offsite services that assist the supportive housing resident in retaining the housing, improving his or her health status, and maximizing his or her ability to live and, when possible, work in the community." Target population refers to persons, including persons with disabilities, youth, and families experiencing homelessness. Transitional housing is intended

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as a middle point between emergency shelters and permanent housing, providing shelter up to two years, in an environment of security and support, which is designed to help residents progress toward self-sufficiency.

Transitional housing and supportive housing must be permitted as a residential use and only subject to those restrictions that apply to other residential dwellings of the same type in the same zone (Government Code Section 65583(a)(5)). The City permits transitional housing by right in residential zoning districts and IL and MHP zones, and conditionally permits transitional housing in CC, IG, IH, and CIV zones. Supportive housing is permitted by right in all residential zones, except HDR-45, and is permitted by right in IL zones. Supportive housing is conditionally permitted in CC, LUA-1, LUA-3, IG, and CIV zones. Currently, transitional housing and supportive housing are not permitted in all mixed-use zoning districts (LUA-2N, LUA-2S, MU-2, and MU-11) that permit residential uses, and supportive housing is not permitted in the HDR-45 zoning district. To comply with Government Code Section 65583(a)(5), the City has included Program 20 to amend the Development Code to permit transitional housing and supportive housing in all zoning districts that permit residential uses, including mixed-use and nonresidential zoning districts, subject only to the same regulations as similar uses in the same zone.

Low-Barrier Navigation Centers

California Government Code Section 65662 requires that the development of Low-Barrier Navigation Centers be developed as a use by right in zones where mixed-uses are allowed or in nonresidential zones that permit multifamily housing. For a navigation center to be considered "low barrier," its operation should incorporate best practices to reduce barriers to entry, which may include, but are not limited to, the following:

- Permitting the presence of partners if it is not a population-specific site, such as for survivors of domestic violence or sexual assault, women, or youth
- Pets
- Ability to store possessions
- Providing privacy, such as private rooms or partitions around beds in a dormitory setting or in larger rooms with multiple beds.

Currently, the City does not recognize low-barrier navigation centers as a permitted use. Program 20 has been included to comply with Government Code Sections 65660-65662.



Farmworker Housing

The City has established an Agricultural Overlay District (AG overlay), which covers the entire Ontario Ranch area. The intent of the Agricultural Overlay District is to allow for the continuation of agricultural uses on an interim basis until such time as a specific plan is proposed for urbanized uses. The Agricultural Overlay District is designed to limit land use activity to uses compatible with and supportive of agricultural uses.

The Health and Safety Code (Section 17021.6) declares that each city must permit and encourage the development and use of sufficient numbers and types of employee housing facilities commensurate with local needs. employee 17021.5 requires that housing providing accommodations for six or fewer employees shall be deemed a singlefamily structure with a residential land use designation, treated as a residential use of property, and that the use not be subject to any regulations or fees not otherwise required of a single-family residence within the same zone. For facilities with 7 to 12 units or spaces, the use shall be considered an agricultural use, subject only to regulations applied to any agricultural use in the same zone, and the permitted occupancy may include employees who do not work on the property where the employee housing is located. Section 17021.8 requires a streamlined, ministerial application process for qualifying agricultural employee housing on land designated as Agricultural (AG) in the City's Policy Plan. While the City no longer has any areas designated as AG, it has included Program 20 to review Development Code Section 5.03.177 to fully comply with the requirements of the Employment Housing Act, including Sections 17021.5, 17021.6, and 17021.8.

The Municipal Code allows for the following uses to provide housing for farmworkers:

• Employee (Farmworker) Housing. Employee housing for farmworkers is only permitted in the AG overlay, where it is allowed by right. Consistent with Section 17021.5 of the Health and Safety Code, the City deems farmworker dwelling units providing accommodations for six or fewer employees, or for one employee and their respective household, a single-family structure. Farmworker dwelling units for six or fewer occupants are permitted by right in the AG overlay and all zoning districts that allow single-family dwellings, subject to the same development standards for single-family dwellings. A farmworker housing complex consisting of up to 36 beds in a group quarter, or 12 units or spaces designed for use by a single family or household, is deemed an agricultural use. The City does not require farmworker housing to be on the same site as the qualifying agricultural operation where the farmworkers are

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employed, but does require a minimum lot size of 10 acres for farmworker housing.

- Caretaker's Quarters. Caretaker's quarters are designed to accommodate employees living on-site to provide security and surveillance, including agricultural operations in the AG overlay. The unit size is restricted to no more than 600 square feet and is limited to one bedroom. These uses are permitted by right in the AG overaly and conditionally permitted in the CN, CC, CR, CCS, OH, BP, IL, IG, and IH zones.
- Accessory Residential Structures (Guest House). Guest houses are permitted by right in all residential zones, LUA-1, LUA-2N, LUA-3, and the AG overlay. Guest houses cannot exceed 650 square feet, only one is permitted per lot containing a single-family dwelling, and quarters are reserved for temporary use (period not exceeding 90 days) of the residents of the property, their nonpaying guests, family, or persons employed on the residence. Guest houses shall not be rented.

Agricultural employment is relatively minor in the community, and the type of agricultural work is year-round and not migrant labor. Ontario's primary agricultural industry is dairy, which is highly automated and generally family-owned and operated. Some dairy farms employ farmworkers to assist with the daily operations, but the use of technology, automation, and family labor has minimized the need for additional farmworkers. Dairy work is relatively constant, and employees, who are often family members, live on-site. Today, many dairy farms have two or more dwellings to accommodate the owner/operator and several key employees.

Housing for People with Disabilities

California Government Code Section 65583 requires that the housing element analyze potential and actual constraints on the development, maintenance, and improvement of housing for persons with disabilities and demonstrate local efforts to remove governmental constraints that hinder the locality from meeting the need for housing for persons with disabilities (California Government Code, Section 65583(a)(4)). As part of the required constraints program, the element must include programs that remove constraints or provide reasonable accommodations for housing designed for persons with disabilities (California Government Code, Section 65583(c)(3)). This section addresses these requirements.



Allowance of Land Uses

State law requires group homes serving six or fewer persons to be (1) treated the same as any residential use; (2) allowed by right in all residential zones; and (3) subject to the same standards, fees, taxes, and permitting procedures as those imposed on the same type of housing in the same zone. These laws ensure that housing opportunities are available for people with disabilities and that such uses are not discriminated against. The City currently permits such uses by right in all residential zones. To expand the use of care facilities for any number of occupants, the City has included Program 20 to explore amending provisions in the Municipal Code to allow state-licensed residential care facilities for seven or more persons only subject to those restrictions that apply to residential uses in the same zone. Residential care facilities would still be subject to state licensing.

State law requires local governments to identify adequate sites, development standards, and a permitting process to facilitate and encourage the development of emergency shelters and transitional housing.

New Construction/Building Codes

Cities that use federal funds must, in all new construction and substantial rehabilitation projects, ensure that at least 5 percent of the units are accessible to persons with mobility impairments and another 2 percent are accessible to persons with hearing or visual impairments. Multiple-family housing must be built so that (1) the public and common-use portions of such units are readily accessible and usable by persons with disabilities, (2) doors allowing passage into and within such units can accommodate wheelchairs, and (3) all units contain adaptive design features.

The United States Department of Housing and Urban Development (HUD) also recommends, but does not require, that all design, construction, and alterations incorporate, wherever practical, the concept of visitability. This recommendation is in addition to requirements of Section 504 and the Fair Housing Act. Recommended construction practices include wide openings for bathrooms and interior doorways and at least one accessible means of egress/ingress per unit. The City enforces federal and state accessibility laws through the building plan check and permit process.

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Rehabilitation of Units

In an older community with many homes built prior to the development of modern accessibility standards for people with disabilities, allowing the retrofit of homes for people with disabilities is an important issue. Federal law requires that substantial rehabilitation projects using federal funds set aside units for disabled people, and HUD encourages visitability standards. Providing options for rehabilitating housing to modern accessibility standards allows people to live in an independent housing arrangement.

To accommodate the needs of persons with disabilities, the City allows property owners to install features that accommodate a disability (e.g., ramp to the front door) without the need to apply for a variance. The City allows retrofit of a residential structure upon submittal of plans and the payment of a normal building plan check and permit issuance fee.

Definition of Family

Fair housing laws prohibit restrictive definitions of family that discriminate against households based on the number, personal characteristics, or the relationship of occupants to one another.

The City's Development Code defines a "family" as a group of individuals not necessarily related by blood, marriage, adoption, or guardianship living together in a dwelling unit as a single housekeeping unit under a common housekeeping management plan based on an intentionally structured relationship providing organization and stability. A "household" is defined as a family living together in a single dwelling unit, with common access to and common use of all living and eating areas and all areas and facilities for the preparation and serving of food within the dwelling unit.

Consistent with state law, the City's family definition states "One or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit."

Spacing and Concentration

The City abides by the spacing and concentration limits set forth by the California Department of Social Services with respect to residential care facilities. The only spacing concentration is for SRO hotels, which shall not be located within 500 feet of any public or private school for children under 18, church, child daycare facility, or other existing SRO facility. The City has included Program 20 to explore amending the Development Code to reduce location constraints for SRO facilities and permitting



SROs through a ministerial process rather than requiring a conditional use permit or administrative use permit.

Development Standards

To facilitate the construction of housing for people with disabilities, including seniors, builders can seek specific development incentives. For instance, the City allows density increases specific to senior citizen housing with affordable units to lower-income seniors. The Development Code allows reduced parking requirements of one space per unit. Boarding and rooming houses have similarly lower standards than other residential uses. Senior citizen housing developments are evaluated based on proximity to essential services, including public transit with frequent service, community centers and/or parks, medical facilities, and pharmacies, among others. Additionally, senior citizen housing developments can determine parking on a project-by-project basis, providing as low as 0.25 parking spaces per rental dwelling unit. While residential care facilities with six or less clients are only subject to development standards for residential uses permitted in the zoning district consistent with Health and Safety Code Section 1500 et seq., large residential care facilities are required to have only 0.5 parking space per unit. The City will seek to amend the Development Code to allow statelicensed residential care facilities for seven or more persons only subject to those restrictions that apply to residential uses in the same zone through Program 20. Further modifications can be sought through administrative exceptions and reasonable accommodation process, detailed below.

Reasonable Accommodation

The federal Fair Housing Act and California Fair Employment and Housing Act impose an affirmative duty on local governments to make reasonable accommodation when such accommodation may be necessary to afford disabled persons an equal opportunity to use and enjoy a dwelling.

In 2006, the City adopted Ordinance No. 2837 to allow reasonable accommodations from certain land use, permitting, and building codes. The ordinance set up a process to evaluate requests for reasonable accommodations related to specific applications of the zoning law to allow for full use and enjoyment of a dwelling and to authorize the application of exceptions to the zoning law, if warranted, to comply with state and federal fair housing law. Application for reasonable accommodation shall be made pursuant to the provisions listed for an administrative exception.

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With respect to the approval process, the applicant must file an application and pay an administrative fee of \$376. Public notice requirements shall be pursuant to the provisions listed for a homeowner variance. The Zoning Administrator may approve, deny, or conditionally approve the request. The Zoning Administrator must issue administrative variance findings to approve such a request. A determination to approve, conditionally approve, or deny a request shall be based on the following:

- The persons who will use the subject property are protected under federal and state fair housing laws.
- The requested exception is necessary to make specific housing available to a person who will occupy the subject property and who is protected under federal and state fair housing laws.
- The requested exception will not impose an undue financial or administrative burden upon the City.
- The requested exception will comply with all applicable Building and Fire Codes and will not result in a fundamental alteration of the planning, zoning, and development laws and procedures of the City.

If the project is deemed to be of significant controversy, the matter may be referred to the Planning Commission. All decisions made on the matter may be appealed to the City Council.

In summary, the City of Ontario continues to ensure that people of all abilities have opportunities to find housing in the community.

Development Standards

The Development Code provides specific residential development standards that determine building height, density, setbacks, parking, etc. These standards are made available to the public online on the City's website. Residential development standards are designed to promote a more livable environment, with adequate yards, height restrictions and setbacks to ensure privacy from adjacent homes, and minimum unit sizes to ensure adequate living areas for families.



Conventional Residential Development

The following tables describe the development standards in Ontario, such as density ranges, lot standards, open space requirements, and building standards, for the multiple types of residential development Ontario supports: Traditional Single-Family (Table 4-9), Small Lot Traditional Single-Family (Table 4-10), Small Lot Alley-loaded Single-Family (Table 4-11), Cluster Single-Family (Table 4-12), and Multifamily Residential (Table 4-13). The following discussion analyzes the City's development standards as they apply to different types of housing.

Detached single-family residential accounts for nearly 60 percent of Ontario's total housing units (Table 2-10). To encourage flexibility in single-family residential subdivision design, higher-density, and use of unconventional, small, and/or infill lots, the City provides development standards for a range of single-family development models. The conventional residential development, called traditional single-family residential, consists of the construction of one or more single-family (detached) dwellings and is permitted in all residential zones. Traditional single-family residential has lower maximum lot coverage and higher setbacks, leading to lower lot utilization than the other single-family use types. Small lot traditional, small lot alley-loaded, and cluster single-family residential development consist of two or more detached dwelling units per lot, allowing for higher density and lot utilization.

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Table 4-9 Traditional Single-Family Residential Development Standards

Development Chandends				Residen	tial Zones			
Development Standards	AR-2	RE-2	RE-4	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
		Den	sity ^{1,2,3}					
Density Ranges	0-2.0	0-2.0	2.1-4.0	2.1-5.0	5.1-11	11.1-18	18.1-25	25.1-45
		Lot S	tandards					
Maximum Lot Coverage	30%	40%	40%	50%	60%	60%	60%	60%
Minimum Lot Size (sf) ⁴	18,000	10,000	10,000	7,200	5,000	5,000	5,000	5,000
Average Lot Size		18,000		8,000	6,000	6,000	6,000	6,000
Min. Lot Dimensions ⁴								
Width (Interior)	100	70	70	60	60	60	60	60
Width (Corner Lots)	120	80	80	65	65	65	65	65
Width (Cul-de-Sac)								
At front property line	40	40	40	40	40	40	40	40
At front building setback	70	70	70	60	40	40	40	40
Depth	135	100	100	75	100	100	100	100
	T	Oper	n Space	ı	ı	T	ı	ı
Min Setback from Street and Alley Property Lines								
Freeways	20	20	20	20	20	20	20	20
Arterial Streets (front) ^{5,6}	30	30	30	30	30	30	30	30
Collector & Local								
Front	30	30	30	20	20	20	20	20
Front garage other	30	30	30	10	10	10	10	10
Street side	30	30	30	10	10	10	10	10
Street rear (1st floor)	30	30	30	10	10	10	10	10
Street rear (2 nd /3 rd floor)	30	30	30	20	20	20	20	20
Front Rear Alleys 1st floor	10	10	10	10	10	10	10	10
2 nd /3 rd floor	20	20	20	20	20	20	20	20
Garage entry	5	5	5	5	5	5	5	5
Garage other	10	10	10	10	10	10	10	10
Minimum Setback from Interior Property Lines								
Front side property lines ⁷								
Rear side property lines (1st floor)	10	10	5	5	5	5	5	5
Rear side property lines (2 nd /3 rd floor)	25	25	10	10	10	10	10	10
Rear side property lines (patio covers)	25	25	20	20	20	20	20	20
	25	25	10	10	10	10	10	10
Landscaping Area Required				ll be fully land:	ntional or sma scaped and pr n system			



Table 4-9 Traditional Single-Family Residential Development Standards

Douglanment Standards				Residen	tial Zones			
Development Standards	AR-2	RE-2	RE-4	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
	Building Standards uilding N/A							
Maximum Units/Building	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
Maximum Height (ft).	35	35	35	35	35	35	35	35

Source: City of Ontario Development Code, 2021.

- 1. A density bonus and other incentives pursuant to Government Code Sections 65915 through 65918 may be granted by the Approving Authority.
- 2. Lots with a maximum density calculation of less than one dwelling shall be allowed the construction of one dwelling unit.
- 3. A residentially zoned lot shall be developed at no less than the minimum number of dwelling units allowed within the specified density range for the applicable zoning district, except that if, as a result of the configuration/design of a lot, the minimum residential density cannot be achieved, the lot may be developed with a maximum of one dwelling unit.
- 4. An existing lot of record that is substandard as to minimum "lot" area and/or dimension(s) shall be granted all development rights of the zoning district in which it is located.
- 5. On a lot having a street adjacent rear property line (arterial, collector, and local streets only), for the purpose of wall placement, each wall shall be setback a minimum of 5 feet behind the street property line to allow for landscaping beyond any required parkway landscaping.
- 6. Refer to Collector and Local Streets standards for street side and rear setbacks
- 7. When vehicle parking is provided at the rear of a lot (whether within a garage or carport, or uncovered) that does not have alley access, a minimum 10-foot interior side building setback, which is clear of meters and mechanical equipment, shall be provided to ensure clear vehicular access to the rear of the lot.

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Table 4-10 Small-Lot Traditional Single-Family Residential Development Standards

				Reside	ntial Zones			
Development Standards	AR-2 ¹	RE-2 ¹	RE-4 ¹	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
			Density ²	2,3,4				
Density Ranges	N/A	N/A	N/A	2.1-5.0	5.1-11	11.1-18	18.1-25	25.1-45
Minimum Project Area	N/A	N/A	N/A	1 acre				
			Lot Stand	ards				
Maximum Lot Coverage	N/A	N/A	N/A	55%	60%	70%	70%	70%
Minimum Lot Size (sf) ⁵ Interior Lot Corner Lot	N/A	N/A	N/A	4,000 4,500	4,000 4,500	2,800 3,200	2,800 3,200	2,800 3,200
Min. Lot Dimensions ⁵ Width (Interior) Width (Corner Lots) Depth	N/A	N/A	N/A	40 45 75	40 45 75	35 40 70	35 40 70	35 40 70
			Open Sp	ace				
Min Setback from Street and Alley Property Lines ⁶ Freeways Arterial Streets (front) ⁷ Collector & Local Street Front (living area) Street Front (garage entry) Street Front (garage other) Street Side ⁸ Street rear (1st floor) ⁸ Street rear (2nd/3rd floor) ⁸ Street rear (garage entry) ⁸ Street rear (garage other – 1st floor only) ⁸ Street rear (patio cover) ⁸ Front Rear Alleys ⁹ 1st floor 2nd/3rd floor	N/A	N/A	N/A	20 30 14 18 10 10 10 15 18 5 5	20 30 14 18 10 10 10 15 18 5 5	20 30 14 18 10 10 10 15 18 5 5	20 30 14 18 10 10 10 15 18 5 5	20 30 14 18 10 10 10 15 18 5 5
Minimum Setback from Interior Property Lines From side property lines From rear property lines ¹⁰	N/A	N/A	N/A	5/4	5/4	5/4	5/4	5/4



Table 4-10 Small-Lot Traditional Single-Family Residential Development Standards

		<u> </u>	,		<u>'</u>			
				Reside	ential Zones			
Development Standards	AR-21	RE-2 ¹	RE-4 ¹	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
Living area				10	10	10	10	10
Garage – 1st floor only				5	5	5	5	5
Patio covers to side or rear				5	5	5	5	5
Landscaping Area Required				nall be fully lar			family project s an undergrour	
			Building Sta	ndards				
Maximum Height (ft).	35	35	35	35	35	35	35	35

Source: City of Ontario Development Code, 2021.

- 1 Small Lot Traditional Single-Family Residential Development is not a permitted use type in the AR-2 and RE-2 zoning districts.
- 2 A density bonus and other incentives pursuant to Government Code Sections 65915 through 65918 may be granted by the Approving Authority.
- 3 Lots with a maximum density calculation of less than one dwelling shall be allowed the construction of one dwelling unit.
- 4 A residentially zoned lot shall be developed at no less than the minimum number of dwelling units allowed within the specified density range for the applicable zoning district, except that if, as a result of the configuration/design of a lot, the minimum residential density cannot be achieved, the lot may be developed with a maximum of one dwelling unit.
- 5 An existing lot of record that is substandard as to minimum "project" area and/or dimension(s) shall be permitted the development rights of the zone in which it is located, except that the maximum density shall be limited to the minimum allowed within the density range.
- 6 The minimum setback from private streets shall be measured from a line running parallel to the street, which is located 12 feet behind face-of-curb (a.k.a., "parkway").
- 7 Refer to Collector and Local Streets standards for street side and rear setbacks.
- 8 On a lot having a street adjacent side or rear property line, for the purpose of wall placement, each wall shall be setback a minimum of 5 feet behind the street property line to allow for landscaping beyond any required parkway landscaping.
- 9 For side alley conditions, refer to the Interior Property Lines standards
- 10 The interior side property line setback may be reduced to 4 feet if the setback area is combined with the side setback area of the adjacent property to create a single minimum 8-foot-wide outdoor use area clear of walls, thereby allowing a minimum 8-foot-wide side-to-side building separation.

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Table 4-11 Small-Lot Alley-loaded Single-Family Residential Development Standards

Development Standards				Reside	ntial Zones			
Development Standards	AR-21	RE-21	RE-41	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
			Density ^{2,3,4}					
Density Ranges	N/A	N/A	N/A	2.1-5.0	5.1-11	11.1-18	18.1-25	25.1-45
Minimum Project Area ²	N/A	N/A	N/A	1 acre	1 acre	1 acre	1 acre	1 acre
Minimum Project Dimensions ² Width Depth	N/A	N/A	N/A	200 200	200 200	200 200	200 200	200 200
		L	ot Standards					
Maximum Lot Coverage	N/A	N/A	N/A	55%	60%	70%	70%	70%
Min. Lot Size Width (Interior) Width (Corner Lots) Depth	N/A	N/A	N/A	40 45 75	40 45 75	35 40 70	35 40 70	35 40 70
			Open Space					
Min Setback from Street Property Lines ⁵ Freeways Arterial Streets ⁶ Collector & Local ⁶ Front Street side Street rear	N/A	N/A	N/A	20 30 10 10 15	20 30 10 10 15	20 30 10 10 15	20 30 10 10 15	20 30 10 10
Minimum Setback from Project Boundary Property Lines ⁷ Project Boundaries Side Rear Patio Cover	N/A	N/A	N/A	10 5/4 10 5	10 5/4 10 5	10 5/4 10 5	10 5/4 10 5	10 5/4 10 5
Minimum Setback from Private Drive ⁵ Living area Porch (single-story) Garage entry Garage other (side/rear)	N/A	N/A	N/A	10 6 18 10	10 6 18 10	10 6 18 10	10 6 18 10	10 6 18 10
Minimum Setback from Private Lanes/Alleyways Living area Garage	N/A	N/A	N/A	10 5	10 5	10 5	10 5	10 5
Minimum Setback from Parking Spaces	N/A	N/A	N/A	10	10	10	10	10
Landscaping Area Required				ıll be fully lar			family project : an undergrour	



Table 4-11 Small-Lot Alley-loaded Single-Family Residential Development Standards

Davalanment Standards				Reside	ntial Zones			
Development Standards	AR-21	RE-21	RE-4 ¹	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
		Buil	ding Standard	ls				
Minimum Separations Between Buildings								
Dwelling Front to Front				25	25	25	25	25
Dwelling Front to Side				20	20	20	20	20
Dwelling Side to Side ⁷				10/8	10/8	10/8	10/8	10/8
Dwelling Rear to Rear	N/A	N/A	N/A	20	20	20	20	20
Garage to Garage ⁷	IN/A	IN/A	IW/A					
Entry to entry				30	30	30	30	30
Entry to side				30	30	30	30	30
Side to side				10/8	10/8	10/8	10/8	10/8
Side to rear				10/8	10/8	10/8	10/8	10/8
Maximum Height (ft).	35	35	35	35	35	35	35	35

Source: City of Ontario Development Code, 2021.

- 1 Small-Lot Alley-Loaded Single-Family Residential Development is not a permitted use type in the AR-2, RE-2, and RE-4 zoning districts.
- 2 A density bonus and other incentives pursuant to Government Code Sections 65915 through 65918 may be granted by the Approving Authority.
- 3 Lots with a maximum density calculation of less than one dwelling shall be allowed the construction of one dwelling unit.
- 4 A residentially zoned lot shall be developed at no less than the minimum number of dwelling units allowed within the specified density range for the applicable zoning district, except that if, as a result of the configuration/design of a lot, the minimum residential density cannot be achieved, the lot may be developed with a maximum of one dwelling unit.
- 5 The minimum setback from private streets shall be measured from a line running parallel to the street, which is located 12 feet behind face-of-curb (a.k.a., "parkway").
- 6 On lot having a street-adjacent side or rear property line, for the purpose of wall placement, each wall shall be setback a minimum of 5 feet behind the street property line to allow for landscaping beyond any required parkway landscaping.
- 7 The interior side property line setback may be reduced to 4 feet if the setback area is combined with the side setback area of the adjacent property to create a single minimum 8-foot-wide outdoor use area clear of walls, thereby allowing a minimum 8-foot-wide side-to-side building separation.

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Table 4-12 Cluster Single-Family Residential Development Standards

December and Characterists				Resid	dential Zones	5		
Development Standards	AR-21	RE-21	RE-4 ¹	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
			Den	sity ^{2,3,4}				
Density Ranges	N/A	N/A	N/A	2.1-5.0	5.1-11	11.1-18	18.1-25	25.1-45
Minimum Project Area ²	N/A	N/A	N/A	1 acre				
Minimum Project Dimensions ² Width Depth	N/A	N/A	N/A	200 200	200 200	200 200	200 200	200 200
			Lot St	andards				
Maximum Lot Coverage	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
Minimum Lot Size	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
Minimum Lot Dimensions	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A
			Open	Space				
Minimum Setback from Public Street Property Lines ⁵ Freeways Arterial Streets ⁶ Collector and Local Streets ⁶ Front Street side Street rear	N/A	N/A	N/A	20 30 20 10 15	20 30 20 10 15	20 30 20 10 15	20 30 20 10 15	20 30 20 10 15
Minimum Setback from Interior Property Lines ⁷ Project Boundaries Sides Rear Patio Cover	N/A	N/A	N/A	10 4 4 4	10 4 4 4	10 4 4 4	10 4 4 4	10 4 4 4
Minimum Setback from Private Drive ⁵ Living area Porch (single-story) Garage entry Garage other	N/A	N/A	N/A	10 5 <5 or >18 10				
Minimum Setback from Private Lanes/Alleyways Living area Porch (single story) Garage	N/A	N/A	N/A	5 5 5	5 5 5	5 5 5	5 5 5	5 5 5



Table 4-12 Cluster Single-Family Residential Development Standards

Davidanment Standards				Resid	dential Zones	5		
Development Standards	AR-21	RE-21	RE-4 ¹	LDR-5	MDR-11	MDR-18	MDR-25	HDR-45
Minimum Setback from Parking Spaces								
Living area	N/A	N/A	N/A	10	10	10	10	10
Porch (single story) Garage				8 5	8 5	8 5	8 5	8 5

Landscaping Area Required

The entirety of a cluster single-family or multiple-family project site, including street parkway and median areas that abut the project site, which is not otherwise devoted to building area and paving, shall be fully landscaped and provided with an underground automatic irrigation system,

	д,												
Building Standards													
Minimum Separations Between Buildings													
Dwelling Front to Front				30	30	30	30	30					
Dwelling Front to Side ⁷				14	14	14	14	14					
Dwelling Side to Side ⁷				8	8	8	8	8					
Dwelling Rear to Rear	N/A	N/A	N/A	8	8	8	8	8					
Garage to Garage ⁷													
Entry to entry				30	30	30	30	30					
Entry to side				30	30	30	30	30					
Side to side				8	8	8	8	8					
Side to rear				8	8	8	8	8					
Maximum Height (ft).	35	35	35	35	35	35	35	35					

Source: City of Ontario Development Code, 2021.

- 1 Cluster Single-Family Residential Development is not a permitted use type in the AR-2, RE-2, and RE-4 zoning districts.
- 2 A density bonus and other incentives pursuant to Government Code Sections 65915 through 65918 may be granted by the Approving Authority.
- 3 Lots with a maximum-density calculation of less than one dwelling shall be allowed the construction of one dwelling unit.
- 4 A residentially zoned lot shall be developed at no less than the minimum number of dwelling units allowed within the specified density range for the applicable zoning district, except that if, as a result of the configuration/design of a lot, the minimum residential density cannot be achieved, the lot may be developed with a maximum of one dwelling unit.
- 5 The minimum setback from private streets shall be measured from a line running parallel to the street, which is located 12 feet behind face-of-curb (a.k.a., "parkway").
- 6 On a lot having a street-adjacent side or rear property line, for the purpose of wall placement, each wall shall be setback a minimum of 5 feet behind the street property line to allow for landscaping beyond any required parkway landscaping.
- 7 The interior side property line setback may be combined with the side setback area of the adjacent property to create a single minimum 8-foot-wide outdoor use area clear of walls, which is defined in the project CC&Rs.

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Table 4-13 Multifamily Residential Development Standards

Development Standards	Residential Zones											
	AR-21	RE-2 ¹	RE-4 ¹	LDR-51	MDR-11	MDR-18	MDR-25	HDR-45				
Density ^{2,3,4,5}												
Density Ranges	N/A	N/A	N/A	N/A	5.1-11	11.1-18	18.1-25	25.1-45				
Minimum Project Area ²	N/A	N/A	N/A	N/A	10,000 sf	10,000 sf	10,000 sf	1 acre				
Minimum Project Dimensions ² Width Depth	N/A	N/A	N/A	N/A	100 100	100 100	100 100	180 200				
Lot Standards												
Maximum Project Coverage	N/A	N/A	N/A	N/A	60%	60%	60%	100%				
Minimum Lot Size	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A				
Minimum Lot Dimensions	N/A	N/A	N/A	N/A	N/A	N/A	N/A	N/A				
	Open Space											
Minimum Setback from Public Street Property Lines ^{6,7} Freeways Arterial Streets Collector and Local Streets	N/A	N/A	N/A	N/A	20 30 20	20 30 20	10 10 10	10 10 10				
Minimum Setback from Interior Project Boundary Property Lines ⁸	N/A	N/A	N/A	N/A	5	5	5	5				
Minimum Setback from Public Alley Property Line	N/A	N/A	N/A	N/A	5	5	5	5				
Minimum Setback from Private Drives/Alleyways (from edge of drive aisle) Living area Garage and other non- habitable structures	N/A	N/A	N/A	N/A	15 5	15 5	15 5	15 5				
Minimum Setback from Parking Spaces or Drive Aisle to Wall or Fence	N/A	N/A	N/A	N/A	5	5	5	5				
Landscaping Area Required	The entirety of a cluster single-family or multiple-family project site, including street parkway and median areas that abut the project site, which is not otherwise devoted to building area and paving, shall be fully landscaped and provided with an underground automatic irrigation system											



Building Standards								
Minimum Separations Between Buildings								
Dwelling Front to Front								
Less than 2 stories					25	25	25	25
Greater than 3 stories					30	30	30	30
Dwelling Front to Side ⁷								
Less than 2 stories					25	25	25	25
Greater than 3 stories					30	30	30	30
Dwelling Side to Side ⁷								
Less than 2 stories					25	25	25	25
Greater than 3 stories	N/A	N/A	N/A	N/A	30	30	30	30
Dwelling Rear to Rear								
Less than 2 stories					10	10	10	10
Greater than 3 stories					15	15	15	15
Dwelling Side to Rear					15	15	15	15
Dwelling Rear to Rear					20	20	20	20
Garage to Garage ⁷								
Entry to entry					30	30	30	30
Entry to side					30	30	30	30
Side to side					10	10	10	10
Side to rear					10	10	10	10
Maximum Height (ft).	N/A	N/A	N/A	N/A	35	45	60	75

Source: City of Ontario Development Code, 2021.

- 1 Multifamily Residential is not a permitted use type in the AR-2, RE-2, RE-4, and LDR-5 zoning districts.
- 2 An existing lot of record that is substandard as to minimum "project" area and/or dimension(s), shall be permitted all of the development rights of the zone in which it is located, except that the maximum density shall be limited to the minimum number of dwelling units allowed within the specified density range for the applicable zoning district.
- 3 A density bonus and other incentives pursuant to Government Code Sections 65915 through 65918 may be granted by the Approving Authority.
- 4 Lots with a maximum density calculation of less than 1.00 may be developed with one dwelling unit.
- 5 A residentially zoned lot shall be developed at no less than the minimum number of dwelling units allowed within the specified density range for the applicable zoning district, except that If, as a result of the configuration/design of a lot, the minimum residential density cannot be achieved, the lot may be developed with a maximum of one dwelling unit.
- 6 A health risk assessment shall be required for multiple-family development projects located within close proximity to a freeway, as determined by the Zoning Administrator.
- 7 The minimum setback from private streets shall be measured from a line running parallel to the street, which is located 12 feet behind face-of-curb (a.k.a., "parkway").
- 8 A dwelling having the primary entry facing onto an interior property line shall maintain a minimum 10-foot setback from the corresponding interior property line.

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Mixed-Use Development

The City of Ontario actively encourages and facilitates the planning and production of mixed-use housing, vertically and horizontally integrated. Mixed-Use developments contain buildings or structures with a variety of complementary uses, such as residential, office, manufacturing, retail, public, or entertainment, in an integrated development project that has significant functional interrelationships and a coherent physical design. As part of The Ontario Plan, prepared in 2010, the City revised its Policy Plan land use designations, most notably expanding the Mixed-Use category to include area-specific designations to create focal points for community activity and identity and facilitate the use of transit. Three of the Policy Plan Mixed-Use designations have corresponding mixed-use zoning districts, shown in Table 4-14. Currently, mixed-use projects can be found along the Interstate (I-) 10 corridor, in the historic Downtown area, and in the newly developing Ontario Ranch area.

The development standards for the Downtown Mixed-Use Area (MU-1) and East Holt Mixed-Use Area (MU-2) rely on a PUD created by the developer at the time of project submittal. The City has the opportunity to develop an area plan or form-based code for the Downtown Mixed-Use Area that would establish development standards or guidelines. The Euclid/Francis Mixed-Use Area (MU-11) uses the same development standards as the MDR-25 zoning district. The Ontario Airport Metro Center area and Ontario Ranch are required to have specific plans. The maximum density indicated in Table 4-14 is derived from the Policy Plan and in some cases has a higher density than is permitted by current zoning. In these areas, specific plans are required, so development standards would be altered to conform to the permitted density in the Policy Plan through the specific plan process (Program 20). Where there are discrepancies, the City has included a program to amend the Development Code to increase the density to match the corresponding Policy Plan land use designation.



Table 4-14 Mixed-Use Development Standards

Wilked Ose Development Standards						
	Mixed-Use Zones					
Development Standards	MU-1 ¹ Downtown Mixed-Use Area	MU-2 East Holt Mixed-Use Area	MU-11 Euclid / Francis Mixed-Use Area			
Density Ranges	25.1 – 40 du/ac	14.1 – 40.0 du/ac	14.1 – 25.0 ²			
Assumed Maximum Build Out for Mixed-Use Area – Residential Density ³						
Dwelling units/acre	60% of the area at 35 du/ac	25% of the area at 30 du/ac	50% of the area at 30 du/ac			
Maximum Units	2,365	428	156			
Floor Area Ratio (Non-residential)						
Commercial-retail	2.0	1.0	1.04			
Commercial-office	2.0	2.0	1.0^{4}			
Development Standards ²						
Minimum Setback from Public Street Property Lines ^{5,6} Freeways Arterial Streets			10			
Collector and Local Streets			10 10			
Minimum Setback from Interior Project Boundary Property Lines ⁷	Governed by planned development	Governed by planned development	5			
Minimum Setback from Public Alley Property Line	regulations or future City area plan or form-based code	regulations	5			
Minimum Setback from Private Drives/Alleyways (from edge of drive aisle)						
Living area			15			
Garage and other non-habitable structures			5			
Minimum Setback from Parking Spaces or Drive Aisle to Wall or Fence			5			

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Table 4-14 Mixed-Use Development Standards

	Mixed-Use Zones				
Development Standards	MU-1 ¹ Downtown Mixed-Use Area	MU-2 East Holt Mixed-Use Area	MU-11 Euclid / Francis Mixed-Use Area		
Landscaping			The entirety of a multiple-family project site, including street parkway and median areas that abut the project site, which is not otherwise devoted to building area and paving, shall be fully landscaped and provided with an underground automatic irrigation system		
Maximum Height (feet)	Governed by planned development regulations or future City area plan or form-based code	Governed by planned development regulations	45 feet		

Source: City of Ontario Development Code, June 2021.

- 1 MU-1 includes sub-zoning districts LUA-1, LUA 2N, LUA-2S, LUA-3, LUA-4; however, residential mixed-use development is not permitted in the LUA-2S and LUA-4 zones.
- 2 Within the MU-11 zoning district, residential development shall be allowed pursuant to the standards of the HDR-25 zoning district.
- 3 Within each Mixed-Use Zoning District, the number of dwelling units allotted shall not exceed the number of units prescribed by the Exhibit LU- 03 (Future Buildout) of the Policy Plan
- 4 Nonresidential development shall be allowed pursuant to the requirements of the CN zoning district.
- 5 A health risk assessment shall be required for multiple-family development projects within close proximity to a freeway, as determined by the Zoning Administrator.
- 6 The minimum setback from private streets shall be measured from a line running parallel to the street, which is located 12 feet behind face-of-curb (a.k.a., "parkway").
- 7 A dwelling having the primary entry facing onto an interior property line shall maintain a minimum 10-foot setback from the corresponding interior property line.



Residential density is often equated with the affordability of housing. For zoning districts primarily permitting single-family residential, the City allows a maximum density of two units per acre in the AR-2 and RE-2 zones, four units per acre in the RE-4 zone, and five units per acre in the LDR-5 zone. For zones intended for multifamily residential, the City permits a maximum density of 11 units in the MDR-11 zone, 18 in the MDR-18 zone, 25 in the MDR-25 zone, and 45 in the MDR-45 zone. The MDR-45 zoning district allows the highest density of zones permitting residential uses, including the mixed-use zoning districts. Recognizing the importance of a variety of densities to facilitate and encourage a range in types and prices of housing, the City offers three key ways to receive additional density increases.

- Planned Unit Development (PUD). In an effort to secure a fuller realization of the Policy Plan component of The Ontario Plan, particularly the Exhibit LU-3, Future Buildout, than would result from strict application of the zoning district regulations, developers can submit PUDs to achieve higher density. Mixed-Use developments in the MU-1 and MU-2 zoning districts require a PUD, leading to establishing development standards on a project-by-project basis, including density standards. The City is currently updating objective design standards to reduce the necessity of PUDs. PUDs require approval by the City Council pursuant to California Government Code Section 65850 based on the following findings of approval:
 - The proposed PUD, or amendment thereto, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan, and City Council Priorities components of The Ontario Plan.
 - The proposed PUD, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the city.
 - In the case of an application affecting specific property(ies), the proposed PUD, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.
 - In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.

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- The proposed PUD is superior to that which could be obtained through the application of the Development Code or a specific plan.
- Senior Citizen Housing Development. The City allows a base density of 25 units per acre for the CC and MU-1 zones, and are subject to the maximum density for the respective residential zoning districts that senior citizen housing developments are permitted in (i.e., MDR-11, MDR-18, MDR-25, and MDR-45). Qualifying senior citizen housing development projects are eligible for a density bonus pursuant to California Government Code Sections 65915 through 65918, permitting a 20-percent density bonus for qualified projects.
- State Density Bonus. In compliance with California Government Code Section 65915, the City allows qualified residential projects to receive a density bonus plus appropriate development incentives when the residential project sets aside the required number of units for affordable housing. Density bonuses are also allowed for senior housing (described in greater detail above). The City processes applications for density bonuses following the City's procedure for Development Agreements, requiring City Council approval unless one or more of the following findings is established:
 - The concession or incentive is not required to provide for affordable housing costs, as defined in Health and Safety Code Section 50052.5, or for rents for the targeted units to be set as specified in California Government Code Section 659159(c).
 - The concession or incentive would have a specific adverse impact, as defined in Government Code Section 65589.5(d)(2), upon public health and safety or the physical environment, or on any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the specific adverse impact without rendering the development unaffordable to low- and moderate-income households.
 - The concession or incentive would be contrary to state or federal law.

Assembly Bills (ABs) 2753, 2372, 1763, 1227, and 2345 were passed in 2018, 2019, and 2020 and revised density bonus law to provide additional benefits for qualifying projects. To ensure the City's development requirements are consistent with recent state law, the City will review Sections 6.01.010(H) and 5.03.360 of the Development Code for



compliance with ABs 2753, 2372, 1227, 1763, and 2345 and to provide requirements within the Development Code sections (Program 20).

Parking Standards

In an urban environment, parking standards are critical to prevent traffic congestion caused by a shortage of parking spaces and the loading and unloading of trucks on public streets to result in maximum efficiency, protect the public safety, provide for the special needs of the physically handicapped, and where appropriate, insulate surrounding land uses from their impact.

City parking standards are also designed to ensure that sufficient on-site spaces are available to accommodate vehicle ownership rates of residents (which is typically more than 2 cars per homeowner and an average of 1.9 vehicles for renters), the needs of the business community, and the rate of overcrowding. Table 4-15 summarizes the common parking standards for residential uses, and the following text describes potential reductions of standards.

Table 4-15
Parking Standards for Housing

Housing Types	Requirement
Traditional Single-Family (one per lot)	2 spaces within enclosed garage
Small Lot and Common Interest Developments	2 resident spaces per dwelling within a garage, plus, 0.2 guest/visitor spaces per dwelling. Guest parking spaces may be provided on-street, immediately adjacent to the development boundary, if available. A minimum of 2 guest spaces shall be provided regardless of the number of dwellings proposed.
Multiple-Family ¹	Resident Parking Spaces: Studio: 1.5 spaces per dwelling, including one space in a garage or carport; One-Bedroom: 1.75 spaces per dwelling, including one space in a garage or carport; Two-Bedrooms: 2.0 spaces per dwelling, including one space in a garage or carport; and Three or more Bedrooms: 2.5 spaces per dwelling, including one space in a garage or carport Guest Parking Spaces: Portion of dwellings < 50: 0.25 spaces per dwelling; Portion of 50 to 100 dwellings: 0.20 spaces per dwelling; Portion of dwellings > 100: 0.17 spaces per dwelling; and A minimum of 3 guest spaces shall be provided for developments consisting of more than 8 dwellings.
Mobile Home Park ¹	Resident Parking Spaces: 2 spaces per unit, tandem allowed Guest Parking Spaces: Portion of dwellings < 50: 0.25 spaces per dwelling; Portion of 50 to 100 dwellings: 0.20 spaces per dwelling; Portion of dwellings > 100: 0.17 spaces per dwelling; and A minimum of 3 guest spaces shall be provided for developments consisting of more than 8 dwellings.
ADUs ^{1,2}	Generally, 1 space per unit
Residential Care 7+ clients	0.5 spaces per bed; plus, one space per employee or staff

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Table 4-15 Parking Standards for Housing

Housing Types	Requirement
SRO	Resident Parking Spaces: One resident space per room; plus, 2 spaces for the resident manager Guest Parking Spaces: Portion of dwellings < 50: 0.25 spaces per dwelling; Portion of 50 to 100 dwellings: 0.20 spaces per dwelling; Portion of dwellings > 100: 0.17 spaces per dwelling; and A minimum of 3 guest spaces shall be provided for developments consisting of more than 8 dwellings.
Boarding, Lodging, or Rooming House	One space per sleeping room; however, provide no fewer than one space per 2 beds
Senior Citizen Housing Development ³ Income Qualified Market Rate Development	Resident Parking Spaces: 0.7 resident space per dwelling Guest Parking Spaces: Portion of dwellings < 50: 0.25 spaces per dwelling; Portion of 50 to 100 dwellings: 0.20 spaces per dwelling; Portion of dwellings > 100: 0.17 spaces per dwelling; and A minimum of 3 guest spaces shall be provided for developments consisting of more than 8 dwellings. Resident Parking Spaces: One resident space per dwelling Guest Parking Spaces: Portion of dwellings < 50: 0.25 spaces per dwelling; Portion of 50 to 100 dwellings: 0.20 spaces per dwelling; Portion of dwellings > 100: 0.17 spaces per dwelling; and A minimum of 3 guest spaces shall be provided for developments consisting of more than 8 dwellings.
Transitional Shelter/Housing	Determined by the Zoning Administrator or Approval Authority
Nursing Care Facilities	Determined by the Zoning Administrator or Approval Authority

Source: City of Ontario Development Code, June 2021

- 1 Tandem parking may be used to satisfy the minimum resident parking requirement for mobile home parks and ADUs and satisfy unenclosed on-site parking for multifamily projects.
- 2 ADUs are not required to provide parking in the following situations:
 - The ADU is within one-half mile walking distance of public transit
 - The ADU is within an architecturally and historically significant historic district.
 - The ADU is part of the proposed or existing primary residence, or an accessory structure.
 - When on-street parking permits are required but not offered to the occupant of the ADU.
 - When there is an established car-share vehicle stop located within one block of the ADU.
- 3 The number of parking spaces required to be provided for senior citizen housing developments may be as low as 0.25 spaces per rental dwelling unit and as high as 1.0 space per for-sale dwelling unit. The actual ratio shall be determined at the time of project approval for the use and shall be based on a parking demand study to be prepared by a qualified traffic consultant or engineer. Of the parking spaces provided, 10 percent shall be designated as parking for the physically impaired.



The Approval Authority may reduce the number of required parking spaces if an applicant is able to provide evidence to substantiate: (1) shared parking (if multiple users use the same joint-parking facilities when operations for the respective uses are not normally conducted during the same hours or when peak use differs); or (2) low demand (when demonstrated via a parking analysis that the use will not use the required number of spaces because of the nature of the specific use or manner in which the use is conducted). The Approval Authority may require a parking analysis to prove either circumstance.

In the Downtown Civic Center, a focus area for revitalization of the City's historic downtown, permits 1.2 spaces/bedroom for residential condominiums, inclusive of guest parking. Developers are encouraged to use a mix of standard and tandem spaces to achieve the required number of parking spaces; however, compact and tandem spaces can each only comprise 20 percent of the total spaces and require the Planning Director to approve a parking study. The City has adopted a flexible parking approach to facilitate revitalization of the city's historic Downtown through a mix of housing types and prices. The Downtown Parking Model continues to provide flexible parking requirements for multiplefamily, mixed-use development, adaptive reuse, and live-work within the Downtown. The model considers parking supply, shared parking, and peak or non-peak demand from any combination of 30 land uses. Downtown parking standards are now performance-based rather than based on a prescriptive standard.

Open Space

The City of Ontario values the incorporation of an appropriate amount and quality of open space in residential projects, particularly higher-density housing. Ensuring an adequate amount of open space enhances higher-density residential projects by providing appropriate levels of privacy, provides green infrastructure that reduces runoff, softens concrete hardscape and beautifies residential projects, improves the value of the property, and creates a more desirable living environment for residents. The City's open space standards are shown in Table 4-16.

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Table 4-16
Open Space Standards for Housing

	•	<u> </u>			
Housing	Lot Coverage	Open Space			
Housing	Lot Coverage	Private	Common		
Single-Family - Small Lot Traditional, Small Lot Alley Loaded, and Cluster Development Projects ¹	20% for all zoning districts permitting use types	Contiguous private open space area for each ground-level dwelling unit that is 225 square feet	Remaining area after Private Open Space per unit shall be used toward Common Open Space		
Multifamily and Mixed-Use Development Projects¹ (sf/unit)					
Zoning Districts: MDR-11 & MDR-18 MDR-25	500 400	200 150	300 250		
HDR-45	310	60	250		
Mobile Home Park	Same as the underlying zone	None specified	300 square feet per pad		
Senior Citizen Housing Development	Based on the development standards applicable to residential uses in the underlying zone				

Source: City of Ontario Development Code, June 2021

The Subdivision Chapter of the Development Code provides additional detail on the appropriate types of private and common open space for multiple-family projects. For instance, common open space does not include driveways, sidewalks, parking areas, or service areas, but may include playgrounds, lawn areas, swimming pools, tennis and sport courts, and other outdoor recreational facilities. Private open space typically is accessible only to occupants of a particular unit and often consists of a fenced yard, fenced patio, or balcony. In addition to project-specific requirements, residential developers must also contribute to the City's goal of providing 3 acres of parks per 1,000 residents through payment of a park impact fee. The ratio is higher in Ontario Ranch, where developers are required to provide 2 acres of park space per 1,000 residents.

The City's zoning allows a variance or administrative exception process, where needed, to provide relief from typical residential development standards that preclude the full enjoyment and use of residential property. However, to obtain density bonus allowances, open space requirements must be met. The variance and administrative exception process is more fully described below.

¹ Open space requirements only apply to residential development projects consisting of more than three dwelling units.



Variance Process

The City has established a variance and administrative exception process to facilitate the resolution of practical difficulties or unnecessary physical hardships that may arise due to the size, shape, or dimensions of a site or the location of existing structures thereon, or from geographic, topographic, or other physical conditions on the site or in the immediate vicinity.

The two primary means of obtaining additional flexibility in residential development standards are:

- Administrative Exception. The City may grant an administrative exception of up to 10 percent from any numerical development standard set forth in the Development Code, except for standards for floor-area ratios and residential density. The Zoning Administrator is empowered to approve the exception.
- Variance. Variances may be granted for the following development requirements: landscaping, screening, site area, site dimensions, yards and projections into yards, heights of structures, distances between buildings, open space, off-street parking, and loading. The Planning Commission can grant the request after a public hearing.

The Planning Commission or Zoning Administrator, as applicable, may grant a variance or administrative exception provided that the following findings can be made:

- 1. Special property circumstances and literal interpretation and enforcement of the code would result in practical difficulties or unnecessary physical hardship inconsistent with the objectives of the City's Development Code or Policy Plan.
- Strict or literal interpretation and enforcement of the specified regulation would deprive the applicant of privileges enjoyed by the owners of other properties classified in the same zoning district.
- Approval of the administrative exception/variance will not constitute a grant of special privilege inconsistent with the limitations of other properties classified in the same zoning district.
- 4. Exceptional/extraordinary circumstances or conditions applicable to the site involved or to the intended use of the property do not apply generally to other properties in the same zone.

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5. Granting of the administrative exception/variance will not be detrimental to the public health, safety, or welfare or materially injurious to the properties or improvements in the vicinity.

Building Codes and Subdivision Improvements

The City implements and enforces building codes, property maintenance standards, subdivision improvement requirements, and other municipal codes to ensure quality housing and neighborhoods for residents. Although building codes and subdivision improvement requirements do raise construction costs, the public interest is best served when buildings adhere to proper construction and engineering practices and neighborhoods have appropriate infrastructure suitable to their design.

Building Codes

Every three years, the State of California adopts new codes that contain the latest advances in construction practices and engineering concepts. The California Building Standards Commission adopts the California Building Codes based on "model" codes produced by professional organizations. Local agencies must adopt these codes, but may make amendments to address geological, climatic, or topographical conditions provided the modifications are no less restrictive than the state standards.

The new state codes incorporate, by reference, the Model Codes published by the International Code Council (ICC), which recently consolidated multiple regional codes into a single set of codes applicable throughout the United States. The City has adopted the most recent building codes to reflect the latest advances in construction technology and building practices. The following codes are currently being implemented:

- 2019 California Building Code/2018 International Building Code
- 2019 California Residential Code/2018 International Residential Code
- 2019 California Electrical Code/2017 National Electrical Code
- 2019 California Mechanical Code/2018 Uniform Mechanical Code
- 2019 California Plumbing Code/2018 Uniform Plumbing Code
- 2019 California Energy Code
- 2019 California Historical Building Code
- 2019 California Green Buildings Standards Code
- 2019 California Fire Code/2018 International Fire Code



According to the local building official, the City has made some minor modifications to the building codes. Local amendments are minimal and related to administrative procedures. Such amendments do not materially increase the cost of residential construction and are similar to the amendments adopted in jurisdictions throughout the county. The City has not imposed any building codes other than those mentioned previously.

Therefore, the new building codes do not present a potential or actual constraint to the development, maintenance, and improvement of housing.

Subdivision Requirements

The City Subdivision Code requires that all new residential developments incorporate a standard set of subdivision requirements and infrastructure improvements to the property in compliance with City specifications and applicable Policy Plan or Specific Plan provisions. This requirement ensures that the subdivision is served by an adequate level of services that contribute to the long-term sustainability of the development.

The type and dimensions of subdivision improvements depend on a number of factors, including topography, density and intensity of development, project size, and other factors. The following list indicates typical infrastructure improvements that are required in subdivisions:

- Dedication of the ultimate street right-of-way if not currently existing at its ultimate width. Most local streets are a 60-foot rightof-way. Arterial streets start at 88-foot rights-of-way.
- Installation of paving, curb, gutter, and sidewalk along the street frontage at the ultimate right-of-way location.
- Installation of streetlights, street trees, fire hydrants, and other needed improvements across the property frontage.
- Undergrounding of all overhead telephone, cable, and electrical lines (less than 34 kilovolts), in accordance with City ordinances.
- Extension and/or installation of existing underground dry utilities needed to serve the development project (such as gas, telephone, cable, and electrical).
- Extension/installation/relocation of wet utilities (sewer, water, storm drain) needed to serve the site, if any. If no storm drain system exists to serve the site, on-site retention would be required.

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Payment of Development Impact Fees (DIF). These fees are used to fund expansions to public facilities and improvements, such as water, sewer, parks, fire and police, transportation systems, and other improvements. Developers may be eligible for DIF credit if they are installing master-planned facilities to serve their sites.

Permit Approval Process

The City uses a standard development review process to ensure that residential projects are of high-quality construction and design. The time frame for processing proposals depends on the complexity of the project, the need for legislative action, and environmental review.

Table 4-17 and the following text describe the steps to process proposals for residential development.

- Initial Project Submittal. The first step in the development review process is the initial submittal of the development application to either the Building Department (for a single-family home) or the Planning Department (for more than two dwellings on a lot or more than four dwellings in total). The initial submittal may be preceded by an initial consultation with the Planning or Building Department as requested by the applicant to determine appropriate submittal requirements.
- Development Plan Review. The Building or Planning Department then routes the application to affected departments for their review and comment. The purpose of the review is to ensure that new development or expansion of existing uses or structures occurs in a manner consistent with the Policy Plan and the objectives and standards of the Development Code, and that reasonable conditions are placed on the project to maintain public health, safety, and welfare.
- Design Review. While the project is reviewed by the affected City departments, the Planning Department conducts design review. Design review is intended to ensure that the proposed architectural treatment of new buildings and structures, including landscaping, open space, and signs, is consistent with the objective and illustrative design guidelines contained in the Development Code and expectations of the City. If the property is designated in a historic area of the community, additional reviews may be required consistent with state and local law.



- Environmental Review. City staff initiates the environmental review process to the extent required by the project. Most standard infill development projects require an initial study and negative declaration or mitigated negative declaration under the California Environmental Quality Act (CEQA). In some cases, an environmental impact report is required for sensitive projects or for specific plans. In these cases, the developer pays a standard fee for the required type of environmental review. The completion of the environmental review is timed to coincide with the forwarding of the application to the Planning Commission.
- Development Advisory Board (DAB). The DAB meets to review the project and its conformance with the previous conditions, the Development Code, Municipal Code, and other requirements of the City. The DAB may review the site in relation to location of buildings on adjoining sites, any physical constraint identified on the site, the characteristics of the area in which the site is located, the degree to which the proposed development will complement or improve the quality of development in the vicinity of the proposed project, and the extent to which adverse impacts to surrounding properties will be minimized.

The DAB has the authority to hear and decide on development plan review applications, substantial modifications to previously approved development plan review applications, environmental assessments associated with any of the above applications, and tentative maps. The DAB may also make recommendations as to the need for variances, conditional use permits, specific plans, etc. Once the review is completed, the DAB makes recommendations to the Planning Commission for appropriate action. To ensure a timely review, the members of the DAB are the same individuals who conducted the initial review of the application.

Planning Commission Action. Planning Commission action is required for single-family tracts, multiple-family projects, specific plans, etc. In most cases, the Planning Commission does not act as a Design Review Board, unless a significant project is proposed or the applicant is appealing recommendations of the Development Advisory Board. The Planning Commission typically approves recommendations of the DAB but may require modifications.

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Table 4-17
Permit Processing Procedures

	Residentia			
Processing Steps	Four or Fewer Housing Units			
Submit Initial Application	Required	Required		
Design Review	N/A	Required	Concurrent with project processing	
Environmental Clearance	N/A	Required		
City Review & Modifications	Required	Required	30 to 60 days	
Developer Makes Modifications	Required	Required	30 to 60 days	
Development Advisory Board	N/A	Required	30 days	
Planning Commission Action	N/A	Required	30 days	
Building Permits Issued	Required	Required	Over the counter	
Total	2 to 3 months	5 to 6 months		

Source: City of Ontario, 2012.

- Approval Findings and Decision. A Development Plan shall be acted upon by the Approving Authority based on the information provided in the submitted application, evidence presented in the Planning Department's written report, and testimony provided during the public hearing, only after considering and clearly establishing all of the following findings and giving supporting reasons for each finding. The application shall be denied if one or more of the following findings cannot be clearly established.
 - The proposed development at the proposed location is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan, and City Council Priorities components of The Ontario Plan:
 - The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located;
 - The proposed development will complement and/or improve on the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety, and general welfare have been required of the proposed project;

^{*} Also applies to more than two units on a single lot



 The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or PUD.

The City is charged with the responsibility of ensuring that residential projects are decent, safe, and well-designed. While the permit approval process adds to the length of time required to process applications, it does not add any unduly constraints on the development of housing.

The City has not received residential development applications proposing to develop housing at a density lower than the minimum density permitted.

In an effort to streamline affordable housing projects, the City is including Program 22 to comply with the requirements of Senate Bill (SB) 35 by establishing a written policy or procedure and other guidance as appropriate to specify the SB 35 (2017) streamlining approval process and standards for eligible projects, as set forth under Government Code Section 65913.4.

Design Review

Design review is a critical component of Ontario's overall housing strategy. Poor quality design, in the long term, leads to the premature deterioration of housing, a decline in the quality of neighborhoods, and resident opposition or "NIMBYism" ("not in my backyard" mentality). However, to achieve the City's housing goals, providing a level of certainty to the development community is important. Developers need to know how to design their projects and neighborhoods to meet City expectations and avoid adverse public opinion and project denials.

Recognizing the need to balance the City's housing goals, neighborhood stabilization, and revitalization goals, the City adopted Residential Design Guidelines in 2006. The guidelines provide guidance, objective standards, and graphics to illustrate the preferred and discouraged methods of planning, neighborhood design, and construction.

Topics include:

- Developments and Subdivisions. Include mixed-use housing, walkable neighborhoods, street networks, and open spaces.
- Open Space and Landscaping. Include common open space, common recreation facilities, pathways, parks, and trails.
- Lots and Buildings. Include size and dimensions, model variety, building orientation, garage placement, and fences and walls.
- Building Design. Include building types, massing and roof form, garage design, accessory structures, and architectural details.

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The Development Advisory Board conducts design review for residential projects exceeding four units. To ensure the process does not unduly lengthen the time period for project approval, design review is conducted concurrently with project review. The majority of multiple-family projects are approved within five to six months of project submittal, which includes the processing of environmental documentation. This process allows approvals to be secured without a public hearing.

For large projects requiring more design creativity, the City has adopted a PUD ordinance or Planned Residential Development Overlay to provide for more flexibility in design. This strategy was successfully employed for six blocks in the Downtown. The City also adopted a performance-based parking model that allows parking requirements to be based on the demand for parking rather than traditional, more rigid standards. This process has resulted in hundreds of new homes in Downtown Ontario.

Regulations Affecting Housing Supply

The City does not regulate short-term rentals and does not have inclusionary housing regulations. No other ordinances or regulatory tools in the city affect the cost and supply of housing.

Energy Conservation Opportunities

Rising energy costs, dependence on fossil fuels, and increasing evidence of the adverse impacts of global warming have provoked the need in California and nationwide to improve energy-management strategies. Buildings use 76 percent of all electricity generated in the United States for their operation and generate 40 percent of carbon dioxide, a major component of greenhouse gases (GHGs), which are primarily responsible for global climate change. How we design, build, and operate buildings thus has profound implications for energy use and resulting global warming.

Although the State of California has long supported energy conservation, recent state laws have been enacted to combat GHG emissions and increase energy independence. In 2006, the State Legislature adopted AB 32, the Global Warming Solutions Act of 2006, which created the first comprehensive, state regulatory program to reduce GHG emissions to 80 percent below 1990 levels by 2050. SB 1368 bars California energy providers from entering into long-term contracts with high-polluting power generators in an effort to encourage the development of the state's renewable energy portfolio.



Promoting energy conservation has become a consistent theme in regulations, green building practices, and general business operations. For Ontario, opportunities abound to promote energy-efficient practices in the siting, design, construction, and renovation of housing stock. These practices not only respond to regulatory requirements but also can generate significant community, environmental, and economic benefits.

In 2018, the City received a Transformative Climate Communities grant for its Ontario Together project, which included a wide range of workforce development, displacement avoidance, and community engagement strategies as well as sustainability projects. These projects included a Rooftop Solar Project, which will install 700 kilowatts of solar power on affordable multifamily developments and single-family homes and will provide paid solar installation internships to grow the renewable energy workforce. The project also intends to plant 365 street trees, which can reduce energy costs to adjacent buildings. As of April 2021, 360 street trees had been planted and 24 solar photovoltaic systems had been installed.

Neighborhood Design

Energy management is rarely a driving consideration for local, land use decision-making authorities. In fact, most land use frameworks—general plans, specific plans, zoning ordinances—do not provide sufficient language for these authorities to require developers to incorporate energy-efficient site planning. The Subdivision Map Act makes references to providing passive or natural heating or cooling opportunities, but no prescriptive guidance is provided. Accordingly, such site planning is often the result of individual developers who recognize the economic and marketing value of an energy-efficient community design.

Strategies to reduce energy demand begin with efficient site planning. Sizing and configuring lots to maximize a building's solar orientation (east-west alignment for southern exposure) facilitates optimal use of passive heating and cooling techniques. Infill development reduces potential energy costs of new infrastructure needed to service the site. Placing housing near jobs, services, and other amenities reduces energy consumption related to transportation. Other design strategies with beneficial energy implications include narrowing street widths to reduce the urban heat island effect, installing broad-canopied trees for shade, and clustering compact development to reduce automobile use.

Building Design

Title 24, Part 6, California Energy Efficiency Standards, requires all residential construction to meet minimum energy conservation standards through either a prescriptive or performance-based approach. The former

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approach requires each individual component of a building to meet an identified minimum energy requirement. The performance-based approach, on the other hand, allows developers to choose a range of measures, which, in totality, meet specified energy-conservation targets. With either of these options, mandatory components must still be installed, such as minimum insulation; heating, ventilation, and air conditioning (HVAC); and efficient water heating equipment.

In addition to California's Title 24 standards, all residential projects are subject to meeting the state building codes, which also include energy conservation standards. The California Building Standards Commission adopted the California Building Codes in 2008 based on "model" codes produced and updated periodically by various professional organizations. The City of Ontario has adopted these standards, which apply to all new residential buildings constructed after January 1, 2010. The City of Ontario enforces Title 24 as the primary means for ensuring new housing incorporates the latest energy-efficient technologies.

Green Standards Design

In 2010, California's Building Standards Commission adopted the California Green Building Code (CALGreen), making California the first state to adopt a uniform green building code. The City of Ontario has adopted the minimum standards of CALGreen to ensure energy efficiency, water conservation, material conservation and resource efficiency, and environmental quality are considered in all new buildings.

The building industry has developed different "green" building programs. The Building Industry Association sponsors a voluntary program called Green Builder. The program focuses on energy efficiency, water conservation, wood conservation, advanced ventilation, and waste diversion. Certified homes incorporate water-efficient landscaping and fixtures, use high-efficiency insulation and ventilation systems, contain environmentally sound building materials, initiate waste-reduction methods during construction, and exceed Title 24 Building Code energy standards by 15 percent.

Other green building programs have also been sponsored by other agencies. The United States Green Building Council (USGBC) sponsors another building certification program called Leadership in Energy and Environmental Design (LEED). The LEED program is a national rating system for green buildings that focuses on commercial and multiple-family residential projects. The USGBC reviews projects for conformance based on various efficiency, sustainability, materials quality, and design factors, and then issues certifications based on points achieved.



RESOURCES

Housing Resources

This section describes the land resources within Ontario that are available to address the City's existing and future housing needs, including its share of the Regional Housing Needs Assessment (RHNA).

Regional Housing Needs

The regional housing needs mandate requires every local government in California to plan for its "fair share" of the region's existing and future housing needs. The California Legislature has stated that housing availability is of vital statewide importance, as is the early attainment of decent housing and a suitable living environment for every Californian. State law, therefore, requires the California Department of Housing and Community Development (HCD) to develop housing needs projections for every region in California. As directed by state law, HCD estimates each region's existing and future needs every eight years; each eight-year period is called a housing cycle. For Ontario and the rest of the Southern California Association of Governments (SCAG) region, the upcoming housing cycle (the 6th cycle) plans for projected housing needs between 2021 and 2029.

For the 6th cycle, the SCAG region has been allocated more than 1.3 million housing units to be planned for by 2029. This estimate is 300 percent higher than the approximately 400,000 units required in the previous housing planning period (the 5th cycle, which covered 2013-2021). The large regional allocation results from underproduction of housing, rapidly increasing housing prices and rents, and increasing levels of housing overpayment among residents. As a result, local governments across the region must plan to accommodate an unprecedented increase in housing goals.

SCAG is responsible for dividing the 1.3-million-unit target between each of the 191 jurisdictions in the region. Under state law, SCAG must consider specific planning factors in allocating the required housing units between jurisdictions, but it can develop a tailored model for the region. SCAG's model for the 6th cycle considers the availability of land, adequacy of infrastructure and services, market demand for housing, fair housing, availability of employment and transit, local population growth estimates, and many other housing and planning considerations; the model, however, weights opportunities to promote fair housing, proximity to employment, and transit accessibility more heavily than other factors, so jurisdictions that were closer to jobs centers, had a lower concentration of poverty, and had more access to transit opportunities

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were generally allocated more units than those with poor jobs access and no public transit.

In Ontario, the RHNA goal has nearly doubled since the last Housing Element planning period. For the 2013-2021 planning period, Ontario's RHNA was 10,861 units; the allocation increased 92 percent to 20,854 units for the 2021-2029 period. Within this goal, the City must plan for housing production at three different income levels: lower-income housing (includes extremely low, very low, and low income), moderate income, and above moderate income. Table 5-1 summarizes the City's 2021–2029 RHNA. Ontario is required to set aside sufficient land, adopt programs, and provide funding (to the extent feasible) to facilitate and encourage housing production commensurate with that need.

Table 5-1
Ontario Regional Housing Need Allocation, 2021–2029

	Lower Income	Moderate Income	Above Moderate Income	Total Capacity	
RHNA Need	8,926	3,329	8,599	20,854	
Source: SCAG, 2021.					

California law holds local governments responsible for planning for their share of the region' housing needs and ensuring that housing is planned commensurate with the total assigned need for each affordability category. However, state law does not require a city to build housing; that is the responsibility of the building industry. Local governments can obtain credit toward meeting their RHNA target in four ways:

- Housing Production. Housing units built and occupied (received a certificate of occupancy) on or after June 30, 2021, when the projection period for the 6th cycle RHNA begins.
- Planned Production. Housing units proposed for construction that are likely to be approved and built during the planning period, from July 2021 to October 2029.
- Available Land. Designation of vacant and underutilized sites with zoning, development standards, services, and public facilities in place so housing can be built.
- **Alternative Credits**. Qualified projects that involve the rehabilitation, preservation, and conversion of non-affordable units to affordable units, subject to conditions.



The City's housing strategies use the first three options. The following section describes how the City will address its housing planning and production goals for the 2021-2029 RHNA.

Relationship of Zoning and Density to Housing Affordability

This section describes the suitability of residential development standards, namely allowed density and zoning districts, for facilitating housing affordable to different income levels.

Zoning and density standards are correlated to the affordability of housing. In general, higher-density housing projects, like apartment buildings, are more affordable than lower-density projects, like single-family homes. While other factors such as location, the size of the units, and quality of the finish materials also contribute to a project's ultimate affordability, the state has determined that density and zoning standards are suitable proxies for identifying the potential affordability level of a site.

The following sections describe the density thresholds identified for each income category and the current zoning districts that allow the identified densities.

Lower Income

Density

Housing element law requires jurisdictions to provide a requisite analysis showing that zones identified for lower-income households provide sufficient density to encourage such development. The law provides two options for preparing the analysis: (1) describe market demand and trends, financial feasibility, and recent development experience; (2) utilize default density standards deemed adequate to meet the appropriate zoning test. According to state law, the default density standard for Ontario is 30 dwelling units per acre. In 2019 and 2020, two 100-percent affordable housing projects were built in Ontario with densities between 25 and 30 dwelling units per acre:

- *Vista Verde*. Completed in 2020. Consists of 101 affordable units (lower income); built at a density of 26 dwelling units per acre.
- Emporia Place. Completed in 2019. Consists of 74 affordable units (lower income); built at a density of 26.7 dwelling units per acre.

Therefore, for the 2021-2029 planning period, the City has determined that the default density adequately demonstrates its capacity to accommodate the lower-income RHNA.

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Zoning

Under the current development standards, residential development within the following zoning districts allows for densities of 30 dwelling units per acre or more.

- Mixed-Use (MU-1) 20–75 dwelling units per acre
- *Mixed-Use (MU-2)* 14-40 dwelling units per acre
 - The City's Development Code allows for stand-alone multifamily residential projects in mixed-use districts.
- Planned Unit Development (PUDs) 25–75 dwelling units per acre
- High-Density Residential (HDR-45) 25.1–45 dwelling units per acre

The City's rezoning program (Program 13) will also create an affordable housing overlay that expands the land use categories that can accommodate at least 30 dwelling units per acre, exempt projects south of Riverside Drive with at least 25 percent of units affordable to lower-incomes from specific plan requirements if no specific plan exists, and revises existing specific plans to allow at least 30 dwelling units per acre on identified sites. Each of these changes will play a key role in fulfilling the City's lower-income RHNA.

Moderate Income

Density

Typically, medium to medium-high multiple-family zoning districts are suitable for facilitating the construction of housing affordable to moderate-income households. To determine the densities needed to accommodate moderate-income housing, nearly 2,000 properties sold or listed on the MLS between September 2020 and September 2021 were analyzed. Of the 2,000 records analyzed, 66 were newer construction (built after 2017) townhomes or condos, most of which were built at densities between 8 and 18 dwelling units per acre. Of those, 10 properties sold for less than \$410,000, the moderate-income threshold, indicating the densities were sufficient to produce moderate-income housing. These sales included four new construction homes in the New Haven master-planned community (two two-bedroom units and two three-bedroom units), where projects are typically 18 units per acre or less.

In recognition of increasing home prices, however, the City has determined that a higher density of 25 units per acre would be more conducive to creating moderate-income housing. In support of this, the



Cities of Chino, Fontana, and Rancho Cucamonga indicate that masterplanned communities with densities up to 25 dwelling units per acre offer homes that are affordable to moderate-income households. Therefore, the City's land inventory will focus on sites that allow for 25 or more dwelling units per acre to fulfill its moderate-income allocation.

Zoning

The same zoning districts identified as suitable for lower-income housing are suitable for moderate-income housing, as well as the following:

- Medium-Density Residential (MDR-25) 18.1 25 dwelling units per acre
- Specific Plan (SP) Sites designated as SP, where no specific plan exists, can facilitate moderate-income housing if the corresponding Policy Plan designation allows densities up to 25 dwelling units per acre. Such Policy Plan designations include:
 - *Medium-Density Residential (MDR)* 11 to 25 dwelling units per acre
 - Mixed-Use (MU) current and proposed designations for various mixed-use areas allow a range of densities with maximums ranging from 25 to 125 dwelling units per acre.

Above Moderate Income

Density and Zoning

The planned residential development projects in the city (see Table 5-2) include a mix of housing types at various densities. Most, however, are anticipated to develop at market rates that are affordable to above-moderate income households. Therefore, it is assumed that any density and any residentially zoned land can support the development of above-moderate housing.

Housing Production and Planned Production

This section details residential development projects currently in the pipeline.

The City of Ontario has several approved residential projects that are anticipated to be built and receive their certificate of occupancy or final inspection on or after June 30, 2021. As allowed under state housing element law, these approved and pending residential projects will be eligible for credit toward the 6th cycle RHNA.

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As described here and listed in Table 5-2, housing pipeline projects represent a mix of residential types, price points, and affordability levels.

- Mix of Housing Types. A broad range of housing products is in the development pipeline, including single-family residential projects, condominiums, townhomes, clustered and motor court homes, and apartment projects. Proposed projects are located on vacant land throughout the city, within existing residential neighborhoods, in underutilized mixed-use areas, and on undeveloped land south of Riverside Drive and east of the Cucamonga Channel (the Channel), where the area is transitioning from agricultural uses to residential communities.
- Project affordability. A project's affordability is based on the type
 of residential product approved/entitled in the community and
 the market sales price or rent charged for recently built projects in
 the city.
- Mix of Vacant and Underused Sites. Similar to the land inventory discussed later, proposed projects are situated on a combination of vacant and underused sites. Many included the subdivision of large lots or the development of a range of products, including multifamily housing, on large lots over 10 acres in size. Further, prior projects that have been built in the city have been within underutilized lots.
- Mix of Housing Locations. The City has approved proposals or issued a certificate of occupancy after July 1, 2021, to develop 1,650 units. While the greatest volume of housing development is in the area south of Riverside Drive and east of the Channel, projects have also been approved in the downtown area, along commercial corridors like Holt Boulevard, within the mixed-use areas along the Interstate (I-) 10 corridor, and scattered throughout established neighborhoods.

Only projects that have been approved for development are included in the pipeline projections. Several projects are currently under review that would add thousands of additional units to the city, but these have not been included. Table 5-2, therefore, represents a conservative estimate of the planned projects.



Table 5-2 Residential Projects in the Pipeline

ID Project Name	Durate at Manage	Accessed Devel Number (ADA)	Туре*	Total Units	Affordability		
ID	Project Name	Assessor's Parcel Number (APN)			Lower	Mod.	Above Mod.
1	Misc. Projects Downtown	104837612, 104839431	SF	2	-	-	2
2	Misc. Projects Between Mission Blvd and Riverside Dr	104955121, 105034106, 105034162, 104931214, 104953154		5	-	-	5
3	West Holt Multi-family	101055106	MF	6	-	-	6
4	Mission & Palmetto	101138204	MF	68	-	-	68
5	890 South Magnolia	101137115-16	MF	49	-	-	49
6	Meredith	11031152, -53	MF	464	-	-	464
7	Piemonte	21020440	MF	22	-	-	22
8	Esperanza	21830203-04, 21830301-17, 21830326-57, 21832201-04, 21832312-33, 21832362-65, 21858501-27, 21858638-41, 21872301-11, 21872339-40, 21872349, 21872401-40, 21872442	SF	200	-	-	200
9	Rich-Haven	21801602-03, 21816115, 21826229-30, 21826235-37, 21826239	SF	60	-	-	60
9	Nicii-i laveii	21801601, 21801604-05, 21809303, 21816115, 21821102, 21826206-07, 21839303-04, 21839308-09	MF	271	-	-	271
10	Subarea 29	107339105-17, 107339129-35, 107339209-12, 107339243-53, 107340214-26, 107340228-30, 107340244-47, 21801501-05, 21801507-16, 21801529-47, 21801549-54, 21801562-64, 21827123, 21833153-54, 21867448-50, 21899125-33, 21899221-27, 21899232-53, 21899331-32, 21899334-42, 21899405-10, 21899477-84, 107339118, 107339126-28, 107339213-15	SF	325	-	-	325
11	The Avenue	21865227	SF	11	-	-	11
12	West Haven	21815111, 21815138	SF	129	-	-	129
13	Harlow Ln	107339216-27, 107340231-38	SF	20	-	-	20
14	Hazel St	107340110-27	SF	18	-	-	18

Source: City of Ontario, 2021.

Notes: The projects included in the table include permits that have been issued but not completed and certificates of occupancy issued on or after 6/30/2021. There are also several projects currently under review that are not included in this table.

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^{*} SF = Single-Family, MF = Multifamily



Ontario's 2021-2029 RHNA includes 8,599 units for the above-moderate income allocation (Table 5-1). As shown in Table 5-2, pipeline projects can address 1,650 units of the above-moderate allocation.

The remainder of this chapter will focus on key strategies that the City will undertake to address the remainder of the above-moderate allocation as well as the lower- and moderate-income RHNA.

2021-2029 RHNA Strategies

While the prior section listed residential projects approved for development in the city, this section describes additional strategies proposed to address the remaining RHNA for the 2021-2029 Housing Element.

Housing Opportunity Areas

The City is required to identify available sites to accommodate its 2021–2029 RHNA. An analysis of the city's available land and existing zoning districts showed that there was not enough land zoned at the correct densities to facilitate the development of enough housing affordable to lower- and moderate-income households to fulfill the RHNA and a rezoning program (Program 13) was needed. The City evaluated all of its land resources for suitability to develop at densities suitable to lower- and moderate-income housing and developed strategies and programs to meet its obligation.

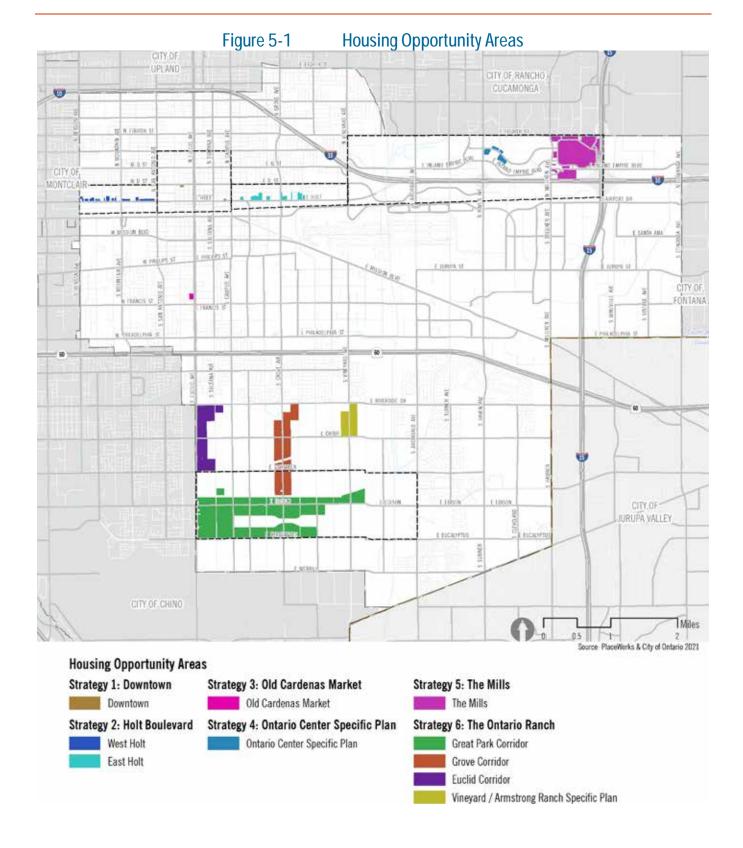
A detailed list of each site by Assessor's Parcel Number (APN) is included in Appendix B.

The analysis identified 10 areas in which housing growth can be accommodated after the City implements its rezoning program (Program 13), grouped into six key strategies, illustrated in Figure 5-1, *Housing Opportunity Areas*. In addition to the Opportunity Areas, the City used accessory dwelling units (ADUs) as the seventh strategy to fulfill the RHNA. The sites identified in the Opportunity Areas further the City's housing goals and align with regional transportation plans by placing higher-density housing along future transit routes. The RHNA strategies also consider fair and equitable housing goals, development feasibility, and infrastructure availability.

Figure 5-1 shows the identified sites categorized into 10 Housing Opportunity Areas and six strategies based on their location within the city.







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Underutilized Sites

The City has included non-vacant sites in its land inventory to meet a significant portion of its regional housing need. These sites are underutilized, and many are located in the undeveloped area south of Riverside Drive and east of the Cucamonga Channel, where the primary use is agricultural; other underutilized sites, like those located in the Downtown area, along Holt Boulevard, and at the old Cardenas market, are parking lots or aging and underused commercial properties that are suitable for residential development and are ready to turn over; finally, the land inventory includes the parking lots and several out parcels around the Ontario Mills Mall, reflecting a national trend of repositioning shopping centers as mixed-use communities.

While the city has seen immense growth in the past few decades, the City is continuously expanding the opportunity for new housing development in response to market demand, developer interest, and state housing law. Interest from property owners and local developers is very high, and projects at densities of 50+ units per acre were recently approved. In 2021, it was estimated that building applications under review would add more than 5,000 dwelling units to the city. Several of the approved projects and projects under review include high-density apartments and condos. This indicates a strong market for higher-density housing products in the inland empire.

To demonstrate a realistic capacity throughout its inventory, but particularly on the underutilized sites, the City calculated the total housing capacity on each site using a realistic density below the maximum allowed by the current or proposed zoning.

Strategy 1: Downtown Housing Opportunity Area

The City developed the Downtown District Plan, as described in Program 8, which provides opportunities for high-density, market-rate, and affordable housing in the Euclid Avenue Entertainment District and Holt Boulevard District. Projects under review, like the Hutton C-Block Mixed-Use development that will bring ground-floor commercial with high-density housing and structured parking to the heart of downtown, and recently completed projects, like the Emporia affordable housing project, embody the vision for housing in the downtown.

Downtown is well-served by infrastructure. The City is also working to build OntarioNet, a new fiber-optic internet service throughout the city. The first fiber lines serving Ontario have already been installed in the areas around downtown, and there are no environmental constraints limiting development potential.



The City has identified two sites in addition to the pipeline projects for inclusion in the land inventory, as shown in Figure 5-2.



Figure 5-2 Downtown Housing Opportunity Area

The two sites, numbered in Figure 5-2, are described below.

- 1. The first site, adjacent to D Street, is zoned for HDR-45, which allows up to 45 dwelling units per acre, satisfying the default density requirements for lower-income housing. This site is currently an underutilized parking lot, which was included in the City's last housing element. In compliance with Assembly Bill (AB) 1397, by-right development will be allowed for any residential project that provides for at least 20 percent of units affordable to lower-income households as described in Program 15. For conservative estimates, the realistic density used to determine the total housing capacity of the site is 25 units per acre.
- The second site, along Holt Boulevard, is currently designated as Neighborhood Commercial (CN), but it will be part of the City's rezoning program (Program 13) with a proposed designation of MU-1, which allows up to 75 dwelling units per acre and is suitable for lower-income housing. This site includes three parcels with consistent ownership, and it is currently operating a used car sales lot. Car lots provide a unique opportunity for redevelopment because limited demolition is necessary; as a result, developers can often afford to pay higher land costs. Furthermore, this lot is currently occupied by an independent car sales operation, so strategic decisions by major corporations would not be necessary. The housing plan also includes a program to encourage lot consolidation along Holt Boulevard (Program 10) and facilitate the development of housing affordable to lower-income residents. To account for potential nonresidential development, only 60 percent of the total land area was assumed suitable for housing. In addition, a realistic density of 35

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dwelling units per acre was used to determine the total housing capacity.

On both sites, only 50 percent of the total realistic capacity was estimated to have the potential to accommodate lower incomes.

Using the metrics described above, the Downtown Opportunity Area has a total housing capacity of 40 units, half of which (20 units) have the capacity to accommodate lower-income housing. The balance (20 units) is presumed to have capacity for housing affordable to moderate-income households.

Strategy 2: Holt Boulevard Opportunity Areas (West and East)

Holt Boulevard is one of the city's original thoroughfares, extending through Ontario and connecting to neighboring communities. It is also an important regional transit corridor. Existing bus lines along Holt Boulevard report some of the highest ridership rates in San Bernardino County. In addition, the San Bernardino County Transportation Authority (SBCTA) plans to provide new bus rapid transit (BRT) service along Holt Boulevard as part of the West Valley Connector Project. Those portions of the project that will serve Holt Boulevard are funded and scheduled for operation in 2023.

Like Downtown, Holt Boulevard is well-served by infrastructure. The development in these areas would constitute urban infill projects on already disturbed land, so there are no environmental constraints limiting development potential.

West Holt Boulevard

The City has identified 36 parcels (18.3 acres) along West Holt Boulevard for inclusion in the land inventory, as shown in Figure 5-3 and described below.



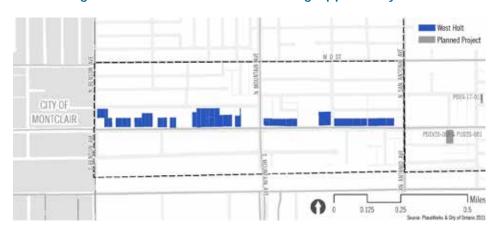


Figure 5-3 West Holt Housing Opportunity Area

Sites along West Holt Boulevard were identified based on proximity to transit, potential for redevelopment, and current zoning.

Approximately 70 percent of the land area identified for lower- and moderate-income housing along West Holt Boulevard is within a 5-minute walk of a planned BRT stop; 100 percent of sites are within a 10-minute walk of a planned BRT stop.

Existing uses include parking lots, aging commercial properties (retail and dining establishments), and used car sales lots. These commercial properties are underperforming and surrounded by residential uses. In general, typical commercial properties are leased on three- to five-year terms. It is unlikely that existing tenants in this area would have lease terms longer than industry standards. Car sales lots, on the other hand, are largely undeveloped, which means the cost and effort required to transition to residential uses are reduced as less site preparation will be necessary.

Existing tenants along West Holt Boulevard are also small independent uses, so redevelopment of these properties would not require strategic decisions by major corporations.

There are 27 parcels (12.8 acres) within the Opportunity Area currently zoned as HDR-45, which satisfies the default density requirements for lower-income housing and is sufficient to support moderate-income housing as well. Several of these parcels were also included in the City's prior housing element (see a detailed list by APN in Appendix B). To further encourage residential development on parcels listed in the last housing element and to comply with AB 1397, by-right development will be allowed for any residential project that includes at least 20 percent of units affordable to lower-income households, as described in Program 15.

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Two parcels (1.5 acres), currently zoned as Community Commercial (CC), are part of the City's rezoning program (Program 13). The City proposes changing the designation to MU-2 with an Affordable Housing Overlay (described in Program 13), which will allow a density range of 20-40 dwelling units per acre and is suitable for lower- and moderate-income housing.

Five parcels (4.0 acres) are currently zoned as MDR-25. These will be included in the City's rezoning program (Program 13) with a proposed designation of HDR-45, which allows up to 45 dwelling units per acre and is suitable for both lower- and moderate-income housing. These sites were used to meet the City's moderate-income RHNA in the prior housing element. Like other sites reused from the prior element, by-right development will be allowed for any residential project that includes at least 20 percent of units affordable to lower-income households as described in Program 15.

All but two sites located in the East Holt Opportunity Area have an existing or proposed designation of HDR-45. The HDR-45 zoning designation allows for multifamily residential projects by-right and densities up to 45 dwelling units per acre. To ensure conservative estimates, however, a realistic density of 25 dwelling units per acre is used.

On the two sites with a proposed designation of MU-2, 75 percent of the total land area is assumed suitable for housing to account for the potential development of non-residential uses while also recognizing that 100-percent residential projects are allowed in the zone. Additionally, a realistic density of 30 dwelling units per acre is used to determine the total housing capacity.

On all sites, only 50 percent of the total realistic capacity was estimated to have the potential to accommodate lower incomes.

Using the metrics described above, the West Holt Opportunity Area has a total housing capacity of 454 units, half of which (227 units) have the capacity to accommodate lower-income housing. The balance (227 units) is presumed to have capacity for housing affordable to moderate-income households.

East Holt

The City has identified 21 parcels (22.2 acres) for inclusion in the land inventory, as shown in Figure 5-4 and described below.





Figure 5-4 East Holt Housing Opportunity Area

Like West Holt Boulevard, sites along East Holt Boulevard were identified based on proximity to transit, potential for redevelopment, and current zoning.

Approximately half of the land area identified for lower- and moderate-income housing in the West Holt Boulevard Opportunity Area is within a 5-minute walk of a planned BRT stop; 100 percent of sites are within a 10-minute walk of a planned BRT stop.

Nine parcels (11 acres) within the Opportunity Area are currently vacant. Of those, five parcels (5.1 acres) are zoned MU-2, which allows 14-40 dwelling units per acre, a density sufficient to support the development of lower- and moderate-income housing. This is evidenced by the Vista Verde affordable housing project, which was developed in the MU-2 district along East Holt Boulevard as a 100-percent affordable housing project, demonstrating that this zone and area can support affordable housing. The remaining four vacant parcels (5.9 acres) are zoned for Business Park (BP) and will be included in the City's rezoning program (Program 13), with a proposed designation of MU-2 with an Affordable Housing Overlay (described in Program 13), which will allow a density range of 20-40 dwelling units per acre.

The balance of the sites identified in the Opportunity Area (11 parcels, 11.2 acres) are currently underutilized and designated as MU-2. Because the current zoning designation allows sufficient density (14-40 dwelling units per acre), redevelopment of these sites could accommodate housing affordable to lower- and moderate-income households without rezoning. The potential for redevelopment on each non-vacant site, numbered by parcel in Figure 5-4, is described below.

1-2. Between Campus Avenue and Allyn Avenue, two parcels (1.5 acres) are identified to have the potential for redevelopment. The first is a used car sales lot, which has potential for

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redevelopment because of the low existing floor-area ratio (FAR) (very small building and large parking area) and the age of the building. The demolition required for redevelopment would be minimal. Adjacent to this is a parcel (0.91 acres) where service commercial uses, including a laundromat and automotive shop, have been identified as candidates for redevelopment due to the age of the buildings (50+ years old) nearing the end of a typical lifespan for commercial buildings. Tenants on both sites are independent operators, so redevelopment of these properties would not require strategic decisions by major corporations.

- 3. Traveling east on Holt Boulevard, another parcel (0.90 acres), just east of the Allyn Avenue intersection, has also been identified to have the potential for redevelopment due to the low existing FAR (large parking area and small building), the age of the structure (48 years old), and because an independent retail use occupies the site.
- 4-6. Continuing east on Holt Boulevard, on the other side of three vacant sites included in the inventory, sit three parcels (4.5 acres), with potential for redevelopment due to low existing FAR, the age of existing structures, and because the sites are currently used by an independent automotive parts retailer and an independent automotive repair shop, but residential uses border the auto-centric services. Converting these sites to housing would improve the land use compatibility for surrounding neighborhoods and mobile home parks.
- 7-9. Further east along Holt Boulevard, three sites (1.8 acres) are identified as candidates for redevelopment. The first two properties include an unused parking lot and a boarded-up identified church. were These sites for redevelopment because of the age of the structure (nearly 100 years old), the poor state of repair of the building, and because the two sites are not currently being used. An automotive repair shop occupies the third parcel, but it was identified as a candidate for redevelopment because it is adjacent to residential uses and has a low existing FAR.
- 10-11. East of the Grove Avenue intersection sits two sites (2.4 acres), each with an existing motel. These sites were identified because both are older, underperforming, budget motels that could be converted into permanent supportive housing or demolished and rebuilt with housing affordable to lower- and moderate-income households.



All of the sites in the East Holt Opportunity Area have either an existing or proposed designation of MU-2. The MU-2 zoning designation allows for 100-percent residential projects, but it also provides for various non-residential uses. To account for the potential development of non-residential uses on the identified sites, 75 percent of the land area for each site is considered in estimating the total housing capacity. Seventy-five percent is estimated because 100-percent residential projects are allowed by the zone, and residential projects in the area do not typically include non-residential uses as well. To further ensure conservative estimates, a realistic density of 30 dwelling units per acre is factored into the total housing capacity of 499 units. Of those, 50 percent of units (250 units) have the capacity to accommodate lower-income housing, while the remaining half (249 units) are presumed to have capacity for housing affordable to moderate-income households.

Strategy 3: Old Cardenas Market Housing Opportunity Area

The Old Cardenas Market site consists of three parcels (3.5 acres) that front Euclid Avenue, as shown in Figure 5-5 and described below.

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W FRANCES ST.

W FRANCES ST.

W FRANCES ST.

W MINING ST.

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W MINING ST.

Figure 5-5 Old Cardenas Market Housing Opportunity Area

The site includes the parking lot behind the gas station, the vacant building that previously housed Cardenas Market and its parking area, and the vacant lot north of the market.

Like Downtown and Holt Boulevard, this site is well-served by infrastructure, and there are no environmental constraints limiting development potential.

All three parcels are currently zoned CN but will be included in the City's rezoning program. The two vacant parcels are proposed to change to HDR-45, while the Cardenas Market is proposed as a mixed-use designation that allows for a density range of 20-30+ dwelling units per acre.

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Parcels with the proposed HDR-designation estimated capacity using a realistic density of 25 dwelling units per acre.

The mixed-use parcel will allow the gas station and a quick-serve restaurant currently operating to remain while also facilitating redevelopment of the site. In acknowledgment of competing non-residential demands on the land, housing is only estimated on 50 percent of the site. Additionally, a realistic density of 30 dwelling units per acre is applied to determine the total housing capacity.

On all sites, only 50 percent of the total realistic capacity was estimated to have the potential to accommodate lower incomes.

Using the metrics described above, the Old Cardenas Market Opportunity Area has a total capacity of 65 units. Only half (33 units) have the capacity to accommodate lower-income housing. The remaining 32 units are presumed to have capacity for housing affordable to moderate-income households.

Strategy 4: Ontario Center Specific Plan Housing Opportunity Area

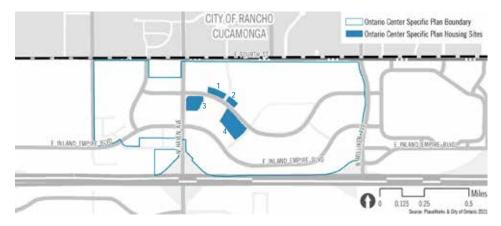
The Ontario Center Specific Plan offers a variety of commercial, retail, entertainment, light industrial, and office uses, as well as a mix of housing opportunities, and outlines a detailed infrastructure plan for the area. Originally adopted in 1981, the Specific Plan has been amended numerous times, including the addition of the Piemonte Overlay in 2006. The most recent amendment was adopted in 2017.

Since much of the Specific Plan area has already been developed, the necessary infrastructure to support housing development is already in place. The remaining vacant parcels are prime candidates for development, and no environmental constraints limit development potential.

After conversations with property owners and developers, the City identified four vacant parcels (16.5 acres) as potential sites for lower- and moderate-income housing development, as shown in Figure 5-6.



Figure 5-6 Ontario Center Specific Plan Housing Opportunity Area



An assessment of the housing capacity and potential affordability for each site, numbered by parcel in Figure 5-6, is described below.

- 1-2. Two of the housing sites (3.6 acres) are regulated by the Piemonte Overlay. In Piemonte, both sites are designated as Office with Residential Development Allowed. While the Piemonte Overlay does not specify allowable densities, it plans for 791 new units, 220 of which could be developed on the two identified housing sites. If 220 units were to develop across the 3.6 acres, the resulting density would be 61 dwelling units per acre, which is sufficient to facilitate the development of lower-and moderate-income housing. Only half of the potential buildout, 110 total units, were projected for housing on these sites. Of those, 55 units were estimated to be affordable to lower-income households, and 55 were counted toward the moderate-income RHNA.
- 3-4. The remaining two sites (12.9 acres) are governed by the Ontario Center Specific Plan (OCSP). Both sites are designated as Urban Commercial, which allows for a mix of tourist-related commercial uses, high- and medium-rise office buildings, entertainment/recreation clusters, and high-density residential uses above the ground floor. While the specific plan does not specify allowable densities, the sites are included in planning area 11, where the maximum building height of 95 feet is regulated only by the height limits set by the Ontario International Airport. Housing products with densities above 60 dwelling units per acre are regularly developed under similar zoning standards, indicating the types of projects that could develop under the specific plan would meet the density thresholds necessary to facilitate the development of lower- and moderate-income housing.

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The specific plan regulates the maximum capacity for Urban Commercial uses based on building square footage rather than dwelling units. As such, the City uses the methodology described to translate the remaining allowable square footage permitted under the current specific plan into an approximate number of units.

Planning area 11 allows a total of 592,700 square feet; it is estimated that 360,455 square feet have already been developed, leaving 232,245 square feet of developable building area. To convert the remaining square footage into potential housing units, the City assumes the residential component would include a range of unit sizes with an average size of 1,000 square feet, inclusive of common areas. Using this factor and reserving approximately 39,000 square feet for ground-floor uses, a total of 193 units are estimated as the total realistic capacity of the two sites.

Of the 193 units, 96 are estimated to be affordable to lower-income households, and 97 are estimated to be affordable to moderate-income households.

Strategy 5: The Mills Housing Opportunity Area

The Ontario Mills Mall and surrounding areas are governed by The California Commerce Center North Specific Plan (The Mills). The specific plan was originally authored in 1992 and has not been amended since. The document envisioned a regional commercial center focused on value-oriented retail and jobs creation that would capitalize on the proximity to the freeways and serve as a local landmark. The Mills has largely achieved this vision, and it remains a high-performing retail hub today (2021), but the economic outlook for retail and office uses in 2021 is vastly different than it was in 1992.

Current trends in the redevelopment of retail centers began with the introduction of online retail, which is currently capturing 20 percent of every retail dollar spent in the US economy. Recent industry reports (Barclay's Bank in October 2020 and Coresight Research in July 2020) find that the current number of retailers will likely be substantially reduced by 2030, with predictions by Coresight that online retail will account for 40 percent of retail sales by 2030. The COVID-19 pandemic has only accelerated the past trends. As retail stores opt to relocate or retail properties become due for major reinvestments/improvements, property owners find few retailers looking to replace existing tenants or find greater value in developing new residential uses. The demand for office space is likewise expected to remain stagnant through the 2021-2029



planning period as more office workers choose to work from home in the wake of the COVID-19 pandemic.

While retail uses, in general, are in decline, the Ontario Mills Mall remains a high-performing shopping center. It is anticipated that much of the existing retail square footage within the mall itself will remain through 2029, while parking areas will intensify with residential uses, and several of the out parcels will redevelop with new uses as existing restaurants, movie theaters, and retailers consolidate locations, relocate, or otherwise reconsider their physical footprint.

Successful shopping center redevelopment efforts, like the Bella Terra Mall in Huntington Beach, show high-performing malls successfully introducing residential uses while retaining and improving their commercial cores.

In addition to this, the Mills area already has the necessary infrastructure in place to support housing development, and no environmental constraints will preclude housing production.

Considering these trends, the City has identified the parking areas and several out parcels around the Ontario Mills Mall as potential redevelopment sites suitable for housing, as shown in Figure 5-7.

CITY OF Carifornia Commerce Center North Specific Plan (The Mills) Boundary
CUCAN
The Mils Housing Sites

E. RURTIN CT

E. HILLAND, EMPIRE, BLVD

Figure 5-7 The Mills Housing Opportunity Area

The existing specific plan does not allow for residential uses, but the City's Policy Plan, last updated in 2010, provides for housing in this area with a density range of 25 to 85 dwelling units per acre. This range would provide sufficient density to allow for the development of housing affordable to lower- and moderate-income households. In addition to establishing the density range, the Policy Plan sets a maximum of 437 units in its buildout table LU-03. As part of its rezoning program (Program 13), the City will update the Policy Plan to increase the

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residential development maximum in the Ontario Mills Mixed-Use area, and it will amend the specific plan consistent with the Policy Plan.

There are 35 parcels (195.5 acres) identified for redevelopment. Collectively, this area could yield up to 16,620 units if the entirety were to develop at the maximum density of 85 dwelling units per acre. However, the City's housing strategy recognizes that many retail properties are expected to remain, with development occurring primarily in the parking areas and on several out parcels. As such, only 40 percent of the land area for each site (78.2 acres total) is estimated as suitable for housing, and a realistic density of 40 dwelling units per acre is used to establish the total development potential of 3,128 units.

AB 1397 requires additional justification to include parcels larger than 10 acres in the sites inventory for lower-income housing. Four of the parcels identified in this area (145.0 acres) are larger than 10 acres. The City has included these sites because larger sites with consistent ownership typically create fewer barriers to the introduction of housing in mall redevelopment projects than smaller sites with many competing landowner interests. Redevelopment efforts for the Westminster Mall, in Westminster, CA, for example, have involved a considerable planning effort to balance the various interests of many disparate property owners. Further, proposed redevelopment plans for malls such as Main Place Mall in Santa Ana, CA, incorporate larger parcels.

Only 50 percent of units (1,564) have the capacity to accommodate lower-income housing, while the balance is presumed to be split between moderate-income (782 units) and above moderate-income (782 units).

Strategy 6: The Ontario Ranch Housing Opportunity Area

The area south of Riverside Drive and west of the Cucamonga Channel is largely undeveloped and represents one of the few remaining greenfield opportunities in the Inland Empire. Upon annexation in 1997, the City envisioned the area as an extension of the existing urban fabric. Residential neighborhoods would be balanced by mixed-use, commercial, and public places and organized around a regional-scale park. Twenty-five years later, that vision has begun to take shape with new development east of the Channel. Over the 2021-2029 planning period, development is expected to spread to the western side of the Channel as the infrastructure becomes available. The City's housing strategies for this area promote the creation of mixed-income communities in the areas west of the Channel while also considering the development constraints associated with greenfield development.



Infrastructure Availability

Backbone infrastructure serving the area west of the Channel is planned. In 2020 and 2021, several large industrial projects were approved, and additional projects are under review along the city's southern border between Merrill and Eucalyptus Avenues. These projects are bringing water and sewer infrastructure to this portion of the city. The initial trunk line, expected to develop in 2022, will travel along Merrill Avenue between Euclid and Walker Avenues. From there, additional sewer lines are planned to extend north along Euclid, Bon View, Grove, and Walker Avenues, with the southern-most sites expected to be first served.

Extension of the city's backbone water infrastructure is also planned throughout the western part of the Ranch in conjunction with the approved industrial projects. Water lines, with an anticipated installation date of 2022, will serve the approved industrial projects and introduce backbone infrastructure throughout the area west of the Channel. While water lines will be installed more extensively throughout the area, service is generally anticipated to follow the extension of the sewer lines, as the backbone sewer infrastructure is more complicated and costly to install.

Similarly, new roadways, recycled water lines, storm drains, and dry utilities are expected to expand at pace with the sewer infrastructure as new development is established.

The sites selected and assumptions applied were developed after discussions with the Ontario Metropolitan Utilities Company (OMUC), taking into consideration the cost and phasing needed to ensure that housing could be developed and served by infrastructure during the 2021-2029 planning period.

The City's housing strategy accounts for the need to extend infrastructure throughout the area by applying a reduction factor based on the distance of sites from the initial trunk line and estimated timeline for infrastructure availability. The applicable reduction factors are detailed under each Opportunity Area.

Environmental Constraints

The sites identified in this portion of the city have no topographical, slope, flood, or fire hazards. A small portion of sites in the Great Park Corridor is vulnerable to liquefaction. The California Building Code provides standards on soils and foundations to ensure new development mitigates the risks of liquefaction zones. Projects within the liquefaction zone are under development.

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Zoning

The current zoning throughout the area west of the Channel is Specific Plan with an Agriculture Overlay (SP-AG).

The Agriculture Overlay District allows for existing agricultural uses to continue operation on an interim basis until development, consistent with the Policy Plan and zoning district, is slated to occur.

The SP district is intended to accommodate the adoption of specific plans that are consistent with the Policy Plan, but the district itself does not set any density or intensity standards when no specific plan exists. With a few exceptions, most parcels in this part of the city are not yet affected by an existing specific plan. However, the Policy Plan specifies land uses and densities throughout the city, including this area. Because both the zoning district and state law require consistency between the City's Policy Plan and zoning, any proposed specific plan must be consistent with the Policy Plan land uses. Therefore, the City's housing strategies use Policy Plan land use categories, including density standards, to estimate the development capacity and affordability potential for sites throughout this portion of the city.

The City's strategy for this area encourages the creation of new mixedincome communities and the integration of affordable housing with new development. As part of this effort, the City will create an Affordable Housing Overlay (AH) Zone. The overlay, described in Program 13, will be applied to all parcels identified in the land inventory south of Riverside Drive. It will establish a minimum density of 20 dwelling units per acre and provide special standards for affordable housing projects where at least 25 percent of the proposed units are affordable to lowerincome households. Provisions for affordable projects would exempt them from the specific plan requirement on parcels that are not already affected by a specific plan. Instead, the developer could apply the development standards for a zone that implements the current or proposed Policy Plan designation. In addition, the overlay would increase the maximum density for parcels with a Policy Plan designation of Medium-Density Residential (MDR) from 25 to 30 dwelling units per acre if the project provided 25 percent of units at a rate affordable to lower-income households. The higher density allowed would effectively increase the base density of the designation and would be applied prior to any state density bonus provisions.

With these changes, any site in the Affordable Housing Overlay District with a Policy Plan designation that allows at least 30 dwelling units per acre (including MDR, as revised) will satisfy the default density requirements and be considered suitable for the development of lower-income housing.



Great Park Corridor

Envisioned as the organizing element for new development and imagined as a focal point for the region, the Ontario Great Park is planned to encompass approximately 340 acres between Campus and Haven Avenues, with its western leg terminating in the Great Park Corridor Housing Opportunity Area. The extent of the Opportunity Area are shown in Figure 5-8 and described in the text that follows.

Figure 5-8 Great Park Corridor Housing Opportunity Area



Sites that comprise the Great Park Corridor Opportunity Area were identified based on proximity to open space and transit, potential for redevelopment, and infrastructure availability.

The Great Park Corridor was identified for higher-density housing development because of its proximity to planned open space, which furthers the City's fair housing goals, and because SBCTA has plans to expand a BRT route along Edison Avenue/Ontario Ranch Road. While potential BRT stops have not yet been identified, rapid transit stops are typically located one-half mile to one mile apart. The City currently envisions (subject to coordination with SBCTA) future transit stops placed at Euclid, Bon View, Grove, Walker, and Vineyard Avenues, each approximately half a mile apart. In that case, roughly 55 percent of the land area identified in the Great Park Corridor Opportunity Area will be within a 5-minute walk of a BRT; nearly 100 percent of the sites will fall within a 10-minute walk of the same BRT stops.

The Opportunity Area consists of 77 parcels (600.6 acres) that are identified as suitable for housing development. Areas that fall within Chino Airport safety zones 1-4, where residential development is limited, have been excluded. Parcels where the property owner or developer has already approached the City with development proposals have also been excluded. In addition, parcels smaller than half an acre are excluded without adding further justification that would otherwise be required by state law enacted through AB1397. Such small sites are insignificant in

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terms of number (of parcels) and total acreage. All such parcels are only excluded due to size, and none of the parcels would inhibit the development of any parcels listed in the inventory (most are owned by the same owners and would be developed along with larger adjacent parcels). Finally, parcels with multiple Policy Plan land use designations have been divided, so only those portions of the parcel deemed suitable for housing development are included. Housing capacity assumptions have been adjusted to reflect only the portion of the parcel included in the inventory.

Several of these sites continue to house the agricultural operations that once dominated this portion of Ontario, but many sites have already begun to transition to interim uses such as truck parking and open storage. There are approximately 28 existing homes that were built in conjunction with other agricultural structures spread across the 600 acres. Due to the extremely low density of housing and the nature of development anticipated, the existing residential uses are not expected to impede development potential. Land with very similar conditions to the sites identified in this area has recently developed on the eastern side of the Cucamonga Channel, forming new residential neighborhoods. The same trend is expected to continue westward as infrastructure expands.

AB1397 requires additional justification to include parcels larger than 10 acres in the sites inventory for lower-income housing. Ten of the parcels identified in this area (183.4 acres) are larger than 10 acres. The City has included these sites because Ontario has ample recent evidence of large greenfield sites developing as housing. Areas east of the Channel had nearly identical parcel patterns to those in this area. Many properties to the east have already been subdivided and developed as housing, and the undeveloped land east of the Channel is expected to develop likewise. Other large urban greenfield projects, such as Irvine's Great Park neighborhood, reflect the same pattern of subdividing larger parcels to create mixed-income communities. Several of the communities in Irvine's Great Park neighborhood include multifamily affordable housing, indicating that this process can produce opportunities for lower-income housing. The Affordable Housing Overlay District that will be applied to all parcels in the Opportunity Area (Program 13) also provides incentives to promote housing affordable to lower-income households.

The sites that comprise the Great Park Opportunity Area are located across Eucalyptus Avenue from the industrial projects that are bringing infrastructure to the area, so this Opportunity Area will have the earliest access to water, sewer, and other utilities.



To account for the need to extend infrastructure and to address the nine larger parcels, only 50 percent of the realistic development capacity in this area is estimated to be affordable to lower-income households. The balance of the capacity is split between moderate-income housing and above moderate-income housing. It is assumed that the above moderate-income housing will help create integrated mixed-income communities and combine with non-residential uses to help finance any necessary subdivision and expansion of infrastructure.

As previously discussed, all sites in this area are currently zoned SP-AG, and they will be rezoned to SP-AG-AH for inclusion in the Affordable Housing Overlay District (Program 13). The Agriculture Overlay District will remain in place until the parcel is ready for development consistent with the Policy Plan and Affordable Housing Overlay District. Property owners and developers alike have expressed interest in redeveloping this area, so existing agricultural operations are not expected to limit development potential.

In addition, Policy Plan land use categories are used to establish each site's capacity and affordability potential.

Forty-one sites (299.4 acres) are currently classified as MDR in the Policy Plan, which will be amended to allow up to 30 dwelling units per acre if at least 25 percent of units are affordable to lower incomes, consistent with the Affordable Housing Overlay District. Projects that do not have housing affordable to lower-income households and projects that include less than 25 percent of affordable units will be subject to a maximum density of 25 units per acre. To account for the development of circulation, sidewalks, and other site requirements, the realistic density of 22 dwelling units per acre is used to establish the maximum capacity on all MDR sites.

Eight sites (81.7 acres) are proposed to be classified as Mixed-Use Eucalyptus / Chino Airport Overlay (MU-EU). This land use category is envisioned to accommodate employee-intensive office, entertainment facilities, live/work, and supporting retail uses in a campus environment designed to leverage proximity to the park and maintain compatibility with surrounding residential areas. Stand-alone and mixed-use residential is also permitted outside of the Chino Airport safety zone. As previously discussed, sites within the Chino Airport safety zones 1-4 are not included in the Opportunity Area. This land use category allows residential development with a density range of 25 to 45 dwelling units per acre, which is sufficient to facilitate the development of lower- and moderate-income housing. To account for potential non-residential development, only 40 percent of the land area on each site is estimated to have residential potential. A realistic density of 35 dwelling units per acre is used to establish the total housing capacity on each site.

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Twenty-nine parcels (219.5 acres) are proposed to be classified as Mixed-Use Great Park (currently called the NMC West Mixed-Use Area). Areas with this land use classification accommodate a vertical and horizontal mixture of commercial, office, entertainment, and residential uses, all connecting to the Great Park in a pedestrian-oriented atmosphere. These mixed-use areas are envisioned as low-rise (3-5 stories) with some midrise (5-10 stories) near the intersection of Euclid and Edison/Ontario Ranch Road. The land use category allows densities up to 65 dwelling units per acre, which is sufficient to facilitate the development of lowerand moderate-income housing. To account for non-residential development, 70 percent of the land area on each site is estimated to have residential potential. Seventy percent is estimated because similar large mixed-use areas on the eastern side of the Channel have developed as primarily residential, and several developers have approached the City with plans to reduce the size of commercial areas in existing specific plans east of the Channel. A realistic density of 35 dwelling units per acre is used to establish the total housing capacity on each site.

Using the metrics described above, the Great Park Corridor Opportunity Area has a total capacity of 13,080 new units, 6,509 of which have the capacity to accommodate lower-income housing, 3,286 are presumed to have capacity for housing affordable to moderate-income households, and 3,286 are presumed to have capacity for housing affordable to above moderate-income households.

Grove Corridor

The Grove Corridor Opportunity Area extends along Grove Avenue north from the Great Park Corridor Opportunity Area and terminating at Riverside Drive. The corridor provides a vital connection between southern Ontario and the city's existing urban fabric. This area is envisioned as a mixed-income residential district with a mixed-use activity node at Chino Avenue and a new community park near Riverside Drive. Housing within the Opportunity Area will benefit from proximity to the Great Park and the new community park. The extents of the Opportunity Area are shown in Figure 5-9 and described below.





Figure 5-9 Grove Corridor Housing Opportunity Area

The sites along the Grove Corridor were identified for higher-density housing development for several reasons, including planning objectives like activating the street and enhancing transit options, development potential, and infrastructure availability.

Bringing a higher concentration of mixed-income residents to the corridor will create energy on the street, distinguishing Grove Avenue as an important community connector. The influx of new residents will also make the corridor a prime candidate for expanded bus service, improving transit options for residents and strengthening the connection between north and south Ontario.

The Opportunity Area consists of 25 parcels (186.8 acres) that are identified as suitable for housing development. Like the Great Park Corridor, several of these sites continue to house the agricultural operations that once dominated this portion of Ontario. Several sites have also already begun to transition to interim uses such as truck parking and open storage. There are approximately six existing homes that were built in conjunction with agricultural operations. Because of the extremely low density of housing and the anticipated type of development, the existing residential uses are not expected to impede development potential. Land with very similar conditions to the sites identified in this area has recently developed on the eastern side of the Cucamonga Channel, forming new residential neighborhoods. The same trend is expected to continue westward as infrastructure expands.

One of the planned sewer lines will travel along Grove Avenue. As service is expanded from the south, the properties identified in this Opportunity Area will have prime access to expanded infrastructure.

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To account for the need to extend infrastructure and to address the greater distance from the planned trunk line and other facilities, the percentage of units estimated to be affordable to lower-income households is reduced based on distance from Merrill Avenue and the estimated timing within the planning period when infrastructure is expected to be available. The corridor is divided into two sections.

- South of Schaefer Avenue. In this area, OMUC estimated that utilities could be available in three to five years, depending on the rate of development. In recognition of the reduced time during the planning period when development is expected to be feasible, only 40 percent of the realistic development capacity is estimated as affordable to lower incomes.
- North of Schaefer Avenue. In this area, OMUC estimated that
 utilities could be available in four to six years, depending on the
 rate of development. In recognition of the reduced time during the
 planning period when development will be feasible, only 25
 percent of the realistic development capacity is estimated as
 affordable to lower incomes.

Throughout the Opportunity Area, the housing capacity that is not counted toward the City's lower-income RHNA is estimated to facilitate the development of above moderate-income housing. It is assumed that the above moderate-income housing will not only help to create integrated mixed-income communities but will also help to finance the expansion of infrastructure.

As previously discussed, all sites in this area are currently zoned SP-AG, and they will be rezoned to SP-AG-AH for inclusion in the Affordable Housing Overlay District (Program 13). The Agriculture Overlay District will remain in place until the parcel is ready for development consistent with the Policy Plan and Affordable Housing Overlay District. Property owners and developers alike have expressed interest in redeveloping this area, so existing agricultural operations are not expected to limit development potential.

In addition, Policy Plan land use categories are used to establish each site's capacity and affordability potential.

There are 20-21 parcels (150.8 acres) currently or proposed to be classified as MDR in the Policy Plan. The definition for MDR will be amended to allow up to 30 dwelling units per acre if at least 25 percent of units are affordable to lower incomes, consistent with the Affordable Housing Overlay District. Projects that do not have housing affordable to lower incomes and projects that include less than 25 percent of affordable units will be subject to a maximum density of 25 units per acre. To account for the development of circulation, sidewalks, and other site requirements,



the realistic density of 22 dwelling units per acre is used to establish the maximum capacity on all MDR sites.

Four parcels (36 acres) are proposed to be classified as Mixed-Use Grove in the Policy Plan. This activity center is envisioned as a low-rise (three to five stories) mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents. The land use category allows densities up to 65 dwelling units per acre, which is sufficient to facilitate the development of lower- and moderate-income housing. To account for non-residential development, only 65 percent of the land area on each site is estimated to have residential potential. A realistic density of 35 dwelling units per acre is used to establish the total housing capacity on each site.

Using the metrics described previously, the Grove Corridor Opportunity Area has a total capacity of 4,130 new units, 1,152 of which have the capacity to accommodate lower-income housing, and 2,978 units are presumed to have capacity for housing affordable to above moderate-income households.

Euclid Corridor

The Euclid Corridor Opportunity Area includes the parcels on the city's western border along Euclid Avenue between Schaefer Avenue and Riverside Drive. The extents of the Opportunity Area are shown in Figure 5-10 and described in the text that follows.





The sites along the Euclid Corridor were identified for higher-density housing development to complement the multifamily housing across the street in Chino, fulfill regional transit goals, and because of their development potential as well as infrastructure availability.

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SBCTA has plans to expand a BRT route along Euclid Avenue. While potential BRT stops have not yet been identified, rapid transit stops are typically one-half mile to one mile apart. If future transit stops are placed at Riverside Drive, and halfway between Chino and Schaefer Avenues approximately three-quarters of a mile apart, 54 percent of the land area identified in the Euclid Corridor Opportunity Area will be within a 5-minute walk of a BRT; nearly 100 percent of the sites will fall within a 10-minute walk of the same BRT stops.

The Opportunity Area includes 22 parcels (132.2 acres) that are identified as suitable for housing development. Two parcels (10.3 acres) are vacant. Eighteen parcels (110.0 acres) continue to house the agricultural operations that once dominated this portion of Ontario. One parcel (8.8 acres) is principally occupied by the parking area that serves the nearby swap meet and livestock sales yard. The final three parcels (3.1 acres) are underutilized commercial sites fronting Euclid Avenue. These sites were identified for redevelopment because they consist of small buildings operated by independent tenants, and they are surrounded by agricultural and vacant land. It is anticipated that these properties will redevelop concurrent with the rest of the Opportunity Area. There are approximately 12 existing homes that were built primarily in conjunction with agricultural operations. Because of the extremely low density of housing and the anticipated type of development, the existing residential uses are not expected to impede development potential. Land with very similar conditions to the sites identified in this area has recently developed on the eastern side of the Cucamonga Channel.

AB 1397 requires additional justification to include parcels larger than 10 acres in the sites inventory for lower-income housing. Five of the parcels identified in this area (68.3 acres) are larger than 10 acres. The City has included these sites because Ontario has ample recent evidence of large greenfield sites developing with residential uses. Areas east of the Channel had nearly identical parcel patterns to those in this area. Many properties to the east have already been subdivided and developed as housing. The remaining undeveloped land east of the Channel is expected to develop likewise. Other large urban greenfield projects, such as Irvine's Great Park neighborhood, reflect the same pattern of subdividing larger parcels to create mixed-income communities. Several of the communities in Irvine's Great Park neighborhood include multifamily affordable housing, indicating that this process can produce opportunities for lower-income housing. The Affordable Housing Overlay District that will be applied to all parcels in the Opportunity Area (Program 13) also provides incentives to promote housing affordable to lower-income households. The development of the Euclid Corridor Opportunity Area is expected to mirror the trends already established on the eastern side of the Channel, with infrastructure availability determining where projects will develop first.



The Euclid Corridor was identified as suitable for development during the 2021-2029 planning period because one of the planned sewer lines will travel along Euclid Avenue. As service is expanded from the south, the properties identified will have prime access to expanded infrastructure.

To account for the need to extend infrastructure and to address the distance from the planned trunk line and other facilities, the percentage of units estimated to be affordable to lower-income households is reduced based on distance from Merrill Avenue, and the time in the planning period when infrastructure is expected to be available. The corridor is divided into two sections.

- Adjacent to Schaefer Avenue. In response to property owner and developer interest, the City is changing the land use immediately south of the Opportunity Area (between Schaefer Avenue and the Great Park Opportunity Area) to allow light industrial, business park, and logistics facilities. Because land for these types of operations is in extremely high demand, it is anticipated that the development of such facilities will bring infrastructure to the Opportunity Area earlier in the planning period than would occur if the area were designated for residential. It is estimated that the seven parcels closest to Schaefer Avenue could have access to infrastructure in three to five years. In recognition of the reduced time during the planning period when development will be feasible, 40 percent of the realistic development capacity is estimated as affordable to lower incomes.
- North to Riverside Drive. For the remaining sites in the Opportunity Area, a more conservative estimate of four to six years, depending on the rate of development, is assumed. In recognition of the reduced time during the planning period when development will be feasible, only 25 percent of the realistic development capacity is estimated as affordable to lower incomes.

Throughout the Opportunity Area, the housing capacity that is not counted toward the City's lower-income RHNA is estimated to facilitate the development of above moderate-income housing. It is assumed that the above moderate-income housing will not only help to create integrated mixed-income communities but will also help to finance the expansion of infrastructure.

As previously discussed, all sites in this area are currently zoned SP-AG, and they will be rezoned to SP-AG-AH for inclusion in the Affordable Housing Overlay District (Program 13). The Agriculture Overlay District will remain in place until the parcel is ready for development consistent with the Policy Plan and Affordable Housing Overlay District. Property owners and developers alike have expressed interest in redeveloping this

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area, so existing agricultural operations are not expected to limit development potential.

In addition, Policy Plan land use categories are used to establish each site's capacity and affordability potential.

There are 21 parcels (117.2 acres) currently or proposed to be classified as MDR in the Policy Plan. The definition for MDR will be amended to allow up to 30 dwelling units per acre if at least 25 percent of units are affordable to lower incomes, consistent with the Affordable Housing Overlay District. Projects that do not have housing affordable to lower incomes and projects that include less than 25 percent of affordable units will be subject to a maximum density of 25 units per acre. To account for the development of circulation, sidewalks, and other site requirements, the realistic density of 22 dwelling units per acre is used to establish the maximum capacity on all MDR sites.

One parcel (15 acres) is proposed to be classified as Mixed-Use Neighborhood Activity Hub (MU-NH) in the Policy Plan. This activity center is envisioned as a low-rise (three to five stories) mixture of retail and residential uses that will create identity and place along the corridor and serve the surrounding residents. The land use category allows densities up to 75 dwelling units per acre, which is sufficient to facilitate the development of lower- and moderate-income housing. To account for non-residential development, 75 percent of the land area on the site is estimated to have residential potential. Seventy-five percent is estimated because recent projects in mixed-use areas on the eastern side of the Channel have developed as primarily residential, and several developers have approached the City with plans to reduce the size of commercial areas in existing specific plans east of the Channel. In addition, a realistic density of 35 dwelling units per acre is used to establish the total housing capacity on the site.

Using the metrics described, the Euclid Corridor Opportunity Area has a total capacity of 2,960 new units, 731 of which have the capacity to accommodate housing affordable to lower-income households, and 2,229 units are presumed to have capacity for housing affordable to above moderate-income households.

Vineyard Corridor / Armstrong Ranch Specific Plan

The Vineyard Corridor / Armstrong Ranch Opportunity Area includes the parcels on either side of Vineyard Avenue between Chino Avenue and Riverside Drive. The extents of the Opportunity Area are shown in Figure 5-11 and described in the text that follows.



Figure 5-11 Vineyard Corridor / Armstrong Ranch Specific Plan Housing Opportunity Area



The sites along Vineyard Avenue were identified as "overflow" sites to help the City maintain a large inventory of potential sites as development occurs throughout the planning period.

The Opportunity Area includes seven parcels (64.3 acres), all of which are currently used for agricultural purposes. Land with very similar conditions to the sites identified in this area has recently developed on the eastern side of the Cucamonga Channel.

The greatest deterrent to the development of these sites is the availability of infrastructure, which may be available in four to six years, depending on the rate of development. In recognition of the reduced time during the planning period when development will be feasible, only 25 percent of the realistic development capacity is estimated as affordable to lower incomes, while the remaining housing capacity is estimated to facilitate the development of above moderate-income housing. It is assumed that the above moderate-income housing will not only help to create an integrated mixed-income community but will also help to finance the expansion of infrastructure.

The three sites (28.2 acres) on the western side of Vineyard Avenue are currently zoned SP-AG. They will be rezoned to SP-AG-AH for inclusion in the Affordable Housing Overlay District (Program 13). The Agriculture Overlay District will remain in place until the parcel is ready for development consistent with the Policy Plan and Affordable Housing Overlay District. Property owners and developers alike have expressed interest in redeveloping this area, so existing agricultural operations are not expected to limit development potential.

In addition, Policy Plan land use categories are used to establish each site's capacity and affordability potential.

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The sites are proposed to be re-classified as MDR in the Policy Plan. As previously noted, the definition for MDR will also be amended to allow up to 30 dwelling units per acre if at least 25 percent of units are affordable to lower incomes, consistent with the Affordable Housing Overlay District. Projects that do not have housing affordable to lower incomes and projects that include less than 25 percent of affordable units, will be subject to a maximum density of 25 units per acre. To account for the development of circulation, sidewalks, and other site requirements, the realistic density of 22 dwelling units per acre is used to establish the maximum capacity on all MDR sites.

The four sites (36.2 acres) on the eastern side of Vineyard Avenue are governed by the Armstrong Ranch Specific Plan. The City will update the Specific Plan as part of its rezoning program (Program 13) to allow at least 30 dwelling units per acre on the sites included in the Opportunity Area. To account for the development of circulation, sidewalks, and other site requirements, the realistic density of 22 dwelling units per acre is used to establish the maximum capacity on these sites.

Using the metrics described, the Vineyard Corridor/Armstrong Ranch Opportunity Area has a total capacity of 1,416 new units, 354 of which have the capacity to accommodate lower-income housing, and 1,062 units are presumed to have capacity for housing affordable to above moderate-income households.

Strategy 7: Accessory Dwelling Units

Accessory dwellings are a part of the strategy for meeting the City's share of its RHNA for housing affordable to low- and moderate-income households. Ontario has many single-family homes on lots that would allow space to build an ADU.

State law has been amended to facilitate and encourage the production of accessory dwellings. In 2016, AB 2299 and SB 1069 required cities to revise their zoning code to conform to the new ADU law. In 2019, SB 68, AB 881, and SB 13 further amended California Government Code Sections 65852.2 and 65852.22, and Health and Safety Code Section 17980.12 pertaining to local regulations for ADUs. In January 2020, the City Council enacted Ordinance 3175 to incorporate recent changes to state law with respect to ADUs.

Ontario has seen an influx in ADU applications, increasing from just 30 units permitted in 2019 to 69 permits issued in 2020. Based on past and current trends, the City expects that 120 to 360 ADUs will be built during the Housing Element period of 2021-2029.



The affordability for ADUs built in Ontario is based on SCAG's ADU survey and affordability assumptions for San Bernardino County. Applying those affordability assumptions to Ontario and the range of production estimates, the City can expect the production of 69 to 207 lower-income ADUs over the Housing Element period. Table 5-3 includes the summary of ADU projection estimates by affordability level. To encourage this level of production, the Housing Plan proposes a program to incentivize the production of ADUs (Program 27).

Table 5-3
Accessory Dwelling Unit Projections, 2021–2029

Household Income	Percent of ADUs	Range of ADU Estimates		
Category	Projected ¹	Conservative Trend ²	Maximum Trend ³	
Lower	57.5%	69	207	
Moderate	34.8%	42	125	
Above Moderate	7.7%	9	28	
ADUS Projected (2021-2029)		120	360	

Source: City of Ontario, 2021.

Notes

- The methodology for the affordability distribution of ADUs can be accessed online at: https://scaq.ca.gov/sites/main/files/file-attachments/adu_affordability_analysis_120120v2.pdf?1606868527
- The conservative trend assumes 15 dwelling units will be built per year. This estimate is based on half of the reduced number of permits issued in 2019.
- The maximum trend assumes 45 dwelling units will be built per year. This estimate is based on the average of the permits issued in 2019 and 2020.

Summary of Capacity to Accommodate RHNA Allocation

Table 5-4 summarizes the City's strategy to achieve its 2021-2029 RHNA. It is recognized, however, that these strategies are deemed conservative—the City expects that additional residential and mixed-use projects will occur within specific plan areas and on underutilized sites throughout the city. The City's RHNA strategy reflects the City's policy to support the creation of mixed-income communities by assuming a mix of lower- and moderate-income housing on identified sites in established neighborhoods and a mix of lower-, moderate-, and above moderate-income housing in areas where there is no existing residential component. The following provides a high-level summary of the City's RHNA strategies.

Above moderate-income. In addition to the capacity identified in pipeline projects and the analysis of strategies, the City has entitled additional capacity for several thousand homes that have not been credited toward the RHNA. Areas with development potential that are not included in the inventory include entitled specific plans, the downtown mixed-use district, and mixed-use

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areas along the I-10 corridor. Several specific plans were also recently amended to allow more housing at higher densities, a trend the City expects to continue. There is sufficient capacity to accommodate the above moderate-income housing requirement of the RHNA.

- Moderate-income. Prior sections contained an analysis of the strategies that will be used to accommodate the moderate- and lower-income RHNA. Sites and densities were demonstrated to be sufficient to accommodate housing affordable to moderateincome households.
- Lower-income. Prior sections contained an analysis of the strategies that will be used to accommodate the lower-income RHNA. Sites and densities were demonstrated to be sufficient to accommodate affordable housing. In addition, specific programs were shown for each strategy that would encourage and facilitate housing production and that would also remove any potential constraints to development.

Table 5-4 Availability of Land to Meet RHNA, 2021–2029

	Site Area	Lower Income	Moderate Income	Above Moderate Income	Total Capacity
2021-	-2029 RHNA	8,926	3,329	8,599	20,854
	Project Credits	-	-	(1,650)	(1,650)
	Remainder	8,926	3,239	6,949	19,204
Strate	egies				
#1	Downtown	20	20	=	40
#2	West Holt	227	227	-	454
#2	East Holt	250	249	-	499
#3	Old Cardenas Market	33	32	-	65
#4	Ontario Center Specific Plan	151	152	-	303
#5	Ontario Mills Specific Plan	1,564	782	782	3,128
	Great Park Corridor	6,509	3,286	3,286	13,081
	Grove Corridor	1,152	-	2,978	4,130
#6	Euclid Corridor	731	-	2,229	2,960
	Vineyard Corridor / Armstrong Ranch Specific Plan	354	-	1,062	1,416
#7	Accessory Dwelling Units	69	42	9	120
Total Capacity		11,060	4,790	10,346	26,196
	Surplus / Buffer	2,134 (24%)	1,461 (44%)	3,397 (40%)	6,992 (34%)
		RHNA met	RHNA met	RHNA met	RHNA met

Source: City of Ontario, 2021.



Administrative and Financial Resources

Funding Sources for Affordable Housing

The City's local housing programs are supported through federal funding and state funding programs that assist first-time homebuyers, build affordable housing, and help special-needs groups, such as seniors and large households, as listed in Table 5-5. In most cases, other entities, including for-profit and nonprofit developers, apply for funds or other program benefits. For example, developers apply directly for Section 202 grants. In general, the City relies on the private sector to develop new affordable units.

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Table 5-5 Federal and State Housing Funding Programs

Program Name	Description	Eligible Activities		
Federal Funding Programs				
Community Development Block Grant (CDBG) Program	The Department of Housing and Urban Development (HUD) awards CDBG annually to entitlement jurisdictions and states for general activities, including housing and economic development activities. HUD also offers various other programs that can be used by the City and nonprofit and for-profit agencies for the preservation of low-income housing units, such as Section 202 and Section 108 loan guarantees. The annual appropriation for CDBG is split between states and local jurisdictions called "entitlement communities."	Acquisition Rehabilitation Homebuyer Assistance Economic Development Assistance Homeless Assistance Public Services Infrastructure Replacement		
HOME Investment Partnerships Program	The Home Investment Partnerships Program (HOME) was created under the Cranston Gonzalez National Affordable Housing Act enacted in November 1990. HOME funds are awarded annually as formula grants to participating jurisdictions. HUD establishes Home Investment Trust Funds for each grantee, providing a line of credit that the jurisdiction may draw upon as needed. The program's flexibility allows states and local governments to use HOME funds for grants, direct loans, loan guarantees, or other forms of credit enhancement or rental assistance or security deposits.	Acquisition Rehabilitation Homebuyer Assistance Rental Assistance		
Housing Choice Voucher (HCV) Rental Assistance (Section 8)	Provides rental assistance payments to owners of market-rate properties on behalf of very low-income tenants.	Rental Assistance		
Section 811	Provides grants to nonprofit developers of supportive housing for disabled persons. The grants may be used to construct or rehabilitate group homes, independent living facilities, and intermediate care facilities. The grants may also have a rental assistance component.	Acquisition Rehabilitation New Construction Rental Assistance		
Section 203(k)	Provides fixed-rate, low-interest loans to organizations wishing to acquire and rehabilitate property.	Land Acquisition Rehabilitation Refinancing of Existing Debt		
Section 202	Grants to private nonprofit developers of supportive housing for very low-income seniors.	New Construction		



Table 5-5 Federal and State Housing Funding Programs

Program Name	Description	Eligible Activities
Low-Income Housing Tax Credits (LIHTC)	In 1986, Congress created the federal LIHTC program to encourage private investment in the acquisition, rehabilitation, and construction of low-income rental housing. Because high housing costs in California make it difficult, even with federal credits, to produce affordable rental housing, the California legislature created a state low-income housing tax credit program to supplement the federal credit. The state credit is essentially identical to the federal credit, the Tax Credit Allocation Committee allocates both, and state credits are only available to projects receiving federal credits. Twenty percent of federal credits are reserved for rural areas and 10 percent for nonprofit sponsors. To compete for the credit, rental housing developments have to reserve units at affordable rents to households at or below 46 percent of area median income. The targeted units must be reserved for the target population for 55 years.	New Construction
Mortgage Credit Certificate Program	Offers income tax credits to first-time homebuyers. The County distributes the credits.	Homebuyer Assistance
Supportive Housing Program (SHP)	Offers grants to agencies who offer supportive housing and services to the homeless.	Transitional Housing Housing for Disabled Persons Supportive Housing Support Services
Community Reinvestment Act	The Community Reinvestment Act (CRA), enacted by Congress in 1977, is intended to encourage depository institutions to help meet the credit needs of the communities in which they operate, including low- and moderate-income neighborhoods, consistent with safe and sound banking operations. The CRA requires that each insured depository institution's record in helping meet the credit needs of its entire community be evaluated periodically. That record is taken into account in considering an institution's application for deposit facilities, including mergers and acquisitions.	New Construction Rehabilitation Acquisition Support Services Supportive Housing Homebuyer Assistance
Emergency Solutions Grant	Awards grants to nonprofits for the provision of shelter support services.	Support Services
Emergency Solutions Grant-CARES (ESG-CV)	One-time funding for programs targeted to homeless or those at-risk for housing to prepare for, respond to, or prevent impacts of the coronavirus pandemic.	Support Services (Homeless Facilities and Program)
Multifamily Housing Program (MHP)	Provides loans for new construction, rehabilitation, and preservation of affordable rental housing. Payments on the loans are deferred for a specified period of time.	New Construction Rehabilitation Preservation
CalHOME	Provides grants to local governments and nonprofit agencies for homebuyer assistance, rehabilitation, and new construction. The agency also finances acquisition, rehabilitation, and replacement of manufactured homes.	Homebuyer Assistance Rehabilitation New Construction

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Table 5-5 Federal and State Housing Funding Programs

Program Name	Description	Eligible Activities
California Self-Help Housing Program	Provides grants for the administration of mutual self-help housing projects.	Homebuyer Assistance New Construction Administrative Costs
Emergency Housing and Assistance Program	Provides grants to support emergency housing.	Shelters Transitional Housing
Affordable Housing and Sustainable Communities Program	Provides funding to support infill development projects with the goal of reducing greenhouse gas emissions.	New Construction Rehabilitation
Veterans Housing and Homeless Prevention Program	Provides funding to buy, construct, rehabilitate or preserve affordable multifamily housing for veterans and their families.	Acquisition Construction Rehabilitation Preservation
Senate Bill 2 – Building Jobs and Homes Act	Provides planning grant funding to jurisdictions for plans and process improvements that will help to accelerate housing production.	Technical Assistance Planning Document Updates
Local Early Action Planning (LEAP) Grants	The (LEAP grants program provides over-the-counter grants complemented with technical assistance to local governments for the preparation and adoption of planning documents, and process improvements that: Accelerate housing production. Facilitate compliance to implement the sixth-cycle Regional Housing Needs Assessment.	Housing element updates Updates to zoning, plans or procedures to increase/accelerate housing production Pre-approved architectural and site plans Establishing State-defined Pro-housing policies See complete list in LEAP program materials
No Place Like Home	Through a County application process, provides loans to acquire, develop, preserve, or rehabilitate permanent supportive housing facilities.	Permanent Supportive Housing
Infrastructure Infill Grant	Provides gap financing for infrastructure improvements necessary to support the development of affordable infill housing.	Infrastructure Improvements



Table 5-5 Federal and State Housing Funding Programs

Program Name	Description	Eligible Activities
Local Housing Trust Fund Program	Provides matching grants to funds provided by Local Housing Trust Funds.	Site Acquisition Site Development Homebuyer Assistance Transitional Housing Emergency Shelter Multifamily Housing
Transit-Oriented Development Program	Supports the development of affordable multifamily rental housing near transit stations through low-interest loans.	New Construction Rehabilitation Infrastructure Improvements
	State Funding Programs	
Affordable Housing Partnership Program (AHPP)	Provides lower interest rate CalHFA loans to homebuyers who receive local secondary financing.	Homebuyer Assistance
Self-Help Builder Assistance Program	Provides lower interest rate CalHFA loans to owner-builders who participate in mutual self-help housing projects. Also provides site acquisition, development financing, and construction financing for self-help projects.	Homebuyer Assistance Site Acquisition Site Development Home Construction
California Housing Assistance Program	Provides 3% silent second loans in conjunction with 97% CalHFA first loans to give eligible homebuyers 100% financing.	Homebuyer Assistance
Extra-Credit Teacher Program	Provides \$7,500 silent second loan with forgivable interest in conjunction with lower-interest-rate CalHFA first loans to assist eligible teachers in buying homes.	Homebuyer Assistance
Housing Enabled by Local Partnerships	Provides 3% interest rate loans, with repayment terms up to 10 years, to local government entities for locally determined affordable housing priorities.	Wide Range of Eligible Activities
Predevelopment Loan Program	The California Department of Housing and Community Development (HCD) administers the program, which provides funds to pay the initial costs of developing affordable housing developments. Priority is given to applications with matching financing from local agencies or federal programs.	Pre-development
Multifamily Housing Program	HCD conducts the acquisition and rehabilitation component of the Multifamily Housing Program to acquire and rehabilitate existing affordable rental housing. Priority is given to projects currently subject to regulatory restrictions that may be terminated. Assistance is provided through low-interest construction and permanent loans. Eligible applicants include local government agencies, private nonprofit organizations, and for-profit organizations.	Rental Acquisition Rental Rehabilitation
Transitional Housing Program for Emancipated Foster/Probation Youth (THP-Plus)	This program provides funds for housing and services for persons who need support services for transition-age youth.	Supportive Housing Foster Care

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Table 5-5 Federal and State Housing Funding Programs

Program Name	Description	Eligible Activities
Special-Needs Housing Program	Allows local governments to use Mental Health Services Act (MHSA) funds to finance the development of permanent supportive rental housing.	New Construction Supportive Housing
Home Mortgage Purchase Program	CalHFA sells bonds to raise funds for providing below-market-rate loans to qualifying first-time homebuyers.	Homebuyer Assistance



Housing Choice Vouchers

The federal government provides approximately \$3 million to the Housing Authority of the County of San Bernardino to administer the Housing Choice Voucher (HCV) program. These funds are used to maintain the affordability of publicly subsidized affordable housing projects in Ontario. This allocation includes funding for approximately 501 Housing Choice Vouchers, and the Family Self-Sufficiency Program. Within Ontario, the approximate number of HCVs have been available: 496 in 2016, 422 in 2017, 527 in 2018, 676 in 2019, and 747 in 2020. As of July 2021, there are 773 vouchers in use for the City of Ontario. This includes 735 in traditional voucher programs and 38 in special voucher programs, such as the Veterans Affairs Supportive Housing. The waitlists, which are for the entire County of San Bernardino remain closed as of July 2021. In March 2021, there were 20,382 tenant-based households on the waitlist and 60,744 total households on the waitlist.

(See Programs 23, 25, 27, and 31)

Community Development Block Grants (CDBG)

The federal government provides funds for a range of housing and community development activities, including acquisition and disposition of real estate or property, public facilities and improvements, relocation, rehabilitation and construction of housing, homeownership assistance, and demolition activities. In addition, these funds can be used to acquire or subsidize at-risk units. The City of Ontario received approximately \$1.8 million in funding in 2019–2020.

(See Programs 3, 4, 6, 7, 9, 27, 31, and 33)

HOME Partnership

Ontario received approximately \$666,000 in 2019–2020 in federal HOME funds. HOME funds can be used for activities that promote affordable rental housing and lower-income homeownership, including building acquisition, new construction, reconstruction, moderate or substantial rehabilitation, first-time homebuyer assistance, and tenant-based assistance, as well as the preservation of affordable housing.

(See Programs 1, 3, 4, 24, 27, 32, and 33)

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Transformative Climate Communities

Ontario received funding through the Transformative Climate Communities (TCC) grant program from the State of California (\$33.2 million) for a variety of project types, including housing. The TCC housing component included gap funding of approximately \$14 million for the development of a 101-unit affordable housing project known as Vista Verde. The construction of the Vista Verde Apartments begun on June 17, 2019, and was completed in March 2021. Within the project area, 24 solar photovoltaic (PV) systems were installed on the single-family homes, providing approximately 31 kilowatts (kw) in renewable energy capacity. Of the 101 units, 11 will be rented to households making below 30 percent of the area median income (AMI), 37 units will be rented to households at 50 percent AMI, and 42 units will be rented out to households at 60 percent AMI. The affordable housing units will be made available through a lottery process. Applicants who already work and live in Ontario will be given preference.

(See Programs 8 and 10)

Permanent Local Housing Allocation

Ontario received funds that will be used for housing programs and homeless programs. The City is planning to use a portion of the Permanent Local Housing Allocation (PLHA) funds for first-time homebuyer programs in conjunction with reuse funds on hand from the CalHome and BEGIN programs formerly offered. Programs are currently in design development to determine income targeting and benefits.

(See Program 24)



PROGRAM EVALUATION

The 2021–2029 Housing Element sets forth goals, policies, and programs to address the community's housing needs. An important step in developing the City's housing strategy is the evaluation of the prior Housing Element in meeting the community's housing needs. This section evaluates progress in meeting the objectives of the 2013–2021 Housing Element.

2013–2021 Housing Element Goals

The 2013–2021 Housing Element defined four general goals to guide the allocation of financial, administrative, and land resources in Ontario. These broad goals and quantified objectives are summarized here.

- Goal 1: Encourage a diverse supply of housing types to accommodate a variety of incomes and lifestyles, support household and job growth, and facilitate mobility.
- Goal 2: Provide housing opportunities to meet the needs of residents, be affordable to all economic segments, and meet the City's share of the region's need for housing.
- Goal 3: Promote and encourage housing opportunity for all, regardless of age, race, sex, ethnicity, ancestry, national origin, marital status, physical condition, or family size.
- **Goal 4:** Promote and encourage the rehabilitation of deteriorated units and the conservation of the currently sound housing stock.

2013–2021 Special Housing Needs Summary of Accomplishments

California Government Code Section 65588 requires that local governments review the effectiveness of the housing element goals, policies, and related actions to meet the community's special housing needs. Special needs are those associated with specific demographic or occupational groups that call for specific program responses, such as preservation of single-room occupancy hotels or the development of units with larger bedroom counts. The statute specifically requires analysis of the special housing needs of people who are elderly or disabled (including developmental disabilities), female-headed households, large families, farmworkers, and people experiencing homelessness. These special-needs groups often spend a disproportionate amount of their income to secure safe and decent housing and are sometimes subject to discrimination based on their specific needs or circumstances.

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As shown in Table 6-1, Review of Previous Housing Element, the 2013 - 2021 Housing Element included several programs that addressed the community's special housing needs. Some of the accomplishments are highlighted below:

- In partnership with the Housing Authority of San Bernardino County, the approximate number of Housing Choice Vouchers (HCV) that were used by Ontario residents are: 496 in 2016, 422 in 2017, 527 in 2018, 676 in 2019, and 747 in 2020.
- In 2020, City staff worked with the National Foundation of Affordable housing to assist with the rehabilitation and preservation of the Ontario Townhouses project including an extension of the Project Based Vouchers (PBV) contract for an additional 20 years.
- From 2016-2020, the city's Homeless Continuum of Care implemented programming for homeless residents through Mercy House providing basic needs and services to 4,662 clients. During Fiscal Year 2019-20, funding for the street outreach team was increased from 10 hours per month to 40 hours per week to better deliver services designed to house persons experiencing homelessness.
- The City has provided a range of supportive services for seniors. Between 2013 and 2020, 182 seniors were assisted with fair housing issues, 1,008 seniors were assisted with landlord/tenant mediation, 1,964 seniors were assisted with supportive services, two seniors received housing rehab grants, nine seniors received tenant-based rental assistance, and 782 units of affordable housing units were restricted for seniors.
- The City worked with two different developers on affordable housing developments that were completed in Fiscal Year 2020-21
 Emporia Place (75 units) and Vista Verde (101 units). These two new developments provide family housing for extremely low-, very low-, and low-income households.
- From 2016-2020, the City, in conjunction with Mercy House Living Centers, implemented the HOME Tenant-Based Rental Assistance program to provide rental assistance for chronically homeless individuals and families through rental assistance subsidies, security deposits, and utility deposits.





Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
	Neighborhoods and	d Housing	
 Code Compliance is an important tool to ensure that the value, character, and quality of neighborhoods, property, and housing are well maintained. Listed below are the programs implemented by the Code Enforcement program specifically designed to improve the quality of Ontario neighborhoods and eliminate health and safety related to building conditions: General Code Enforcement: The City utilizes an interdepartmental approach for inspecting properties for compliance with state and local regulations regarding the condition and maintenance of residential buildings and properties. If deficiencies are found, the property owner is notified of the code deficiency and compliance measures required, and the property owner is granted a period of time to correct the matter. To facilitate timely compliance, City staff direct the property owners to City—administered rehabilitation loans and/or other nonprofit housing loan programs, where available. Community Improvement Team: This team has been specifically designed to proactively implement an intensive code compliance program to address serious code violations within focus areas. As part of this team approach, various City departments work together to bring a myriad of resources to the focus area to arrest neighborhood decline and improve the living conditions within the area. Systematic Health and Safety Inspection Program: The program is designed to ensure the quality of the rental stock and reduce substandard building conditions. Through this program, all rental housing units over seven 	Objectives: Continue code enforcement using a progressive approach of voluntary compliance, citations, and court action if needed. Continue to apply for funding. Responsible Agencies: Housing and Neighborhood Improvement, Police, Fire, Economic Development, Building, and Planning Departments Funding: CDBG, HOME, ORA, and CalHOME funds Timing: Ongoing, inspect properties annually	The Community Improvement Team completed the following: Inspections: 5,305 in 2016, 4,646 in 2017, 5,201 in 2018, 5,283 in 2019, and 3,878 in 2020 Cases closed: 2,076 in 2016, 2,166 in 2017, 1,925 in 2018, 2,145 in 2019, and 1,607 in 2020. The Rental Inspection Program completed the following: Inspections: 4,018 in 2016, 7,477 in 2017, 6,948 in 2018, 5,134 in 2019, and 1,372 in 2020. Abated violations: 1,963 in 2016, 4,190 in 2017, 5,960 in 2018, 4,083 in 2019, and 317 in 2020. The COVID-19 crisis severely hindered Community Improvement Department activities in 2020.	Continue

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Table 6-1 Review of Previous Housing Element

Review of Previous Housing Element				
Program	Implementation	Result/Effectiveness	Continue/Modify/Delete	
years old are inspected on a four-year schedule unless it is necessary to inspect more frequently due to substandard conditions. Abandoned and Distressed Property Program				
and Foreclosure Opportunities Response Team (FORT) Program: These programs were established to protect Ontario neighborhoods from becoming blighted through the lack of adequate maintenance and security of abandoned and distressed properties.				
Residential neighborhoods located directly west of Ontario International Airport experience high noise levels. In the early 1990s, the Federal Aviation Administration, City of Los Angeles, and City of Ontario created a program to improve the quality of life in noise-impacted neighborhoods and community/airport compatibility. Eligible homes are outfitted with sound insulation to reduce the interior noise levels to 45db CNEL. The second component consists of the voluntary acquisition of eligible properties and reuse of properties in a manner compatible with the airport. Eligibility is restricted to properties located within the noise contour map. Currently, the Los Angeles World Airport is updating the Part 150 Study, which may impact the eligibility area. The study is anticipated to be completed within 2014–2015.	Objectives: Continue to implement program. Responsible Agencies: Ontario Housing Authority Funding: Federal Aviation Administration, Los Angeles World Airport Timing: Ongoing	Since the program began in 1994, 1,599 units have been insulated and 256 properties were acquired. The program has sold 30 parcels for future airport-compatible development. The program was terminated in September 2015 because of updated noise exposure maps (NEM) eligibility noise contour, which eliminated funding for the program.	Delete – program no longer exists, and the noise contours have been modified in such a way that there is not a qualifying area for the funding from the Federal Aviation Administration.	
3. Historic Preservation Known as the Original Model Colony, Ontario is rich in local history. The City operates a comprehensive historic preservation program. It is a certified local government, a designation that signifies that the City's program meets state and federal historic preservation standards. The City has six historic districts and is surveying nine additional areas for the potential of	Objectives: Continue to implement program. Responsible Agencies: Planning Department Funding: General Fund, state and federal grants Timing: Ongoing	As of 2020, 7 properties on the Ontario Register were reviewed for historic significance, 4 were removed, and 3 received a Tier Determination. Of the 3 properties that received a Tier Determination, 2 received Local Landmark designation, and 1 received a Local Historic District designation. Additionally, 1 Mills Act Contract (preservation agreement) was approved, and 6 contracts of the Annual Mills Act were completed. Staff continues to	Continue	





Table 6-1 Review of Previous Housing Element

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Program	Implementation	Result/Effectiveness	Continue/Modify/Delete	
historic district designation. It encourages historic preservation efforts through Mills Act contracts,		implement design review for historic properties and as of 2018, completed 237 projects for design review.		
surveys of potentially historic structures, and an adaptive reuse program (for the Emporia District and Downtown). The City also implements an awardwinning web-based historical resource management system that catalogs local historical resources and eventually offers interface capacities for the public to search the database.		Staff participated in or coordinated the following community outreach activities: Model Colony Awards program, Historic Plaque program, Historic Preservation Month Photo Contest, development of an ESRI Storymap geographically displaying photographs and information for local historical points of interest, Ontario Showcase/Heritage Celebration, and the Ontario Festival of the Arts.		
4. Housing Rehabilitation Loan and Grants When funding is available, the City offers housing rehabilitation loans and grants to qualified	Objectives: Continue to implement program, as funding is available.	The City of Ontario designed the Community Improvement Code Abatement Loan Program. In 2019 and 2020, no homes were rehabilitated through this program.	Modify to only include Conservation Home Improvement (CHIP) Loan	
rehabilitation loans and grants to qualified homeowners. Due to the State elimination of redevelopment funding and recent federal funding cutbacks, the City of Ontario is not currently able to	Responsible Agencies: Housing Department Funding: CDBG, HOME, CalHOME Timing: Ongoing	Under implementation of the CIT Homeowner Occupied Rehabilitation Loan Program, no homes were rehabilitated from 2016 to 2020.	Program	
provide owner-occupied rehabilitation programs. Should funding become available, the City will re- establish this program and provide associated quantified objectives.		Under implementation of the CIT Emergency Grant Program, the following number of homeowners received assistance: 1 in 2016, 1 in 2017, and 0 from 2018-2020.		
		The City's largest housing rehabilitation program, the CARES Program, continues to remain on hold. Funding for this program had been provided through the Ontario Redevelopment Agency's Low- and Moderate-Income Housing Fund (LMIHF). To date, no replacement funding has been identified and secured.		
		Using Community Development Block Grant (CDBG) funds, the City designed the Conservation Home Improvement Program (CHIP Loan). The program was launched in June 2020 to assist homeowners with exterior home improvements. As of July 2021, two loans were approved, with construction in progress and another two homeowners were proceeding through the eligibility process.		

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Table 6-1 Review of Previous Housing Element

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Program	Implementation	Result/Effectiveness	Continue/Modify/Delete	
 CARES The City of Ontario implements the comprehensive CARES Neighborhood Revitalization Program within selected focus neighborhoods. The components of this comprehensive, multiagency program include code enforcement, arterial street improvement, relief program, exterior improvement program, and sidewalk or safe routes to school program. The program seeks to stabilize neighborhoods through a comprehensive approach to building community. The program has several components: Single-Family Improvement Loans. The City offers two low-interest deferred loan programs for homeowners (with a one- to five-year deferment) to make exterior improvements to 	Objectives: Continue program implementation, as funding is available. Responsible Agencies: Housing and Neighborhood Revitalization Agency Funding: CDBG, HOME, General Fund Timing: Ongoing	As of July 2021, the program is on hold. This program was previously funded with LMIHF, HOME, and General Funds. However, limited availability of funding during the 5th cycle planning period hindered the City's ability to implement this program.	Continue and modify – City will seek funding to continue this program and restructure it as needed.	
 Neighborhood Improvements. The City improves streets (e.g., resurfacing, replacing curb and gutter, improving sidewalks and drainage), plants trees and greenways, and enforces codes. 				
6. Neighborhood Plans Ontario's neighborhoods define the sense of identity and community for residents, the quality of life experienced, and the image and role of Ontario. The City currently implements many programs to improve neighborhoods. However, the City has identified a need to foster a stronger sense of neighborhood identity in the community. While this goal is being achieved in CDBG-eligible areas (CARES program) and in historic areas, efforts need to be expanded to other neighborhoods. During the planning period, the City will begin a public outreach effort to solicit input from neighborhood leaders and residents as to particular needs and goals. This process may result in the establishment of ongoing dialog with the City,	Objectives: Designate focus neighborhoods, outreach plan and process, and initiate survey efforts. Evaluate the potential of creating neighborhood improvement plans. Responsible Agencies: Planning Department, Housing and Neighborhood Revitalization Agency Funding: General Fund Timing: Ongoing	The primary Neighborhood Planning programs implemented from 2016 to 2020 include the HEAL Zone and Transformative Climate Communities (TCC), Zoning Consistency Program, and Active Transportation Program (ATP). Through the HEAL Zone and TCC programs, feedback from community leaders helped inform decision makers on policy and capital improvements. In 2020, over 6,000 people were invited to a variety of virtual workshops from topics including urban forestry to affordable housing and solar installation. Over four stakeholder meetings were attended through zoom. The Zoning Consistency program was completed in 2018. A total of 552 properties were rezoned to either be consistent with existing residential uses or be more	Continue and modify to include that the City will continue to work on a Multimodal Transportation Center (MTC) Needs Assessment and Siting Criteria project. This assessment will assist in determining the optimum location for an MTC on or near the Ontario International Airport connecting future modes of transportation, including light-rail opportunities.	





Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
neighborhood organizations, or the preparation of neighborhood improvement plans.	imponionation	compatible with adjacent residential development by limiting uses. Members of a local community garden helped to develop language for a new Urban Agriculture section of the Development Code that went into effect January 2016.	Communication
		The City received funding for infrastructure improvements as part of the ATP Cycle 1, Cycle 2, and Cycle 3. The ATP Cycle 1 and Cycle 2 were completed with improvements to pedestrian safety and access around Euclid, Bon View, Corona, Vineyard, and El Camino Elementary Schools. As part of ATP Cycle 3, design and right-of-way (ROW) acquisition was completed for the area around Sultana Elementary and De Anza Middle School.	
		Additionally, the City acquired the Systemic Safety Analysis Report Program (SAARP) Caltrans Grant to examine how to incorporate improved pedestrian, biking, and transit opportunities along the Euclid Avenue corridor. The City also received a GoHuman demonstration grant through the Southern California Association of Governments (SCAG) to provide an opportunity for public input to pedestrian and bicycle improvements being considered in downtown.	
		As part of the Active Transportation Master Plan, community outreach and most of the walk audits around public schools was completed. The City continues to move forward on the Multimodal Transportation Center Needs Assessment and Siting Criteria project that began in 2019 that will centralize multimodal options for residents and employees of and visitors to Ontario.	
		The City is also in the process of updating the City Parks Master Plan and initiated a city-wide parks survey in 2020. As of July 2021, the City anticipates that the City Council will adopt the Master Plan soon.	

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
7. Neighborhood Stabilization The Dodd-Frank Wall Street Reform and Consumer Protection Act of 2010 provided an additional \$1 billion for the Neighborhood Stabilization Program (NSP) that was originally established under the Housing and Economic Recovery Act of 2008. HUD awarded grants to 270 states and selected local governments to mitigate the negative impact of the nation's economic decline and housing market collapse and to stabilize and revitalize communities/areas hit the hardest. The City of Ontario was provided an allocation of \$1,872, 853 in NSP3 funds. The City will utilize these funds (1) to acquire, rehabilitate, and resell single-family homes; (2) to acquire and rehabilitate multiple-family properties; (3) to provide financial assistance; (4) to establish land banks; (5) to demolish blighted structures; (6) to redevelop demolished or vacant properties; and (7) for administration (capped at 10 percent).	Objectives: Designate focus neighborhoods, outreach plan and process, and initiate survey efforts. Evaluate the potential of creating neighborhood improvement plans. Responsible Agencies: Housing and Neighborhood Revitalization Agency Funding: Federal NSP3 funds Timing: Ongoing	Federal NSP3 funds were exhausted in 2013 and all NSP grant funding was closed out in 2018. In 2019, the City Council approved the Neighborhood Preservation Strategy Plan, which designated four focus neighborhoods: Downtown, Nocta, Mission-Mountain, and Fourth-Grove.	Modify to switch from federal NSP to implementing Ontario's 2019 Neighborhood Preservation Strategy Plan
The City of Ontario Police Department uses CDBG funds to implement a community-oriented policing program in designated low- and moderate-income neighborhoods. This partnership involves working with community leaders, businesses, and property owners to address neighborhood issues including code enforcement, crime-free multi-family housing, safe and clean streets, and school interventions. With respect to housing, the Police Department implements the Crime-Free Multifamily Housing Program to control and eliminate crime in apartment buildings. Under this program, the Police Department will provide training to apartment owners, conduct a property inspection to identify and eliminate potential crime hazards, and certify properties where the owner signs a written agreement and commitment to maintain the program.	Objectives: Continue implementation of COPs program; coordinate marketing efforts with the new Quadrennial Inspection Program. Responsible Agencies: Housing and Neighborhood Revitalization Agency, Code Enforcement, and Police Department Funding: General Fund, CDBG Timing: Ongoing	 The Community Engagement Team takes a pro-active approach by partnering with building and property owners to prevent, deter, and solve crimes. The Crime-Free Housing and Trespassing program completed the following activities: In 2016, Code Enforcement presented the Systematic Rental Inspection Program at the Multifamily Crime-Free Training for apartment complex property owners and managers. In 2018, 80 apartment buildings and 8 businesses were enrolled in the Crime-Free Housing and Trespassing program. 280 individuals were served with trespassing forms. In 2019, two Crime-Free Multi-Housing classes were hosted by the Ontario Police Department. A total of 13 properties are enrolled in the Crime-Free Multi-Housing Program. A total of 16 properties are enrolled in the Trespassing Enforcement Program 	Continue





Table 6-1 Review of Previous Housing Element

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Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
		 and 527 individuals were served with forbidden trespass forms. In 2020, no Crime-Free Multi-Housing classes were hosted due to COVID-19. A total of 13 properties are enrolled in the Crime-Free Multi-Housing Program. A total of 16 properties are enrolled in the Trespassing Enforcement Program and 427 individuals were served with forbidden trespass forms. Additionally, during inspections, on-site improvements are identified under the concept of Crime Prevention Through Environmental Design (CPTED). In 2019 and 2020, the Community Engagement Team prepared 3 comprehensive CPTED reports for businesses in the city. These reports outlined positive aspects of the businesses, challenges, and recommendations for improvement. 	
	Housing Supply and	d Diversity	
Ontario's Downtown covers 12 blocks along Euclid Boulevard. In 1983, the City adopted the Center City Redevelopment Project area to encourage development of a high-intensity, multiuse central business district and surrounding neighborhoods that maximize the productivity of commercial areas and housing opportunities. The \$200 million Town Center Square project will provide a mix of housing, educational, retail, office, and government uses that will stimulate the renewal of Downtown. Although the General Plan redesignates a majority of the area for new housing and mixed uses, a comprehensive planning process is necessary to ensure the sensitive integration of new housing, commercial uses, open space, pedestrian paths, and transportation into the fabric of Downtown.	Objectives: Create a Downtown Plan to facilitate new mixed-use and residential development; continue to acquire property and assemble sites to facilitate new housing. Responsible Agencies: Planning Department Funding: General Fund, Tax Increment Timing: 2015	The objective of the downtown planning effort is to facilitate new mixed-use and residential development and continue to acquire property and assemble sites to facilitate new housing. To facilitate new development and establish new businesses within the downtown area, the HEART (Historic Euclid Avenue Revitalization Team) Initiative was established in 2015. Through improved transit and placemaking efforts that integrate arts and culture, the HEART Initiative is creating an environment that attracts new housing, improves existing housing, and encourages a mix of uses and activities. A strategic plan to diversify land uses and improve mobility and connectivity in the downtown area was approved in 2020. In 2020, new affordable housing development was completed, including a 100% affordable (low and very low income) 101-unit transit-oriented development (TOD) project on Virginia/Holt and 100% affordable (low and very low income) 75-unit project on Vine/Holt. A 153-unit	Continue

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
		mixed-use development was in the entitlement phase at the end of 2020. As of July 2021, the City was continuing to review and the project had not been approved by the Planning Commission.	
		The Downtown Plan area is included in a \$35 million Transformative Climate Communities (TCC) Grant awarded to the City to increase prosperity and improve transportation and housing within a disadvantaged community. The TCC program includes affordable housing, active transportation improvements, mobility hub, urban greening, carbon farm, solar photovoltaic, and transit improvements. This grant is a collaborative effort with public and community-based organizations. Projects in the Downtown include the planting of approximately 300 right-of-way trees and development of the Vista Verde 101-unit Affordable Housing project.	
Euclid Avenue and Mountain Avenue extend the entire length of Ontario. In recent years, developers have expressed interest in building residential and commercial projects along these corridors. Mountain Avenue has had numerous senior and affordable housing projects built adjacent or near to the corridor, and developers have begun to show interest in Euclid Avenue. Both corridors have commercial property that is proposed for redesignation as residential. To facilitate corridor development, the City will redesignate properties along Euclid Avenue and Mountain Avenue for medium- and high-density residential development as shown on the Official Land Use Plan (LU-01). The City will also develop a lot-consolidation ordinance to incentivize the assemblage of parcels. Incentives may include fee modifications, flexibility in design, expedited permit processing, or others.	Objectives: Redesignate corridors for medium- and high-density residential uses and develop a lot consolidation ordinance to facilitate the assemblage of lots into larger parcels. Responsible Agencies: Planning Department Funding: General Fund Timing: Summer 2014	All sites on the Housing Element Available Land Inventory along corridors Euclid Avenue and Mountain Avenue have been rezoned to medium-density residential, high-density residential, or mixed-use designations, and are consistent with the General Plan. The City continues to monitor the on-going status of development in the Euclid Avenue and Mountain Avenue corridors.	Modify to reflect that rezoning was completed. Continue lot consolidation ordinance. Add that the City will continue to monitor the ongoing status of development in the Euclid Avenue and Mountain Avenue corridors.





Table 6-1 Review of Previous Housing Element

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Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
Holt Boulevard Holt Boulevard is one of the original corridors paralleling the railroad and extending through Ontario and neighboring communities. With the development and success of commercial uses fronting the freeways, the commercial viability of Holt Boulevard has gradually eroded, leaving a significant number of underutilized uses on small parcels. The General Plan has declared Holt Boulevard as a focus area for mixed uses, both perpendicular to Mountain Avenue, at the base of Downtown, and in the East Holt Boulevard Study Area. To stimulate investment in these areas, the City will adopt a lot consolidation ordinance and incentives to encourage the recycling of land to residential uses. The City will also explore the use of density incentives to encourage mixed-use development, offering higher densities for quality projects of a certain size.	Objectives: Redesignate Holt Boulevard for high-density residential and mixed uses, and develop a lot consolidation ordinance to facilitate the assemblage of lots into larger parcels. Responsible Agencies: Planning Department Funding: General Fund Timing: 2015	All sites on the Housing Element Available Land Inventory along the Holt Boulevard corridor have been rezoned to accommodate higher densities. The City continues to monitor the on-going status of development in the Holt Boulevard area.	Modify to reflect that rezoning was completed. Continue lot consolidation ordinance. Add that the City will continue to monitor the ongoing status of development in the Holt Boulevard area.
The New Model Colony covers 8,200 acres of the former San Bernardino Agricultural Preserve. This area is intended to provide a range of housing opportunities for the city's emerging regional and national employment centers. Buildout of this area is contingent on completion of infrastructure, approval of specific plans, and cancellation of Williamson Act contracts. The City has entered into an agreement with a consortium to fund \$430 million in infrastructure serving the eastern New Model Colony. Many specific plans for this area have been approved. Some of the original Williamson Act contracts will also expire during the planning period. The General Plan has designated much of the area for medium- and high-density residential and mixed use. Although development is not expected to occur during the planning period, the City will continue to process specific plan applications and work with developers to	Objectives: Continue to review, approve, and implement plans to develop the New Model Colony. Responsible Agencies: Planning Department Funding: General Fund Timing: Ongoing	New Model Colony is now known as Ontario Ranch. City staff continues to review and process applications for development in Ontario Ranch. Within Ontario Ranch, the following number of permits for new single-family and multifamily homes were issued: 482 in 2016, 762 in 2017, 1,063 in 2018, 1,398 in 2019, and 864 in 2020. Completion of infrastructure, approval of specific plans, and cancellation of the Williamson Act contracts remain ongoing as of July 2021. The City continues to coordinate with developers on planned developments.	Continue and modify to update name to Ontario Ranch.

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
address outstanding issues, in particular the financing of infrastructure in the western New Model Colony.			
13. Downtown Core Catalyst Project The City of Ontario has embarked on a strategy for a large-scale undertaking that would act as the catalyst for the resurgence of Downtown Ontario. The City of Ontario was awarded one of only 13 prestigious Catalyst awards from the State of California in 2010 for efforts to revitalize downtowns through this strategy. Upon completion of all of the activities included in the Downtown Core Catalyst, 519 housing units will be developed.	Objectives: Continue to implement the programs identified in the Downtown Core Catalyst Project as funding is available. Responsible Agencies: Housing and Neighborhood Revitalization Agency Funding: State and federal Timing: By 2018	The Catalyst program was completed in 2017. During the Catalyst program, 375 of the anticipated 519 housing units were completed in the Downtown area and the City met all data collection and monitoring requirements of the program. This program did not provide funding for development or program implementation and with the loss of redevelopment funds, the remainder of the planned development was unable to be completed during the project term.	Delete
The City implements a design review program to ensure quality housing, maintain property values, stabilize neighborhoods, and improve quality of life. For standard projects, the City's Residential Design Guidelines provide objective standards and graphics to illustrate the preferred methods of planning, neighborhood design, and construction for subdivisions, open space and landscaping, lots and buildings, architecture, and other aspects. For certain infill projects in the Downtown or other focus areas of the community, the City may adopt a PUD ordinance or Planned Residential Development Overlay to provide for more flexibility in design. Specific plans provide another means to address the design of large-scale projects. The General Plan includes a Community Design Element that provides unifying and broader principles of community design.	Objectives: Continue to implement design review process. Responsible Agencies: Planning Department Funding: General Fund Timing: Ongoing	City staff continues to implement a design review process. The City is in the process of updating its The Ontario Plan, including the Policy Plan. The update includes Objective Design and Development Standards for single-family residential, multifamily residential, and mixed-use developments.	Continue and modify to refer to Objective Design and Development Standards that are being developed with the General Plan Update that is currently in process as of July 2021. Update program name to "Senate Bill 2 Implementation"
15. Green Building Green building means creating structures and using materials that are environmentally responsible and resource efficient, considering a building's entire life cycle. To reduce per capita energy use, the City will promote conservation and renewable energy generation techniques in public facilities and private	Objectives: - Promote green building practices in the private sector and explore point-of-sale energy retrofits for residences. - Renewable energy incentive and energy efficiency programs.	The City continues to encourage opportunities in the private sector for point-of-sale retro-fits. The City is in the process of updating its General Plan, including the section on energy discussion. The City continued to trend towards the target of 30% greenhouse gas reduction below Year 2020 business as usual by Year 2020. The City is actively	Continue





Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
development. The City will require new construction to reduce energy demand by incorporating building and site design strategies. Conservation will be the priority strategy for renovation of existing facilities. The General Plan also includes land planning strategies that impact energy demand reduction, including narrowing street widths, installing broad-canopied trees for shade, and clustering compact development to reduce automobile use.	Develop a citywide 20-year energy plan. Support pilot development project as a net-zero-energy community and formulate solar site orientation guidelines. Responsible Agencies: Planning/Building/Public Works Funding: General Fund Timing: Ongoing	participating in the TCC Ontario Shines Program that has resulted in over 24 low-income solar installations. Overall, City consumption of potable water has been reduced by 21% through a combination of water use reduction, transition to recycled water irrigation systems, and drought-tolerant landscaping. The City is continuing to reduce emissions through participation in GGRF Cal Fire Grant that provided additional planting of 150 tress acting as a carbon sink and supporting reductions in the heat island effect.	
The City is in the process of updating the Development Code for consistency with the Land Use designations of The Ontario Plan. This program will implement a land monitoring program to ensure that the City has enough land to meet its Regional Housing Needs Allocation, through out the planning period. The City has identified 83 acres to be rezoned to allow development to occur at a density of 25–45 dwelling units per acre. This program will ensure that the proposed sites are rezoned to appropriate densities and identify additional sites to be rezoned if any of the proposed sites cannot be rezoned. All rezoned sites will permit owner-occupied and rental multi-family developments by right and will not require a conditional use permit, a planned unit development permit, or any other discretionary review. All sites will accommodate a minimum of 20 units per acre and at least 16 units per site, per state law requirements. In addition, the City will ensure that at least 50% of its lower- income RHNA shortfall is accommodated on sites designated for exclusively residential uses.	Objectives: Ensure there is a sufficient supply of multi-family zoned land to meet the housing needs identified in the Regional Housing Needs Allocation. Responsible Agencies: Planning Funding: General Fund Timing: Within the first three years of the planning period.	The Development Code was updated for consistency with Land Use Designations in 2018 or 2019. The rezoning required to meet the 5th cycle Regional Housing Needs Allocation (RHNA) was completed. City staff monitors entitlement applications to ensure that the Available Land Inventory is maintained and verifies that development of identified sites complies with the minimum density indicated in the Available Land Inventory or identifies alternate sites to meet the City's RHNA needs. Safeguards have been incorporated into the City's Discretionary Permit Application, which includes an affidavit regarding compliance with the Available Land Inventory.	Continue and update, include No-Net Loss and, if necessary, another rezone program.

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
	Governmental Regulatio	ns/Constraints	
 17. Incentives The City of Ontario offers several different types of incentives to facilitate housing production, including: Financial Incentives: The City makes available financial incentives that meet certain criteria. For instance, impact fee reductions are allowed for projects built in the Downtown. The City is financially assisting a variety of nonprofit organizations to provide senior housing, housing for homeless people, and other services. Density bonuses allowed for qualified projects work as a financial incentive by increasing the revenue stream of projects. The City also has established its Community Housing Development Organization (CHDO) program to leverage the nonprofit sector resources with available HOME CHDO funding. The intent of the CHDO funding is to work with nonprofit CHDOs to help preserve, enhance, and improve existing neighborhoods through acquisition, rehabilitation, and/or new housing construction activities. Finally, the City continues to grant low-cost leases (e.g., \$1 per year leases) to qualified organizations to provide senior housing and homeless housing. These types of financial incentives will be provided to allow the City to meet its community development and housing objectives. Regulatory Incentives: The regulatory incentive program is intended to realize improved value, a rich palette of amenities, landmarks, and identifiable places. While the underlying land use designations still apply, the City may offer various incentives through a discretionary permit. Special incentives may be granted for mixed-use developments, residential 	Objectives: Offer financial and regulatory incentives for residential projects that meet City housing and affordable housing goals. Responsible Agencies: Housing and Neighborhood Revitalization Agency Funding: General Fund Timing: Ongoing and at least annual outreach to developers of affordable housing, including non-profit.	The City continues to offer financial incentives for affordable housing projects where feasible and as funding is available. Housing incentives have also been included in the comprehensive Development Code update, which was adopted in 2016.	Continue





Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
infill projects near transit facilities, the replacement of underperforming commercial uses with new residential use, the improvement and/or intensification of existing, mid-block residential uses, or lot consolidation and development of desired projects. The menu of incentives may include density transfers, modifications in development standards, increased residential density, and other incentives to be negotiated on a case-by-case basis.			
Land Acquisition Land acquisition for residential development is perhaps one of the greatest challenges to creating affordable housing. Over the past five years, the City of Ontario has seen increasing land prices. To facilitate the development of affordable housing, the City has actively purchased land and made it available at a low cost (typically a \$1 per year lease) to affordable housing developers and nonprofit agencies to create affordable senior housing, emergency shelters, affordable attached ownership projects, and other affordable housing projects. As situations merit and projects are proposed that meet the City's housing goals and the public interest, the City of Ontario will continue to acquire residential land that can be leased or sold at below-market rates for the production of affordable housing.	Objectives: Continue to approve financial incentives for residential projects that meet City housing and affordable housing goals. Responsible Agencies: Ontario Housing Authority Funding: General Fund, NSP3, and other funding as available Timing: Ongoing	City staff continues to implement programs as funds become available. The City has acquired some sites for affordable housing development and then sold those properties to qualified affordable housing developers, including sites for Emporia Place Phase I, Emporia Place Phase II, and Vista Verde.	Continue
19. Planned Unit Development (PUD) Within an established suburban fabric, there are considerable challenges to creating affordable housing. As development standards and lot standards change over time, it is not uncommon to have irregularly shaped and nonconforming parcels that are simply not conducive to redevelopment. The City has adopted a Planned Unit Development Ordinance that permits a variety of housing types in every residential zone. The City may conditionally permit attached and	Objectives: Continue to utilize the PUD to create tailored development standards to facilitate new housing. Responsible Agencies: Planning Department Funding: General Fund Timing: Ongoing, 2010	The PUD continues to be a viable tool to implement new multifamily housing. Building permits for multifamily homes by year include 2 in 2014, 0 in 2015, 98 in 2016, 81 in 2017, 246 in 2018, 458 in 2019, and 188 in 2020, for a total of 1,173.	Continue

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
detached single-family residences, town homes, patio homes, zero lot line, and any other type of housing product permitted by the regulations of the underlying zone. The PUD is a tool that has been successfully used for Town Square to encourage and facilitate innovative design, variety, and flexibility in the types of housing products, including the provision of affordable housing, that would otherwise not be allowed or possible through standards in the underlying zoning districts.			
20. Mixed-Use and High-Density Residential Zone and Standards The General Plan directs significant housing growth to mixed-use areas. These areas include the Downtown, Euclid Avenue, the I-10 Corridor, the New Model Colony, and Holt Boulevard. These mixed-use areas each have a distinct mix of land uses and density ranges (see Policy Plan Land Use Exhibit LU-11, Land Use Designation Summary Table). To facilitate the development of quality housing and exemplary design, the City will create mixed-use zoning and development standards allowing up to 125 units per acre and a high-density residential zone and standards allowing 25 to 45 units per acre. The parameters of the ordinance have yet to be designed; however, the intent of the ordinance is to facilitate high-density housing. In both these zones, high-density residential and mixed use will be allowed by right.	Objectives: Develop new mixed-use and high- density residential development zone and standards to implement the General Plan. Allow residential uses by right in both zones. Responsible Agencies: Planning Department Funding: General Fund Timing: 2014	New General Plan land use designations were adopted in 2010. The 2016 comprehensive update to the Development Code implements the General Plan land use designations and allows residential uses by right within the High-Density Residential and Mixed-Use zones. The City continues its efforts in processing zone changes to bring alignment with the adopted General Plan. In 2020, no additional parcels were rezoned to High-Density Residential or Mixed-Use zones.	Continue and modify to refer to Objective Design and Development Standards that are being developed with the General Plan Update that is currently in process, as of July 2021.



Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
	Housing Assist	ance	
21. Public Housing The Housing Authority of the County of San Bernardino administers the Housing Voucher rental program for the City of Ontario. Funded by the US Department of Housing and Urban Development, the Housing Voucher program extends rental subsidies to very low-income households by offering the tenant a voucher that pays the difference between the current fair market rent (FMR) established by the Housing Authority and 30 percent of the tenant's income. A tenant has the option to choose housing that costs more than the FMR, if the tenant pays the extra rent above the payment standard. The Housing Authority also implements the scattered site program, Family Self-Sufficiency program, Section 8 project-based assistance, and HUD-assisted multiple-family housing units. This program serves up to 600 individuals and families in the City of Ontario.	Objectives: Continue to assist up to 600 households under the public housing program and seek additional vouchers as available. Responsible Agencies: Housing Authority of the County of San Bernardino Funding: US Department of Housing and Urban Development Timing: Ongoing	Public housing programs in Ontario are administered through the Housing Authority of the County of San Bernardino (HACSB). Within Ontario, the approximate number of Housing Choice Vouchers have been available: 496 in 2016, 422 in 2017, 527 in 2018, 676 in 2019, and 747 in 2020.	Continue, add mobility objectives and commitment to facilitating the movement of voucher holders to areas of high opportunity and resources.
22. Homeownership The City has a broad-based homeownership program for residents. The City uses a combination of funds (BEGIN, HOME, CalHome, and other available funding) to provide down payment assistance to homebuyers seeking to purchase homes in Ontario. The City of Ontario also works in conjunction with Neighborhood Partnership Housing Services (NPHS), a nonprofit organization, and the Inland Fair Housing and Mediation Board (IFHMB) to further the City's homeownership goals through homebuyer education, counseling, and down payment assistance.	Objectives: Implement down payment assistance programs citywide. Responsible Agencies: Housing and Neighborhood Revitalization Funding: HCD, BEGIN, CalHOME Timing: Ongoing	In 2016, the City secured \$1 million in CalHome Mortgage Assistance Program funds to offer down payment assistance to qualified low-income families, assisting a total of one household. The CalHome Mortgage Assistance program ended in 2017. City staff continue tracking the use of loan funds paid off in a reuse account for use on eligible projects. The City is planning to use a portion of the Permanent Local Housing Allocation (PLHA) funds for first-time homebuyer programs in conjunction with reuse funds on hand from the CalHome and BEGIN programs formerly offered. Programs are currently in design development to determine income targeting and benefits.	Continue and modify

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Table 6-1 Review of Previous Housing Element

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Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
23. Preservation of At-Risk Housing The City maintains more than 1,500 units of rental housing affordable to seniors, families, and individuals earning lower incomes. The City is committed to preserving its stock of affordable housing, some of which is at risk of conversion and/or needs significant renovation and improvement. The City remains committed to preserving its affordable housing and will monitor the status of the affordable housing projects, provide technical assistance, and consider appropriate actions should these projects be at imminent risk of conversion.	Objectives: Monitor the status of at-risk projects and, if they are at imminent risk of conversion, provide technical assistance and/or financial assistance to preserve the properties as deemed feasible. Responsible Agencies: Ontario Housing Authority Funding: Federal government Timing: Ongoing	The 2013-2021 Housing Element reported the existence of one development with affordable units that were at risk of converting to market rate within 10 years of the start of the planning period. That development, the Ontario Townhouses project, a project-based Section 8 voucher property, was preserved in 2020. City staff worked with the National Foundation for Affordable Housing to assist with the rehabilitation of the Ontario Townhouses project, a project-based Section 8 voucher property, including an extension of the Project Based Vouchers (PBV) contract for an additional 20 years. The project was completed during early 2020.	Continue
		As of September 2021, there are a total of 3,539 assisted, multifamily rental units in the city, of which, 460 units were "at-risk" of conversion to market rate. To address the preservation of public housing for very low- and low-income persons, the City maintains contact with owners of at-risk units as the use restriction expiration date approaches to communicate with the owner the importance of the units to the supply of affordable housing in Ontario, as well as its desire to preserve the units as affordable. The City will make every effort in using local incentives that can be offered to property owners to preserve any at-risk units.	
24. Jack Galvin Accord The City of Ontario has more than 2,100 mobile homes, which provide affordable market-rate housing for lower-income families, seniors, and individuals. In 1990, the City Council adopted an ordinance to regulate mobile home space rents but later repealed that ordinance per state law. Subsequently, in working with mobile home park owners and tenants, the City drafted the Jack Galvin Mobile Home Park Accord, which was accepted by park owners. The accord places limits on the allowable increases based on the Consumer Price Index; allows for additional adjustments for changes utilities, taxes, and capital improvements; provides a process for requesting rent	Objectives: Continue to implement the Jack Galvin Accord and monitor the effectiveness of the accord. Responsible Agencies: Housing and Neighborhood Revitalization Funding: General Fund Timing: Ongoing	City staff administered the Accord that covers 1,697 mobile home units in 10 mobile home parks throughout Ontario. City staff distributed the annual rent adjustments allowed as part of the Accord and designed to limit rental increases within the participating mobile home parks. The Accord was approved for a 5-year extension on November 15, 2019, with an expiration date of January 5, 2025.	Continue



Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
reductions for service reductions; and allows for rent adjustments for resale. The term of the agreement was adopted in 1999, and per extensions continues in effect today. The City will continue to implement and enforce this ordinance.			
	Special-Needs H	ousing	
Ontario is committed to furthering fair housing opportunities so that people in all walks of life have the opportunity to find suitable housing in the community. To that end, the City contracts with a fair housing service provider to provide landlord/tenant education, conduct testing of the rental and ownership market, and investigate and mediate housing complaints where needed. The City periodically prepares the required federal planning reports, including the Analysis of Impediments to Fair Housing Choice (Al), to document the City's progress in improving and maintaining fair housing opportunities. As part of the Al update, the City will review its Municipal Code, local government regulations, and other practices such as the definition of a family. Recommendations will be made to eliminate potential constraints and further fair housing in Ontario.	Objectives: - Continue to contract with local fair housing providers to provide educational, outreach, advocacy, and mediation services. - Conduct Al concurrently with the development of the Consolidated Plan, and review and change potential impediments, including the definition of a family. - Provide fair housing information at City Hall, the Ontario Senior Center, and the Ontario Housing Authority. Responsible Agencies: Ontario Housing Authority Funding: CDBG Timing: Ongoing	The City of Ontario has worked in conjunction with the Inland Fair Housing and Mediation Board to affirmatively further fair housing opportunities in this community. The Inland Fair Housing and Mediation Board "actively supports and promotes freedom of residence through education, advocacy, and litigation to the end that all persons have the opportunity to secure the housing they desire and can afford, without regard to their race, color, religion, gender, sexual orientation, national origin, familial status, marital status, disability, ancestry, age, source of income or other characteristics protected by law." The definition of the family has been updated to one or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit. In addition, the City has provided fair housing information at City Hall, the Ontario Senior Center, and the Ontario Housing Authority.	Continue and update to comply with Assembly Bill 686. Modify to remove reference to definition of family.
26. Homeless Continuum of Care The City implements a Homeless Services Continuum of Care to prevent homelessness and assist people in becoming self-sufficient. Working together with homeless service providers, the City has developed a full-service homeless continuum of care consisting of a homeless outreach service center, transitional housing, permanent housing, and supportive housing services. The City funds other programs that assist homeless people utilizing Emergency Solutions Grant funds.	Objectives: Continue to fund Mercy House to implement the Continuum of Care program for homeless residents and other programs as funding is available. Responsible Agencies: Housing and Neighborhood Revitalization Funding: Federal funds and private financing Timing: Ongoing	The City's Continuum of Care implements programming for homeless residents. The Mercy House Ontario Access Center has provided basic needs and services to the following number of (unduplicated) clients: 1,385 in 2016, 1,041 in 2017, 809 in 2018, 683 in 2019, and 744 in 2020. The Assisi House and Aftercare Services Program has provided transitional housing and aftercare services to the following number of (unduplicated) clients: 47 in 2016, 59 in 2017, 38 in 2018, 59 in 2019, and 50 in 2020.	Continue

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Table 6-1 Review of Previous Housing Element

Review of Frevious Housing Element			0 11 12 110 15 1 1
Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
The City also actively participates in regional homeless efforts, including the Interagency Council on Homelessness, which is a countywide effort of governmental and nonprofit organizations working to		HOME Tenant-Based Rental Assistance (TBRA) has provided tenant-based rental assistance to the following number of households: 15 in 2016, 17 in 2017, 23 in 2018, 25 in 2019, and 34 in 2020.	
end homelessness within the County of San Bernardino.		Through HUD's Supportive Housing Program, Project Gateway has helped secure permanent housing with wrap-around services for chronically homeless individuals with disabilities and their families. The following number of households have been served: 13 in 2016, 12 in 2017, 12 in 2018, 12 in 2019, and 13 in 2020.	
		In cooperation with Ontario Housing Authority, Mercy House, and Mercy House CHDO, a total of 76 permanent housing units continue to be provided for priority occupancy to participants in the CoC.	
		Ontario has created new programs to assist in the delivery of services designed to house persons experiencing homelessness within the city. In Fiscal Year 2019-20, the funding for the street outreach team was increased from 10 hours per month to 40 hours per week.	
		The Extreme Weather Motel Voucher Program assisted 11 persons with a total of 58 bed nights in 2019, and 14 persons in 2020. The Program was adapted to begin the COVID-19 motel voucher program in March 2020, serving 57 households from March 2020 to September 2020. The Emergency Motel Voucher Program was implemented in November 2020 to provide shelter to unhoused individuals and families. During Fiscal Year 2020-21, 92 households were served by the Emergency Motel Voucher Program. All individuals assisted are provided with the opportunity for case management focused on connecting the individuals to housing.	
		The LMIHF Utility Assistance Program assists persons experiencing homelessness with \$0 income to participate in the existing HOME TBRA program operated as part of the CoC. This program was	





Table 6-1 Review of Previous Housing Element

Program	Program Implementation Result/Effectiveness Continue/Modifi		
Program 27. Senior Housing	Implementation Objectives: Continue to provide a full range of	 Result/Effectiveness canceled in March 2020 in an effort to focus resources on housing unsheltered persons during the COVID-19 pandemic. The City partnered with a local school district to identify homeless families and assist these families with rental subsidies through the HOME TBRA Program. The City facilitated monthly meetings with Ontario focused homeless providers and governmental agencies to coordinate services to transition individuals/families from homelessness into a stable housing program. The COVID-19 Rapid Re-Housing Program finds housing solutions for persons at-risk of homelessness during the pandemic. During 2020, 2 households received assistance. 	Continue/Modify/Delete Continue
The City is actively working with nonprofit housing groups to build senior housing projects in the community. In addition to facilitating housing construction, the City also provides a range of supportive services for seniors. These include fair housing services, housing rehabilitation grants, preservation of subsidized senior housing, low-cost transportation services, and a range of other services tailored to meet the unique needs of Ontario's senior population.	housing support services. Responsible Agencies: Housing and Neighborhood Revitalization Funding: State and federal funds Timing: Ongoing	senior housing. The City continued to work with non-profit housing groups to build senior housing projects in the community. The City provided a range of supportive services for seniors. Between 2013 and 2020, 182 seniors were assisted with fair housing issues, 1,008 seniors were assisted with landlord/tenant mediation, 1,964 seniors were assisted with support services, 2 seniors received housing rehab grants, 9 seniors received tenant-based rental assistance, and 782 units of affordable housing were restricted for seniors.	Cultinue
28. Housing for People with Disabilities The City enforces state and federal accessibility laws to facilitate the improvement of housing for disabled people. The City also prepares a Transition Plan to comply with state and federal accessibility laws. The City has adopted a reasonable accommodation process and administratively allows modifications to land use, building codes, and the permitting process to facilitate the reasonable accommodations without going through a standard variance process. However,	Objectives: - Continue to assist with the development of housing for persons with disabilities, including those with developmental disabilities. - Update the definition of family to comply with state law. Responsible Agencies: Building and Planning Department	The City enforces state and federal accessibility laws to facilitate the improvement of housing for persons with disabilities and encourages reasonable accessibility accommodations. The definition of the family has been updated to one or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit. The City's Tenant-Based Rental Assistance (TBRA) Program has served 17 disabled households since inception in 2014. For the next cycle, the definition of a	Continue and modify to remove reference to definition of family. Add reference to Inland Regional Center.

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
given the large number of people with disabilities, the growing need for housing opportunities, and changing legal context for housing planning, additional efforts are needed. Many homes were built before the advent of modern accessibility standards and thus many homes remain inaccessible to people with disabilities and persons with developmental disabilities. To address this issue, the City will evaluate the feasibility and appropriateness of modifying building standards to encourage visitability concepts in new housing. Additionally, to ensure compliance with state law, the City will update its definition of "family" to state "One or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit."	Funding: General Fund Timing: Ongoing, update the definition of family within one year of adoption of the Housing Element.	family will be changed to one or more persons living together in a dwelling unit, with common access to, and common use of all living, kitchen, and eating areas within the dwelling unit. This program will continue to assist with the development of housing for persons with disabilities, including those with developmental disabilities.	
Ontario has a large number of family households, specifically large families with five or more members. The City has a multifaceted program for increasing and maintaining the supply of family housing. The Housing Authority of San Bernardino County allocates housing choice vouchers to lower-income families in Ontario, many of whom are large families. Another key effort is the City's program to acquire, rehabilitate, and preserve existing affordable housing units that accommodate families and large families. Over the past five years, the City and the Housing Authority have preserved the vast majority of publicly subsidized affordable units for families. Finally, the City funds through its Community Development Block Program programs such as child care, after-school programs, food programs, and other services targeted for lower-income households, including large families.	Objectives: Continue program implementation. Responsible Agencies: Housing and Neighborhood Revitalization, Housing Authority of the County of San Bernardino Funding: General Fund, CDBG Timing: Ongoing	The City continues to monitor 1,228 units of affordable family housing. In addition, Ontario worked with two different developers to construct two affordable housing developments with construction that were completed during Fiscal Year 2020-21 - Emporia Place (75 units) and Vista Verde (101 units). These two new developments will provide for extremely low-, very low-, and low-income units for families.	Continue





Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
30. Extremely Low-Income Households The City offers programs to address the housing needs of extremely low-income (ELI) households. As funding is available, the City provides a number of incentives to encourage the production of ELI housing. The City offers fee reductions for ELI housing, supports grant applications to increase the supply of affordable housing, works with nonprofit organizations to build affordable housing, and provides land writedowns.	Implementation Objectives: - Work with nonprofits and/or for-profit developers to build housing for ELI households through supporting grants and funding applications. - Offer fee reductions and land writedowns for new affordable housing for low-income, very low-income, and ELI households. Responsible Agencies: Planning Department, Housing and Neighborhood Revitalization Funding: CDBG, HOME, federal and state grants Timing: Annually	Result/Effectiveness The City continues to provide housing assistance to extremely low-income households. The City has restricted the following number of housing units for extremely low-income occupants: 12 in 2016, 12 in 2017, 12 in 2018, 20 in 2019, and 20 in 2020. Under implementation of Project Gateway, the following number of housing units were occupied by extremely low-income households: 8 in 2016, 10 in 2017, 10 in 2018, 6 in 2019, and 13 in 2020. The HOME Tenant Based Rental Assistance (TBRA) has assisted the following total number of households: 9 in 2016, 11 in 2017, 14 in 2018, 13 in 2019, and 34 in 2020. The CoC Permanent Housing (excluding Project Gateway and HOME TBRA) has assisted the following number of households: 6 in 2016, 6 in 2017, 6 in 2018, 2 in 2019, and 2 in 2020. The Assisi House and Aftercare Services program has served the following number of (unduplicated) persons: 47 persons in 2016, 29 in 2017, 38 in 2018, 59 in 2019, and 50 in 2020. The Ontario Access Center has served the following number of (unduplicated) persons: 1,385 in 2016, 1,039 in 2017, 808 in 2018, 683 in 2019, and 744 in 2020. Foothill Family Shelter has assisted the following number of (unduplicated) persons: 12 in 2016, 26 in 2017, and 7 in 2018. Services for Victims of Domestic Violence and Their Children has assisted the following number of (unduplicated) persons: 54 in 2016, 25 in 2017, 74 in 2018, 52 in 2019, and 86 in 2020.	Continue/Modify/Delete Continue

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Table 6-1 Review of Previous Housing Element

Program	Implementation	Result/Effectiveness	Continue/Modify/Delete
, rogram	Importation	(unduplicated) persons: 2,444 in 2016, 2,589 in 2017, 2,503 in 2018, 1,890 in 2019, and 2,776 in 2020.	Sommuonnoun yr Dollete
		Fair Housing services has assisted the following number of (unduplicated) households: 101 in 2016, 71 in 2017, 106 in 2018, 105 in 2019, and 126 in 2020.	
		Landlord-Tenant Mediation services has assisted the following number of (unduplicated) households: 863 in 2016, 740 in 2017, 718 in 2018, 539 in 2019, and 892 in 2020.	
		Senior Services has assisted the following number of (unduplicated) persons: 142 in 2016, 133 in 2017, 166 in 2018, 122 in 2019, and 157 in 2020.	
		Child Care Subsidies has assisted the following number of (unduplicated) persons: 18 in 2016, 17 in 2017, 6 in 2018, 14 in 2019, and 56 in 2020.	
In implementing affordable housing programs, the City will work with housing providers to ensure that special housing needs are addressed for seniors, large families, female-headed households, single-parent households with children, persons with disabilities and developmental disabilities, homeless individuals and families, and farmworker families. The City will seek to meet these special housing needs through a combination of regulatory incentives, zoning standards, new housing construction programs, housing rehabilitation, homebuyer assistance programs, and supportive services programs. In addition, the City may seek funding under the federal Housing Opportunities for Persons with AIDS, California Child Care Facilities Finance Program, and other state and federal programs designated specifically for special needs groups such as seniors, persons with disabilities, and persons at risk for homelessness.	Objectives: Collaborate with affordable housing developers and secure funding, if feasible, to assist with the development of special needs housing projects. Responsible Agencies: Planning Department, Housing and Neighborhood Revitalization Funding: CDBG, HOME, federal and state grants Timing: Annually	In conjunction with public agencies and community organizations, the following number of Supportive Housing Program project-based vouchers are available each year for chronically homeless with disabilities and their families: 12 in 2016, 12 in 2017, 12 in 2018, 12 in 2019, and 13 in 2020. The City, in conjunction with Mercy House Living Centers, implemented the HOME Tenant-Based Rental Assistance to provide rental assistance and assistance with security deposits and utility deposits to chronically homeless individuals and households. Each year, the following number of homeless households have received assistance for permanent housing: 15 in 2016, 17 in 2017, 23 in 2018, 25 in 2019, and 34 in 2020. The City continues to pursue funding under the federal Housing Opportunities for Persons with AIDS, California Child Care Facilities Finance Program, and other state and federal programs designated specifically for special-needs groups such as seniors, persons with disabilities, and persons at risk for homelessness.	Continue



7 HOUSING ELEMENT OUTREACH

The City of Ontario conducted a housing element outreach program that included a combination of public meetings, consultations, and surveys.

2020-2024 Consolidated Plan

The community outreach process for the preparation of the 2020-2024 Consolidated Plan and 2020-2021 One-Year Action Plan offered numerous opportunities for public input, comment, and review. The City hosted and coordinated a public hearing, three community forums, resident surveying, and stakeholder consultations. The culmination of public input provided the City with priority areas to help address housing needs and homelessness.

Public Outreach

A public hearing was held on February 3, 2020, to provide the public an overview of the process and components of a Consolidated Plan and for the public to submit comments and input on the Community Needs Assessment Survey. Bilingual staff was available for translation. Display advertisements and legal advertisements to promote the public hearing were published on January 9 and January 17, 2020, in the local newspaper *Inland Valley Daily*.

Three community forums were held on February 19, 2020, at Veterans Memorial Park Community Center, February 25, 2020, at Dorothy Quesada Community Center, and February 28, 2020, at De Anza Park Community and Teen Center. Residents were provided with the Community Needs Assessment Survey and information about the Consolidated Plan. Attendees were invited to participate in an engagement activity where residents selected their highest priority in select categories among activities eligible for Community Development Block Grant (CDBG), HOME, and Emergency Solutions Grant (ESG) funding.

The City held a community fair on February 29, 2020, with over 500 residents in attendance. At the Housing department booth, attendees were invited to complete the Community Needs Assessment Survey and participate in the engagement activity presented at the community forums.

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Stakeholder Consultations

The City consulted with 18 stakeholder groups representing public and private entities, service providers, or community organizations. Table 7-1 provides more information on the stakeholder consultation process, including the type of stakeholder, specific housing topics discussed, and anticipated outcomes of the consultation.

Table 7-1 Stakeholder Consultations

Agency/Group/ Organization	What section of the 2020-2024 Consolidated Plan was addressed by Consultation?	How was the Stakeholder consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?
Ontario Housing Authority	Housing Need Assessment Public Housing Needs	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing affordable housing.
Neighborhood Partnership Housing Services, Inc.	Housing Need Assessment	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination with first-time homebuyer and owner-occupied rehabilitation loan programs, and other programs and services relating to homebuyer education, financial literacy, and homeowner assistance.
AOF/Golden State Community Development Corp.	Housing Need Assessment	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination with a possible Community Housing Development Organization (CHDO).
Mercy Housing Living Centers	Homelessness Strategy Homeless Needs – Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing homeless services and permanent supportive housing.
Inland Fair Housing and Mediation Board	Housing Need Assessment Non-Homeless Special Needs Fair Housing Strategy	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing increased services for senior citizens, fair housing services, landlord/tenant mediation services, fair housing education, testing, and enforcement, first-time homebuyer education, and financial literacy programs.
County of San Bernardino Office of Homeless Services	Homelessness Strategy Homeless Needs – Chronically homeless Homeless Needs - Families with children Homelessness Needs - Veterans Homelessness Needs - Unaccompanied youth	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing homeless services in the region, HMIS services, Continuum of Care coordination, and coordinated assessment system.
Ontario-Montclair YMCA	Non-Housing Community Development Strategy	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination of childcare for affordable housing residents.
Ontario-Montclair School District	Homeless Needs - Families with children	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Identification and coordination of services for homeless families within the school district.



Table 7-1 Stakeholder Consultations

Agency/Group/ Organization	What section of the 2020-2024 Consolidated Plan was addressed by Consultation?	How was the Stakeholder consulted and what are the anticipated outcomes of the consultation or areas for improved coordination?
Ontario Senior Center	Non-Homeless Special Needs	Invited to participate in community meetings/public hearings. Coordination of Homeowner Energy Assistance Program (HEAP) services and social services for senior residents within affordable senior housing projects.
Rolling Start, Inc.	Non-Homeless Special Needs	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination of Section 504 outreach and identifying potential participants for affirmative marketing outreach for affordable housing units.
San Bernardino County Department of Behavioral Health	Housing Need Assessment Homelessness Strategy Homeless Needs – Chronically homeless	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination of Project Gateway (Shelter Plus Care vouchers) and proactively working together to secure additional resources to serve Ontario residents.
Foothill AIDS Project	Housing Need Assessment Homelessness Strategy	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination and identification of potential participants for affirmative marketing outreach for affordable housing units.
County of San Bernardino Children and Family Services	Non-Housing Community Development Strategy	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination for potential bridge housing resources.
Inland Valley Hope Partners	Homelessness Strategy Homeless Needs – Chronically homeless Homeless Needs - Families with children	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing services for the homeless and those at-risk of homelessness and identification of potential participants for affirmative marketing outreach for affordable housing units.
House of Ruth, Inc.	Homelessness Strategy Homeless Needs - Families with children	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination of providing services for victims of domestic violence and potential identification of participants for affirmative marketing outreach for affordable housing units.
State of California Housing and Community Development	Housing Need Assessment	The City reviews all possible leveraging resources and will investigate opportunities to use various programs that will fund identified community needs (Affordable Housing and Sustainable Communities (AHSC), Veteran Housing and Homeless Prevention (VHHP), Emergency Housing and Assistance Program Capital Development (EHAP-CD), and others). Actively support efforts to create permanent affordable housing funding sources.
Foothill Family Shelter	Homelessness Strategy Homeless Needs - Families with children	Invited to participate in community meetings/public hearings and a copy of the executive summary was provided for feedback and comments. Coordination in providing homeless services, transitional housing, and identification of potential participants for affirmative marketing outreach for affordable housing units.
Housing Authority of the County of San Bernardino	Public Housing Needs	Requested review of public housing needs section and a copy of the executive summary was provided for feedback and comments. Implement permanent supportive housing programs and work together to secure state and federal resources for the region.

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Summary of Priority Needs

The Community Needs Assessment Survey, in concert with the feedback at meetings and consultations, highlight the City's clear and detailed need for investment in affordable housing for both owner-occupied and renter-occupied households, programs for homeless persons, and homelessness prevention. Table 7-2 summarizes the city's housing needs, including level of priority, identified throughout the outreach process.

Table 7-2 Summary of Priority Needs

Need	Priority Level	Description	Basis for Priority Level
Affordable housing- rental assistance	High	Housing Choice Vouchers (Section 8) extend rental subsidies to very low-income households by offering the tenant a voucher that pays the difference between the current fair-market rent (FMR) established by the Housing Authority and 30 percent of the tenant's income.	Rental assistance programs are a high priority because they are a way to meet the pressing need for affordable housing. (See Program 23.)
Affordable housing- production of new units	High	Projects to produce new affordable housing units may be targeted to owner-occupied or renter-occupied housing types.	Production of new units is a high priority because they are a way to meet the pressing need for affordable housing. (See Programs 8, 9, 10, 11, 15, and 32)
Affordable housing- rehab of existing units	Medium	Rehabilitation of existing units could be targeted to owner-occupied or renter-occupied households. The programs would preserve the existing housing stock.	Rehab of existing units is important because preservation and maintenance are critical to maintain quality housing conditions. Nearly 60% of existing units were built prior to 1979, indicating a potential need for rehab. (See Programs 1, 3, 31, and 33)
Affordable housing – acquisition of existing units	High	The acquisition or preservation of existing units may assist in creating new affordable housing units or ensuring the continued affordability of units preserved.	Acquisition of existing units is a high priority because they are a cost-effective way to meet the pressing need for affordable housing (See Programs 16, 25, and 26)
Homelessness- outreach	High	The City has developed a Continuum of Care in Ontario that serves all segments of the homeless population and those at risk of homelessness. Outreach programs provide basic needs to homeless individuals and families and also provide appropriate referrals to services needed by specific populations, such as those with chronic substance abuse, victims of domestic violence, veterans, and persons with HIV/AIDS.	Outreach services are a high priority because they can provide the necessary first step toward the goal of transitioning someone from homelessness to stable housing. (See Programs 27, 28, 32 and 33)
Homelessness- emergency/ transitional shelter	High	The City has developed a Continuum of Care in Ontario that serves all segments of the homeless population and those at risk of homelessness. Emergency shelter and transitional housing programs provide temporary housing for homeless individuals and families.	An emergency/transitional shelter is a high priority because it can provide a temporary roof as someone works toward the goal of transitioning from homelessness to stable, permanent housing. (See Programs 17, 20, 27, 28, and 33)
Homelessness- rapid re-housing	High	The City has developed a Continuum of Care in Ontario that serves all segments of the homeless population and those at risk of homelessness.	Rapid re-housing is a high priority because it can provide a temporary roof as someone works toward the goal of transitioning from homelessness to stable, permanent housing. Ontario will continue to work with its partners at the Housing Authority for the County of San Bernardino and the County of San Bernardino



Table 7-2 Summary of Priority Needs

Need	Priority Level	Description	Basis for Priority Level
		Rapid re-housing programs provide a needed resource to quickly move those living on the streets or in shelters into permanent housing.	Department of Behavioral Health to provide rapid rehousing services. (See Programs 17, 20, 27, 28, and 33)
Homelessness- prevention	High	The City has developed a Continuum of Care in Ontario that serves all segments of the homeless population and those at risk of homelessness. Prevention services can often be the link to keep a household in their current housing and diminish the growth of the homeless population.	Services that keep someone who is at risk of becoming homeless in their home is a high priority because it meets a pressing need and is more costeffective than providing housing for someone once they've become homeless. (See Programs 17, 20, 27, 28, and 33)

Assessment of Fair Housing

The City of Ontario's outreach process for the Consolidation Plan also included outreach for fair housing. A broad array of outreach was conducted, such as community forums, focus groups, and public hearings to ensure that the analysis contained in the Fair Housing Assessment truly reflects conditions in a community and that the goals and strategies are targeted and feasible.

Stakeholder Consultations and Surveys

In preparation of the Fair Housing Assessment, the City reached out to a wide array of stakeholders to hear directly about fair housing issues affecting residents. Stakeholders included Spanish-speaking groups, tenants, homeowners, fair housing organizations, civil rights and advocacy groups, organizations serving people with disabilities (including physical disabilities and people with HIV/AIDs), organizations serving domestic violence survivors, social services providers, and homeless providers. All community meetings had translation services available in Spanish. In addition, all meetings were held in locations accessible to people with mobility issues.

Fair housing surveys were conducted in-person both in English and Spanish at the community meetings and community fair. The majority of respondents were members of protected classes. Of the 73 respondents, 21 found housing discrimination to be an issue in Ontario, and 14 directly experienced discrimination. Survey respondents cited race as the reason for discrimination, followed by color, familial status, national origin, and disability.

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Contributing Factors to Fair Housing Issues

The 2020 Assessment of Fair Housing Choice identified the following contributing factors to fair housing issues:

- Lack of meaningful language access for individuals with limited English proficiency (See Programs 24 and 27)
- Lack of affordable housing in moderate- and high-resource areas of the city (See Programs 13 and 23)
- Lack of investment/community revitalization strategies in lowresource areas of the city (See Programs 4, 10, and 27)
- Availability of rentals that accept HCV in moderate- and highresource areas of the city (See Program 23 and 31)
- Displacement of residents in moderate- and/or high-resource areas of the city because of economic pressure (See Programs 23, 27, and 32)
- Concentration of affordable housing in low-resource areas of the city (See Programs 13 and 23)
- Lack of access to opportunity due to high housing cost (See Programs 11 and 27)
- Instances of private discrimination (See Programs 20 and 27)
- Lack of accessible affordable housing appropriate for persons with disabilities (See Programs 3, 27, 30, and 33)
- Availability of affordable housing units in a range of sizes (See Programs 23 and 31)
- Age of housing stock in northwest area of the city (See Programs 1, 3, 16, 31, and 33)
- · Cost of home repairs (See Programs 1, 3, 16, 29, 31, and 33)
- Availability of affordable housing in the form of accessory dwelling units (ADUs) and junior accessory dwelling units (JADUs) (See Programs 20, 27)
- Lack of meaningful language access for individuals with limited English proficiency to learn about rehabilitation options (Program 27)



- Lack of investment/community revitalization strategies in lowresource areas of the city that would improve health outcomes for residents (See Programs 1, 4, 6, 10, and 27)
- Lack of investment/community revitalization strategies in lowresource areas of the City to improve economic outcomes for residents (See Programs 1, 4, 6, 10, and 27)

Fair Housing Goals and Priorities

The following goals and strategies will serve as an effective basis for affirmatively furthering fair housing by reducing patterns of segregation, mitigating displacement, addressing disproportionate housing needs, and increasing access to opportunity for members of protected classes.

Goal 1: Increase the supply of affordable housing in high opportunity areas.

Ontario has a significant portion of its residents who are rent-burdened and facing severe housing problems. Additionally, publicly supported affordable housing accounts for slightly less than 3 percent of the total housing stock, and Ontario and its environs are experiencing rapidly rising housing costs. Members of protected classes, particularly Hispanic and Black residents, experience these problems most acutely. These indicate a need to expand the supply of affordable housing. The following strategies address Goal 1.

- Explore the creation of new funding sources of affordable housing.
- Using best practices from other jurisdictions, explore policies and programs that increase the supply of affordable housing, such as linkage fees, inclusionary housing, public land set-aside, community land trusts, transit-oriented development, expedited permitting and review, and reduced building permit fees for nonprofit developers.
- Explore opportunities to provide low-interest loans to singlefamily homeowners and grants to homeowners with household incomes of up to 120 percent of the Area Median Income to develop ADUs with affordability restriction on their property.
- Align zoning codes to conform to recent California affordable housing legislation.

(See Programs 1, 6, 16, 24, and 27)

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Goal 2: Increase community integration for persons with disabilities.

There is a lack of permanent supportive housing for non-elderly persons with disabilities in Ontario. By prioritizing HOME funding for such projects, which should ideally set aside 10 to 25 percent of units for persons with disabilities who need supportive services, the City can help make development proposals more competitive for Low-Income Housing Tax Credit (LIHTC) and Mental Health Services Act assistance. The following strategy addresses Goal 2.

 Prioritize HOME funding for developments that include permanent supportive housing for non-elderly persons with disabilities.

(See Programs 3, 16, 27, and 33)

Goal 3: Ensure equal access to housing for persons with protected characteristics, who are disproportionately likely to be lower-income and to experience homelessness.

Although California law provides strong legal tools to combat source of income discrimination, some landlords violate these laws, as they do housing discrimination laws more generally. Targeted education efforts would help to reduce the incidence of unlawful source of income discrimination. Attendees at community stakeholder meetings were unaware that landlords are required to accept vouchers and third-party checks and would benefit from fair housing education. The following strategies address Goal 3.

 Conduct fair housing training for landlords and tenants on California's Source of Income Discrimination protections to reduce the number of voucher holders turned away.

(See Program 23 and 27)

Homelessness

The 2020 San Bernardino County Homeless Count identified 102 homeless persons residing in Ontario, including 74 persons unsheltered and 28 homeless individuals living in emergency shelters or transitional housing.

Neighborhood Preservation Strategy Plan

In 2019, the City developed the Neighborhood Preservation Strategy Plan (NPSP) to focus on improving the overall quality of life in neighborhoods and develop a nexus between the conditions of the neighborhood and the solutions to improve it. The NPSP focused on four target neighborhoods



where the NPSP could significantly address neighborhood conditions: Downtown, Nocta, Mission-Mountain, and Fourth-Grove neighborhoods. Each of these neighborhoods were identified based on demographics, land use data, and Community Improvement active cases, including outcomes from Systematic Health and Safety Inspection Program inspections, select My Ontario app reports, and calls for police service data. In reviewing the base conditions of these neighborhoods, the City determined that:

- All target neighborhoods have a lower median income than the city as a whole.
- Three of the four target neighborhoods have a higher proportion of renter-occupied households than owner-occupied households.
- The majority of the target neighborhoods have both renteroccupied and owner-occupied households that are experiencing housing cost burden at a rate higher than the city as a whole, where households are paying more than 30 percent of their income for housing costs.
- Two of the four target neighborhoods have a higher proportion of housing stock built prior to 1980 than the city as a whole.
- My Ontario App has the highest number of reports for debris in the public right-of-way among all reports evaluated for all four target neighborhoods.

To strengthen and improve quality of life in the four target neighborhoods, the City identified six strategic components to address over a three-year span: community engagement, neighborhood fairs, new resident marketing initiatives, establishment of the Neighborhood Action Team, evaluation of opportunities for affordable homeownership, and evaluation of community resources and infrastructure.

Strengthening community engagement is a primary objective of the NPSP. Community engagement strengthening efforts can be divided into three main components: capitalizing on existing community outreach and engagement, creating new events for engagement opportunities, and non-event based ongoing engagement in a variety of mediums. To accomplish this, the City will undertake the following actions.

The Community Improvement Department will work in collaboration with the Neighborhood Action Team to identify existing events, workshops, and meetings that impact the four target neighborhoods. These include Integrated Waste quarterly clean-up days, Ontario Night Out, Neighborhood Watch meetings, Crime-Free Multi-Housing meetings, Community Life

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and Culture events (Culture Fest, Arts Festival, etc.), Downtown Strategic Plan workshops, and Community Health Workers monthly community engagement forums.

- One neighborhood fair is planned to be held annually within targeted neighborhoods. This event will bring together various City, County, and School District agencies, along with business owners, the Ontario Chamber of Commerce, the Ontario Hispanic Chamber of Commerce, and community organizations for a oneday event designed to bring services and information to the residents within their neighborhood.
- The City will purchase and fund a new mobile recreation program, including the purchase of a customized vehicle, related supplies, and staffing for 1,500 hours of operation per year.
- The City will develop an initial community survey that will be distributed at workshops, meetings, and engagement events to residents within the target neighborhoods to identify the opinions and needs of those residents, with the purpose of developing more focused engagement and programs/projects within each target neighborhood.

(See Program 6)

Neighborhood fairs are a key strategy to educate neighborhood residents about city programs available to them, develop relationships between the City and residents, and obtain feedback from residents through day-of activities and surveys. The City will hold at least one "block-party" style neighborhood fair over the course of the three years. The City will partner with other public sector agencies that impact community life within the target neighborhoods, such as San Bernardino County Department of Public Health, San Bernardino County Workforce Development Department, Ontario-Montclair School District, and Chaffey Joint Union School District, private local business owners, including the Ontario Chamber of Commerce, Ontario Hispanic Chamber of Commerce, and community organizations and non-profit agencies that work in the community, such as Habitat for Humanity, Neighborhood Partnership Housing Services, Boy Scouts and Girl Scouts, Inland Fair Housing and Mediation Board, Neighborhood Partnership Housing Services, Rotary Club. and Kiwanis.

(See Program 6)



The Neighborhood Action Team is the implementing entity for this strategy. The core team will include staff members from Housing Department, Community Improvement Department, Ontario Police Department, Community Life and Culture, Public Works, Ontario Municipal Utilities Company, Information Technology, Management Services, Ontario Fire Department, San Bernardino County Department of Public Health, Ontario-Montclair School District, and Chaffey Joint Union School District. In addition to the monthly meetings and solutions or other options to address "flash point" properties or blocks, the Neighborhood Action Team will implement community clean-up programs to address property maintenance issues within the targeted neighborhoods. These include Property Clean-Up Programs, Neighbors Helping Neighbors Program, and Community Clean-Up Days.

(See Program 6)

An important component of this strategy will be identifying possible means to create opportunities to expand homeownership within the target neighborhoods and throughout Ontario. Housing Department staff, along with strategic community partners, have evaluated homeownership models, from down-payment assistance programs to innovative in-fill development opportunities that would promote more affordable homeowner housing. Each target neighborhood has undeveloped land that may be suitable for housing and during the course of the strategy, undeveloped lots will be evaluated to determine possible housing options for affordable homeownership. Homeownership models may include small-lot development and community land trusts. In addition, opportunities for funding down-payment assistance loans to assist lower-income households purchase their first home from the State of California or other federal resources will be explored to develop new homeownership assistance programs. To educate residents of the four target neighborhoods of tools available through the city, the NPSP includes developing educational material to distribute to new homeowners and renters within the target neighborhoods. Again, this will be a multi-agency effort to provide residents with information that would be helpful to new residents in the community.

The final component of the NPSP will be to implement a small commercial façade improvement program within Downtown Ontario.

(See Program 6)

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8. HOUSING GOALS AND POLICIES

The City of Ontario aspires to be the premier city of the Inland Empire. Building from the Ontario International Airport, the Ontario Ranch, the City's rich cultural and historic heritage, and transportation and economic assets, the City seeks to define a prosperous future through design. How we design our housing, neighborhoods, and community, and how we provide public services are critical to the achievement of that vision.

The City's vision is underpinned by four principles supporting Ontario as a unified and prosperous community:

- A dynamic balance that enables our community to confront the continued dynamic growth of the region and technological change with confidence and a sense of opportunity.
- A prosperous economy that sustains the reality of prosperity across our entire community that positively impacts all the people of Ontario.
- Distinctive development that integrates our varied and diverse focal points, districts, villages, and neighborhoods to provide a feeling of coherence without sacrificing uniqueness.
- Recognized leadership in local governance that stimulates excellence and serves to unify the people.

The Housing Element plays a critical role in achieving this vision. Housing Ontario residents and the workforce, creating quality neighborhoods of distinctive design, assisting residents with special needs, and responsibly accommodating growth and community development are fundamental to achieving the City's long-term vision of prosperity.

The Housing Plan sets forth goals and policies to achieve this end. This includes goals and policies for housing and neighborhood quality, housing diversity and supply, removal of governmental constraints, housing assistance, and special needs. Within this framework, this chapter proposes both existing and new programs to implement these goals and policies.

Table 8-1 at the end of the chapter lists the programs, key planning objectives, funding sources, implementing agency, time frame for implementation, and quantified program objectives, where feasible.



Goal 1: Neighborhoods and Housing

Ontario's neighborhoods determine our quality of life and reflect the value we place in our community. Neighborhoods differ in lot sizes, housing types, history, purpose, and environment. Whether rural residential, suburban, historic, or urban, Ontario's neighborhoods should provide a nurturing environment for all residents to enjoy their lives. Residential neighborhoods should provide quality housing, ample parks and recreational opportunities, tree-lined streets and sidewalks for walking, safety and security, and public facilities and services.

As an established community, Ontario is committed to improving its older neighborhoods. This goal may be achieved through redevelopment, housing rehabilitation, code enforcement, and neighborhood improvement projects. Ontario will facilitate the development of new neighborhoods consistent with their unique purpose, such as the Ontario Ranch, the Ontario Airport Metro Center, and other areas. Taken together, Ontario is committed to creating and strengthening neighborhoods to promote a high quality of life for residents.

Goal H1: Stable neighborhoods of quality housing, ample community services and public facilities, well-maintained infrastructure, and public safety that foster a positive sense of identity.

Policies

- H1-1 Housing Rehabilitation. We support the rehabilitation, maintenance, and improvement of single-family, multiple-family, and mobile homes through code compliance, removal of blight where necessary, and provision of rehabilitation assistance where feasible.
- H1-2 Neighborhood Conditions. We direct efforts to improve the long-term sustainability of neighborhoods through comprehensive planning, provision of neighborhood amenities, rehabilitation and maintenance of housing, and community building efforts.
- H1-3 Community Amenities. We shall provide adequate public services, infrastructure, open space, parking and traffic management, pedestrian, bicycle, and equestrian routes, and public safety for neighborhoods consistent with City master plans and neighborhood plans.

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- H1-4 Historical Preservation. We support the preservation and enhancement of residential structures, properties, street designs, lot configurations, and other reminders of Ontario's past that are considered to be local historical or cultural resources.
- H1-5 Neighborhood Identity. We strengthen neighborhood identity through creating parks and recreational outlets, sponsoring neighborhood events, and encouraging resident participation in the planning and improvement of their neighborhoods.

Goal 2: Housing Supply and Diversity

Bolstered by its International Airport, burgeoning employment sector, the Ontario Ranch, and unparalleled transportation access, Ontario aspires to be the urban center of the Inland Empire. Housing diversity is critical to achieving this goal. Ontario is committed to ensuring the provision of the widest range of housing choices for the varied lifestyles of its residents and future workforce. This includes single-family and multiple-family housing, mixed- and multi-use housing, senior housing, live-work units, and other types of housing opportunities.

Housing production is to be encouraged in a responsible manner that furthers citywide and neighborhood goals. New housing will be creatively designed, sustainable, and accessible. Residential and mixed-use growth is strategically directed to the Downtown, corridors, Ontario Airport Metro Center area, Ontario Ranch, and other areas. By encouraging an adequate supply and diversity of housing, Ontario will accommodate its changing housing needs, support economic prosperity, foster an inclusive community, and become the urban center of the Inland Empire.

Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

Policies

H2-1 Corridor Housing. We revitalize transportation corridors by encouraging the production of higher-density residential and mixed uses that are architecturally, functionally, and aesthetically suited to corridors.



- **H2-2 Historic Downtown**. We foster a vibrant historic downtown by facilitating a wide range of housing types and affordability levels for households of all ages, housing preferences, and income levels.
- H2-3 Ontario Airport Metro Center. We foster a vibrant, urban, intense, and highly amenitized community in the Ontario Airport Metro Center area through a mix of residential, entertainment, retail, and office-oriented uses.
- **H2-4 Ontario Ranch**. We support a premier lifestyle community in the Ontario Ranch, distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.
- **H2-5 Housing Design.** We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.
- **H2-6 Infill Development.** We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Goal 3: Governmental Regulations

The City is committed to facilitating and encouraging the production, maintenance, and improvement of housing in a responsible manner; however, various factors may limit the City's ability to address its housing needs, such as governmental regulations or environmental considerations. Market factors may also affect the feasibility of building housing or the affordability of housing in the community. Moreover, housing goals may at times conflict with the need to promote other important City goals, including open space or the provision of jobs for the region.

Whereas City land use policy and municipal codes provide a regulatory framework for addressing housing, existing regulations cannot address every situation. To facilitate the type of development desired and to realize the greatest community benefits, the City's regulatory framework must be flexible and incentive based. The development review process must be time sensitive, predictable, and thorough. The review process must support long-term community benefits, rather than just short-term gain. Finally, the regulatory framework must contain a broad range of incentives to stimulate desired development and private investment and realize the community features that improve quality of life.

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Goal H3:

A City regulatory environment that balances the need for creativity and excellence in residential design, flexibility and predictability in the project approval process, and the provision of an adequate supply and prices of housing.

Policies

- H3-1 Incentives. We maintain incentive programs that can be offered to projects that provide benefits to the community such as exceptional design quality, economic advantages, environmental sustainability, or other benefits that would otherwise be unrealized.
- H3-2 Flexible Standards. We allow flexibility in the application of residential and mixed-use development standards to gain benefits such as exceptional design quality, economic advantages, sustainability, or other benefits that would otherwise be unrealized.
- H3-3 Development Review. We maintain a residential development review process that provides certainty and transparency for project stakeholders and the public, yet allows for the appropriate review to facilitate quality housing development.
- **H3-4 Financial Incentives.** We consider financial incentives to facilitate and encourage the production, rehabilitation, or improvement of housing, or the provision of services where such activity furthers housing and community-wide goals.

Goal 4: Housing Assistance

Ontario recognizes the importance of an adequate supply of affordable housing and its importance to the quality of life of residents. Residential developments in the Ontario Ranch and Ontario Airport Metro Center area will provide quality housing opportunities to attract and retain Ontario's workforce and support citywide economic development goals. Lower- and moderate-income residents will require homeownership and rental assistance to secure and maintain housing.

Housing prices and rents in Ontario and across the region continue to lead to lower homeownership rates, longer commutes, increased traffic congestion, higher cost burdens, and overcrowding in neighborhoods. Working with partners and the state and federal governments, the City of Ontario is committed to providing a range of housing types and prices affordable to all economic segments of the city and assisting residents and



the workforce to secure and maintain housing that is affordable and appropriate to their needs.

Goal H4:

Increased opportunities for low- and moderate-income households and families to afford and maintain quality ownership and rental housing opportunities, including move-up opportunities. Inclusive communities, racial equity, fair housing choice, and access to opportunity.

Policies

- **H4-1 Preservation of Affordable Apartments.** We strive to facilitate the preservation of the affordability of publicly assisted apartments for lower-income households through financial assistance, technical assistance, rehabilitation, and collaborative partnerships.
- H4-2 Homeownership Opportunities. We increase and expand homeownership rates for lower- and moderate-income households by offering financial assistance, low-interest loans, and educational resources, and by working in collaboration with partnerships.
- **Rental Assistance**. We support the provision of rental assistance for individuals and families earning extremely low, very low, and low income with funding from the state and federal government.
- **H4-4 Mixed-Income Housing**. We encourage the integration of affordable housing in the Ontario Ranch, Ontario Airport Metro Center area, and existing neighborhoods.
- **H4-5 Collaborative Partnerships**. We support collaborative partnerships of nonprofit organizations, affordable housing developers, major employers, and for-profit developers to produce affordable housing.
- **H4-6 Fair Housing**. We further fair housing by prohibiting discrimination in the housing market, lifting barriers that restrict access to housing, and providing education, support, and enforcement services to address discriminatory practices.

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Goal 5: Special Needs

The City is home to a large number of people with special housing needs. These special needs may be related to occupation, income, family characteristics, disability, veteran status, or other characteristics. Special needs groups include, but are not limited to, seniors, large families with children, people with disabilities, single-parent families, college students, veterans, and people who are homeless. Though each group is markedly different, they share the challenge of finding suitable and affordable housing.

Ontario aspires to be the premier city the Inland Empire. As such, the city's population will become increasingly diverse, with people of many cultures, backgrounds, family types, ages, and experiences. The housing needs of Ontario's residents will be equally diverse. Recognizing the contributions of this diversity to the community, Ontario has the opportunity to demonstrate leadership in addressing the housing and support needs of all residents. Ontario is thus committed to creating a community that allows people to live in the city for their entire life, regardless of their special needs.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.

Policies

- **H5-1 Senior Housing.** We support the development of accessible and affordable senior housing and provide financial assistance for seniors to maintain and improve their homes.
- **H5-2 Family Housing.** We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities.
- **H5-3 Disabled People.** We increase the supply of permanent, affordable, and accessible housing for people with disabilities, and provide assistance to allow them to maintain and improve their homes.
- **H5-4 Homeless People.** We partner with nonprofit partners to provide emergency shelters, transitional housing, permanent supportive housing, and supportive services for people who are homeless.



- **H5-5 Supportive Services.** We financially support organizations, as feasible, that provide support services that meet the needs of those with special needs and further the greatest level of independence.
- **H5-6 Partnerships.** We collaborate with nonprofit organizations, private developers, employers, government agencies, and other interested parties to develop affordable housing and provide support services.

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HOUSING PROGRAMS

Neighborhoods and Housing

1. Code Enforcement

Code compliance is an important tool to ensure that the value, character, and quality of neighborhoods, property, and housing are well maintained. Listed below are the programs implemented by the Code Enforcement program specifically designed to improve the quality of Ontario neighborhoods and eliminate health and safety related to building conditions:

- General Code Enforcement: The City uses an interdepartmental approach for inspecting properties for compliance with state and local regulations regarding the condition and maintenance of residential buildings and properties. If deficiencies are found, the property owner is notified of the code deficiency and compliance measures required, and the property owner is granted a period of time to correct the matter. To facilitate timely compliance, City staff direct the property owners to City-administered rehabilitation loans and/or other nonprofit housing loan programs, where available.
- Community Improvement Team: This team has been specifically designed to proactively implement an intensive code compliance program to address serious code violations within focus areas. As part of this team approach, various City departments work together to bring a myriad of resources to the focus area to arrest neighborhood decline and improve the living conditions within the area.
- Systematic Health and Safety Inspection Program: The program is designed to ensure the quality of the rental stock and reduce substandard building conditions. Through this program, all rental housing units over seven years old are inspected on a four-year schedule unless it is necessary to inspect more frequently due to substandard conditions.
- Abandoned and Distressed Property Program and Foreclosure Opportunities Response Team (FORT) Program: These programs were established to protect Ontario neighborhoods from becoming blighted through the lack of adequate maintenance and security of abandoned and distressed properties.



The City will focus efforts throughout the city, with a particular emphasis on areas to the north and northwest of the Ontario International Airport.

Implementation

- Objectives: Continue code enforcement using a progressive approach of voluntary compliance, citations, and court action if needed. Continue to apply for funding.
- Responsible Agencies: Housing and Neighborhood Preservation Departments, Police, Fire, Economic Development, Building, and Planning Departments.
- Funding: Community Development Block Grant (CDBG), HOME, and CalHOME funds.
- · *Timing:* Ongoing; inspect properties annually.

2. Historic Preservation

Known as the Original Model Colony, Ontario is rich in local history. The City operates a comprehensive historic preservation program. It is a certified local government, a designation that signifies that the City's program meets state and federal historic preservation standards. The City has eight historic districts and is surveying nine additional areas for the potential of historic district designation. It encourages historic preservation efforts through Mills Act contracts, surveys of potentially historic structures, and an adaptive reuse program (for the Emporia District and Downtown).

Implementation

- · Objectives: Continue to implement program.
- · Responsible Agencies: Planning Department
- · Funding: General Fund, state and federal grants
- · Timing: Ongoing

3. Housing Rehabilitation Loan and Grants

When funding is available, the City offers housing rehabilitation loans and grants to qualified homeowners to pay for accessibility improvements, emergency repairs, home renovations, and other services that improve the homes and lives of Ontario residents, including seniors and persons with disabilities. The City launched the Conservation Home Improvement Program (CHIP) loan in 2020 with Community Development Block Grant (CDBG) funding provided through the

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Department of Housing and Urban Development (HUD). CHIP offers rehabilitation loans to qualifying owner-occupied households to make energy and/or water conservation improvements to the exterior of properties. As of 2021, the City approved two CHIP loans that have progressed to construction and are reviewing two additional applications. The City will continue to implement CHIP as funding is available and seek other sources of funding to support rehabilitation loans and grant programs.

Implementation

- Objectives: Support 30 rehabilitated units through CHIP and/or future rehabilitation programs.
- Responsible Agencies: Housing Department
- · Funding: CDBG, HOME, CalHOME
- · Timing: Ongoing

4. CARES

The City of Ontario has previously implemented the comprehensive CARES Neighborhood Revitalization Program within selected focus neighborhoods. The components of this comprehensive, multi-agency program have included code enforcement, arterial street improvement, relief program, exterior improvement program, and sidewalk or safe routes to school program. The program sought to stabilize neighborhoods through a comprehensive approach to building community. The program is currently on hold because of limited availability of funds. The City will seek funding opportunities to continue the program and restructure it as needed, depending on the requirements of the funding program. If funding can be secured to continue the CARES program, the City will focus efforts throughout the city, with a particular emphasis on areas to the north and northwest of the Ontario International Airport.

Implementation

- *Objectives*: Seek funding to continue program implementation, as funding is available, and restructure as needed.
- Responsible Agencies: Housing and Neighborhood Preservation Departments Funding: CDBG, HOME, General Fund
- · Timing: Ongoing



5. Neighborhood Plans

Ontario's neighborhoods define the sense of identity and community for residents, the quality of life experienced, and the image and role of Ontario. The City currently implements many programs to improve neighborhoods; however; the City has identified a need to foster a stronger sense of neighborhood identity in the community. While this goal is being achieved in CDBG-eligible areas (CARES program) and in historic areas, efforts need to be expanded to other neighborhoods. During the planning period, the City will begin a public outreach effort to solicit input from neighborhood leaders and residents as to particular needs and goals. This process may result in the establishment of ongoing dialog with the City, neighborhood organizations, or the preparation of neighborhood improvement plans. In addition, the City will continue the Multimodal Transportation Center (MTC) Needs Assessment and Siting Criteria project. This assessment will assist in determining the optimum location for an MTC on or near the Ontario International Airport connecting future modes of transportation, including light-rail opportunities.

Implementation

- Objectives: Designate focus neighborhoods, outreach plan and process, and initiate survey efforts. Evaluate the potential of creating neighborhood improvement plans.
- Responsible Agencies: Planning, Housing, and Community Improvement Departments.
- · Funding: General Fund
- Timing: Ongoing

6. Neighborhood Stabilization

In July 2019, the City adopted the Neighborhood Preservation Strategy Plan (NPSP) to develop community partnerships between the City, its residents. business owners. community organizations, neighborhoods. The NPSP identified four initial target neighborhoods (Downtown, Nocta, Mission-Mountain, Fourth Grove) based on baseline demographic information related to economic, education, housing, languages, and families that will be the focus of the NPSP efforts. The NPSP has been designed to meet objectives over a three-year span within the four target neighborhoods. Components of the strategy include community engagement, neighborhood fairs, new resident marketing initiatives, establishment of the Neighborhood Action Team, evaluation of opportunities for affordable homeownership, and evaluation of community resources and infrastructure.

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Implementation

- Objectives: Implement the key components of the NPSP to strengthen the four target neighborhoods, improving neighborhood conditions and resident quality of life: community engagement, neighborhood fairs, neighborhood action team, homeownership opportunities, community education materials, and economic development activities.
 - The Housing and Community Improvement Departments will report on the progress of the NPSP at the end of each fiscal year for the life of the program.
 - Community Engagement: The Housing and Community Improvement Departments will collaborate with the Neighborhood Action team to attend existing events in the target neighborhoods (e.g., Integrated Waste quarterly clean-up days, Ontario Night Out, Neighborhood Watch meetings, Crime-Free Multi-Housing meetings, Community Life and Culture events [Culture Fest, Arts Festival, etc.], Downtown Strategic Plan workshops, Community Health Workers monthly community engagement forums). The City will develop an ongoing community engagement plan through distributing an initial community survey in the first year that will inform focused engagement plan, programs, and projects within each of the target neighborhoods.
 - Neighborhood Fairs: Conduct at least one neighborhood fair each year over the three-year term of the NPSP.
 - Neighborhood Action Team: The Neighborhood Action Team will be composed of staff from Housing and Community Improvement Departments, Ontario Police Department, Community Life and Culture, Public Works, Ontario Municipal Utilities Company, Information Technology, Administrative Services, Ontario Fire Department, San Bernardino County Department of Public Health, Ontario-Montclair School District, and Chaffey Joint Union School District. The Neighborhood Action Team will meet monthly to upcoming planned community engagement opportunities, discuss partnerships and updates, strategize solutions for the target neighborhoods. The Neighborhood Action Team will also implement community clean-up programs, including the Property Clean-Up Program (grant fund program with up to \$3,000 per property for exterior improvements), Neighbors Helping Neighbors Program (partnership opportunity with local housing



- nonprofits and community volunteer groups), and Community Clean-Up Days (a minimum of two clean-up days per program year).
- Homeownership Opportunities: Housing Department staff will evaluate homeownership model programs to promote homeownership in each of the target neighborhoods. Homeownership models may include small-lot development, community land trusts, and opportunities for funding down payment assistance loans.
- Community Education Materials: Housing and Community Improvement Department staff will develop a packet of materials (available physically and online through the City's website) to be distributed to new homeowners and renters within the target neighborhoods. In addition to resident information packages, develop a business user's guide to inform local business owners of Ontario Municipal Code and Ontario Development Code requirements. The business user's guide would be developed in coordination with the Economic Development Agency, the Community Development Agency, and Ontario Municipal Utilities Company.
- Economic Development Activities: For the initial year of NPSP, the Housing and Economic Development Agency will implement a small commercial façade program for qualifying businesses within the 100-600 blocks of North Euclid Avenue. The commercial façade program will support four businesses.
- Responsible Agencies: Housing and Community Improvement Departments, Economic Development Agency
- · Funding: CDBG
- *Timing*: Within three years of Housing Element adoption

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7. Community-Oriented Policing

The City of Ontario Police Department uses CDBG funds to implement a community-oriented policing program in designated low- and moderate-income neighborhoods. This partnership involves working with community leaders, businesses, and property owners to address neighborhood issues, including code enforcement, crime-free multifamily housing, safe and clean streets, and school interventions. With respect to housing, the Police Department implements the Crime-Free Multifamily Housing Program to control and eliminate crime in apartment buildings. Under this program, the Police Department will provide training to apartment owners, conduct a property inspection to identify and eliminate potential crime hazards, and certify properties where the owner signs a written agreement and commitment to maintain the program.

Implementation

- Objectives: Continue implementation of Community-Oriented Policing (COPs) program; coordinate marketing efforts with the new Quadrennial Inspection Program.
- Responsible Agencies: Housing and Community Improvement Departments, and Ontario Police Department
- Funding: General Fund, CDBG
- · *Timing*: Ongoing

Housing Supply and Diversity

8. Downtown Plan

Ontario's Downtown covers 12 blocks along Euclid Boulevard. The City developed a Downtown District Plan that encompasses four downtown districts that create sub-zones within the Mixed-Use (MU-1) zoning district: Euclid Avenue Entertainment District (LUA-1), Arts District North and South (LUA-2S and LUA-2N), Holt Boulevard District (LUA-3), and Civic Center District (LUA-4). The Downtown District Plan vision provides opportunities for high-density, market-rate and affordable housing in the Euclid Avenue Entertainment District and Holt Boulevard District, offering convenient access to downtown amenities, local commercial centers, public services, open space, and public transportation. The Downtown Plan area is included in a \$35 million Transformative Climate Communities (TCC) Grant awarded to the City to increase prosperity and improve transportation and housing within a disadvantaged community. The TCC program includes affordable



housing, active transportation improvements, mobility hub, urban greening, carbon farm, solar photovoltaic, and transit improvements. This grant is a collaborative effort with public and community-based organizations.

Implementation

- Objectives: Adopt the Downtown District Plan to facilitate new mixed-use and residential development; continue to acquire property and assemble sites to facilitate new housing.
- · Responsible Agencies: Planning Department
- Funding: General Fund, Tax Increment, Transformative Climate Communities (TCC)
- **Timing:** Within one year of Housing Element adoption

9. Mountain and Euclid Corridors

Euclid Avenue and Mountain Avenue extend the entire length of Ontario. In recent years, developers have expressed interest in building residential and commercial projects along these corridors. Mountain Avenue has had numerous senior and affordable housing projects built adjacent to or near the corridor, and developers have begun to show interest in Euclid Avenue. Both corridors have commercial property that is proposed for redesignation as residential. To facilitate corridor development, the City previously rezoned properties along Euclid Avenue and Mountain Avenue for medium- and high-density residential development, as shown on the Official Land Use Plan (LU-01). During the planning period, the City will develop a lot-consolidation ordinance to incentivize the assemblage of parcels. Incentives may include fee modifications, flexibility in design, expedited permit processing, or others. The City will continue to monitor the ongoing status of development in the Euclid Avenue and Mountain Avenue corridors.

Implementation

- Objectives: Develop a lot consolidation ordinance to facilitate the assemblage of lots into larger parcels. Continue to monitor the ongoing status of development in the Euclid Avenue and Mountain Avenue corridors.
- · Responsible Agencies: Planning Department
- Funding: General Fund
- *Timing*: Within two years of Housing Element adoption

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10. Holt Boulevard

Holt Boulevard is one of the original corridors paralleling the railroad and extending through Ontario and neighboring communities. With the development and success of commercial uses fronting the freeways, the commercial viability of Holt Boulevard has gradually eroded, leaving a significant number of underutilized uses on small parcels. The Policy Plan has declared Holt Boulevard as a focus area for mixed uses, both perpendicular to Mountain Avenue, at the base of Downtown, and in the East Holt Boulevard Study Area. Sites along Holt Boulevard have been previously rezoned to accommodate higher densities. To stimulate investment in these areas, the City will adopt a lot consolidation ordinance and incentives to encourage the recycling of land to residential uses. The City will continue to monitor the ongoing status of development in the Holt Boulevard area. The Holt Boulevard area is included in a \$35 million Transformative Climate Communities (TCC) grant awarded to the City to increase prosperity and improve transportation and housing within a disadvantaged community. The TCC program includes affordable housing, active transportation improvements, mobility hub, urban greening, carbon farm, solar photovoltaic, and transit improvements. This grant is a collaborative effort with public and community-based organizations and includes the development of the 101-unit Vista Verde Affordable Housing project.

Implementation

- Objectives: Develop a lot consolidation ordinance to facilitate the assemblage of lots into larger parcels. Continue to monitor the ongoing status of development in the Holt Boulevard area.
- Responsible Agencies: Planning Department
- Funding: General Fund
- Timing: Within two years of Housing Element adoption, ongoing monitoring of development

11. Ontario Ranch

Ontario Ranch covers 8,200 acres of the former San Bernardino Agricultural Preserve. This area is intended to provide a range of housing opportunities for the City's emerging regional and national employment centers. Buildout of this area is contingent on completion of infrastructure, approval of specific plans, and cancellation of Williamson Act contracts. The City has entered into an agreement with a consortium to fund \$430 million in infrastructure serving the eastern Ontario Ranch. Many specific plans for this area have been approved. Some of the original Williamson Act contracts will also expire during the planning



period. The Policy Plan has designated much of the area for medium- and high-density residential and mixed-use. Within western Ontario Ranch (largely undeveloped), the Policy Plan lays groundwork to promote a mixed-income community, with low-density, medium-density, high-density, and mixed-use well integrated with one another. Ontario Ranch offers important opportunity to integrate housing affordable to all income levels, especially lower-income households. The City will continue to process specific plan applications and work with developers to address outstanding issues, in particular the financing of infrastructure in western Ontario Ranch. In addition, the City will meet with potential developers to explore opportunities to integrate affordable housing in new developments.

Implementation

- Objectives: Continue to review, approve, and implement plans to develop Ontario Ranch, and meet with potential developers to encourage the development of affordable housing.
- · Responsible Agencies: Planning Department
- · Funding: General Fund
- · Timing: Ongoing

12. Green Building

Green building means creating structures and using materials that are environmentally responsible and resource efficient, considering a building's entire life cycle. To reduce per-capita energy use, the City will promote conservation and renewable energy generation techniques in public facilities and private development. The City will require new construction to reduce energy demand by incorporating building and site design strategies. Conservation will be the priority strategy for renovation of existing facilities. The Policy Plan also includes land planning strategies that impact energy demand reduction, including narrowing street widths, installing broad-canopied trees for shade, and clustering compact development to reduce automobile use.

Implementation

- · Objectives:
 - Promote green building practices in the private sector and explore point-of-sale energy retrofits for residences.
 - Renewable energy incentive and energy-efficiency programs.

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- Develop a citywide 20-year energy plan.
- Support pilot development project as a net-zero-energy community and formulate solar site orientation guidelines.
- Responsible Agencies: Planning Department, Building Department, Public Works
- Funding: General Fund
- Timing: Ongoing

13. 2021–2029 Regional Housing Need

To meet state law requirements (California Government Code Sections 65583(c)(1)(A) and 65583(c)(1)(B)) to address the 2021-2029 Regional Housing Needs Allocation (RHNA) and to ensure a sufficient selection of sites are available for higher-density development, the City shall amend the Policy Plan and the Development Code, as needed, to provide adequate sites for 8,333 lower-income units and 2,735 moderate-income units (11,068 total units). The City will increase maximum density to at least 30 dwelling units per acre (du/ac) on a minimum of 270-370 acres depending on the mix of densities employed. Sites for lower-income units must allow at least 30 du/ac and are subject to the requirements of California Government Code Section 65583.2(h), including allowing owner-occupied and rental multifamily housing "by right" without discretionary review if 20 percent or more of the units in a project proposed on the site are affordable to those with lower incomes. The sites rezoned to accommodate lower-income RHNA must be able to accommodate a minimum of 16 units per site. At least half (50 percent) of the sites rezoned to accommodate lower-income RHNA shall be designated/zoned for residential uses only, except that all of the very low- and low-income housing need may be accommodated on sites designated for mixed uses if those sites allow 100-percent residential use and require that residential uses occupy 50 percent of the total floor area of a mixed-use project. The applications can be subject to design review as long as the project does not trigger the California Environmental Quality Act review process.

An **Affordable Housing Overlay** zone will be created and applied to all parcels in the sites inventory that are zoned MU-2 along Holt Boulevard and parcels in the area south of Riverside Drive. The Overlay establishes a minimum density of 20 dwelling units per acre for all development and provides for special standards for affordable housing projects where at least 25% of proposed units are restricted for lower income households, including:



- No specific plan shall be required unless the parcels are already affected by an adopted specific plan
- The project can apply the development standards for the zone that implements the current or proposed Policy Plan designation
- For parcels with a Policy Plan designation of MDR, the maximum density shall increase from 25 to 30 units per acre (before application of state density bonus provisions)
- For mixed-use projects, at least 75 percent of the project area must be dedicated to residential uses, and densities shall be consistent with the applicable Policy Plan designation.

The following **specific plans** will be updated to require properties within these Specific Plan areas that are also included in the sites inventory include a minimum density of 20 du/ac and allow at least 30 du/ac:

- · The Ontario Mills Specific Plan
- The Armstrong Ranch Specific Plan

The City will also update the policy plan designations and associated tables as necessary to reflect the intent of the overlay zone, and the changes to the specific plans.

Implementation

- Objectives: Accommodate Ontario's share of the 2021-2029 RHNA.
- · Responsible Agencies: Planning Department
- Funding: General Fund
- Timing: Specific sites will be rezoned prior to February 12, 2025

14. Land Monitoring Program to Meet the RHNA

The City is in the process of updating the Development Code for consistency with the Land Use designations of its Policy Plan. This program will implement a land monitoring program to ensure that the city has enough land to meet its RHNA throughout the planning period.

California Government Code Section 65863 stipulates that a jurisdiction must ensure that its Housing Element inventory can accommodate its share of the RHNA by income level throughout the planning period. If a jurisdiction approves a housing project at a lower density or with fewer units by income category than identified in the Housing Element, it must

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quantify at the time of approval the remaining unmet housing need at each income level and determine whether there is sufficient capacity to meet that need. If not, the city must "identify and make available" additional adequate sites to accommodate the jurisdiction's share of housing need by income level within 180 days of approving the reduceddensity project. The City will evaluate residential development proposals for consistency with goals and policies of the Policy Plan and the 2021-2029 Housing Element sites inventory and make written findings that the density reduction is consistent with the Policy Plan and that the remaining sites identified in the Housing Element are adequate to accommodate the RHNA by income level. If a proposed reduction of residential density will result in the residential sites inventory failing to accommodate the RHNA by income level, the City will identify and make available additional adequate sites to accommodate its share of housing need by income level within 180 days of approving the reduced-density project.

Implementation

- *Objectives:* Ensure there is a sufficient supply of multifamily zoned land to meet the housing needs identified in the RHNA.
- Responsible Agencies: Planning Department
- · Funding: General Fund
- · *Timing:* Throughout the 2021-2029 planning period.

15. Residential By-Right for Developments with 20-Percent Affordable

To comply with Assembly Bill (AB) 1397, sites with Assessor's Parcel Numbers (APNs) listed in Table 9-1-in the land inventory in this 6th cycle Housing Element shall be allowed to be developed for residential use byright, in accordance with Government Code Section 65583.2(c). This byright (without discretionary review) requirement is only for housing developments in which at least 20 percent of the units are affordable to lower-income households. The application of the requirement should not be used to further constrain the development of housing. As such, housing developments that do not contain the requisite 20 percent would still be allowed to be developed according to the underlying (base) zoning.



Table 9-1 Assessor's Parcel Numbers

104857602	104857601	101050176	101054306	101055234	101055233	101055232	101052217
101052206	101054301	101054327	101054304	101054305	101055216	101054302	101054313
101054314	101055237	104860414	104860415	101049116	101049102	101049103	101050207

Implementation

 Objectives: Comply with California Government Code Section 65583.2(c)

· Responsible Agencies: Planning Department

· Funding: General Fund

• *Timing:* Continuously during the planning period.

Governmental Constraints

16. Incentives

The City of Ontario offers several different types of incentives to facilitate housing production, including:

Financial Incentives: The City makes available financial incentives that meet certain criteria. For instance, impact fee reductions are allowed for projects built in the Downtown. The City is financially assisting a variety of nonprofit organizations to provide senior housing, housing for homeless people, and other services. Density bonuses allowed for qualified projects work as a financial incentive by increasing the revenue stream of projects. The City also has established its Community Housing Development Organization (CHDO) program to leverage the nonprofit sector resources with available HOME CHDO funding. The intent of the CHDO funding is to work with nonprofit CHDOs to help preserve, enhance, and improve existing neighborhoods through acquisition, rehabilitation, and/or new housing construction activities. Finally, the City continues to grant lowcost leases (e.g., \$1 per-year leases) to qualified organizations to provide senior housing and homeless housing. These types of financial incentives will be provided to allow the City to meet its community development and housing objectives.

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Regulatory Incentives: The regulatory incentive program is intended to realize improved value, a rich palette of amenities, landmarks, and identifiable places. While the underlying land use designations still apply, the City may offer various incentives through a discretionary permit. Special incentives may be granted for mixed-use developments; residential infill projects near transit facilities; the replacement of underperforming commercial uses with new residential uses; the improvement and/or intensification of existing, mid-block residential uses; or lot consolidation and development of desired projects. The menu of incentives may include density transfers, modifications in development standards, increased residential density, and other incentives to be negotiated on a case-by-case basis.

Implementation

- Objectives: Offer financial and regulatory incentives for residential projects that meet City housing and affordable housing goals.
- Responsible Agencies: Housing Department
- · Funding: General Fund
- Timing: Ongoing and at least annual outreach to developers of affordable housing, including nonprofit.

17. Land Acquisition

Land acquisition for residential development is perhaps one of the greatest challenges to creating affordable housing. Over the past five years, the City of Ontario has seen increasing land prices. To facilitate the development of affordable housing, the City has actively purchased land and made it available at a low cost (typically a \$1 per-year lease) to affordable housing developers and nonprofit agencies to create affordable senior housing, emergency shelters, affordable attached ownership projects, and other affordable housing projects. As situations merit and projects are proposed that meet the City's housing goals and the public interest, the City will continue to acquire residential land that can be leased or sold at below-market rates for the production of affordable housing.

Implementation

 Objectives: Continue to approve financial incentives for residential projects that meet City housing and affordable housing goals.



- · Responsible Agencies: Ontario Housing Authority
- · Funding: General Fund, and other funding as available
- Timing: Ongoing

18. Planned Unit Development

Within an established suburban fabric, there are considerable challenges to creating affordable housing. As development standards and lot standards change over time, it is not uncommon to have irregularly shaped and nonconforming parcels that are simply not conducive to redevelopment. The City has adopted a Planned Unit Development (PUD) Ordinance that permits a variety of housing types in every residential zone. The City may conditionally permit attached and detached single-family residences, town homes, patio homes, zero lot line, and any other type of housing product permitted by the regulations of the underlying zone. The PUD is a tool that has been successfully used for Town Square to encourage and facilitate innovative design, variety, and flexibility in the types of housing products, including the provision of affordable housing, that would otherwise not be allowed or possible through standards in the underlying zoning districts.

Implementation

- Objectives: Continue to use the PUD Ordinance to create tailored development standards to facilitate new housing.
- Responsible Agencies: Planning Department
- · Funding: General Fund
- · Timing: Ongoing

19. Mixed-Use and High-Density Residential Zone and Standards

The Policy Plan directs significant housing growth to mixed-use areas. These areas include the Downtown, Euclid Avenue, the Interstate (I-) 10 Corridor, Ontario Ranch, and Holt Boulevard. These mixed-use areas each have a distinct mix of land uses and density ranges (see Policy Plan Land Use Exhibit LU-11, Land Use Designation Summary Table). The City will continue its efforts in processing Development Code changes to align with the updated Policy Plan following the Ontario Plan update, which is in progress as of 2021. This includes reviewing the Euclid Francis Mixed-Use Area land use designation that has an assumed build-out of 156 units based on 50 percent of the area developed at 30 du/ac, which exceeds the allowed density range for the corresponding zoning district, Mixed Use-11 (allows a maximum of 25 units per acre). Additionally, as

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a part of The Ontario Plan, the City will develop Objective Design and Development Standards for multifamily residential and mixed-use developments to replace subjective standards/policies as required by California Government Code Section 65589 (see Program 21, Senate Bill 2 Implementation).

Implementation

- Objectives: Process necessary changes to the Development Code to ensure consistency with land use designations for mixed-use and multifamily residential in the updated Policy Plan, including adopting Objective Design and Development Standards with The Ontario Plan.
- · Responsible Agencies: Planning Department
- · Funding: General Fund
- *Timing*: Complete by December 31, 2023

20. Development Code Amendments

To comply with state law, address identified constraints to residential development and remove barriers to housing for special-needs groups, including, but not limited to, low-, very low-, and extremely low-income households, the City will amend its Development Code, as described herein:

- Density Bonuses. Within two years of Housing Element adoption, pursuant to California Government Code Section 65915 et seq., amend the Development Code to update the density bonus ordinance to address recent updates to state law. If additional changes to state law occur during the planning period, the density bonus ordinance will be updated to comply with those changes.
- Accessory Dwelling Units (ADUs). The City adopted updates to the ADU ordinance as a part of the Development Code update in 2020 to comply with state law. During the planning period, the City will implement the ADU ordinance and update it to comply with any new state requirements.
- Specific Plan Requirements. The City will amend its Development Code to eliminate the Specific Plan requirement in the Ontario Ranch for residential development projects that include a minimum of 25-percent affordable units.



- Transitional and Supportive Housing. Allow transitional and supportive housing as a residential use in all zones allowing residential uses, subject only to those restrictions that apply to other residential uses of the same type in the same zone and without any discretionary action. Additionally, permit supportive housing by right in any nonresidential or mixed-use zone that permits multifamily (AB 2162, Government Code Section 65583(c)(3)).
- Employee Housing. Treat employee housing that serves six or fewer persons as a single-family structure and permitted in the same manner as other single-family structures of the same type in the same zone (Health and Safety Code Section 17021.5). The Development Code will also be amended to treat employee housing consisting of no more than 12 units or 36 beds as an agricultural use and permitted in the same manner as other agricultural uses in the same zone (Section 17021.6) in zones where agricultural uses are permitted, specifically removing minimum lot size of 10 acres. Review Development Code for compliance with Section 17021.8, which requires a streamlined ministerial process for qualifying agricultural employee housing on land designated as Agricultural in the City's Policy Plan.
- Single-Room Occupancy Units. Allow single-room occupancy (SRO) housing in one or more zones without a use permit or administrative use permit, as other residential uses are regulated in the City's Development Code (Government Code Section 65583(c)(1)). Review location requirements for SRO facilities to expand affordable housing opportunities for extremely low-income households.
- Emergency Shelter Parking. The City's zoning permits an emergency shelter by right in the IL zone and conditionally permits an emergency shelter in the MDR-11, MDR-18, MDR-25, CC, LUA-3, IG, and IH zones. Emergency shelters are also permitted by right in the Emergency Shelter Overlay, subject to the base zone standards and consistent with Government Code Section 65583(4)(A). The City will review and revise parking requirements for emergency shelters to ensure that parking standards are sufficient to accommodate all staff, provided standards do not require more parking for emergency shelters than other residential or commercial uses within the same zone, per California Government Code Section 65583(a)(4).

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- Low-Barrier Navigation Centers. Allow low-barrier navigation centers, a type of emergency shelter with wrap-around services, by right in zones that allow for mixed-use and nonresidential zones permitting multifamily uses, per California Government Code Section 65662.
- **Residential Care Facilities.** Define residential care facilities. Allow for residential care facilities with seven or more persons and subject them to the same restrictions that apply to other residential uses of the same type in the same zone.
- Boarding, Lodging, and Rooming Houses. Review and amend Development Code standards for Boarding, Lodging, or Rooming House to remove the restrictions that the homes cannot be occupied by more than one federal, state, or youth authority parolee and add requirement that homes shall be required to sign a "Crime-Free Lease Addendum" to their rental or lease agreement, in order to prevent discrimination based on criminal history, complying with California Code of Regulations, Title 2, Section 12264-12271.

Implementation

- Objectives: Comply with state law, address identified constraints to residential development, and remove barriers to housing for special-needs groups
- Responsible Agencies: Planning Department
- Funding: General Fund
- *Timing*: Within two years of Housing Element adoption, evaluation of potential constraints ongoing

21. Senate Bill 2 Implementation

As a part of the City's Senate Bill (SB) 2 grant workplan, the City plans to develop Objective Design and Development Standards (ODDS) for residential projects: mixed use, multifamily, and single-family developments. As the City's current Development Code does not have standards for Mixed-Use areas and detached developments at multifamily densities, the project will include developing comprehensive numerical development standards for these uses, such as setbacks, building heights, lot coverage, etc. This will also include the elimination of subjective development standards/policies, which shall be replaced with objective design standards, as required by Government Code Section 65589.



The City will review existing standards and guidelines in the Development Code, Downtown Design Guidelines, and applicable Specific Plans that incorporate residential development and assess the potentially needed amendments to create ODDS for all types of housing development. The City will also prepare a compliance checklist and style sheet for applicants to provide a clear list of minimum design requirements and architectural examples to illustrate design requirements.

Implementation

- Objectives: Complete tasks associated with SB 2 grant workplan, including preparing and adopting ODDS to replace subjective standards/policies for all types of residential development, and making supplemental compliance checklist and style sheet available for applicant use.
- · Responsible Agencies: Planning Department
- Funding: SB 2
- · Timing: Complete by December 31, 2023

22. Streamline Housing Development Process

Establish a written policy or procedure and other guidance as appropriate to specify the SB 35 (2017) streamlining approval process and standards for eligible projects, as set forth under Government Code Section 65913.4.

Implementation

- Objectives: Establish written policy or procedure and other guidance as appropriate to specify SB 35 streamlining approval process.
- · Responsible Agencies: Planning Department
- · Funding: General Fund
- · Timing: Within two years of Housing Element adoption

Housing Assistance

23. Public Housing

The Housing Authority of the County of San Bernardino administers the Housing Voucher rental program for the City of Ontario. Funded by HUD, the Housing Voucher program extends rental subsidies to very

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low-income households by offering the tenant a voucher that pays the difference between the current fair-market rent (FMR) established by the Housing Authority, and 30 percent of the tenant's income. A tenant has the option to choose housing that costs more than the FMR, if the tenant pays the extra rent above the payment standard. The Housing Authority also implements the Family Self-Sufficiency program, Section 8 project-based assistance, and HUD-assisted multiple-family housing units. As of 2021, program serves nearly 800 individuals and families in the City of Ontario. The City will work with the Housing Authority of San Bernardino to provide biannual training to landlords regarding fair-housing requirements, including the requirement that they accept vouchers, and encourage them to market available units at their rental properties in high-resource areas to voucher holders to increase mobility from low- to high-resource areas.

Implementation

- Objectives: Continue to assist up to 800 households under the public housing program and seek additional vouchers as available. Hold biannual trainings to educate landlords on fair-housing requirements to expand the use of Housing Choice Vouchers to moderate- and high-resource areas of the city.
- Responsible Agencies: Housing Authority of the County of San Bernardino
- Funding: US Department of Housing and Urban Development
- Timing: Ongoing

24. Homeownership

The City has a broad-based homeownership program for residents. The City uses a combination of funds (BEGIN, HOME, CalHome, and other available funding) to provide down payment assistance to homebuyers seeking to purchase homes in Ontario. The City also works in conjunction with Neighborhood Partnership Housing Services (NPHS), a nonprofit organization, and the Inland Fair Housing and Mediation Board (IFHMB) to further the City's homeownership goals through homebuyer education, counseling, and down payment assistance. The City is planning to use a portion of the Permanent Local Housing Allocation (PLHA) funds for first-time homebuyer programs in conjunction with reuse funds on hand from the CalHome and BEGIN programs formerly offered. Programs are currently in design development to determine income targeting and benefits. Materials to promote the first-time homebuyer program will be offered in English and Spanish to reduce language barriers.



Implementation

- Objectives: Implement down payment assistance programs citywide, assisting at least 20 households with first-time homebuyer loans.
- · Responsible Agencies: Housing Department
- Funding: California Department of Housing and Community Development (HCD), BEGIN, CalHOME, PLHA
- · Timing: Ongoing

25. Preservation of At-Risk Housing

As of 2020, the City maintains 1,910 units of rental housing affordable to seniors, families, and individuals earning lower incomes, with 807 units at risk of conversion during the planning period. To address the preservation of public housing for very low- and low-income persons, the City maintains contact with owners of at-risk units as the use restriction expiration date approaches to communicate with the owner the importance of the units to the supply of affordable housing in Ontario, as well as its desire to preserve the units as affordable. The City will make every effort in using local incentives that can be offered to property owners to preserve any at-risk units.

Implementation

· Objectives:

- Monitor the status of the 807 units that are at-risk to maintain at least 1,910 units. For the 807 units at imminent risk of conversion during the planning period, provide technical assistance and/or financial assistance to preserve the properties as deemed feasible.
- Meet with the owners (or their representatives) of the subsidized rental housing developments that are facing unexpected risk to the affordable units in a timely fashion, to discuss their plans for maintaining, converting, or selling their properties. If any of the owners indicate that the affordability of the units is at risk of conversion to market-rate housing or that the owner intends to sell the property, the City will seek to facilitate the acquisition of the property by another forprofit or nonprofit entity to preserve the rental units as affordable housing. The City will not take part directly in negotiations regarding the property but will apply for state or federal funding on behalf of an interested nonprofit entity, if

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necessary, to protect the affordability of the rental units. The City will request that the property owners provide evidence that they have complied with state and federal regulations regarding notice to tenants and other procedural matters related to conversion, and the City will contact HUD, if necessary, to verify compliance with notice requirements.

- Work with the Housing Authority to ensure that low-income tenants displaced as a result of a conversion receive priority for federal housing vouchers.
- Ensure that tenants are adequately notified throughout the preservation/acquisition process as to the status of their housing units, impacts of the ownership change or preservation process on occupancy and rents, their rights and responsibilities as tenants, and who to contact with questions or concerns. The City will work with the responsible entity (whether the existing property owner, the Housing Authority, a nonprofit entity, or a new for-profit entity) to distribute information and conduct tenant meetings, as needed, to keep residents informed of the preservation process, tenant options, and what to expect once the process has been completed.
- · Responsible Agencies: Ontario Housing Authority
- · Funding: Federal government
- Timing: Ongoing

Jack Galvin Accord

The City of Ontario has more than 2,100 mobile homes, which provide affordable market-rate housing for lower-income families, seniors, and individuals. In 1990, the City Council adopted an ordinance to regulate mobile home space rents but later repealed that ordinance per state law. Subsequently, in working with mobile home park owners and tenants, the City drafted the Jack Galvin Mobile Home Park Accord, which was accepted by park owners. The accord places limits on the allowable increases based on the Consumer Price Index; allows for additional adjustments for changes in utilities, taxes, and capital improvements; provides a process for requesting rent reductions for service reductions; and allows for rent adjustments for resale. The agreement was adopted in 1999, and per extensions, continues in effect today. The most recent extension was approved for a five-year term on November 15, 2019, with an expiration date of January 5, 2025. The City will continue to implement and enforce this ordinance.



Implementation

- Objectives: Continue to implement the Jack Galvin Accord and monitor the effectiveness of the accord.
- Responsible Agencies: Housing and Neighborhood Preservation Agency
- · Funding: General Fund
- · Timing: Ongoing

Special-Needs Housing

27. Fair Housing

Ontario is committed to furthering fair housing opportunities so that people in all walks of life have the opportunity to find suitable housing in the community. To that end, the City contracts with a fair-housing service provider to provide landlord/tenant education, conduct testing of the rental and ownership market, and investigate and mediate housing complaints where needed. The City periodically prepares the required federal planning reports, including the Analysis of Impediments to Fair Housing Choice (AI), to document the City's progress in improving and maintaining fair housing opportunities. Recommendations will be made to eliminate potential constraints and further fair housing in Ontario. The City adopted an updated Assessment of Fair Housing (AFH) in 2020. This Housing Element incorporates the goals and priorities of the Ontario 2020 AFH. The goals of the Ontario 2020 AFH include:

- <u>Goal 1:</u> Increase the supply of affordable housing in highopportunity areas.
- <u>Goal 2:</u> Increase community integration for persons with disabilities.
- <u>Goal 3:</u> Ensure equal access to housing for persons with protected characteristics, who are disproportionately likely to be lower-income and to experience homelessness.

To further comply with AB 686, the City will implement actions to Affirmatively Furthering Fair Housing (AFFH). The ongoing and additional actions the City will take to address AFFH shall take actions to address significant disparities in housing needs and in access to opportunity for all persons regardless of race, color, religion, sex, gender, sexual orientation, marital status, national origin, ancestry, familial status, source of income, or disability, and other characteristic protected by the California Fair Employment and Housing Act (Part 2.8,

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commencing with Section 12900, of Division 3 of Title 2), Section 65008, and any other state and federal fair housing and planning law.

Implementation

Objectives:

- Continue to contract with local fair housing providers to provide educational, outreach, advocacy, and mediation services. Through the partnership, develop a method to track fair housing issues to identify patterns in the City.
- Conduct AI concurrently with the development of the Consolidated Plan, and review and change potential impediments.
- Provide fair housing information at City Hall, the Ontario Senior Center, and the Ontario Housing Authority. Fair Housing information shall be provided in multiple languages, including Spanish, to reduce barriers to education due to primary language.
- Implement Goal 1 of the 2020 Ontario AFH to increase the supply of affordable housing by committing to (1) explore the creation of new funding sources of affordable housing; (2) use best practices from other jurisdictions, explore policies and programs that increase the supply of affordable housing, such as linkage fees, inclusionary housing, public land set-aside, community land trusts, transit-oriented development, and expedited permitting and review; (3) explore opportunities to provide low-interest loans to single-family homeowners and grants to homeowners with household incomes of up to 120 percent of the Area Median Income to develop ADUs with affordability restriction on their property; and (4) align Development Codes to conform to recent California affordable housing legislation.
- Implement Goal 2 of the 2020 Ontario AFH to increase community integration for persons with disabilities by committing to (1) prioritize HOME funding for such projects, which should ideally set aside 10 to 25 percent of units for persons with disabilities who need supportive services, the City can help make development proposals more competitive for low-income housing tax credit (LIHTC) and Mental Health Services Act assistance.



- Implement Goal 3 of the 2020 Ontario AFH to ensure equal access to housing for persons with protected characteristics, who are disproportionately likely to be lower-income and to experience homelessness by committing to conduct fair housing training for landlords and tenants on California's Source of Income Discrimination protections to reduce the number of voucher holders turned away.
- Partner with San Bernardino County to promote the CalWorks program to provide assistance for eligible low-income families with children to meet basic needs and enter, or re-enter, the workforce, particularly for residents of northwest Ontario that have lower access to job centers compared to residents in southwest Ontario.
- Meet biannually with Omnitrans to assess if any new unmet transit needs have developed and, if so, will provide technical assistance in applying for state and federal funding for expansions.
- As described in Program 20, Development Code Amendments, review and amend Development Code standards for Boarding, Lodging, and Rooming Houses to remove the restrictions that the homes cannot be occupied by more than one federal, state, or youth authority parolee and add requirement that homes shall require to sign a "Crime-Free Lease Addendum" to their rental or lease agreement, in order to prevent discrimination based on criminal history, complying with California Code of Regulations, Title 2, Section 12264-12271.
- The City shall promote accessory dwelling units (including junior accessory dwelling units) (ADUs/JADUs) as an affordable housing option in high-resource areas and an economic mobility opportunity in Ontario through the following actions.
 - Consider a program to waive, reduce, or defer connection or impact fees for ADUs that agree to affordability covenants for a set period of time.
 - Continue to provide information such as Frequently Asked Questions (FAQ) on the City's website. Expand information on the City's website to provide educational materials and additional guidance, including permitting procedures and construction resources.

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- Assess the feasibility of establishing a loan program to help homeowners finance the construction of ADUs. The City shall consider incentives to encourage homeowners to deed restrict ADUs for lower-income households.
- O Actively market ADU guidance and materials in areas with high access to resources to encourage the development of new affordable housing in areas of opportunity as a strategy to enhance mobility and reduce displacement of low-income households seeking affordable housing options.
- Develop and implement a monitoring program. The program will track ADU approvals and affordability. The City will use this monitoring program to track progress in ADU development and adjust or expand the focus of its education and outreach efforts through the 2021-2029 planning period. The City will evaluate ADU production and affordability two years into the planning period (2023) and if it is determined these units are not meeting the lower-income housing need, the City will consider other housing sites that are available to accommodate the unmet portion of the lower-income RHNA.
- Responsible Agencies: Housing and Planning Departments, Ontario Housing Authority
- · Funding: General Fund, CDBG
- · Timing: Ongoing

28. Homeless Continuum of Care

The City implements a Homeless Services Continuum of Care to prevent homelessness and assist people in becoming self-sufficient. Working together with homeless service providers, the City has developed a full-service homeless continuum of care consisting of a homeless outreach service center, transitional housing, permanent housing, and supportive housing services. The City funds other programs that assist homeless people using Emergency Solutions Grant funds.

The City also actively participates in regional homeless efforts, including the Interagency Council on Homelessness, which is a countywide effort of governmental and nonprofit organizations working to end homelessness within the County of San Bernardino.



Implementation

- Objectives: Continue to fund Mercy House to implement the Continuum of Care program for homeless residents and other programs as funding is available.
- · Responsible Agencies: Housing Department
- Funding: Federal funds and private financing
- Timing: Ongoing

29. Senior Housing

The City is actively working with nonprofit housing groups to build senior housing projects in the community. In addition to facilitating housing construction, the City also provides a range of supportive services for seniors. These include fair housing services, housing rehabilitation grants, preservation of subsidized senior housing, low-cost transportation services, and a range of other services tailored to meet the unique needs of Ontario's senior population.

Implementation

- Objectives: Continue to provide a full range of housing support services.
- · Responsible Agencies: Housing Department
- Funding: State and federal funds
- · Timing: Ongoing

30. Housing for People with Disabilities

The City enforces state and federal accessibility laws to facilitate the improvement of housing for disabled people. The City also prepares a Transition Plan to comply with state and federal accessibility laws. The City has adopted a reasonable accommodation process and administratively allows modifications to land use, building codes, and the permitting process to facilitate the reasonable accommodations without going through a standard variance process. However, given the large number of people with disabilities, the growing need for housing opportunities, and changing legal context for housing planning, additional efforts are needed. Many homes were built before the advent of modern accessibility standards and thus many homes remain inaccessible to people with disabilities and persons with developmental disabilities. To address this issue, the City will evaluate the feasibility and

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appropriateness of modifying building standards to encourage visitability concepts in new housing.

The City partners with the Inland Regional Center, one of 21 regional centers in California that provide point-of-entry services for people with developmental disabilities. The City will continue to partner with the Inland Regional Center to provide services to its residents with disabilities.

Implementation

- Objectives: Continue to assist with the development of housing for persons with disabilities, including those with developmental disabilities.
- · Responsible Agencies: Building and Planning Department
- Funding: General Fund
- · Timing: Ongoing

31. Family Housing

Ontario has a large number of family households, specifically large families with five or more members. The City has a multifaceted program for increasing and maintaining the supply of family housing. The Housing Authority of San Bernardino County allocates housing choice vouchers to lower-income families in Ontario, many of whom are large families. Another key effort is the City's program to acquire, rehabilitate, and preserve existing affordable housing units that accommodate families and large families, with a focus on expanding affordable housing units in high resources areas of the City. Over the past five years, the City and the Housing Authority have preserved the vast majority of publicly subsidized affordable units for families. Finally, the City funds through its CDBG programs such as childcare, after-school programs, food programs, and other services targeted for lower-income households, including large families.

Implementation

- · *Objectives*: Continue program implementation.
- Responsible Agencies: Housing Department, Housing Authority of the County of San Bernardino
- Funding: General Fund, CDBG
- · Timing: Ongoing



32. Extremely Low-Income Households

The City offers programs to address the housing needs of extremely low-income (ELI) households. As funding is available, the City provides a number of incentives to encourage the production of ELI housing. The City offers fee reductions for ELI housing, supports grant applications to increase the supply of affordable housing, works with nonprofit organizations to build affordable housing, and provides land writedowns.

Implementation

Objectives:

- Work with nonprofits and/or for-profit developers to build housing for ELI households through supporting grants and funding applications.
- Offer fee reductions and land writedowns for new affordable housing for low-income, very low-income, and ELI households.
- **Responsible Agencies:** Housing and Planning Departments.
- · Funding: CDBG, HOME, federal and state grants
- · **Timing**: Annually

33. Special-Needs Housing

In implementing affordable housing programs, the City will work with housing providers to ensure that special housing needs are addressed for seniors, large families, female-headed households, single-parent households with children, persons with disabilities and developmental disabilities, homeless individuals and families, and farmworker families. The City will seek to meet these special housing needs through a combination of regulatory incentives, zoning standards, new housing construction programs, housing rehabilitation, homebuyer assistance programs, and supportive services programs. In addition, the City may seek funding under the federal Housing Opportunities for Persons with AIDS, California Child Care Facilities Finance Program, and other state and federal programs designated specifically for special-needs groups such as seniors, persons with disabilities, and persons at risk for homelessness.

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Implementation

- Objectives: Collaborate with affordable housing developers and secure funding, if feasible, to assist with the development of special-needs housing projects.
- · Responsible Agencies: Housing and Planning Departments
- · Funding: CDBG, HOME, federal and state grants
- Timing: Annually

34. Infrastructure Provision and Financing

The City will seek opportunities minimize infrastructure costs for residential development by identifying infrastructure needs and available sources of funding for infrastructure improvements. The City will analyze existing and potential infrastructure financing measures for their ability to meet infrastructure needs without an adverse impact to housing costs and identify and apply for state or United States Department of Agriculture (USDA) infrastructure funding programs to support improvement efforts. The City shall monitor the cost of infrastructure and associated fees on development to determine whether they impact the production of housing and will develop a strategy to reduce costs for developers, if needed. The City will focus the implementation of this program in areas of concentrated poverty, including northwest Ontario.

Implementation

- Objectives: Seek opportunities to minimize infrastructure costs for residential development, such as identifying available sources of funding for infrastructure improvements.
- Responsible Agencies: Planning Department, Ontario Municipal Utilities Company
- · Funding: General Fund, federal and state grants
- · Timing: Ongoing



Quantified Objectives

Identifying quantified objectives refers to the number of new units that may potentially be constructed over the planning period, the number of existing units that can be expected to be rehabilitated, and the conservation of existing affordable housing stock. This information is presented in Table 9-2.

Table 9-2
Quantified Objectives for the 2021–2029 Housing Element

	Quantified Objectives by Income Group										
Housing Program	Extremely Low Income	Very Low Income	Low Income	Moderate Income	Above Moderate	Totals					
New Construction	2,820	2,820	3,286	3,329	8,599 ¹	20,854 ¹					
Rehabilitation ²	10	10	10	0	0	30					
Housing Conservation ³	269	269	269	0	0	807					

Source: City of Ontario 2021

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¹ This total is based on the 6tht Cycle RHNA identified for the City by SCAG.

² See Program 3.

³ A total of 807 units have been identified as at-risk during the planning period. See Program 25 for additional details.



APPENDIX A: LIST OF QUALIFIED ENTITIES

County	Organization	code	Address	City	ST	Zip	Phone Number	Contact Person	E-Mail Address	FAX Number	Type of Organization	Confirm	Date Revised	List Added
FRESNO	Fresno Housing Authority	3	P.O. Box 11985	Fresno	CA	93776	(559) 443-8475	Edward Stacy	ned@pacbell.net	(559) 445-8981	Local, regional, national nonprofit org.		6/14/17	12/16/98
ALL COUNTIES	American Baptist Homes of the West	2	6120 Stoneridge Mall Road, 3rd Fir.	Pleasanton	CA	94588	(925) 924-7162	Ancel Romero		(925) 924-7233	Local, regional, national public agency	×	8/14/14	12/22/98
ALAMEDA	Housing Authority of City of Alameda	2	701 Atlantic Ave	Alameda	CA	94501	(510) 747-4300	Denise Connors	dconnors@alamedahsq.org	(510) 522-7848	Local, regional, national nonprofit org.	×	8/14/14	12/23/98
ALAMEDA	Housing Authority of the City of Livermore	2	3203 Leahy Way	Livermore	CA	94550	(925) 447-3600	Jon D. Hovey	livhsa@prodigy.net	(925) 447-0942	Local, regional, national nonprofit org.	х	8/14/14	12/23/98
ALAMEDA	Housing Authority of County of Alameda	3	22941 Atherton St	Hayward	CA	94541	(510) 538-8876		obasgak@aol.com	(510) 727-8554	Local, regional, national nonprofit org.			12/23/98
ALL COUNTIES	USA Properties Fund		7530 Santa Monica Blvd, Suite 1	West Hollywood	CA	90046	(323) 650-8771	Jesse Slansky		(323) 650-4745	Local, regional, national public agency	×	12/13/13	12/23/98
CONTRA COSTA	Rubicon Programs, Inc.	3	2500 Bissell Ave	Richmond	CA	90804	(510) 235-1516	Tom Matthews	TomM@Rubic onpams.org	(510) 235-2025	Local, regional, national nonprofit org.		6/5/17	12/23/98
KERN	Golden Empire Affordable Housing, Inc	3	3600 CheSte.r Ave. Ste. B	Bakersfield	CA	93301	(805) 633-1533	Gary Kammer		(805) 633-1617	Local, regional, national nonprofit org.		6/14/17	12/23/98
LAKE	Lake County Housing Services Dept	э	255 N. Forbes St.	Lakeport	CA	95453	(707) 263-2510	Linda Hedstrom	linda h@co.lake.ca.us	(707) 263-2751	Local, regional, national nonprofit org.		6/14/17	12/23/98
LOS ANGELES	West Hollywood Community Housing Corp.	2	7530 Santa Monica Blvd, Suite 1	West Hollywood	CA	90046	(323) 650-8771	Robin Conerty	robin@whchc.org	(323) 650-4745	Local, regional, public agency	х	8/14/14	12/23/98
LOS ANGELES	City of Pomona Housing Authority	3	505 South Garey Ave	Pomona	CA	91766	(909) 620-2368	Benita DeFrank, Neighborhood Services Director		(909) 620-3702	Local, regional, national nonprofit org.	×	6/14/17	12/23/98
LOS ANGELES	Hollywood Community Housing Corp.	3	1726 N. Whitley Ave	Hollywood	CA	90028	(323) 469-0710	Christina V. Duncan		(323) 469-1899	Local, regional, national nonprofit org.	x		12/23/98
LOS ANGELES	Hope - Net	3	760 S. Westmoreland Ave	Los Angeles	CA	90005	(213) 389-9949	Canoace Whalen	hope-netr@pacbell.net	(213) 389-0098	Local, regional, national nonprofit org.	×		12/23/98
LOS ANGELES	Skid Row Housing Trust	3	1317 E. 7th St	Los Angeles	CA	90021	(213) 683-0522	Jim Bonar		(213) 683-0781	Local, regional, national nonprofit org.	×		12/23/98
LOS ANGELES	The Long Beach Housing Development Co.	э	333 W. Ocean Blvd., 2nd Fir	Long Beach	CA	90802	(562) 570-6926	Diana V. McNeel		(562)570-6746	Local, regional, national nonprofit org.	×		12/23/98
MARIN	Housing Authority of the County of Marin	2	4020 CMc Center Drive	San Rafael	CA	94903	(415) 491-2530	Anna Semenova	ASemenova@marinhousing.org	(415) 491-2530	Local, regional, national nonprofit org.	×	2/26/14	12/23/98
MARIN	Canal Community Alliance	3	91 Larkspur St	San Rafael	CA	94901	(415) 454-2640	Tom Wilson	canalca@aol.com	(415) 454-3967	Local, regional, national nonprofit org.	×		12/23/98
ORANGE	Neighborhood Housing Services of Orange County Inc.	э	350 Hillcrest	La Habra	CA	90631	(562) 694-2051	Diane Ste.wart	nhs@aol.com	(562) 694-2052	Local, regional, national nonprofit org.	х		12/23/98
SAN DIEGO	Housing Development Partners of San Diego	2	1122 Broadway, Suite 300	San Diego	CA	92101	(619) 578-7555	Dottie Pierce	dottiep@sdhc.org	(619) 578-7360	Local, regional, national nonprofit org.	x	2/26/14	12/23/98
SAN DIEGO	San Diego Housing Commission	2	1122 Broadway, Suite 300	San Diego	CA	92101		Dottie Pierce	dottiep@s.dhc.org	(619) 578-7360	Local, regional, national nonprofit org.	×	8/14/14	12/23/98
SAN DIEGO COUNTY	Housing Development Partners of San Diego		1122 Broadway, Suite 300	San Diego	CA	92101		Dottie Pierce	dottiep@sdhc.org	(619) 578-7356	Local, regional, national nonprofit org.	×	2/26/14	12/23/98
SAN FRANCISCO	Tenderloin Neighborhood Development Corp.	э	201 Eddy St	San Francisco	CA		(415) 776-2151	Don Falk	tndc@ix-netcom.com	(415) 776-3952	Local, regional, national nonprofit org.	×		12/23/98
SAN JOAQUIN	Stockton Shelter for the Homeless	3	P.O. Box 4803	Stockton	CA	95204		Bill Mendelson		(209) 943-4806	Local, regional, national nonprofit org.	×		12/23/98
SANTA CLARA	Cambrian Center, Inc.	э	2360 Samaritan Place	SanJose	CA	95124	, ,	Dale J. Harrington	dale2360@ix.netcom.com	(408) 377-0478	Local, regional, national nonprofit org.	×		12/23/98
SANTA CLARA	Charities Housing Development Corp.	3	195 East San Fernando St	SanJose	CA	95112		Chris Block	chblock@aol.com	(408) 282-1130	Local, regional, national nonprofit org.	×		12/23/98
SANTA CLARA	Palo Alto Senior Housing Project, Inc.	3	455 E. Charleston Rd	Palo Alto	CA	94306		Genie Dee	gxdee@california.com	(650) 493-7437	Local, regional, national nonprofit org.	x		12/23/98
SONOMA	Burbank Housing Development Corp.	э	3432 Mendocino Ave	Santa Rosa	CA	95403	(707) 526-9782	John Lowry	burbank@sonic.net	(707) 526-9811	Local, regional, national nonprofit org.	×		12/23/98
ALAMEDA	Affordable Housing Associates	3	1250 Addison St., Ste. G	Berkeley	CA	94702		Susan Friedlow		(510) 649-0312	Local, regional, national nonprofit org.		5/9/17	12/24/98
ALL COUNTIES	Resources for Community Development	4	2220 Oxford St	Berkeley	CA	94704	(510) 841-4410	Dan Sawislak	dsawislak@rcdhousing.org	(510) 548-3502	Local, regional, national nonprofit org.	×	6/5/17	12/24/98
ALPINE	Amador-Tuolumne Community Action Agency	3	935 South State Highway 49	Jackson	CA	95642	(209) 223-1485 Ext. 22	Raj Rambob	rrambob@atcaa.org	(209) 223-4178	Local, regional, national public agency	×	5/24/17	12/24/98
CALAVERAS	ACLC, Inc	3	315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
COLUSA	Eskaton Properties Inc.	3	5105 Marganita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
CONTRA COSTA	ACLC, Inc		315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811	, , , , , , , , , , , , , , , , , , , ,		(209) 466-3465	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
CONTRA COSTA	East Bay NHS	3	2320 Cutting Blvd	Richmond	CA	94804	(510) 237-6459			(510) 237-6482	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
CONTRA COSTA	Affordable Housing Associates		1250 Addison St., Ste. G	Berkeley	CA	94702	(510) 649-8500	Susan Friedow		(510) 649-0312	Local, regional, national nonprofit org.		5/24/17	12/24/98
CONTRA COSTA	Eskaton Properties Inc.		5105 Marganita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
EL DORADO	Eskaton Properties Inc.		5105 Marganita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
FRESNO	ACLC, Inc		315 N San Joaquin St	Stockton	CA		(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	x	9/27/10	12/24/98
FRESNO	Better Opportunities Builder, Inc.	2	P.O. Box 11863	Fresno	CA		(559) 443-8400	Tracewell Hanrahan	nandersen@bobinc.org	(559) 443-8495	Local, regional, national nonprofit org.	×	8/14/14	12/24/98
GLENN	Eskaton Properties Inc.		5105 Marganita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
KERN	Self-Help Enterprises		8445 W. Elowin Court/P.O. Box 6520	Visalia	CA		(559) 802-1620	Thomas J. Collishaw	tomc@selfhelpenterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	x	4/10/18	12/24/98
KINGS	Self-Help Enterprises		8445 W. Elowin Court/P.O. Box 6520	Visalia	CA		(559) 802-1620	Thomas J. Collishaw	tomo@seifhelpenterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	×	4/10/18	12/24/98
LASSEN	Eskaton Properties Inc.		5105 Marzanita Ave	Carmichael	CA		(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
LOS ANGELES	Housing Authority of the City of Los Angeles	3	2500 Wilshire Blvd, PHA	Los Angeles	CA		(213) 252-4269	Larry Goins			Local, regional, national public agency	x	3/3/11	12/24/98
LOS ANGELES	Century Housing Corporation	2	1000 Corporate Pointe	Culver City	CA		(310) 642-2007	Ronald A. Griffith		(310) 258-0710	Local, regional, national public agency	×	8/14/14	12/24/98
MADERA	ACLC, Inc		315 N San Joaquin St	Stockton	CA		(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	x	9/27/10	12/24/98
MADERA	Self-Help Enterprises		8445 W. Elowin Court/P.O. Box 6520		CA		(559) 802-1620	Thomas J. Collishaw	tomc@seifhelpenterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	x	4/10/18	12/24/98
MARIN	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA		(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
MARIN	Affordable Housing Foundation	3	P.O. Box 26516	San Francisco	CA		(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.			12/24/98





County	<u>Organization</u>	code	<u>Address</u>	City	<u>ST</u>	<u>Zip</u>	Phone Number	Contact Person	E-Mail Address	<u>FAX Number</u>	Type of Organization	Confirm	Date Revised	List Added
MARIPOSA	Self-Help Enterprises		8445 W. Elowin Court/P.O. Bax 6520	∨isala	CA	93290	(559) 802-1620	Thomas J. Collishaw	tomc@sethelperiterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	х	4/10/18	12/24/98
MERCED	ACLC, Inc		315 N San Joaquín St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	X	9/27/10	12/24/98
MERCED	Eskaton Properties Inc.		5105 Marganita Ave.	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	х	5/24/17	12/24/98
MERCED	Self-Help Enterprises		8445 W. Elowin Court/P.O. Bax 6520	∨isala	CA	93290	(559) 802-1620	Thomas J. Collishaw	tomc@sethelperiterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	×	4/10/18	12/24/98
MONTEREY	Mid-Peninsula Housing Coalition	3	303 Vintage Park Drive, #250	Foster City	CA	94404	(650) 356-2900			(650) 357-9766	Local, regional, national public agency	×	8/14/14	12/24/98
MONTEREY	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	etloanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
MONTEREY	South County Housing, Inc.		7455 Carmel St	Gilray	CA	95020	(408) 842-9181	Jan Lindenthal	jan@scounty.com	(408) 842-0277	Local, regional, national nonprofit org.	×		12/24/98
NAPA	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
ORANGE	Century Housing Corporation		1000 Corporate Pointe	Culver City	CA	90230	(310) 642-2007	Ronald A. Griffith		(310) 258-0710	Local, regional, national public agency	х	8/14/14	12/24/98
ORANGE	Civic Center Barrio Housing Corp.		1665 E. 4th St, Ste. 210	Santa Ana	CA	92701	(714) 835-0406	Helen Brown	ccbho@msn.com	(714) 835-7354	Local, regional, national public agency	×		12/24/98
PLACER	ACLC, Inc		315 N San Joaquín St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
PLACER	Eskaton Properties Inc.		5105 Manzanita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
PLACER	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	etloanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
RIVERSIDE	St. Vincent de Paul Village	2	3350 E St	San Diego	CA	92102	(619) 687-1029	Bill Boistad		(619) 687-1010	Local, regional, national nonprofit org.	х	8/14/14	12/24/98
RIVERSIDE	Neighborhood Housing Services of the Inland Empire, Inc.	э	1390 North D St	San Bernardino	CA	92405	(909) 884-6891	Edward Moncrief	edward@nhsie.org	(909) 884-6893		×		12/24/98
SACRAMENTO	ACLC, Inc		315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
SACRAMENTO	Eskaton Properties Inc.		5105 Marizanita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98
SACRAMENTO	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
SAN BENITO	South County Housing, Inc.		7455 Carmel St.	Gilray	CA	95020		Jan Lindenthal	jan@scounty.com	(408) 842-0277	Local, regional, national nonprofit org.	×		12/24/98
SAN BERNARDINO	Century Housing Corporation		1000 Corporate Pointe	Culver City	CA	90230		Ronald A. Griffith		(310) 258-0710	Local, regional, national public agency	×	8/14/14	12/24/98
SAN BERNARDINO	Neighborhood Housing Services of the Inland Empire, Inc.		1390 North D St	San Bernardino	CA	92405		Edward Moncrief	edward@nhsie.org	(909) 884-6893	Econ, regiona, resiona paone agency	×	211111	12/24/98
SAN DIEGO	St. Vincent de Paul Village		3350 E St	San Diego	CA	92102	, ,	Harvey Mandel	hmandel@neighbor.org	(619) 687-1010	Local, regional, national nonprofit org.	×	B/14/14	12/24/98
SAN DIEGO	Civic Center Barrio Housing Corp	3	1665 E. 4th St. Ste. 210	Santa Ana	CA	92701		Helen Brown	c.cbhc@msn.com	(714) 835-7354	Local, regional, national public agency	×	0.140.14	12/24/98
SAN FRANCISCO	Affordable Housing Foundation		P.O. Bax 26516	San Francisco	CA		(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	Û		12/24/98
SAN JOAQUIN	ACLC, Inc		315 N San Joaquin St	Stockton	CA		(209) 466-6811	Life rang		(209) 466-3465	Local, regional, national nonprofit org.	v	9/27/10	12/24/98
SAN JOAQUIN	Eskaton Properties Inc.		5105 Marganita Ave	Carmichael	CA	95608		Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248		v	5/24/17	12/24/98
SAN MATEO	The second secon				CA	94404	<u> </u>	Codiney Tatali, Kaniy Salidi (elinei dile)	Cool il ey Catolitoges Patolitory	(650) 357-9766	Local, regional, national nonprofit org.	Û	8/14/14	12/24/98
	Mid-Peninsula Housing Coalition		303 Vintage Park Drive, #250	Foster City	CA		(415) 387-7834	Edo Tono	alianamash/Rasil sam		Local, regional, national public agency	V	0/14/14	12/24/98
SAN MATEO	Affordable Housing Foundation	3	P.O. Box 26516 725 Alma St	San Francisco Palo Alto				Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	Č.	 	12/24/98
SAN MATEO	Pato Atto Housing Corp	-			CA	94301		Marlene H. Prendergast		(650) 321-4341	Local, regional, national nonprofit org.	×	0.114.114	
SANTA CLARA	Mid-Peninsula Housing Coalition		303 Vintage Park Drive, #250	Foster City	CA	94404		54		(650) 357-9766	Local, regional, national public agency		8/14/14	12/24/98
SANTA CLARA	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA		(415) 387-7834	Eric Tang	etioanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	X	 	12/24/98
SANTA CLARA	Palo Alto Housing Corp	_	725 Alma St	Palo Alto	CA	94301	, , , , , , , , , , , , , , , , , , , ,	Marlene H. Prendergast		(650) 321-4341	Local, regional, national nonprofit org.	×		12/24/98
SANTA CLARA	South County Housing, Inc	ů,	7455 Carmel St	Gilray	CA	95020	1	Jan Lindenthal	jan@scounty.com	(408) 842-0277	Local, regional, national nonprofit org.	X		12/24/98
SANTA CRUZ	Mid-Peninsula Housing Coalition		303 Vintage Park Drive, #250	Foster City	CA	94404	1			(650) 357-9766	Local, regional, national public agency	X	8/14/14	12/24/98
SANTA CRUZ	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA		(415) 387-7834	Eric Tang	etloanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
SANTA CRUZ	South County Housing, Inc		7455 Carmel St	Gilray	CA	95020	1	Jan Linderthal	jan@scounty.com	(408) 842-0277	Local, regional, national nonprofit org.	X		12/24/98
SISKIYOU	Eskaton Properties Inc .		5105 Marizanita Ave	Carmichael	CA		(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	X	5/24/17	12/24/98
SOLANO	ACLC, Inc		315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	X	9/27/10	12/24/98
SOLANO	Mid-Peninsula Housing Coalition		303 Vintage Park Drive, #250	Foster City	CA	94404	(650) 356-2900			(650) 357-9766	Local, regional, national public agency	×	8/14/14	12/24/98
SOLANO	Affordable Housing Associates		1250 Addison St., Ste. G	Berkeley	CA	94702	(510) 649-8500	Susan Friedlow		(510) 649-0312	Local, regional, national nonprofit org.	-	5/24/17	12/24/98
SOLANO	Affordable Housing Foundation		P.O. Bax 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	etioanmach@sol.com	(415) 752-9902	Local, regional, national nonprofit org.	×		12/24/98
SONOMA	Affordable Housing Foundation		P.O. Box 26516	San Francisco	CA	94126	(415) 387-7834	Eric Tang	ettoanmach@aol.com	(415) 752-9902	Local, regional, national nonprofit org.	X		12/24/98
STANISLAUS	ACLC, Inc		315 N San Joaquín St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	х	9/27/10	12/24/98
STANISLAUS	Self-Help Enterprises		8445 W. Elowin Court/P.O. Bax 6520	Visala	CA	93290	(559) 802-1620	Thomas J. Collishaw	tomc@selfhelperterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	×	4/10/18	12/24/98
TULARE	Self-Help Enterprises		8445 W. Elowin Court/P.O. Box 6520	Visala	CA	93290	(559) 802-1620	Thomas J. Collishaw	tomc@selfhelpenterprises.org	(559) 651-3634	Local, regional, national nonprofit org.	X	4/10/18	12/24/98
TUOLUMNE	ACLC, Inc		315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	x	9/27/10	12/24/98
VENTURA	Century Housing Corporation		1000 Corporate Pointe	Culver City	CA	90230	(310) 642-2007	Ronald A. Griffith		(310) 258-0710	Local, regional, national public agency	×	8/14/14	12/24/98
YOLO	ACLC, Inc		315 N San Joaquin St	Stockton	CA	95202	(209) 466-6811			(209) 466-3465	Local, regional, national nonprofit org.	×	9/27/10	12/24/98
YOLO	Eskaton Properties Inc.		5105 Marganita Ave	Carmichael	CA	95608	(916) 334-0810	Courtney Tatum, Kathy Sailor (either one)	courtney.tatum@eskaton.org	(916) 338-1248	Local, regional, national nonprofit org.	×	5/24/17	12/24/98

A-2 Draft October 2021



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Gounty	Organization	code	<u>Address</u>	<u>G/ty</u>	<u>sr</u>	Zio	Phone Number	Contact Person	E-Mail Address	FAXNumber	Type of Organization	<u>Confirm</u>	Date Revised	List Added
ALL COUNTIES	BRIDGE Housing Corporation	3	345 Spear Strett, Suite 700	San Francisco	CA		(415) 989-1111	Brad Wiblin	-	(415) 495-4898	Local, regional, national public agency	X	12/24/10	12/28/98
AMADOR	Rural California Housing Corp	3	3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
CALAVERAS	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	X	8/14/14	12/28/98
COLUSA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
COLUSA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
CONTRA COSTA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
EL DORADO	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
GLENN	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
LOS ANGELES	FAME Corporation	2	1968 W. Adams Blvd.	Los Angeles	CA	90018	(323) 730-7727	Sandra Hernandez	sandrah@famecorporations.org	(323) 737-5717	Local, regional, national nonprofit org.	×	8/14/14	12/28/98
NAPA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
NEVADA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
PLACER	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
SACRAMENTO	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
SAN JOAQUIN	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
SHASTA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
SUTTER	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
TEHAMA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
YOLO	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	×	8/14/14	12/28/98
YUBA	Rural California Housing Corp		3120 Freeboard Drive, Suite 201	West Sacramento	CA	95691	(916) 414-4436				Local, regional, national public agency	v	8/14/14	12/28/98
RIVERSIDE			1280 Bison, Ste. B9-200	Newport Beach	CA	92660	, ,	Tracy Green	tio-build@msn.com	(949) 720-7434		Û	CO TAD TA	12/29/98
	BUILD Leadership Development Inc.	3							scock@pacbell.net		Local, regional, national nonprofit org.	· ·	+	
SACRAMENTO	Sacramento Valley Organizing Community		3263 1st Ave	Sacramento	CA	95817		Larry Ferlazzo		(916) 457-0207	Local, regional, national nonprofit org.	<u></u>	+	12/29/98
SAN BERNARDINO	BUILD Leadership Development Inc.		1280 Bison, Ste. B9-200	Newport Beach	CA	92660	-	Tracy Green	tto-build@msn.com	(949) 720-7434	Local, regional, national nonprofit org.	×	+	12/29/98
SAN FRANCISCO	BUILD Leadership Development Inc.		1280 Bison, Ste. B9-200	Newport Beach	CA	92660		Tracy Green	tic-build@msn.com	(949) 720-7434	Local, regional, national nonprofit org.	X	+	12/29/98
SOLANO	Sacramento Valley Organizing Community		3263 1st Ave	Sacramento	CA	95817		Larry Feriazzo	scock@pacbell.net	(916) 457-0207	Local, regional, national nonprofit org.	×	+	12/29/98
YOLO	Sacramento Valley Organizing Community		3263 1st Ave	Sacramento	CA	95817		Larry Feriazzo	scoci@pacbell.net	(916) 457-0207	Local, regional, national nonprofit org.	X	+	12/29/98
ALL COUNTIES	Foundation for Affordable Housing, Inc.	2	384 Forest Ave., Suite 14	Laguna Beach	CA	92651	(949) 443-9101	Darrin Willard		(949) 443-9133	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
BUTTE	Community Housing Improvement Program, Inc		1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	X	8/14/14	12/30/98
GLENN	Community Housing Improvement Program, Inc		1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
LASSEN	Community Housing Improvement Program, Inc	2	1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
PLUMAS	Community Housing Improvement Program, Inc		1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
SHASTA	Community Housing Improvement Program, Inc.		1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
SUTTER	Community Housing Improvement Program, Inc.		1001 Willow St	Chica	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
TEHAMA	Community Housing Improvement Program, Inc.		1001 Willow St	Chico	CA	95928	(530) 891-6931	Kris Zappettini	chip@sunset.net	(530) 891-8547	Local, regional, national nonprofit org.	×	8/14/14	12/30/98
ALAMEDA	East Bay Asian Local Development Corporation	э	310 Eighth Street, Ste. 200	Oakland	CA	94607		Lynette Jung Lee	junglee@ebaldc.com	(510) 763-4143	Local, regional, national nonprofit org.		5/9/17	1/5/99
CONTRA COSTA	East Bay Asian Local Development Corporation		310 Eighth Street, Suite 200	Oakland	CA	94607	(510) 287-5353			(510) 763-4143	Local, regional, national nonprofit org.		6/5/17	1/5/99
FRESNO	Fresna Ca. Economic Opportunities Commission	3	3120 W. Nielsen Ave., Ste. 102	Fresno	CA	93706	(559) 485-3733	George Egawa	eocnisn@psnw.com	(559) 485-3737	Local, regional, national nonprofit org.		6/14/17	1/5/99
SOLANO	Fairfield Redevelopment Agency, Planning Department	3	1000 WebSteir St., 2nd Floor	Fairfield	CA	94533	(707) 428-7688	Lark Solis	Isolis@ci fairfield.ca	(707) 428-7621	Local, regional, national nonprofit org.	×		1/5/99
EL DORADO	El Dorado County Housing Authority	з	937 Spring St	Placerville	CA	95667		Joyce Aldrich	laidrich@innercite.com		Local, regional, national nonprofit org.		6/14/17	1/6/99
LOS ANGELES	American Family Housing	2	15161 Jackson St.	Midway City	CA		(714) 897-3221	Donna Gallup	info@compuall.net	(714) 893-6858	Local, regional, national nonprofit org.	×	1/5/17	1/6/99
SAN FRANCISCO	Bernal Heights Neighborhood Center	3	515 Cortland Ave	San Francisco	CA		(415) 206-2140	Housing Director		(415) 648-0793	Local, regional, national nonprofit org.	×		1/6/99
SAN FRANCISCO	Mission Housing Development Corp	3	474 Valencia St, Ste. 280	San Francisco	CA		(415) 864-6432	Philip Dockow		(415) 864-0378	Local, regional, national nonprofit org.	×		1/8/99
STANISLAUS	Modesto Redevelopment Agency	3	940 11th St	Modesto	CA		(209) 577-5247	Bill Cooper	bcooper@ci.modesto.ca.us	(209) 544-3982	Local, regional, national nonprofit org.	×	1	1/8/99
SAN FRANCISCO	Chinatown Community Development Center	3	1525 Grant Ave	San Francisco	CA		(415) 984-1450		cchd@hooked.net	(415) 984-1494		×	1	1/11/99
	, , , , , , , , , , , , , , , , , , , ,	2					(925) 385-0754	Joanne Lee			Local, regional, national nonprofit org.	Û	2/26/44	1/12/99
ALL COUNTIES	The Trinity Housing Foundation	-	836 Avalon Ave	Lafayette	CA			Bill Leone	bleone@apr.com	(925) 215-2403	Local, regional, national public agency	V	2/26/14	
LOS ANGELES	The Long Beach Housing Development Co.	_	836 Avaion Ave	Lafayette	CA		(925) 385-0754	Bil Leone	bleone@apr.com	(925) 215-2403	Local, regional, national public agency	X	6/14/12	1/12/99
LOS ANGELES	PICO Union Housing Corporation	2	1038 Venice BMd.	Los Angeles	CA		(213) 747-2790	Jesus Torres	torres@puhc.org	(213) 743-3819	Local, regional, national nonprofit org.	X	8/14/14	1/12/99
VENTURA	A Community of Friends		836 Avalon Ave	Lafayette	CA		(925) 385-0754	Bill Leone	bleone@apr.com	(925) 215-2403	Local, regional, national public agency	X	6/14/12	1/12/99
LOS ANGELES	Korean Youth & Community Center, Inc. (KYCC)	3	680 S. Wilton Place	Los Angeles	CA	90005	(213) 365-7400	Jimmy Lee		(213) 353-1280	Local, regional, national nonprofit org.	X		1/19/99
CONTRA COSTA	Pacific Community Services, Inc.	3	329 Railroad Ave, P.O. Box 1397	Pittsburg	CA	94565	(925) 439-1056	Tom LaFleur	tomlf@earthlink.net	(925) 439-0831	Local, regional, national nonprofit org.	×	6/5/17	1/21/99



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County	Organization	code		City	<u>27</u>	Zip	Phone Number	Contact Person	E-Mail Address	FAXNumber	Type of Organization	Confirm	Date Revised	List Added
IMPERIAL	Coachella Valley Housing Coalition	3	45-701 Monroe St, Ste. G., Plaza I	Indio	CA	92201	(760) 347-3157	Emilia Mojica	Emolic a@cvhc.org	(760) 342-6466	Local, regional, national nonprofit org.	X	6/14/17	1/21/99
RIVERSIDE	Coachella Valley Housing Coalition	-	45-701 Monroe St, Ste. G., Plaza I	Indio	CA	92201	(760) 347-3157	Emilia Mojca	Emojica@cvhc.org	(760) 342-6466	Local, regional, national nonprofit org.	X	6/14/17	1/21/99
SAN BERNARDINO	Coachella Valley Housing Coalition		45-701 Monroe St, Ste. G., Plaza I	Indio	CA	92201	(760) 347-3157	Emilia Mojica	Emojic a@cvhc.org	(760) 342-6466	Local, regional, national nonprofit org.	×	6/14/17	1/21/99
SAN DIEGO	Coachella Valley Housing Coalition		45-701 Monroe St, Ste. G., Plaza I	Indio	CA	92201	(760) 347-3157	Emilia Mojca	Emojica@cvhc.org	(760) 342-6466	Local, regional, national nonprofit org.	×	6/14/17	1/21/99
SOLANO	Pacific Community Services, Inc.		329 Railroad Ave, P.O. Box 1397	Pittsburg	CA	94565	(925) 439-1056	Tom LaFleur	Pacomseru@aol.cn	(925) 439-0831	Local, regional, national nonprofit org.	×		1/21/99
SONOMA	Pacific Community Services, Inc.	_	329 Railroad Ave, P.O. Box 1397	Pittsburg	CA	94565	(925) 439-1056	Tom LaFleur	pac omseru@aol.com	(925) 439-0831	Local, regional, national nonprofit org.	×		1/21/99
ALAMEDA	Community and Economic Development Agency	3	250 Frank H. Ogawa Plaza Ste. 5313	Oakland	CA	94612	(510) 238-3502	Jefferey P. Levin	iplevin@oaklandhet.com	(510) 238-3691	Local, regional, national nonprofit org.		5/9/17	1/27/99
ALAMEDA	Bay Area Community Services	3	629 Oakland Ave	Oakland	CA	94611	(510) 499-0365	Daniel Cooperman	dcooperman@bayareacs.org	(510) 569-4589	Local, regional, national nonprofit org.	×	5/9/17	1/28/99
ALL COUNTIES	National Community Renaissance	3	9421 Haven Avenue	Rancho Cucamonga	CA	91730	(909) 204-3508	Tracey Williams	twilliams@nationalcore.org	(909) 483-6524	Local, regional, national public agency	x	5/24/17	5/17/99
ALL COUNTIES	Community Housing Assistance Program, Inc.	2	3803 E. Casselle Ave	Orange	CA	92869		Ken Robertson	chapahq1@aol.com	(714) 744-6850	Local, regional, national public agency	×	8/14/14	5/18/99
CONTRA COSTA	Community Housing Development Corp.	3	1535 Fred Jackson Way ste A	Richmond	CA	94801	(510) 412-9290	Donald Gilmore	Dailmore@chdcnr.com	(510) 215-9276	Local, regional, national nonprofit org.	×	5/24/17	5/19/99
DEL NORTE	Redwood Community Action Agency	2	904 G St.	Eureka	CA	95501	(707) 269-2021	Bill Rodstrom	planning@rcaa.org		Local, regional, national nonprofit org.	×	8/14/14	5/19/99
HUMBOLDT	Redwood Community Action Agency		904 G St	Eureka	CA	95501	(707) 269-2021	Bill Rodstrom	planning@rcaa.org		Local, regional, national nonprofit org.	×	8/14/14	5/19/99
LOS ANGELES	Long Beach Affordable Housing Coalition, Inc	3	5855 Naples Plaza, Suite 209	Long Beach	CA	90803	(562) 434-3333	H. Kim Huntley		(562) 434-3330	Local, regional, national public agency	x	9/26/08	5/19/99
MONTEREY	Peoples' Self-Help Housing Corp.		3533 Empleo St	San Luis Obispo	CA	93401	(805) 540-2452	John Fowler	admin@pshhc.org	(805) 544-1901	Local, regional, national nonprofit org.	×	8/14/14	5/19/99
ORANGE	Long Beach Affordable Housing Coalition, Inc		5855 Naples Plaza, Suite 209	Long Beach	CA	90803	(562) 434-3333	H. Kim Huntley		(562) 434-3330	Local, regional, national public agency	v	9/26/08	5/19/99
SAN BENITO	Peoples' Self-Help Housing Corp	2	3533 Empleo St.	San Luis Obispo	CA	93401	(805) 540-2452	John Fowler	http://www.pshh.org/	(805) 544-1901	Local, regional, national nonprofit org.	Ĉ.	8/14/14	5/19/99
SAN DIEGO		_	5855 Naples Plaza, Suite 209	Long Beach	CA	90803	(562) 434-3333	H. Kim Huntley		(562) 434-3330		v	9/26/08	5/19/99
	Long Beach Affordable Housing Coalition, Inc.	3	5100 Federal Blvd, 2nd Floor		CA	92105	(619) 262-8403	Stasi Williams		(619) 262-7836	Local, regional, national public agency	Û	5/20/00	5/19/99
SAN DIEGO	Bayview CDC	-		San Diego			,		a decision Constitution and		Local, regional, national nonprofit org.	L.	0.1.4.1.4	0
SAN LUIS OBISPO	Peoples' Self-Help Housing Corp.		3533 Empleo St.	San Luis Obispo	CA	93401	(805) 540-2452	John Fowler	admin@pshhc.org	(805) 544-1901	Local, regional, national nonprofit org.	×	8/14/14	5/19/99
SANTA BARBARA	Long Beach Affordable Housing Coalition, Inc.	_	5855 Naples Plaza, Suite 209	Long Beach	CA	90803	(562) 434-3333	H. Kim Huntley		(562) 434-3330	Local, regional, national public agency	X	9/26/08	5/19/99
SANTA BARBARA	Peoples' Self-Help Housing Corp.	-	3533 Empleo St.	San Luis Obispo	CA	93401	(805) 540-2452	John Fowler	admin@pshhc.org	(805) 544-1901	Local, regional, national nonprofit org.	X	8/14/14	5/19/99
VENTURA	Long Beach Affordable Housing Coalition, Inc		5855 Naples Plaza, Suite 209	Long Beach	CA	90803	(562) 434-3333	H. Kim Huntley		(562) 434-3330	Local, regional, national public agency	X	9/26/08	5/19/99
VENTURA	Peoples' Self-Help Housing Corp.		3533 Empleo St.	San Luis Obispo	CA	93401	(805) 540-2452	John Fowler	admin@pshhc.org	(805) 544-1901	Local, regional, national nonprofit org.	X	8/14/14	5/19/99
ALL COUNTIES	DML & Associates Foundation	9	6043 Tampa Ave, Ste. 101A	Tarzana	CA	91356	(818) 708-2710	Myron Lieberman		(818) 708-1944	Local, regional, national nonprofit org.		5/23/17	5/21/99
ALL COUNTIES	EAH, Inc.	3	22 Pelican Way	San Rafael	CA	94901	(415) 599-2712	Scott Johnson	scott.johnson@eahhousing.org	(415) 453-3683	Local, regional, national public agency	X	5/23/18	5/21/99
ALAMEDA	Satellite Affordable Housing Associates	2	1835 Alcatraz Ave.	Berkeley	CA	94703	(510) 647-0700	Susan Friedland	Susanfriedland@sahahomes.org	(510) 647-0820	Local, regional, national nonprofit org.	X	2/26/14	6/10/99
CONTRA COSTA	Anka Behavioral Health	3	1850 Gateway Blvd., Suite 900	Concord	CA	94520	(925) 825-4700			(925) 825-2610	Local, regional, national nonprofit org.	x	9/27/10	6/10/99
CONTRA COSTA	Anka Behavioral Health		1850 Gateway Blvd., Suite 900	Concord	CA	94520	(925) 825-4700			(925) 825-2610	Local, regional, national nonprofit org.	×	9/27/10	6/10/99
CONTRA COSTA	Satelite Housing Inc.		2526 Martin Luther King., Jr Way	Berkeley	CA	94704	(510) 647-0700	Susan Friedland	staff@sathomes.org	(510) 647-0820	Local, regional, national nonprofit org.	×	2/26/14	6/10/99
KERN	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
LOS ANGELES	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
NAPA	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccromar@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
ORANGE	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
RIVERSIDE	Anka Behavioral Health		1850 Gateway Blvd., Suite 900	Concord	CA	94520	(925) 825-4700		zzcheap@aol.com	(925) 825-2610	Local, regional, national nonprofit org.	×	9/27/10	6/10/99
RIVERSIDE	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
SACRAMENTO	Satellite Housing Inc.		2526 Martin Luther King., Jr Way	Berkeley			(510) 647-0700	Susan Friedland,	staff@sathomes.org	(510) 647-0820	Local, regional, national nonprofit org.	×	2/26/14	6/10/99
SACRAMENTO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com	(2.2)	Local, regional, national public agency		6/14/17	6/10/99
SAN BERNARDINO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
			-											
SAN DIEGO SAN FRANCISCO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach Berkeley	CA		(323) 726-9672 (510) 647-0700	Carol Cromar	hcaccroman@dessretonline.com	(510) 647-0820	Local, regional, national public agency	L .	6/14/17	6/10/99
	Satellite Housing Inc.	-	2526 Martin Luther King., Jr Way					Susan Friedland,	staff@sathomes.org	(510) 647-0820	Local, regional, national nonprofit org.	^	2/26/14	
SAN FRANCISCO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency	+	6/14/17	6/10/99
SAN JOAQUIN	Housing Corporation of America	-	31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccromar@dessretonline.com		Local, regional, national public agency	+	6/14/17	6/10/99
SAN MATEO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com	_	Local, regional, national public agency	+	6/14/17	6/10/99
SANTA BARBARA	Housing Corporation of America	-	31423 Coast Highway, Ste. 7100	Laguna Beach	CA		(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
SANTA CLARA	Satellite Housing Inc.	-	2526 Martin Luther King., Jr Way	Berkeley	CA	94704	(510) 647-0700	Susan Friedland,	staff@sathomes.org	(510) 647-0820	Local, regional, national nonprofit org.	X	2/26/14	6/10/99
SOLANO	Anka Behavioral Health	-	1850 Gateway Blvd., Suite 900	Concord	CA	94520	(925) 825-4700		zzcheap@aol.com	(925) 825-2610	Local, regional, national nonprofit org.	×	9/27/10	6/10/99
SOLANO	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
STANISLAUS	Housing Authority of the County of Stanislaus	3	P.O. Box 581918	Madesto	CA	95358	(209) 523-0705	Rich Chubon	Rchubon@stancoha.net	(209) 522-8637	Local, regional, national nonprofit org.	×		6/10/99

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City of Ontario Policy Plan Housing Element Technical Report



County	Organization	code	Address	City	<u>sr</u>	Zio	Phone Number	Contact Person	E-Mail Address	FAXNumber	Type of Organization	Confirm	Date Revised	List Added
VENTURA	Housing Corporation of America		31423 Coast Highway, Ste. 7100	Laguna Beach	CA	92677	(323) 726-9672	Carol Cromar	hcaccroman@dessretonline.com		Local, regional, national public agency		6/14/17	6/10/99
LOS ANGELES	Abode Communities	2	701 E. Third St., Ste. 400	Los Angeles	CA	90015	(213) 629-2702	Holly Benson		(213) 627-6407	Local, regional, national public agency	×	8/14/14	3/9/00
ALL COUNTIES	University River Village	3	7901 La Riviera Drive	Sacramento	CA	95826	(916) 381-2001	Kaci Walsh		(916) 381-7321	Local, regional, national public agency		5/10/17	6/1/00
ALL COUNTIES	National Affordable Housing Trust	э	2335 North Bank Drive	Columbus	ОН	43220	(614) 451-9929	Robert Snow	bsnow@naht.org	(614) 451-3370	Local, regional, national public agency		5/24/17	6/1/00
SAN DIEGO	San Diego Co. Dept. of Housing & Cmty Development	3	3989 Ruffin Road	San Diego	CA	92123		Alfredo Ybarra	avbarrcd@co.san.dego.ca.us	(858) 694-4871	Local, regional, national nonprofit org.	×		9/19/00
ALL COUNTIES	California Housing Partnership Corporation	2	369 Pine Street, Suite 300	San Francisco	CA		(415) 433-6804	Matt Schwartz	mschwartz/@chp.net	(415) 433-6805	Local, regional, public agency	x	8/14/14	10/23/00
ALL COUNTIES	Solari Enterprises, Inc.	3	1544 W. Yale Ave	Orange	CA	92687	(714) 282-2520	Bruce Solari	solari@solari-ent.com	(714) 282-2521	Profit-motivated individual or organization	-	5/24/17	12/29/00
LOS ANGELES	Southern California Presbyterian Homes	2	516 Burchett St	Glendale	CA		(818) 247-0420	Jacqueline A Seegobm		(818) 247-0420	Local, regional, national public agency	×	8/14/14	12/29/00
RIVERSIDE	Southern California Presbyterian Homes		516 Burchett St	Glendale	CA		(818) 247-0420	Jacqueline A Seegobm		(818) 247-0420	Local, regional, national public agency	×	8/14/14	12/29/00
SAN BERNARDINO	Southern California Presbyterian Homes		516 Burchett St	Glendale	CA	91203		Jacqueline A Seegobm		(818) 247-0420	Local, regional, national public agency	x	8/14/14	12/29/00
ALL COUNTIES	Mercy Housing, Inc.	3	1999 Broadway, Suite 1000	Deriver	co	80202	303-830-3300	Janet Gaf	info@mercyhousing.org	(303) 830-3301	Local, regional, national public agency	-	5/24/17	1/17/01
ALL COUNTIES	California Community Reinvestment Corp.	2	100 West Broadway Ste. 1000	Glendale	CA	91210	818-844-4951	David Saltzman	david.saltzman@e-ccrc.org	(818) 550-9806	Local, regional, national nonprofit org.	×	5/23/17	1/18/01
FRESNO	The East Los Angeles Community Union (TELACU)	2	1248 Goodrich Blvd.	Los Angeles	CA	90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	(323) 838-0548		Û	8/14/14	1/29/01
KERN		_	1248 Goodrich BMd.	Los Angeles	CA	90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	(323) 838-0548	Local, regional, national public agency	Ĉ.	8/14/14	1/29/01
LOS ANGELES	The East Los Angeles Community Union (TELACU)				CA					(323) 838-0548	Local, regional, national public agency	Û		1/29/01
	The East Los Angeles Community Union (TELACU)		1248 Goodrich BMd.	Los Angeles		90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	1	Local, regional, national public agency	Û	8/14/14	
RIVERSIDE	The East Los Angeles Community Union (TELACU)		1248 Goodrich BMd.	Los Angeles	CA	90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	(323) 838-0548	Local, regional, national public agency	X	8/14/14	1/29/01
SAN BERNARDINO	The East Los Angeles Community Union (TELACU)		1248 Goodrich BMd.	Los Angeles	CA	90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	(323) 838-0548	Local, regional, national public agency	X	8/14/14	1/29/01
SAN DIEGO	The East Los Angeles Community Union (TELACU)	_	1248 Goodrich BMd.	Los Angeles	ÇA	90022	(323) 838-8556	Jasmine Borrego	trmreception@telacu.com	(323) 838-0548	Local, regional, national public agency	×	8/14/14	1/29/01
ALAMEDA	Christian Church Homes of Northern California, Inc.	3	303 Hegenberger Road, Ste. 201	Oakland	CA	94621	(510) 632-6712	William F. Pickel	bpickel@cchnc.org	(510) 632-6755	Local, regional, national public agency	<u> </u>	5/9/17	2/6/01
ALL COUNTIES	Goldrich & Kest Industries , LLC	2	5150 Overland Avenue	Culver City	CA	90230	(310) 204-2050	Carole Glodney	Carole@Gkind.com	(310) 280-5767	Profit-motivated individual or organization	X	8/14/14	4/25/01
LOS ANGELES	LTSC Community Development Corporation	2	231 East Third Street, Ste. G 106	Los Angeles	CA	90013	(213) 473-1606	Takao Suzuki	tsuzukk@ltsc.org	(213) 473-1681	Local, regional, national nonprofit org.	X	8/14/14	4/25/01
ALL COUNTIES	East Los Angeles Community Corporation	3	530 South Boyle Avenue	Los Angeles	CA	90033	(323) 269-4214	Emesto Espinoza		(323) 261-1065	Local, regional, national nonprofit org.	X	12/24/10	7/13/01
LOS ANGELES	Nexus for Affordable Housing	2	1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	bruce@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	X	8/14/14	7/13/01
ORANGE	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	pruce@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	X	8/14/14	7/13/01
RIVERSIDE	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	<u>bruc ඇති solari-ent.com</u>	(714) 282-2521	Local, regional, national nonprofit org.	X	8/14/14	7/13/01
SAN BERNARDINO	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	bruce@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	×	8/14/14	7/13/01
SAN DIEGO	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	bruce@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	×	8/14/14	7/13/01
SANTA BARBARA	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	bruc er@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	×	8/14/14	7/13/01
VENTURA	Nexus for Affordable Housing		1572 N. Main Street	Orange	CA	92867	(714) 282-2520	Bruce Solari	bruc et@solari-ent.com	(714) 282-2521	Local, regional, national nonprofit org.	х	8/14/14	7/13/01
SACRAMENTO	Norwood Family Housing	3	630 I Street, Second Floor	Sacramento	CA	95814	(916) 440-1328	Darren Bobrowsky	dbobrowsky@shra.org	(916) 442-6736	Local, regional, national nonprofit org.	×		1/9/02
ALL COUNTIES	Bayside Communities	9	1990 North California Blvd., Ste. 1070	Walnut Creek	CA	94596	(925) 482-9406	Basil Rallis	brallis@baysidecommunities.com	(510) 891-9004	Profit-motivated individual or organization	×	5/10/17	9/10/02
ALL COUNTIES	Newport Development, LLC	3	9 Cushing, Ste. 200	Irvine	CA	92618	(949) 923-7812	Warren Allen	waller@newportpartners.com	(949) 585-0449	Profit-motivated individual or organization	×	5/24/17	9/10/02
ALL COUNTIES	Mercy Housing California	Э	1360 Mission St., Suite 300	San Francisco	CA	94103	213-743-5830	Ed Holder	eholder@mercyhousing.org	(415) 553-6373	Local, regional, national nonprofit org.	х	5/24/17	12/9/02
ALL COUNTIES	KDF Communities, LLC	3	1301 Dove St., Suite 720	Newport Beach	CA	92660	(949) 622-1888	John Bemard		(949) 851-1819	Profit-motivated individual or organization		5/23/17	12/13/02
SONOMA	Divine Senior Apartments	3	P.O. Box 148	Occidental	CA	95465	(707) 874-3538	Richard W. Blanz		(707) 874-3538	Local, regional, national nonprofit org.	×		5/29/03
ALL COUNTIES	California Human Development Corporation	2	3315 Airway Drive	Santa Rosa	CA	95403	(707) 521-4788	John M. Way	CaliforniaHumanDevelopment.org	(707) 523-3776	Local, regional, national nonprofit org.	×	8/14/14	6/30/03
ALL COUNTIES	Reiner Communities LLC	э	100 Spectrum Center Dr. Suite 830	Irvine	CA	92618	(949) 753-0555	Dylan Feliciano	df@reinertc.com		Profit-motivated individual or organization	×	5/16/17	6/30/03
ALL COUNTIES	Petaluma Ecumenical Properties Inc.		1400 Caulfield Lane	Petaluma	CA	94954	(707) 762-2336	Vera R. Ciammetti	pep@pephousing.org	(707) 762-4657	Local, regional, national nonprofit org.		5/10/17	8/19/03
ALL COUNTIES	The John Stewert Company	3	1388 Sutter St., 11th Floor	San Francisco	CA	94109	(415) 345-4400	Margaret Miller	mmillen@isw.net	(415) 614-9175	State-wide, for-profit	×	5/24/17	8/19/03
ALL COUNTIES	William G. Ayyad, Inc.	3	9252 Chesepeake Dr., Suite 100	San Diego	CA	92123	(858) 244-0900 *4	Rebecca Ayyad	rayyad@udgi.net	(858) 244-0909	Profit-motivated individual or organization		5/24/17	8/19/03
MENDOCINO	CDC of Mendocino County	2	1076 North State Street	Ukiah	CA	95482		Todd Crabtree	crabtret@cdchousing.org	(707) 463-4188	Public housing authority		2/26/14	8/25/03
SONOMA	Sonoma County Community Development Commission	2	1440 Guemeville Road	Santa Rosa	CA		(707) 565-7901	Nick Stewart	Nick Stewart@sonoma-county.org				8/14/14	9/8/03
ALL COUNTIES	Linc Housing Corporation	2	100 Pine Avenue, # 500	Long Beach	CA		(562) 684-1100	Sid Paul		(562) 684-1137			8/14/14	9/15/03
LOS ANGELES	Francis R. Hardy, Jr.	3	2735 W. 94th Street	Inglewood	CA		(323) 756-6533	Francis R. Hardy, Jr.		(323) 756-6533				9/18/03
ALL COUNTIES	Domus Development, LLC	2	594 Howard St., Ste 204	San Francisco	CA		(415) 856-0010	Meea Kang		(415) 856-0264		×	8/14/14	2/4/04
ALL COUNTIES	BUILD Leadership Development, Inc.	3	P.O. Box 9414	Newport Beach	CA		(877) 644-9422	Tracy Green		(949) 719-9711	Local, regional, national nonprofit org.	1	5/16/17	2/4/04
ALL COUNTIES	Skyline Real Estate Development & Acquisitions, Inc.	3	P.O. Box 7613	Newport Beach	CA		(949) 293-4705	Lynn Miller	skylinerealestate@cox.net	(949) 719-9711	Profit-motivated individual or organization	1	5/24/17	2/4/04
OS ANGELES		Ť		Irvine	CA		(415) 856-0010			(415) 856-0264	Tone-incuraced marriada di diganzasidh	V	8/27/13	2/4/04
JUS ANGELES	A Community of Friends		9 Cushing, Ste. 200	III AILUS	CA	92618	(413) 836-0010	Meea Kang	meea@domusd.com	(413) 036-0264		^	0/2//13	2/4/04



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County	<u>Organization</u>	code	Address	City	<u>sr</u>	Zio	Phone Number	Contact Person	E-Mail Address	FAX Number	Type of Organization	Confirm	Date Revised	List Added
ALL COUNTIES	Cabouchon Properties, LLC	3	Pier 9, Suite 114	San Francisco	CA	94111	(415) 433-2000	Susan Terrado		(415) 433-2000			5/23/17	4/28/04
ALL COUNTIES	Maximus Properties, LLC	3	23586 Calabasas Road, Ste. 103	Calabasas	CA	91302	(818)449-4004	Jeffrey S. McGuire	Imcquire@remax.net	(818) 449-4004			5/23/17	4/28/04
ALL COUNTIES	Squier Properties, LLC	3	1157 Lake Street	Venice	CA	90291	(310) 418-6389	Scott Richards√	gsquier@earthlink.net	(310) 418-6389	Profit-motivated individual or organization		5/24/17	4/28/04
LOS ANGELES	Many Mansions, Inc.	3	1459 E. Thousand Oaks Blvd.,Ste.C	Thousand Oaks	CA	91362	(805) 496-4948	Neil McGuffin	danhardv@west.net	(805) 496-4948	Local, regional, national nonprofit org.	×		4/28/04
LOS ANGELES	Wirnetka King, LLC	3	23586 Calabasas Road, Ste. 100	Los Angeles	CA	91302	(818) 222-2800	Rick Macaya		(818) 222-2800				4/28/04
SANTA BARBARA	Many Mansions, Inc.		1459 E. Thousand Oaks Blvd.,Ste.C	Thousand Oaks	CA	91362	(805) 496-4948	Neil McGuffin		(805) 496-4948				4/28/04
VENTURA	Many Mansions, Inc.		1459 E. Thousand Oaks BMd., Ste.C	Thousand Oaks	CA	91362	(805) 496-4948	Neil McGuffin	danhardv@west.net	(805) 496-4948	Local, regional, national nonprofit org.	×		4/28/04
ALL COUNTIES	Creative Housing Coalition	3	4612 Ata Canyada Road	La Canada	CA	91011	(805) 736-9342	Jane Anderson		(805) 736-9342			5/23/17	5/19/04
ALL COUNTIES	Fallbrook Capital Corporation	3	6700 Fallbrook Avenue, #111	West Hills	CA	91307	(818) 712-6931	Brandt Blaken		(818) 712-6931			5/23/17	6/1/04
LOS ANGELES	Los Angeles Housing & Community Invest Dept	2	1200 W.7th Street, 9th Floor	Los Angeles	CA	90017	(213) 808-8654	Franklin Campos	fcampos@lahd.lacity.org	(213) 808-8999		x	8/14/14	3/15/05
ALL COUNTIES	West Bay Housing Corporation	3	1390 Market Street, Ste. 405	San Francisco	CA	94102	(415) 618-0012	Bill Pickel		(415) 618-0228		x	9/25/12	4/6/05
SAN DIEGO	A Community of Friends		1390 Market Street, Ste. 405	San Francisco	CA	94102	(415) 618-0012	Bill Pickel		(415) 618-0228		x	9/25/12	4/6/05
ALAMEDA	Northern California Land Trust, Inc.	2	3122 Shattuck Avenue	Berkeley	CA	94705	(510) 548-7878	Erin Coyle	erin.covie@nct.org	(510) 548-7562		×	8/14/14	6/10/05
CONTRA COSTA	Northern California Land Trust, Inc.		3122 Shattuck Avenue	Berkeley	CA	94705	(510) 548-7878			(510) 548-7562		х	8/14/14	6/10/05
LOS ANGELES	Orange Housing Development Corporation	3	414 E. Chapman Avenue	Orange	CA	92866	(714) 288-7600	Todd Cottle		(714) 242-2092		×		6/10/05
MARIN	Northern California Land Trust, Inc.		3122 Shattuck Avenue	Berkeley	CA	94705	(510) 548-7878			(510) 548-7562		×	8/14/14	6/10/05
ORANGE	Orange Housing Development Corporation		414 E. Chapman Avenue	Orange	CA	92866	(714) 288-7600	Todd Cottle		(714) 242-2092		×		6/10/05
RIVERSIDE	Orange Housing Development Corporation		414 E. Chapman Avenue	Orange	CA	92866	(714) 288-7600x 25	Todd Cottle		(714) 242-2092		×		6/10/05
SAN BERNARDINO	Orange Housing Development Corporation		414 E. Chapman Avenue	Orange	CA	92866	(714) 288-7600	Todd Cottle		(714) 242-2092		х		6/10/05
SAN DIEGO	Orange Housing Development Corporation		414 E. Chapman Avenue	Orange	CA	92866	(714) 288-7600	Todd Cottle		(714) 242-2092		×		6/10/05
SAN FRANCISCO	Northern California Land Trust, Inc.		3122 Shattuck Avenue	Berkeley	CA	94705		10000000		(510) 548-7562		×	8/14/14	6/10/05
SAN MATEO	Northern California Land Trust, Inc.		3122 Shattuck Avenue	Berkeley	CA		(510) 548-7878			(510) 548-7562		×	8/14/14	6/10/05
ORANGE	Riverside Chartable Corporation	2	3803 E. Casselle Ave	Orange	CA	92869		Kenneth S. Robertson		(714) 628-1657			8/14/14	9/2/05
LOS ANGELES	Home and Community	3	2425 Riverside Place	Los Angeles	CA	90039		Sabrina Williams		(213) 913-5819				11/28/05
ALL COUNTIES	Wakeland Housing & Development Corporation	3	1230 Columbia St. Ste. 950	San Diego	CA	92101		Tim Wray		(619) 235-5386		×	5/24/17	12/27/05
LOS ANGELES	Hart Community Homes	3	2807 E. Lincoln Ave	Anaheim	CA	92086		William Hart		(714) 630-3714		v v	972-977	12/27/05
ORANGE	Hart Community Homes	<u> </u>	2807 E. Lincoln Ave	Anaheim	CA	92086	(714) 630-1007	William Hart		(714) 630-3714		v		12/27/05
KERN	Keller & Company	3	4309 Argos Drive	San Diego	CA	92116	(714) 000-1001	Chad Keller		(619) 795-7151		<u> </u>	6/14/17	2/8/06
KERN	Poker Flats LLC	3	1726 Webster	Los Angeles	CA	90026		Jennifer B. Luria		(323) 661-2936			6/14/17	2/8/06
LOS ANGELES	Keller & Company	_	4309 Argos Drive	_	CA	92116		Chad Keller		(619) 795-7151			0/14/1/	2/8/06
LOS ANGELES	Poker Flats LLC		1726 Webster	San Diego	CA	90026		Jennifer B. Luria		(323) 661-2936		_	+	2/8/06
ORANGE			4309 Argos Drive	Los Angeles	CA			Chad Keller		(619) 795-7151		_		2/8/06
	Keller & Company			San Diego	CA	92116				1		_		
ORANGE	Poker Flats LLC		1726 Webster	Los Angeles		90026		Jennifer B. Luria		(323) 661-2936		+		2/8/06
RIVERSIDE	Keller 8. Company		4309 Argos Drive	San Diego	CA	92116		Chad Keller		(619) 795-7151			+	2/8/06
RIVERSIDE	Poker Flats LLC		1726 Webster	Los Angeles	CA	90026		Jennifer B. Luria		(323) 661-2936		+		
SAN BERNARDINO	Keller & Company		4309 Argos Drive	San Diego	CA	92116		Chad Keller		(619) 795-7151		_	_	2/8/06
SAN BERNARDINO	Poker Flats LLC		1726 Webster	Los Angeles		90026		Jennifer B. Luria		(323) 661-2936		+		2/8/06
SAN DIEGO	Keller 8. Company	_	4309 Argos Drive	San Diego	CA	92116		Chad Keller		(619) 795-7151		+	+	2/8/06
SAN DIEGO	Poker Flats LLC	_	1726 Webster	Los Angeles	CA	90026		Jennifer B. Luria		(323) 661-2936		· ·		2/8/06
ALL COUNTIES	Hampstead Development Group, Inc.	3	3413 30th Street	San Diego	CA		(619) 543-4200	Chris Foster		(619) 543-4220		X	8/24/11	5/5/06
KERN	Housing Authority of the County of Kern	2	601 24th Street	Bakersfield	CA		(661) 631-8500	Stephen M. Pelz		(661) 631-9500		+	8/14/14	5/5/06
SANTA CRUZ	Housing Authority of the County of Santa Cruz	2	2931 Mission Street	Santa Cruz	CA		(831) 454-5901	Ken Cole			-		8/14/14	5/5/06
LOS ANGELES	Coalition for Economic Survival	3	514 Shatto Place, Suite 270	Los Angeles	CA		(213) 252-4411	Alison Dickson		(213) 252-4422	-	X	+	6/8/06
ORANGE	Coalition for Economic Survival		514 Shatto Place, Suite 270	Los Angeles	CA		(213) 252-4411	Alison Dickson		(213) 252-4422		X	+	6/8/06
RIVERSIDE	Coalition for Economic Survival		514 Shatto Place, Suite 270	Los Angeles	CA		(213) 252-4411	Allson Dickson		(213) 252-4422		X	+	6/8/06
SAN BERNARDINO	Coalition for Economic Survival		514 Shatto Place, Suite 270	Los Angeles	CA		(213) 252-4411	Alison Dickson		(213) 252-4422		X	+	6/8/06
VENTURA	Coalition for Economic Survival		514 Shatto Place, Suite 270	Los Angeles	CA		(213) 252-4411	Alison Dickson		(213) 252-4422	-	X	+	6/8/06
ALL COUNTIES	A. F. Evans Development, Inc.		4305 Univeristy Ave. Suite 550	San Diego	CA		(619) 282-6647	Anne Wilson		(619) 282-4145	-	X	1/26/12	10/11/06
ALL COUNTIES	California Coalition for Rural Housing	3	717 K Street, Suite 400	Sacramento	CA	95814	(916) 443-4448	Alicia Sebastian	alicia@calrurathousing.org	(916) 447-0458		×	5/23/17	10/11/06

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City of Ontario Policy Plan Housing Element Technical Report



County	<u>Organization</u>	code	<u>Address</u>	City	<u>sr</u>	Zip Phone Number	Contact Person	E-Mail Address	FAX Number	Type of Organization	Confirm	Date Revised	List Added
ALL COUNTIES	Chelsea Investment Corporation	3	725 South Coast Highway 101	Encinitas	CA	92024 (760) 456-6000	Jim Schmid		(760) 456-6001			5/23/17	10/11/06
ALL COUNTIES	Corporation for Better Housing	3	15303 Ventura Blvd., Suite 1100	Sherman Oaks	CA	91403 (818) 905-2430	Mary Silverstein		(818) 905-2440			5/23/17	10/11/06
ALL COUNTIES	Community HousingWorks	2	2815 Camino Del Rio South, Ste. 350	San Diego	CA	92108 (619) 858-9031	Daniel Marcus	dmarcus@chworks.org	(619) 282-4145	Local, regional, national nonprofit organization	х	6/7/18	10/11/06
IMPERIAL	Calexico Community Action Council Inc.	3	2306 M.L. King	Calexico	CA	92231 (760) 357-2995	Steve F. Rivera		(760) 357-2923			6/14/17	10/11/06
LOS ANGELES	Clifford Beers Housing, Inc.	3	1200 Wilshire Blvd. Ste. 205	Los Angeles	CA	90017	James Bonar		(213) 316-0111		х		5/3/07
ALL COUNTIES	USA Properties Fund	3	2440 Professional Drive	Roseville	CA	95661 (916) 773-5866	Geoffrey C. Brown		(916) 773-5866			5/24/17	7/12/07
CONTRA COSTA	City of Walnut Creek	2	1666 N. Main Street	Walnut Creek	CA	94596 (925) 943-5899	Laura Simpson	Simpson@walnut-creek.org	(925) 256-3500		х	2/26/14	9/11/07
ALL COUNTIES	Bank of America, N.A.	3	555 California St., 6th Floor	San Francisco	CA	94104 (415) 953-2631	Gabriel Speyer		(415) 622-1671			5/16/17	12/4/07
ALL COUNTIES	Fairfied Residential LLC	3	5510 Morehouse Drive, Suite 200	San Diego	CA	92121 (858) 824-6406	Paul Kudirka	pkudirka@ffres.com	(858) 635-8606			5/23/17	12/4/07
MONTEREY	CHISPAInc.	ą.	295 Main Street, Suite 100	Salinas	CA	93901 (831) 757-6251	Normand V. Kalpin		(831) 757-7537				5/29/08
ALL COUNTIES	Allied Pacific Development, LLC	3	169 Saxony Road, Suite 103	Encinitas	CA	92024 (760) 557-1480			(760) 557-1480		x		5/27/10
ALL COUNTIES	Belveron Real Estate Partners, LLC	3	268 Bush St., #3534	San Francisco	CA	94104 (415) 273-6801			(415) 520-5688		x		5/27/10
DEL NORTE	Humbold Bay Housing Development Corporation	2	PO Bax 4655	Arcata	CA	95518 (707) 826-7312	Bonnie Hughes	bhughes@housinghumboldt.org	(707) 826-7319		x	8/14/14	5/27/10
HUMBOLDT	Housing Humboldt)		PO Box 4655	Arcata	CA	95518 (707) 826-7312	Elizabeth Matsumoto	bmatsumoto@housinghumboldt.org	(707) 826-7319	Local, regional non-profit	х	8/14/14	5/27/10
ALL COUNTIES	Richman Group of California, LLC.	3	21520 Yorba Linda Blvd, Suite G-548	Yorba Linda	CA	92887 (714) 837-6138	Pamela Mikus	MikusP@therichmangroup.com			x		5/28/10
ALL COUNTIES	Renaissance Housing Communities	3	110 Pacific Avenue, Suite 292	San Francisco	CA	94111 (415)0419-4027	David Silver		(415) 789-448		x		8/9/10
ALAMEDA	Alameda County Allied Housing Program	3	224 W. Winton Avenue, Room 108	Hayward	CA	94541 (510) 670-5404	Linda Gardiner	linda.gardinen@ac.gov.org	(510) 670-6378	Local, regional, national nonprofit org.		5/9/17	9/27/10
ALL COUNTIES	Mesa Realty Advisors	3	56 Cbana Blanca	Henderson	NV	89012 (310) 213-5310	Rick W. Toney		(510)		×	9/25/12	9/27/10
CONTRA COSTA	Alameda County Allied Housing Program	_	224 W. Winton Avenue, Room 108	Hayward	CA	94541 (510) 670-5404	Property : Forey		(510) 670-6378	Local, regional, national nonprofit org.	x	9/27/10	9/27/10
LOS ANGELES	CSI Support & Development Services	3	201 E. Huntington Drive	Monrovia	CA	91016 (626) 599-8464	Isa Woods		(626) 599-8463	Estat, regarde, nasonal nation of g	v	0.21110	9/27/10
ORANGE	A Community of Friends	Ť	56 Chana Blanca	Henderson	NV	89012 (310) 213-5310	Rick W. Toney		(020) 333-0403		Û	9/25/12	9/27/10
ORANGE	CSI Support & Development Services		201 E. Huntington Drive	Monrovia	CA	91016 (626) 599-8464	Isa Woods		(626) 599-8463		Û	5120/12	9/27/10
RIVERSIDE	CSI Support & Development Services		201 E. Huntington Drive	Monrovia	CA	91016 (626) 599-8464	Isa Woods		(626) 599-8463		Û		9/27/10
					CA						Û		9/27/10
SAN BERNARDINO	CSI Support & Development Services	9	201 E. Huntington Drive	Monrovia		, , , , , , , , , , , , , , , , , , , ,	Isa Woods		(626) 599-8463		ů	0.0.4.0.4	
ALL COUNTIES	California Commercial Investment Group		4530 E. Thousand Oaks BMd., Ste. 10	Westlake Village	CA	91362 (805) 495-8400	To Shorten		(805) 495-5471			8/14/14	12/24/10
ALL COUNTIES	Dawson Holdings, Inc.	2	300 Turney Street, 2nd Floor	Sausalito	CA	94965 (801) 244-6658	Tim Fluetsch		(801) 733-6116			8/14/14	12/24/10
ALL COUNTIES	WNC Community Preservation Partners, LLC	3	17782 Sky Park Circle	Irvine	CA	92620 (714) 662-5565			(714) 662-4412		×		3/3/11
AMADOR	Mutual Housing California	2	8001 Fruitridge Road, Suite A	Sacramento	CA	95820 (916) 453-8400	Rachel Isladow	rache@mutuathousing.com	(916) 453-8401	Local, regional, national public agency	×	4/21/16	3/3/11
SOLANO	Mutual Housing California	2	8001 Fruitridge Road, Suite A	Sacramento	CA	95820 (916) 453-8400	Holly Wunder Stiles	hotv@mutuahousing.com			X	8/14/14	3/3/11
ALAMEDA	ROEM Development Corporation	3	1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600	Erin Caputo	ec aputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
BUTTE	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	, ,	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
CONTRA COSTA	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	, ,	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
EL DORADO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
FRESNO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
IMPERIAL	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
KERN	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
KINGS	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		X	5/24/17	3/30/11
LOS ANGELES	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
MADERA	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
MENDOCINO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
MERCED	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
MONTEREY	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
ORANGE	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
PLACER	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
RIVERSIDE	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
SACRAMENTO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
SAN BENITO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
SAN BERNARDINO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
SAN DIEGO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050 (408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11



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County	<u>Organization</u>	code	<u>Address</u>	City	<u>st</u>	Zip.	Phone Number	Contact Person	E-Mail Address	FAXNumber	Type of Organization	<u>Confirm</u>	Date Revised	List Added
SAN FRANCISCO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
SAN JOAQUIN	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
SAN LUIS OBISPO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	ÇA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
SAN MATEO	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
SANTA BARBARA	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
SANTA CLARA	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
SANTA CRUZ	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
STANISLAUS	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		x	5/24/17	3/30/11
TULARE	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		×	5/24/17	3/30/11
VENTURA	ROEM Development Corporation		1650 Lafayette Circle	Santa Clara	CA	65050	(408) 984-5600 Ext 17	Erin Caputo	ecaputo@roemcorp.com	(408) 984-3111		х	5/24/17	3/30/11
KERN	Abbey Road Inc.		15305 Rayen Street	North Hills	ÇA	91343	(818) 332-8008	Jonathon Dilworth		(818) 332-8101			6/14/17	3/28/12
LOS ANGELES	Abbey Road Inc.		15305 Rayen Street	North Hills	CA	91343	(818) 332-8008	Jonathon Dilworth		(818) 332-8101		x	6/14/17	3/28/12
NAPA	SWJ Housing	3	PO Box 815	Sebastopol	CA	95473	(707) 823-9884	Scott Johnson		(707) 634-1422		×	8/14/14	3/28/12
ORANGE	City of Newport Beach	3	100 Civic Center Drive	Newport Beach	CA	92660	(949) 644-3221	Melinda Whelan				х	B/14/14	3/28/12
ORANGE	Abbey Road Inc.		15305 Rayen Street	North Hills	CA	91343	(818) 332-8008	Jonathon Dilworth		(818) 332-8101		x	6/14/17	3/28/12
SAN BERNARDINO	Abbey Road Inc.		15305 Rayen Street	North Hills	CA	91343	(818) 332-8008	Jonathon Dilworth		(818) 332-8101		x	6/14/17	3/28/12
SOLANO	SWJ Housing	3	PO Box 815	Sebastopol	CA	95473	(707) 823-9884	Scott Johnson		(707) 634-1422		×	8/14/14	3/28/12
SONOMA	SWJ Housing	3	PO Bax 815	Sebastopol	CA	95473	(707) 823-9884	Scott Johnson		(707) 634-1422		x	8/14/14	3/28/12
VENTURA	Abbey Road Inc.		15305 Rayen Street	North Hills	CA	91343	(818) 332-8008	Jonathon Dilworth		(818) 332-8101		×	6/14/17	3/28/12
ALL COUNTIES	Preservation Partners Development	2	21515 Hawthorne Blvd. Suite 125	Torrance	CA	90503	(310) 802-6681	Chuck Treatch	Chuck@preservationpartners.org	(310) 802-6680	A California limited partnership, for-profit	x	8/14/14	1/16/13
ALL COUNTIES	Berladia	2	823 Colby Drive	Davis	CA	95616	(916) 769-7768	AIR Inouye	Al inouve@inouveapartments.com			×	8/1/16	2/26/14
ORANGE	Jamboree Housing Corporation		17701 Cowan Ave, #200	Irvine	CA	92614	(949) 214-2395	Roger Kinoshita	rkinoshita@jamboreehousing.com	(949)214-2395	Local, region, national, nonprofit org		4/22/16	4/22/16
ALL COUNTIES	American Community Developers, Inc.		20250 Harper Avenue	Detroit	MI	48225	(313) 884-0722	Derek M. Skrzynski	derek@acdmail.com	(313) 884-0722	Profit-motivated individual or organization	x	9/2/16	9/2/16
ALL COUNTIES	Highland Property Development, LLC		250 W. Colorado Bv. Suite 210	Arcadia	CA	91007	(626) 698-6357	Paul Patierno	p_patierno@highlandcompanies.com	(626) 698-6365	Profit-motivated individual or organization			9/27/16
ALL COUNTIES	GAL Affordable LP		250 W. Colorado Bv. Suite 210	Arcadia	CA	91007	(626) 698-6357	Paul Patierno		(626) 698-6365	Profit-motivated individual or organization			10/24/16
ALL COUNTIES	Eden Housing, Inc.		22645 Grand Street	Hayward	CA	94541	(510) 582-1460	Andrea Osgood	aos good@edenhousing.org	(510) 582-6523	Local, regional, national nonprofit org.	x		11/8/16
LOS ANGELES	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	x		4/6/17
ORANGE	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	x		4/6/17
RIVERSIDE	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	×		4/6/17
SAN BERNARDINO	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	x		4/6/17
VENTURA	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	x		4/6/17
SAN DIEGO	Innovative Housing Opportunities, Inc.		19772 Macarthur Bv., Ste. 110	Irvine	CA	92612	(949) 863-9740	Patricia Whitaker		(949) 863-9746	Local, regional, national nonprofit org.	x		4/7/17
ALL COUNTIES	Lincoln Avenue Capitol, LLC		680 5th Avenue, 17th Floor	New York	NY	10019	(646) 585-5524	Andrew Mika	andrew@lincolnavecap.com		Profit-motivated individual or organization	×	8/30/19	5/3/17
LOS ANGELES	Community Development Commission		700 W. Main Street	Los Angeles	CA	91801	(626) 586-1812	Larry Newnam	larry.newnam@lacdc.org	(626) 943-3815	Loca., regional, national public agency		11/6/17	8/17/17
FRESNO	Self-Help Enterprises		8445 W. Elowin Court/P.O. Box 6520	Visala	CA	93290	(559) 802-1620	Thomas J. Collishaw	tomc@sethelpenterprises.org	(559) 651-3634	Local, regional, national nonprofit org.		4/10/18	4/10/18
ALL COUNTIES	Colrich Multifamily Investments, LLC		444 West Beach St.	San Diego	CA	92101	(858) 490-2300	Danny Gabriel	dannyo@colrich.com	(858) 490-0264	Profit-motivated individual or organization			4/16/18
ALL COUNTIES	Standard Property Company, INC. (DBA Standard Commun	nities)	1901 Avenue of the Stars, Suite 395	Los Angeles	CA	90067	(310) 553-5711	Brad Martinson	bmartinson@standard-companies.com	(310) 551-1666	Profit-motivated individual or organization			6/4/18
ALL COUNTIES	Jonathon Rose Companies		551 Fifth Ave, 23rd Floor	New York	NY		(917) 542-3600	Nathan Taft	nathan@rosecompanies.com	(917) 542-3601	Profit-motivated individual or organization			6/27/18
ALL COUNTIES	JEMCOR Development Partners		1700 El Camino Real Suite #400	Sna Mateo	CA		(415) 941-5847	Michael McDermott	mmcdeormott@iemcorpartners.com		Profit-motivated individual or organization			8/8/18
ALL COUNTIES	Catalyst Housing Group, LLC		21 Ward Street, Suite 2	Larkspur	CA		(415) 205-4702	Jordan Moss	Jordan@CatalystHousing.com		Profit-motivated individual or organization			12/11/18
LOS ANGELES	Los Angeles County Development Authority		700 W. Main Street	Alhambra	CA		(626) 586-1816	KeAndra Cylear-Dodds	keandra.cyleardodds@łacda.org		Local, regional, national public agency			4/18/19
ALL COUNTIES	Veritas Urban Properties LLC		2050 Hancock Street, Suite B	San Diego	CA		(619) 746-5191	Gilman Bishop	gbishop@bishopventures.com		Profit-motivated individual or organization			4/24/19
LOS ANGELES	Santa Fe Art Colony Tenants Association		2415 S. Sante Fe Avenue, Unit 2	Los Angeles	CA		(310) 663-6665	Sylvia Tidwell	sylvia@sylviatidwell.net		Tenants' Association			5/2/19
SANTA CLARA	Silicon Valley at Home		350 West Julian Street, Building 5	San Jose	CA		(669) 254-1009	Mathew Reed	mathew@siliconvallevathome.org		Local, regional, national nonprofit org.			7/10/19
LOS ANGELES	San Gabriel Valley Habitat for Humanity, Inc.		400 S Irwindale Ave	Azusa			(626) 709-3277	Mark Van Lue	mvanlue@sgvhabitat.org		Local, regional, national nonprofit org.			8/12/2019

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City of Ontario Policy Plan Housing Element Technical Report



APPENDIX B: HOUSING ELEMENT SITES INVENTORY (PARCEL LIST)

Draft October 2021 B-1

Opp Area	Site Address/Intersection	APN	Consol-dated Sites	Current GP	Current Zone	Min Density (du/ac)	Max Density (du/ac)	Parcel Size (Acres)	Existing Use/Vacancy	Infrast Available	Publicly- Owned	Site Status	Identified in Last/Last 2 Cycle(s)	Lower	Mod	Abov Mod	Total	Notes
Downtown	315 W D ST	104857601		HDR	HDR-45	25	45	0.70	Parking lot	Yes	NO	Available	Prior/Non-vacant	9	9	-	18	
Downtown	325 N PALM AVE	104857602		HDR	HDR-45	25	45	0.47	Parking lot	Yes	NO	Available	Prior/Non-vacant	6	6	-	12	
West Holt	900 W HOLT BLVD	101050176		HDR	HDR-45	25	45	1.48	Auto sales	Yes	NO	Available	Prior/Non-vacant	18	18	-	37	
West Holt	1034 W HOLT BLVD	101050206	J	HDR	HDR-45	25	45	0.34	Auto sales	Yes	NO	Available	No	4	4	-	9	
West Holt	1020 W HOLT BLVD	101050207	J	HDR	HDR-45	25	45	0.43	Auto sales	Yes	NO	Available	Prior/Non-vacant	5	5	-	11	
West Holt	1134 W HOLT BLVD	101052206		HDR	HDR-45	25	45	0.67	Strip retail	Yes	NO	Available	Prior/Non-vacant	8	8	-	17	
West Holt	1156 W HOLT BLVD	101052217		HDR	HDR-45	25	45	0.95	Auto sales	Yes	NO	Available	Prior/Non-vacant	12	12	-	24	
West Holt	1206 W HOLT BLVD	101054302	С	HDR	HDR-45	25	45	0.47	Parking lot	Yes	NO	Available	Prior/Non-vacant	6	6	-	12	
West Holt	W OF 1206 W HOLT BLVD	101054303		HDR	HDR-45	25	45	0.25	Auto sales	Yes	NO	Available	Prior/Non-vacant	3	3	-	6	
West Holt	1240 W HOLT BLVD	101054304		HDR	HDR-45	25	45	0.87	Auto sales	Yes	NO	Available	Prior/Non-vacant	11	11	-	22	
West Holt	1258 W HOLT BLVD	101054306		HDR	HDR-45	25	45	0.56	Automotive	Yes	NO	Available	Prior/Non-vacant	7	7	-	14	
West Holt	1328 W HOLT BLVD	101054310	E	HDR	HDR-45	25	45	0.30	Auto sales	Yes	NO	Available	Not Used	4	4	-	7	
West Holt	1328 W HOLT BLVD	101054311	E	HDR	HDR-45	25	45	0.28	Auto sales	Yes	NO	Available	Not Used	3	3	-	7	
West Holt	1350 W HOLT BLVD	101054313	F	HDR	HDR-45	25	45	0.35	Building supply	Yes	NO	Available	Prior/Non-vacant	4	4	-	9	
West Holt	1360 W HOLT BLVD	101054314	F	HDR	HDR-45	25	45	0.44	Building supply	Yes	NO	Available	Prior/Non-vacant	6	6	-	11	
West Holt	W OF SWC STONERIDGE CT & MOUNTAIN AVE	101054326	С	HDR	HDR-45	25	45	0.26	Parking lot	Yes	NO	Available	Not Used	3	3	-	7	
West Holt	1203 W STONERIDGE CT	101054327	С	HDR	HDR-45	25	45	0.42	Parking lot	Yes	NO	Available	Prior/Non-vacant	5	5	-	11	
West Holt	1424 W HOLT BLVD	101055204	G	HDR	HDR-45	25	45	0.31	Automotive	Yes	NO	Available	Not Used	4	4	-	8	
West Holt	1426 W HOLT BLVD	101055205	G	HDR	HDR-45	25	45	0.31	Automotive	Yes	NO	Available	Not Used	4	4	-	8	
West Holt	1448 W HOLT BLVD	101055207	Н	HDR	HDR-45	25	45	0.31	Auto sales	Yes	NO	Available	Not Used	4	4	-	8	
West Holt	1528 HOLT BLVD	101055212	I	HDR	HDR-45	25	45	0.32	Retail	Yes	NO	Available	Not Used	4	4	-	8	
West Holt	1538 HOLT BLVD	101055213	I	HDR	HDR-45	25	45	0.32	Retail	Yes	NO	Available	Not Used	4	4	-	8	
West Holt	120 N BENSON AVE	101055216	D	HDR	HDR-45	25	45	0.68	Retail	Yes	NO	Available	Prior/Non-vacant	8	8	-	17	
West Holt	1535 W STONERIDGE CT	101055217	D	HDR	HDR-45	25	45	0.21	Retail	Yes	NO	Available	Not Used	3	3	-	5	
West Holt	1420 W HOLT BLVD	101055232	А	HDR	HDR-45	25	45	0.46	Parking lot	Yes	NO	Available	Prior/Non-vacant	6	6	-	12	
West Holt	1414 W HOLT BLVD	101055233	А	HDR	HDR-45	25	45	0.46	Building supply	Yes	NO	Available	Prior/Non-vacant	6	6	-	12	
West Holt	1414 W HOLT BLVD	101055234	А	HDR	HDR-45	25	45	0.42	Building supply	Yes	NO	Available	Prior/Non-vacant	5	5	-	11	
West Holt	1502 W HOLT BLVD	101055237	Н	HDR	HDR-45	25	45	0.39	Auto sales	Yes	NO	Available	Prior/Non-vacant	5	5	-	10	
West Holt	1512 W HOLT BLVD	101055238		HDR	HDR-45	25	45	0.56	Auto sales	Yes	NO	Available	Prior/Non-vacant	7	7	-	14	
East Holt	1323 E HOLT BLVD	11006104		MU-HE	MU-2	14	40	0.99	Vacant	Yes	NO	Available	Not Used	11	11	-	22	
East Holt	1207 E HOLT BLVD	11006110		MU-HE	MU-2	14	40	1.11	Vacant	Yes	NO	Available	Not Used	13	13	-	25	Note 1
East Holt	1217 E HOLT BLVD	11006121		MU-HE	MU-2	14	40	1.19	Hotel	Yes	NO	Available	Not Used	13	13	-	27	Note 1
East Holt	1241 E HOLT BLVD	11006125		MU-HE	MU-2	14	40	1.20	Hotel	Yes	NO	Available	Not Used	14	14	-	27	Note 1
East Holt	1111 E HOLT BLVD	104847115		MU-HE	MU-2	14	40	0.52	Automotive / Open Storage	Yes	NO	Available	Not Used	6	6	-	12	Note 1
East Holt	1101 E HOLT BLVD	104847122	В	MU-HE	MU-2	14	40	0.83	Parking Lot	Yes	NO	Available	Not Used	9	9	-	19	Note 1
East Holt	1101 E HOLT BLVD	104847123	В	MU-HE	MU-2	14	40	0.48	Vacant Church	Yes	NO	Available	Not Used	5	5	-	11	Note 1
East Holt	1031 E HOLT BLVD	104848102		MU-HE	MU-2	14	40	1.83	Automotive repair	Yes	NO	Available	Not Used	21	21	-	41	Note 1
East Holt	1015 E HOLT BLVD	104848103		MU-HE	MU-2	14	40	0.78	Auto parts store	Yes	NO	Available	Not Used	9	9	-	17	Note 2
East Holt	E OF NEC HOLT BLVD & ALLYN AVE	104848106		MU-HE	MU-2	14	40	0.63	Vacant	Yes	NO	Available	Not Used	7	7	-	14	Note 1
East Holt	935 E HOLT BLVD	104848107		MU-HE	MU-2	14	40	0.90	Independent Convenience Store	Yes	NO	Available	Not Used	10	10	-	20	Note 1

East Holt	1025 E HOLT BLVD	104848122	MU-HE	MU-2	14	40	1.90	Automotive repair	Yes	NO	Available	Not Used	21	21	-	43	Note 1
East Holt	957 E HOLT BLVD	104848128	MU-HE	MU-2	14	40	1.18	Vacant	Yes	NO	Available	Not Used	13	13	-	27	Note 1
East Holt	957 E HOLT BLVD	104848129	MU-HE	MU-2	14	40	1.18	Vacant	Yes	NO	Available	Not Used	13	13	-	26	Note 1
East Holt	803 E HOLT AVE	104851212	MU-HE	MU-2	14	40	0.61	Car Sales	Yes	NO	Available	Not Used	7	7	-	14	Note 1
East Holt	813 E HOLT BLVD	104851213	MU-HE	MU-2	14	40	0.91	Laundromat / Automotive	Yes	NO	Available	Not Used	10	10	-	21	Note 1
Ontario Ctr SP	SWC CONCOURS ST & MERCEDES LN	21020404	MU-OC	SP	see notes	see notes	8.44	Vacant	Yes	NO	Available	Not Used	63	63	-	126	Note 2
Ontario Ctr SP	888 HAVEN AVE	21020407	MU-OC	SP	see notes	see notes	4.47	Vacant	Yes	NO	Available	Not Used	33	33	-	67	Note 2
Ontario Ctr SP	NWC CONCOURS ST & DUESENBERG DR	21053115	MU-OC	SP	see notes	see notes	2.28	Vacant	Yes	NO	Available	Not Used	34	34	-	68	Note 3
Ontario Ctr SP	NEC CONCOURS ST & DUESENBERG DR	21053116	MU-OC	SP	see notes	see notes	1.36	Vacant	Yes	NO	Available	Not Used	20	20	-	41	Note 3

Notes:

Note 1: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use)- 75%

Note 2: Specific Plan does not set density standards, however the types of projects allowed by zoning can accommodate products with 60+ du/ac. The Policy Plan Category allows for densities up to 125 du/ac

Note 3: Specific Plan/ Piemonte Overlay does not set density standards, however the types of projects allowed by zoning can accommodate products with 60+ du/ac. The Policy Plan Category allows for densities up to 125 du/ac

Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation	Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Capacity	Vacant/ Nonvacant	Description of Existing Uses	Notes
Downtown	111 N MIRAMONTE AVE	104852414	1	1	1	0	0.13 1	VC	CN	MU-DT	MU-1	25	75	3	Nonvacant	Auto sales	Notes 1, 4
Downtown	617 E HOLT BLVD	104852415	1	1	1	0	0.13 1	NC	CN	MU-DT	MU-1	25	75	3	Nonvacant	Auto sales	Notes 1, 4
Downtown	609 E HOLT BLVD	104852416	2	1	3	0	0.26 1	NC	CN	MU-DT	MU-1	25	75	6	Nonvacant	Auto sales	Notes 1, 4
West Holt	828 W HOLT BLVD	101049102	4	2	3	3	0.53 1	MDR	MDR-25	HDR	HDR-45	25	45	13	Nonvacant	Auto sales	
West Holt	830 W HOLT BLVD	101049103	10	6	16	0	1.29 [MDR	MDR-25	HDR	HDR-45	25	45	32	Nonvacant	Auto sales	
West Holt	802 W HOLT BLVD	101049116	3	2	5	0	0.43 1	MDR	MDR-25	HDR	HDR-45	25	45	11	Nonvacant	Automotive	
West Holt	1050 W HOLT BLVD	101050208	8	5	13	0	1.18 (GC	СС	MU-HW	MU-2-AH	20	40	27	Nonvacant	Auto sales	Note 5
West Holt	1050 W HOLT BLVD	101050209	2	1	3	0	0.28 (GC	СС	MU-HW	MU-2-AH	20	40	6	Nonvacant	Auto sales	Note 5
West Holt	724 W HOLT BLVD	104860414	4	2	6	0	0.52 1	MDR	MDR-25	HDR	HDR-45	25	45	13	Nonvacant	Restaurant	
West Holt	740 W HOLT AVE	104860415	10	6	16	0	1.27 [MDR	MDR-25	HDR	HDR-45	25	45	32	Nonvacant	Auto sales	
East Holt	NWC HOLT BLVD & IMPERIAL AVE	11007102	15	9	23	0	2.07 E	3P	ВР	MU-HE	MU-2-AH	20	40	47	' Vacant	Vacant	Note 5
East Holt	1381 E HOLT BLVD	11007106	13	8	21	0	1.83 E	3P	ВР	MU-HE	MU-2-AH	20	40	41	. Vacant	Vacant	Note 5
East Holt	1387 E HOLT BLVD	11007107	4	2	6	0	0.55 E	3P	ВР	MU-HE	MU-2-AH	20	40	12	Vacant	Vacant	Note 5
East Holt	1405 E HOLT BLVD	11007210	10	6	17	0	1.48 E	3P	ВР	MU-HE	MU-2-AH	20	40	33	Vacant	Vacant	Note 5
Old Cardenas Market	N OF NEC MAPLE ST & EUCLID AVE	105027201	5	3	8	0	0.63	NC	CN	HDR	HDR-45	25	45	16	Vacant	Vacant	
Old Cardenas Market	1652 EUCLID AVE	105027220	5	3	8	0	0.63 1	NC	CN	HDR	HDR-45	25	45	16	Vacant	Vacant	
Old Cardenas Market	1714 S EUCLID AVE	105028401	11	6	17	0	2.23 1	NC	CN	MU-EF	MU*	20	30	33	Nonvacant	Vacant Grocery	Note 6
Ontario Mills SP	1050 N ONTARIO MILLS DR	23801402	6	4	5	5	1.28	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	20	Nonvacant	Automotive	Note 7
Ontario Mills SP	990 N ONTARIO MILLS DR	23801403	9	5	7	7	1.81	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	29	Nonvacant	Retail	Note 7
Ontario Mills SP	960 N ONTARIO MILLS DR	23801404	6	4	5	5	1.28 [MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	20	Nonvacant	Restaurant	Note 7
Ontario Mills SP	4900 E FOURTH ST	23801405	94	55	74	74	18.62	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	298	Nonvacant	Movie theater	Note 7
Ontario Mills SP	980 N ONTARIO MILLS DR	23801406	6	4	5	5	1.24	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	20	Nonvacant	Retail	Note 7
Ontario Mills SP	880 N ROCHESTER AVE	23801407	7	4	5	5	1.29	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	21	. Nonvacant	Retail	Note 7
Ontario Mills SP	4421 ONTARIO MILLS PKWY	23801410	17	10	13	13	3.33	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	53	Nonvacant	Retail	Note 7
Ontario Mills SP	950 N ONTARIO MILLS DR	23801418	7	4	5	5	1.37 [MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	22	Nonvacant	Restaurant	Note 7
Ontario Mills SP	4549 MILLS CIR	23801419	74	43	59	59	14.67	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	235	Nonvacant	Movie theater & mall parking lot	Note 7
Ontario Mills SP	4364 E MILLS CIR	23801420	4	2	3	3	0.81	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	13	Nonvacant	Retail	Note 7
Ontario Mills SP	4340 MILLS CIRCLE DR	23801423	3	2	3	3	0.65	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	10	Nonvacant	Restaurant	Note 7

City of Ontario Policy Plan Housing Element Technical Report

Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation	Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Vacant Capacity Nonvac	•	Notes
Ontario Mills SP	4453 MILLS CIRCLE CIR	23801428	4	2	3	3	0.84	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	13 Nonvacan	: Restaurant	Note 7
Ontario Mills SP	4485 MILLS CIRCLE DR	23801429	7	4	6	6	1.44	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	23 Nonvacan	Restaurant	Note 7
Ontario Mills SP	4551 EAST MILLS CIRCLE 0	23801430	10	6	8	8	1.92	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	31 Nonvacan	Restaurant	Note 7
Ontario Mills SP	4567 MILLS CIR	23801431	3	2	2	2	0.60	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	10 Nonvacan	. Automotive	Note 7
Ontario Mills SP	4583 MILLS CIR	23801432	4	2	3	3	0.79	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	13 Nonvacan	. Automotive	Note 7
Ontario Mills SP	4605 MILLS CIR	23801433	4	2	3	3	0.74	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	12 Nonvacan	: Restaurant	Note 7
Ontario Mills SP	4649 ONTARIO MILLS DR	23801434	5	3	4	4	0.90	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	14 Nonvacan	t Restaurant	Note 7
Ontario Mills SP	1 E MILLS CIR	23801436	486	285	385	385	96.34	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	1541 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	4534 MILLS CIR	23801437	6	4	5	5	1.21	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	19 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	4449 MILLS CIR	23801438	7	4	5	5	1.37	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	22 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	1 MILLS CIR	23801439	6	3	5	5	1.15	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	18 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	4380 MILLS CIR	23801440	9	5	7	7	1.73	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	28 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	4380 MILLS CIR	23801441	2	1	1	1	0.31	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	5 Nonvacan	Ontario Mills Mal & parking lot	ll Note 7
Ontario Mills SP	4423 EAST MILLS CIRCLE 0	23801445	7	4	6	6	1.38	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	22 Nonvacan	Restaurant	Note 7
Ontario Mills SP	4320 MILLS CIR	23801446	15	9	12	12	2.94	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	47 Nonvacan	: Retail	Note 7
Ontario Mills SP	4403 E MILLS CIR	23801454	12	7	10	10	2.45	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	39 Nonvacan	t Restaurant	Note 7
Ontario Mills SP	4413 E MILLS CIR	23801455	12	7	10	10	2.40	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	38 Nonvacan	: Restaurant	Note 7
Ontario Mills SP	4351 ONTARIO MILLS PKWY	23804113	5	3	4	4	1.01	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	16 Nonvacan	: Restaurant	Note 7
Ontario Mills SP	4371 ONTARIO MILLS PKWY	23804127	5	3	4	4	1.03	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	16 Nonvacan	: Restaurant	Note 7

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Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation	l Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Capacity	Vacant/ Nonvacant	Description of Existing Uses	Notes
Ontario Mills SP	4400 ONTARIO MILLS PKWY	23804129	13	8	10	10	2.57	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	41	Nonvacant	Retail	Note 7
Ontario Mills SP	4440 ONTARIO MILLS PKWY	23804130	78	46	62	62	15.38	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	246	Nonvacant	Retail	Note 7
Ontario Mills SP	ONTARIO MILLS PKWY & INLAND EMPIRE BLVD (T-INTERSECTION)	23804132	23	13	18	18	4.53	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	72	Nonvacant	Retail	Note 7
Ontario Mills SP	4750 MILLS CIR	23826101	24	14	19	19	4.75	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	76	Nonvacant	Ontario Mills Mall & parking lot	Note 7
Ontario Mills SP	4449 MILLS CIR	23826102	7	4	6	6	1.42	MU-OM	SP	MU-OM	SP - City to ammend California Commerce Center North Specific Plan (The Mills)	25	85	23	Nonvacant	Ontario Mills Mall & parking lot	Note 7
Great Park Corridor	8270 EDISON AVE	21631109	50	29	40	40	7.33	MDR	SP-AG	MDR	SP-AG-AH	20	30	159	Nonvacant	Agriculture	Note 2
Great Park Corridor	NWC EDISON AVE & WALKER AVE	21631208	49	29	39	39	7.08	MDR	SP-AG	MDR	SP-AG-AH	20	30	156	Nonvacant	Agriculture	Note 2
Great Park Corridor	8314 EDISON AVE	21631209	23	13	19	19	3.45	MDR	SP-AG	MDR	SP-AG-AH	20	30	74	Nonvacant	Agriculture	Note 2
Great Park Corridor	8354 EDISON AVE	21631210	26	15	21	21	3.77	MDR	SP-AG	MDR	SP-AG-AH	20	30	83	Nonvacant	Agriculture	Note 2
Great Park Corridor	NEC EDISON AVE & WALKER AVE	21631301	48	28	38	38	6.96	MDR	SP-AG	MDR	SP-AG-AH	20	30	153	Vacant	Vacant	Note 2
Great Park Corridor	NWC EDISON AVE & BAKER AVE	21631309	48	28	38	38	6.94	MDR	SP-AG	MDR	SP-AG-AH	20	30	153	Nonvacant	Agriculture	Note 2
Great Park Corridor	NEC EDISON AVE & BAKER AVE	21631401	47	28	37	37	6.82	MDR	SP-AG	MDR	SP-AG-AH	20	30	150	Nonvacant	Agriculture	Note 2
Great Park Corridor	N OF NWC EDISON AVE & VINEYARD AVE	21631408	7	4	6	6	1.06	MDR	SP-AG	MDR	SP-AG-AH	20	30	23	Nonvacant	Agriculture	Note 2
Great Park Corridor	NWC EDISON AVE & VINEYARD AVE	21631409	63	37	50	50	9.04	MDR	SP-AG	MDR	SP-AG-AH	20	30	199	Nonvacant	Agriculture	
Great Park Corridor	8311 EDISON AVE	21632108	106	62	85	85	15.46	MDR	SP-AG	MDR	SP-AG-AH	20	30	338	Nonvacant	Agriculture	Note 2
Great Park Corridor	S OF SEC EDISON AVE & PARCO AVE	21632203	20	12	16	16	2.87	MDR	SP-AG	MDR	SP-AG-AH	20	30	63	Nonvacant	Agriculture	Note 2
Great Park Corridor	8335 EDISON AVE	21632204	62	37	50	50	9.08	MDR	SP-AG	MDR	SP-AG-AH	20	30	199	Nonvacant	Agriculture	
Great Park Corridor	S OF SEC EDISON AVE & WALKER AVE	21632303	21	12	17	17	3.03	MDR	SP-AG	MDR	SP-AG-AH	20	30	67	Nonvacant	Agriculture	Note 2
Great Park Corridor	8535 EDISON 0	21632304	61	36	49	49	8.87	MDR	SP-AG	MDR	SP-AG-AH	20	30	195	Nonvacant	Agriculture	
Great Park Corridor	NEC EDISON AVE & VINEYARD AVE	21818101	63	37	50	50	9.03	MDR	SP-AG	MDR	SP-AG-AH	20	30	199	Nonvacant	Agriculture	
Great Park Corridor	N OF NEC EDISON AVE & VINEYARD AVE	21818102	28	16	22	22	4.05	MDR	SP-AG	MDR	SP-AG-AH	20	30	89	Nonvacant	Agriculture	Note 2
Great Park Corridor	9064 EDISON AVE	21818111	37	22	29	29	5.31	MDR	SP-AG	MDR	SP-AG-AH	20	30	117	Nonvacant	Agriculture	Note 2
Great Park Corridor	9060 EDISON AVE	21818112	50	29	40	40	7.29	MDR	SP-AG	MDR	SP-AG-AH	20	30	159	Nonvacant	Agriculture	
Great Park Corridor	NWC EDISON AVE & BON VIEW AVE	105330101	54	32	43	43	7.86	MDR	SP-AG	MDR	SP-AG-AH	20	30	173	Nonvacant	Agriculture	Note 2
Great Park Corridor	NWC EDISON AVE & CAMPUS AVE	105330102	56	33	44	44	8.05	MDR	SP-AG	MDR	SP-AG-AH	20	30	177	Nonvacant	Agriculture	Note 2
Great Park Corridor	NWC EDISON AVE & CUCAMONGA AVE	105331101	54	32	43	43	7.79	MDR	SP-AG	MDR	SP-AG-AH	20	30	171	Nonvacant	Agriculture	Note 2
Great Park Corridor	13905 BON VIEW AVE	105331102	54	32	43	43	7.78	MDR	SP-AG	MDR	SP-AG-AH	20	30	171	Nonvacant	Agriculture	Note 2
Great Park Corridor	7914 EDISON AVE	105332102	30	18	24	24	4.36	MDR	SP-AG	MDR	SP-AG-AH	20	30	96	Nonvacant	Agriculture	
Great Park Corridor	N OF NEC EDISON AVE & CUCAMONGA AVE	105332103	23	14	18	18	3.34	MDR	SP-AG	MDR	SP-AG-AH	20	30	74	Nonvacant	Agriculture	Note 2
Great Park Corridor	SEC EDISON AVE & CUCAMONGA AVE	105333101	53	31	42	42	7.63	MDR	SP-AG	MDR	SP-AG-AH	20	30	168	Nonvacant	Agriculture	
Great Park Corridor	E OF NEC EDISON AVE & CUCAMONGA AVE	105333103	8	5	6	6	1.14	MDR	SP-AG	MDR	SP-AG-AH	20	30	25	Nonvacant	Agriculture	
Great Park Corridor	S OF SEC EDISON AVE & CUCAMONGA AVE	105334101	23	14	18	18	3.35	MDR	SP-AG	MDR	SP-AG-AH	20	30	74	Nonvacant	Agriculture	Note 2
Great Park Corridor	14213 BON VIEW AVE	105335101	19	11	16	16	2.97	MDR	SP-AG	MDR	SP-AG-AH	20	30	63	Nonvacant	Agriculture	Note 2
Great Park Corridor	S OF SWC EDISON AVE & CUCAMONGA AVE	105335102	21	12			3.07	MDR	SP-AG	MDR	SP-AG-AH	20	30	67	Nonvacant	Agriculture	Note 2
Great Park Corridor	7721 EDISON AVE	105336101	62	36	50	50	9.02	MDR	SP-AG	MDR	SP-AG-AH	20	30	197	Nonvacant	Agriculture	

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Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation		Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Capacity	Vacant/ Nonvacant	Description of Existing Uses	Notes
Great Park Corridor	SWC EDISON AVE & CUCAMONGA AVE	105336102	62	36	49	49	8.95 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	197	Nonvacant	Agriculture	
Great Park Corridor	7587 EDISON AVE	105337101	162	95	130	130	23.60 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	517	Nonvacant	Agriculture	Note 2
Great Park Corridor	N OF NEC EUCALYPTUS AVE & CAMPUS AVE	105353101	21	12	17	17	3.02 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	66	Nonvacant	Agriculture	Note 2
Great Park Corridor	14366 BON VIEW AVE	105353104	20	12	16	16	2.89 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	64	Nonvacant	Agriculture	Note 2
Great Park Corridor	14450 BON VIEW AVE	105354101	38	22	30	30	5.52 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	121	Nonvacant	Agriculture	Note 2
Great Park Corridor	14451 BON VIEW AVE	105357101	62	37	50	50	9.10 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	199	Nonvacant	Agriculture	
Great Park Corridor	NEC EUCALYPTUS AVE & BON VIEW AVE	105357102	63	37	50	50	9.10 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	200	Nonvacant	Agriculture	
Great Park Corridor	7556 EUCALYPTUS AVE	105358103	63	37	50	50	9.14 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	201	Nonvacant	Agriculture	
Great Park Corridor	14330 BON VIEW 0	105358104	62	36	50	50	9.03 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	198	Nonvacant	Agriculture	
Great Park Corridor	7330 EDISON AVE	105320101	150	88	138	138	23.54 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	515	Nonvacant	Agriculture	Note 2
Great Park Corridor	14350 WALKER AVE	21632210a	81	48	65	65	11.80 N	/IDR	SP-AG	MDR	SP-AG-AH		20	30	259	Nonvacant	Agriculture	Note 3
Great Park Corridor	14474 GROVE AVE	21632101	38	22	31	31	8.78 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	122	Nonvacant	Agriculture	Note 7
Great Park Corridor	14361 GROVE AVE	21632102	32	19	26	26	7.47 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	104	Nonvacant	Agriculture	Notes 2, 7
Great Park Corridor	N OF NWC EUCALYPTUS AVE & PARCO AVE	21632106	25	15	20	20	5.71 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	80	Nonvacant	Agriculture	Notes 2, 7
Great Park Corridor	NWC EUCALYPTUS AVE & PARCO AVE	21632107	40	24	32	32	9.14 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	128	Nonvacant	Agriculture	Note 7
Great Park Corridor	NEC EUCALYPTUS AVE & PARCO AVE	21632201	40	24	32	32	9.08 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	127	Nonvacant	Agriculture	Notes 2, 7
Great Park Corridor	N OF NEC EUCALYPTUS AVE & PARCO AVE	21632202	13	8	11	11	3.06 N	/IDR	SP-AG	MU-BC	SP-AG-AH		25	45	43	Nonvacant	Agriculture	Notes 2, 7
Great Park Corridor	14400 S GROVE AVE	105355101	118	69	93	93	26.67 N	//DR	SP-AG	MU-BC	SP-AG-AH		25	45	373	Nonvacant	Agriculture	Notes 2, 7
Great Park Corridor	14350 WALKER AVE	21632210b	52	31	41	41	11.80 N	//DR	SP-AG	MU-BC	SP-AG-AH		25	45	165	Nonvacant	Agriculture	Notes 3, 7
Great Park Corridor	E OF NEC EDISON AVE & GROVE AVE	21631101	29	17	23	23	3.69 G	iC	SP-AG	MU-GP	SP-AG-AH		20	65	90	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	14049 GROVE AVE	21631102	29	17	23	23	3.72 G	ic	SP-AG	MU-GP	SP-AG-AH		20	65	91	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	8185 EDISON AVE	21632103	120	70	96	96	15.67 G	ic	SP-AG	MU-GP	SP-AG-AH		20	65	382	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	7244 EDISON AVE	105328101	65	38	51	51	8.37 N	/IDR	SP-AG	MU-GP	SP-AG-AH		20	65	205	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	7218 EDISON AVE	105328102	16	9	13	13	2.06 C	C	SP-AG	MU-GP	SP-AG-AH		20	65	50	Nonvacant	Open storage	Note 8
Great Park Corridor	NWC EDISON AVE & EUCLID AVE	105328103	21	12	17	17	2.73 C	С	SP-AG	MU-GP	SP-AG-AH		20	65	67	Nonvacant	Vacant w/ concrete pad & fence	Note 8
Great Park Corridor	N OF NWC EDISON AVE & EUCLID AVE	105328108	40	24	32	32	5.23 C)C	SP-AG	MU-GP	SP-AG-AH		20	65	128	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	14050 GROVE AVE	105332101	55	32	44	44	7.13 G	iC	SP-AG	MU-GP	SP-AG-AH		20	65	175	Nonvacant	Open storage	Notes 2, 8
Great Park Corridor	8061 EDISON AVE	105333104	65	38	52	52	8.48 G	iC	SP-AG	MU-GP	SP-AG-AH		20	65	208	Nonvacant	Agriculture	Note 8
Great Park Corridor	S OF SWC EDISON AVE & GROVE AVE	105334102	43	25	34	34	5.62 G	iC	SP-AG	MU-GP	SP-AG-AH		20	65	138	Nonvacant	Agriculture	Notes 2, 8
Great Park Corridor	7325 EDISON AVE	105339101	281	165	223	223	36.45 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	892	Nonvacant	Agriculture	Note 8
Great Park Corridor	7225 EDISON AVE	105341101	142	83	112	112	18.35 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	450	Nonvacant	Agriculture	Note 8
Great Park Corridor	14251 EUCLID AVE	105342101	50	29	39	39	6.45 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	158	Nonvacant	Agriculture	Note 8
Great Park Corridor	14211 S EUCLID 0	105342102	42	25	33	33	5.42 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	133	Nonvacant	Open storage	Note 8
Great Park Corridor	14187 EUCLID AVE	105342104	44	26	35	35	5.71 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	140	Nonvacant	Agriculture	Note 8
Great Park Corridor	S OF SEC EDISON AVE & EUCLID AVE	105342107	5	3	4	4	0.61 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	15	Nonvacant	Open storage	Note 8
Great Park Corridor	14107 EUCLID AVE	105342108	9	5	7	7	1.18 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	29	Nonvacant	rruck rentar / oper	n Note 8
Great Park Corridor	7145 EDISON AVE	105342109	27	16	22	22	3.54 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	87	Nonvacant	Open storage	Note 8
Great Park Corridor	14393 S EUCLID AVE	105351101	46	27	36	36	5.92 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	145	Nonvacant	Truck wash	Note 8
Great Park Corridor	N OF NEC EUCALYPTUS AVE & EDISON AVE	105351105	45	26	36	36	5.84 N	ЛU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	143	Nonvacant	Agriculture	Note 8

Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation		Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Capacity	Vacant/ Nonvacant	Description of Existing Uses	Notes
Great Park Corridor	N OF NWC EUCALYPTUS AVE & SULTANA AVE	105351106	73	43	58	58	9.49	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	233	Nonvacant	Agriculture	Note 8
Great Park Corridor	14389 SULTANA AVE	105352101	73	43	58	58	9.49	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	232	Nonvacant	Agriculture	Note 8
Great Park Corridor	N OF NWC EUCALYPTUS AVE & CAMPUS AVE	105352102	72	42	57	57	9.36	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	229	Nonvacant	Agriculture	Note 8
Great Park Corridor	7511 EUCALYPTUS AVE	105359101	69	41	56	56	9.17	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	223	Nonvacant	Agriculture	Note 8
Great Park Corridor	7388 EUCALYPTUS AVE	105359102	69	40	55	55	9.01	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	220	Nonvacant	Agriculture	Note 8
Great Park Corridor	7280 EUCALYPTUS 0	105360101	70	41	55	55	9.02	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	221	Nonvacant	Agriculture	Note 8
Great Park Corridor	NEC EUCALYPTUS AVE & EDISON AVE	105360102	43	25	34	34	5.58	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	137	Nonvacant	Agriculture	Note 8
Great Park Corridor	14437 EUCLID AVE	105360103	22	13	17	17	2.82	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	69	Nonvacant	Agriculture	Note 8
Great Park Corridor	14411 EUCLID AVE	105360104	26	15	20	20	3.33	MU-GP	SP-AG	MU-GP	SP-AG-AH		20	65	82	Nonvacant	Truck wash	Note 8
Grove Corridor	N OF NEC CHINO AVE & GROVE AVE	21617102	16	10	0	79	4.76	LMDR	SP-AG	MDR	SP-AG-AH		20	30	105	Nonvacant	Agriculture	
Grove Corridor	S OF SEC RIVERSIDE DR & GROVE AVE	21617103	33	19	0	157	9.51	MDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Grove Corridor	SWC RIVERSIDE DR & PARCO AVE	21617105	32	19	0	151	9.18	MDR	SP-AG	MDR	SP-AG-AH		20	30	202	Nonvacant	Agriculture	
Grove Corridor	0 COMET AVE	21617106	33	20	0	159	9.62	MDR	SP-AG	MDR	SP-AG-AH		20	30	212	Nonvacant	Agriculture	
Grove Corridor	N OF NEC CHINO AVE & GROVE AVE	21617111	16	10	0	79	4.76	LMDR	SP-AG	MDR	SP-AG-AH		20	30	105	Nonvacant	Agriculture	
Grove Corridor	13605 GROVE AVE	21621101	30	18	0	147	8.92	LMDR	SP-AG	MDR	SP-AG-AH		20	30	195	Nonvacant	Open storage	
Grove Corridor	13403 GROVE AVE	21621104	33	19	0	157	9.50	LMDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Grove Corridor	N OF NEC EDISON AVE & GROVE AVE	21631103	48	28	0	114	8.64	LMDR	SP-AG	MDR	SP-AG-AH		20	30	190	Nonvacant	Agriculture	Note 2
Grove Corridor	13817 GROVE AVE	21631104	53	31	0	126	9.52	LMDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Grove Corridor	13715 GROVE AVE	21631105	50	29	0	120	9.06	LMDR	SP-AG	MDR	SP-AG-AH		20	30	199	Nonvacant	Agriculture	
Grove Corridor	N OF NWC CHINO AVE & GROVE AVE	105217102	33	19	0	157	9.51	LMDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Grove Corridor	S OF SEC CHINO AVE & GROVE AVE	105247102	33	19	0	157	9.51	LMDR	SP-AG	MDR	SP-AG-AH		20	30	209	Vacant	Vacant	
Grove Corridor	13524 S GROVE AVE	105248102	24	14	0	114	6.94	LMDR	SP-AG	MDR	SP-AG-AH		20	30	153	Nonvacant	Agriculture & oper	n Note 2
Grove Corridor	13608 GROVE AVE	105249105	15	9	0	73	4.43	NC	SP-AG	MDR	SP-AG-AH		20	30	96	Nonvacant	Open storage	Note 3
Grove Corridor	8010 SCHAEFER AVE	105249106	14	8	0	70	4.25	NC	SP-AG	MDR	SP-AG-AH		20	30	92	Nonvacant	Open storage	Note 4
Grove Corridor	8025 E SCHAEFER AVE	105315104	26	15	0	61	4.63	LMDR	SP-AG	MDR	SP-AG-AH		20	30	102	Nonvacant	Open storage	
Grove Corridor	8087 E SCHAEFER AVE	105315105	22	13	0	52	3.92	LMDR	SP-AG	MDR	SP-AG-AH		20	30	86	Nonvacant	Open storage	
Grove Corridor	13814 GROVE AVE	105316102	53	31	0	125	9.50	LMDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Building supply	
Grove Corridor	13908 GROVE AVE	105317103	24	14	0	56	4.28	LMDR	SP-AG	MDR	SP-AG-AH		20	30	94	Nonvacant	Open storage	
Grove Corridor	13960 GROVE AVE	105317105	20	12	0	48	3.61	LMDR	SP-AG	MDR	SP-AG-AH		20	30	79	Nonvacant	Agriculture	Note 2
Grove Corridor	13519 S GROVE AVE	21621124	23	14	0	112	6.77	LMDR	SP-AG	MDR	SP-AG-AH		20	30	149	Nonvacant	Open storage	Note 2
Grove Corridor	8146 CHINO AVE	21617101	32	19	0	155	9.09	LMDR	SP-AG	MU-GR	SP-AG-AH		20	65	206	Nonvacant	Agriculture	Note 9
Grove Corridor	8113 CHINO AVE	21621105	32	19	0	153	8.96	LMDR	SP-AG	MU-GR	SP-AG-AH		20	65	204	Nonvacant	Agriculture	Note 9
Grove Corridor	8074 CHINO AVE	105218101	31	18	0	153	8.94	LMDR	SP-AG	MU-GR	SP-AG-AH		20	65	202	Nonvacant	Agriculture	Note 9
Grove Corridor	8089 CHINO AVE	105246103	-1	0	0	205	8.99	LMDR	SP-AG	MU-GR	SP-AG-AH		20	65	204	Nonvacant	Agriculture	Note 9
Euclid Corridor	7247 RIVERSIDE DR	105207105	5	3	0	31	1.90	MDR	SP-AG	MDR	SP-AG-AH		20	30	40	Nonvacant	Agriculture	
Euclid Corridor	SEC RIVERSIDE DR & EUCLIDE AVE	105207107	62	36	0	295	14.98	GC	SP-AG	MU-ER	SP-AG-AH		20	75	393	Nonvacant	Agriculture	Note 10
Euclid Corridor	E OF SEC RIVERSIDE DR & EUCLIDE AVE	105207108	25	15	0	121	7.32	MDR	SP-AG	MDR	SP-AG-AH		20	30	161	Nonvacant	Agriculture	
Euclid Corridor	E OF SEC RIVERSIDE DR & EUCLIDE AVE	105207109	16	9	0	76	4.61	MDR	SP-AG	MDR	SP-AG-AH		20	30	101	Nonvacant	Agriculture	
Euclid Corridor	7297 RIVERSIDE DR	105207110	7	4	0	38	2.30	MDR	SP-AG	MDR	SP-AG-AH		20	30	49	Nonvacant	Agriculture	
Euclid Corridor	S OF SWC RIVERSIDE DR & SULTANA AVE	105208104	33	19	0	157	9.50	MDR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Euclid Corridor	7325 RIVERSIDE DR	105210101	27	16	0	145	8.81	MDR	SP-AG	MDR	SP-AG-AH		20	30	189	Nonvacant	Parking, vacant field w/ unused ag bldg	3

Opportunity Area	Site Address/Intersection	APN	Very Low- Income	Low- Income	Moderate- Income	Above Moderate- Income	Parcel Size (Acres)	Current General Plan	Current Zoning	Proposed General Plan (GP) Designation		Proposed Zoning	Minimum Density Allowed	Maximum Density Allowed	Total Capacity	Vacant/ Nonvacant	Description of Existing Uses	Notes
Euclid Corridor	7192 CHINO AVE	105226101	18	10	0	84	5.07 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	112	Nonvacant	Agriculture	
Euclid Corridor	N OF NEC CHINO AVE & EUCLID AVE	105226102	20	12	0	96	5.84 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	128	Nonvacant	Agriculture	
Euclid Corridor	13165 EUCLID AVE	105226106	40	24	0	192	11.65 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	256	Nonvacant	Agriculture	
Euclid Corridor	13647 EUCLID AVE	105238101	54	32	0	258	15.65 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	344	Nonvacant	Agriculture	
Euclid Corridor	N OF NEC SCHAEFER AVE & EUCLID AVE	105238102	3	2	0	16	0.97 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	20	Nonvacant	Agriculture	
Euclid Corridor	13583 EUCLID AVE	105238104	14	8	0	66	3.98 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	87	Vacant	Vacant	
Euclid Corridor	13573 EUCLID AVE	105238105	2	1	0	11	0.64 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	14	Nonvacant	Agriculture	
Euclid Corridor	13555 EUCLID AVE	105238106	2	1	0	12	0.71 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	15	Nonvacant	Agriculture	
Euclid Corridor	13545 EUCLID AVE	105238108	3	2	0	13	0.81 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	18	Nonvacant	Building supply	
Euclid Corridor	N OF NWC SCHAEFER AVE & SULTANA AVE	105238111	22	13	0	105	6.34 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	140	Vacant	Vacant	
Euclid Corridor	N OF NWC SCHAEFER AVE & SULTANA AVE	105238112	10	6	0	47	2.82 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	62	Nonvacant	Agriculture	
Euclid Corridor	13525 S EUCLID AVE	105238113	5	3	0	22	1.35 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	30	Nonvacant	Veterinarian	
Euclid Corridor	13529 EUCLID AVE	105238116	3	2	0	15	0.90 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	20	Nonvacant	Auto rentals	
Euclid Corridor	7220 CHINO AVE	105238110	51	30	0	244	14.76 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	325	Nonvacant	Agriculture	Note 2
Euclid Corridor	7110 CHINO AVE	105238114	38	23	0	186	11.27 N	1DR	SP-AG	MDR	SP-AG-AH		20	30	247	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	S OF SWC RIVERSIDE DR & VINEYARD AVE	21617406	33	19	0	157	9.50 LI	DR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	N OF NWC CHINO AVE & VINEYARD AVE	21617407	33	19	0	157	9.51 LI	DR	SP-AG	MDR	SP-AG-AH		20	30	209	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	NWC CHINO AVE & VINEYARD AVE	21617408	32	19	0	151	9.15 LI	DR	SP-AG	MDR	SP-AG-AH		20	30	201	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	NEC CHINO AVE & VINEYARD AVE	21810101	31	18	0	149	9.05 LI	DR	SP	MDR	SP- City to a Specific Pla	ammend Armstrong Ranch in	20	30	199	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	N OF NEC CHINO AVE & VINEYARD AVE	21810102	31	18	0	149	9.05 LI	DR	SP	MDR	SP- City to a Specific Pla	ammend Armstrong Ranch In	20	30	199	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	S OF SEC RIVERSIDE DR & VINEYARD AVE	21810103	31	18	0	149	9.05 LI	DR	SP	MDR	SP- City to a Specific Pla	ammend Armstrong Ranch in	20	30	199	Nonvacant	Agriculture	
Vineyard / Armstrong Ranch Specific Plan	SEC RIVERSIDE DR & VINEYARD AVE	21810104	31	18	0	149	9.05 LI	DR	SP	MDR	SP- City to Specific Pla	ammend Armstrong Ranch In	20	30	199	Nonvacant	Agriculture	

Notes:

Note 1: Lot Consolidation Proposed: Site A

Note 2: This parcel has multiple land use designations. Only the portion of the parcel that allows residential uses were included in the acreage in this table and used to calculate capacity.

Note 3: This parcel has been split in the inventory to reflect multiple proposed land uses. Only the portions of the parcel that allow residential uses were included in the acreage in this table and used to calculate capacity.

Note 4: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 60%

Note 5: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 75%

Note 6: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 50%

Note 7: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 40%

Note 8: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 70%

Note 9: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 65%

Note 10: Percent of acreage estimated to accommodate housing (reduction factors for mixed-use): 75%







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RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVE FILE NO. PZC-21-002, A ZONE CHANGE TO MODIFY THE ZONING MAP TO ESTABLISH AN AH (AFFORDABLE HOUSING) ZONING OVERLAY DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF.

WHEREAS, the City of Ontario ("Applicant") has filed an Application for the approval of a Zone Change, File No. PZC-21-002, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application proposes to establish an Affordable Housing Overlay District to facilitate housing opportunities within the City through the implementation of required rezone programs pursuant to the City's adopted Housing Element where required for compliance with State Housing Element law; and

WHEREAS, the Affordable Housing Overlay District applies to sites identified in the housing inventory, part of the City's Housing Element, located (1) south of Riverside Drive, (2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and (3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue; and

WHEREAS, a related General Plan Amendment (File No. PGPA21-004) is being processed concurrently with this application for the Housing Element update to the Policy Plan (General Plan) component of The Ontario Plan addressing State mandates and the 6th Cycle Regional Housing Needs Allocation ("RHNA") and this application also proposes to modify the Policy Plan Land Use Plan (Exhibit LU-01) to establish an Affordable Housing Overlay; and

WHEREAS, a related Development Code Amendment (File No. PDCA21-001) is being processed concurrently with this application to establish density standards applicable to the Affordable Housing Overlay District; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies

and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to City Council on the subject Application; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission recommended City Council approval of a resolution adopting an Addendum to a previous Environmental Impact Report prepared pursuant to CEQA, the State CEQA Guidelines and the City of Ontario Local CEQA Guidelines, which indicated that all potential environmental impacts from the Project were less than significant or could be mitigated to a level of less than significance; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: **Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report — State Clearinghouse

No. 2008101140 ("Certified EIR"), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and

- (2) The Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (5) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- (6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Certified EIR, are incorporated herein by this reference.
- <u>SECTION 2</u>: **Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:
- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: **Housing Element Compliance.** Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) The proposed Zone Change is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan as follows:

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
- ➤ <u>LU1-6: Complete Community</u>. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

Compliance: The proposed Zone Change reflects the existing uses of the properties or closely coordinates with land use designations in the surrounding area and provides opportunities for choice in living environments.

- Goal LU2: Compatibility between a wide range of uses.
- LU2-1: Land Use Decisions. We minimize adverse impacts on adjacent properties when considering land use and zoning requests.

Compliance: The proposed Zone Change reflects the existing uses of the properties or closely coordinates with land use designations in the surrounding area and will not create adverse impacts on adjacent properties.

- <u>Goal LU5</u>: Integrated airport systems and facilities that minimize negative impacts to the community and maximize economic benefits.
- LU5-7: ALUCP Consistency with Land Use Regulations. We comply with state law that requires general plans, specific plans and all new development be consistent with the policies and criteria set forth within an Airport Land Use Compatibility Plan for any public use airport.

Compliance: The proposed Zone Change is consistent with the adopted Airport Land Use Compatibility Plan for both Ontario International Airport and Chino Airport.

Housing Element:

- <u>Goal H2</u>: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
- ➤ <u>H2-1: Corridor Housing</u>. We revitalize transportation corridors by encouraging the production of higher density residential and mixed-uses that are architecturally, functionally and aesthetically suited to corridors.

Compliance: The proposed Zone Change supports higher density residential and mixed-use development that is functionally and aesthetically suited along transportation corridors.

➤ <u>H2-4: New Model Colony</u>. We support a premier lifestyle community in the New Model Colony distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

Compliance: The proposed Zone Change is consistent with the policy to support a diverse housing type throughout Ontario.

➤ <u>H2-6: Infill Development</u>. We support the revitalization of neighborhoods through the construction of higher-density residential developments on underutilized residential and commercial sites.

Compliance: The proposed Zone Change supports the revitalization of neighborhoods through the infill development of higher-density residential and mixed-use developments on underutilized properties.

- Goal H3: A City regulatory environment that balances the need for creativity and excellence in residential design, flexibility and predictability in the project approval process, and the provision of an adequate supply and prices of housing.
- ➤ H3-2: Flexible Standards. We allow flexibility in the application of residential and mixed-use development standards in order to gain benefits such as exceptional design quality, economic advantages, sustainability, or other benefits that would otherwise be unrealized.

Compliance: The proposed Zone Change supports flexibility in the application of development standards to achieve a variety of residential development on higher density residential and mixed-use properties.

- <u>Goal H5</u>: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.
- ➤ <u>H5-2: Family Housing</u>. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Compliance: The proposed Zone Change supports the development of larger rental apartments on higher density residential and mixed-use properties to meet the housing needs of all individuals and families in Ontario.

- (2) The proposed Zone Change would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City. The proposed zoning designation is compatible with the zoning and land uses in the surrounding areas.
- (3) The proposed Zone Change will not adversely affect the harmonious relationship with adjacent properties and land uses. The allowed uses of the properties will be similar to other properties in the areas.
- (4) The subject site is physically suitable, including, but not limited to, parcel sizes, shapes, access, and availability of utilities, for the request and anticipated development. The sites identified for the AH Overlay District are of adequate size to accommodate a variety of residential development and are physically suitable for the anticipated future development.
- SECTION 6: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Application, as detailed in "Attachment A: Affordable Housing Overlay" and "Attachment B: Affordable Housing Overlay and Current Zoning" attached hereto and incorporated herein by this reference.
- <u>SECTION 7</u>: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 8</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

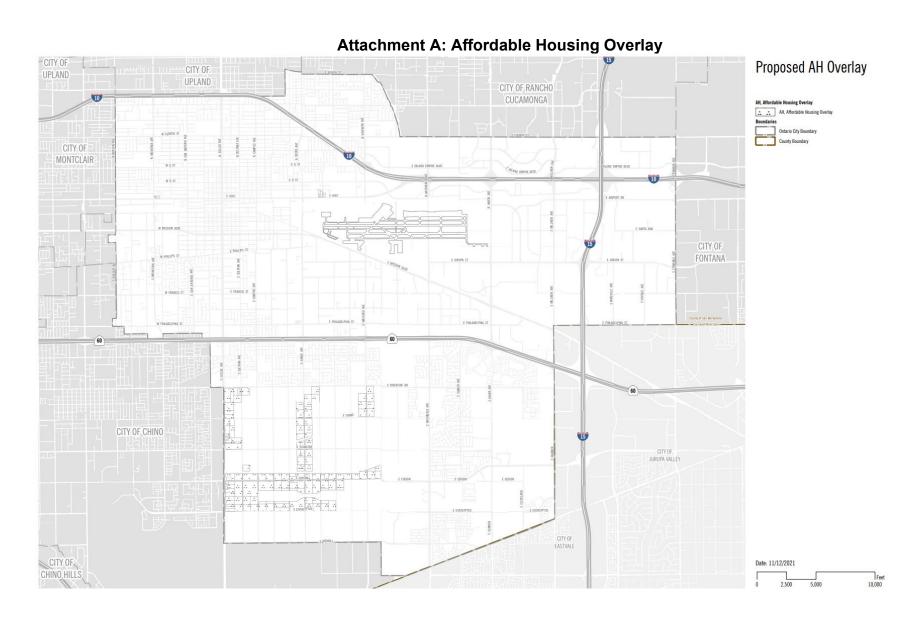
I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Richard Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

mpore of the Planning Commission of the pregoing Resolution No., was duly passed the City of Ontario at their special meeting roll call vote, to wit:
Curan Parandaan
Gwen Berendsen Secretary Pro Tempore



Attachment B: Affordable Housing Overlay and Current Zoning -CITY OF Proposed AH Overlay and CITY OF UPLAND **Current Zoning** UPLAND CITY OF RANCHO CUCAMONGA Zoning By Parcel RE-2, Rural Estate RE-4, Residential Estate CITY OF LDR-5, Low Density Residential MONTCLAIR MDR-25, Medium-High Density Residentia HDR-45, High Density Residential MHP, Mobile Home Park Mixed Use / Special Areas MU-1, Downtown Mixed-Use CITY OF MU-2, East Holt Mixed-Use MU-11, Euclid/Francis Mixed-Use FONTANA CS. Corner Store CN. Nerighborhood Commercial CC, Community Commercial CR, Regional Commercial CCS, Conv. Ctr. Support Commercial OL, Low Intensity Office OH, High Intensity Office BP, Business Park IP Industrial Park IL, Light Industrial IG, General Industrial IH, Heavy Industrial CIV, Civic CITY OF CHINO OS-R, Open Space-Recreation OS-C, Open Space-Cemetery JURUPA VALLEY UC, Utilities Corridor SP, Specific Plan RC, Rail Corridor AH, Affordable Housing Overlay AH, Affordable Housing Overla Ontario City Boundary County Boundary CITY OF EASTVALE CITY OF Date: 11/12/2021 CHINO, HILLS Feet 10,000

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVES FILE NO. PDCA21-001, A DEVELOPMENT CODE AMENDMENT REVISING SECTION 6.01.035 FOR THE PURPOSE OF ADDING PROVISIONS TO ESTABLISH AN AH (AFFORDABLE HOUSING) OVERLAY DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF.

WHEREAS, the City of Ontario ("Applicant") has initiated a Development Code Amendment, File No. PDCA21-001, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application is of Citywide impact, affecting approximately 50 square miles (31,789 acres) of land, which is generally bordered by Benson Avenue and Euclid Avenue on the west; Interstate 10 Freeway, Eighth Street, and Fourth Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south; and

WHEREAS, certain minor changes are proposed to the City of Ontario Development Code, as follows:

- Section 6.01.035 (Overlay Zoning Districts) is to be amended, adding Subparagraph C.6 to establish an Affordable Housing Overlay District,
- The purpose of the Affordable Housing Overlay District is to facilitate housing opportunities within the City through the implementation of required rezone programs pursuant to the City's adopted Housing Element where required for compliance with State Housing Element law,
- The Affordable Housing Overlay District applies to sites identified in the housing inventory, part of the City's Housing Element, located 1) south of Riverside Drive, 2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and 3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue; and

WHEREAS, the Development Code (Ontario Municipal Code Title 9) provides the legislative framework for the implementation of The Ontario Plan, which states long-term principles, goals, and policies for guiding the growth and development of the City in a manner that achieves Ontario's vision, and promotes and protects the public health, safety, comfort, convenience, prosperity, and welfare of its citizens; and

WHEREAS, a related General Plan Amendment (File No. PGPA21-004) is being processed concurrently with this application for the Housing Element update to the Policy

Plan (General Plan) component of The Ontario Plan addressing State mandates and the 6th Cycle Regional Housing Needs Allocation (RHNA) and this application also proposes to modify the Policy Plan Land Use Plan (Exhibit LU-01) to establish an Affordable Housing Overlay; and

WHEREAS, a related Zone Change (File No. PZC-21-002) is being processed concurrently with this application to establish an Affordable Housing Overlay District; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA"); and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) assigns the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on December 20, 2021, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, as the first action on the Project, on December 20, 2021, the Planning Commission recommended approval of a Resolution recommending City Council adopt an Addendum to the Environmental Impact Report (State Clearinghouse No. 2008101140) adopted by City Council on January 27, 2010 for File No. PGPA06-001.

The Addendum finds that the proposed project introduces no new significant environmental impacts, and all previously adopted mitigation measures are incorporated into the Project by reference; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

<u>SECTION 1</u>: **Environmental Determination and Findings.** As the recommending body for the Project, the Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

- (1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report State Clearinghouse No. 2008101140 ("Certified EIR"), which was certified by the Ontario City Council on January 27, 2010, in conjunction with File No. PGPA06-001; and
- (2) The Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and
- (3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and
- (4) The Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and
- (5) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts; and
- (6) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the Certified EIR, and all mitigation measures previously adopted by the Certified EIR, are incorporated herein by this reference.

<u>SECTION 2</u>: **Additional Environmental Review Not Required.** Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental environmental impact report is not required for the Project, as the Project:

- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
- (a) The project will have one or more significant effects not discussed in the Certified EIR; or
- (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
- (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
- (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.
- <u>SECTION 3</u>: *Housing Element Compliance.* The Project will be consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project furthers the purposes, principals, goals, and policies of the Housing Element, in that it will expand upon the types of housing that may be constructed throughout residential and mixed use zoning districts of the City and will allow for alternate forms of home rental and fee-simple homeownership.
- <u>SECTION 4</u>: *Ontario International Airport Land Use Compatibility Plan* ("ALUCP") Compliance. The California State Aeronautics Act (Public Utilities Code

Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport ("ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

<u>SECTION 5</u>: **Concluding Facts and Reasons.** Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 through 4, above, the Planning Commission hereby concludes as follows:

- (1) The proposed Development Code Amendment is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The Development Code Amendment proposes:
- (a) The identification of specific sites to be included in the AH Overlay District that are included in the housing inventory, which is part of the City's Housing Element, and located (1) south of Riverside Drive, (2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and (3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue that are suitable for affordable housing development projects; and
- (b) Defining that affordable housing projects for the purposes of the AH Overlay provides that a certain identified percentage of the total units within each development are designated as affordable to lower income households and guaranteed for a minimum of 30 years; and

- (c) Establishing that development standards applicable to proposed development in the AH Overlay District shall be in accordance with the applicable base zone, other overlay zones, and all applicable provisions of the Development Code; and
- (d) Establishing that all new residential development, within the AH Overlay District, regardless of affordability, requires a minimum density of 20 dwelling units per acre; and
- (e) Establishing that affordable housing projects on properties designated as MDR in the Policy Plan shall allow a maximum density of 25 to 30 dwelling units per acre.
- (2) The proposed Development Code Amendment would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City. The Development Code Amendment incorporates safeguards intended to ensure that the purposes of the Development Code are preserved; the project will not be contrary to or damage the public health, safety, convenience, or general welfare; the project will not result in any significant environmental impacts; and the project will be in full conformity with the Vision, City Council Priorities, and Policy Plan components of The Ontario Plan.
- <u>SECTION 6</u>: **Planning Commission Action.** Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVES the herein described Development Code Amendment, included as "Attachment A" attached hereto and incorporated herein by this reference.
- <u>SECTION 7</u>: **Custodian of Records.** The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

<u>SECTION 8</u>: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a special meeting thereof held on the 20th day of December 2021, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Rick Gage Planning Commission Chairman

ATTEST:

Rudy Zeledon
Planning Director and
Secretary to the Planning Commission

Planning Commission Resolution File No. PDCA21-001 December 20, 2021 Page 8	
STATE OF CALIFORNIA COUNTY OF SAN BERNARDINO CITY OF ONTARIO)))
City of Ontario, DO HEREBY CERTI	Pro Tempore of the Planning Commission of the IFY that foregoing Resolution No, was duly Commission of the City of Ontario at their special by the following roll call vote, to wit:
AYES:	
NOES:	
ABSENT:	
ABSTAIN:	
	Gwen Berendsen Secretary Pro Tempore

ATTACHMENT A: Development Code Amendment File No. PDCA21-001

Development Code Section 6.01.035 (Overlay Zoning Districts) is hereby amended, adding Subparagraph C.6 to read as follows:

"6. AH (Affordable Housing) Overlay District.

- **a.** *Purpose.* The purpose of the AH Overlay District is to facilitate housing opportunities within the community via implementation of required rezone programs pursuant to the City's adopted Housing Element where required for compliance with State Housing Element law.
- **b.** Applicability. The herein established rights and responsibilities applicable to the AH Overlay District shall apply to sites identified in the housing inventory, part of the City's Housing Element, located 1) south of Riverside Drive, 2) along East Holt Boulevard between the Cucamonga Creek Channel and Corona Avenue, and 3) areas designated as MU-2 at the northeast corner of West Holt Boulevard and Mountain Avenue.

c. Definitions.

- (1) Affordable Housing Project. Affordable housing projects for the purposes of the affordable housing overlay are defined as follows:
- (a) Tier 1. Any projects with a residential component with at least 20% of total units are designated as affordable to lower incomes via deed restriction or another mechanism to guarantee affordability for a minimum of 30 years.
- **(b)** *Tier 2.* Any projects with a residential component with at least 25% of total units are designated as affordable to lower incomes via deed restriction or another mechanism to guarantee affordability for a minimum of 30 years.
- **d.** Development Standards. The development standards applicable to proposed development in the Affordable Housing Overlay District shall be in accordance with the applicable base zone, other overlay zones, and all applicable provisions of the Development Code, unless otherwise indicated herein, or where necessary to comply with Federal and State law.
- (1) Minimum Density. Within the AH Overlay District, all new residential development, regardless of affordability, requires a minimum density of 20

dwelling units per acre. The minimum density shall be calculated as an average over the entire project area on sites south of Riverside Drive.

- (2) Affordable Housing Projects. Development of affordable housing projects, as defined for the purposes of the affordable housing overlay, shall be allowed by-right within the AH Overlay District and are encouraged in accordance with the following provisions:
- (a) Specific Plan Base Zoning. Affordable housing projects with a base zoning designation of SP may develop as follows:
 - (i) Tier 1 Affordable housing projects:
- Medium Density Residential. Affordable housing projects on properties designated as MDR in the Policy Plan may be developed in accordance with either the SP zoning district or the MDR-25 zoning district.
- Mixed Use. Affordable housing projects on properties designated as MU in the Policy Plan may be developed in accordance with either the SP zoning district or a comparable existing zoning implementation tool that aligns with the density and intensity of the proposed project. The following exceptions would apply:
- o Maximum Density. The maximum density allowed is equivalent to the maximum density identified for the applicable Mixed-Use area in the Land Use Designations Summary Table (Table LU-02) of the Policy Plan component of The Ontario Plan.
- Calculating Minimum Density. If a project with a base designation of SP develops without a specific plan, minimum density shall be calculated as an average over the entire portion of the project area that is within the AH Overlay District.
 - (ii) Tier 2 Affordable housing projects:
 - Same as Tier 1, with the following

additions:

Medium Density Residential.
 Affordable housing projects on properties designated as MDR in the Policy Plan shall allow a maximum density of 25 to 30 dwelling units per acre."



CITY OF ONTARIO MEMORANDUM

TO:

Planning Commission

FROM:

Rudy Zeledon, Planning Director

DATE:

December 20, 2021

SUBJECT: Monthly Activity Reports

The Monthly Activity Reports were not available at the time of agenda packet distribution. These Reports will be made available at the January 25, 2022 Planning Commission meeting.