



## **CITY OF ONTARIO DEVELOPMENT ADVISORY BOARD**

### **AGENDA**

**May 15, 2023**

- ▶ **All documents for public review are on file in the Planning Department located in City Hall at 303 East “B” St., Ontario, CA 91764 and on the city’s website at [ontarioca.gov/Agendas/DAB](http://ontarioca.gov/Agendas/DAB)**

**MEETING WILL BE HELD AT 1:30 PM IN ONTARIO CITY COUNCIL CHAMBERS  
LOCATED AT 303 East “B” St.**

Scott Ochoa, City Manager  
Scott Murphy, Executive Director, Community Development Agency  
Jennifer McLain Hiramoto, Economic Development Director  
James Caro, Building Official  
Rudy Zeledon, Planning Director  
Khoi Do, City Engineer  
Chief Michael Lorenz, Police Department  
Fire Marshal Paul Ehrman, Fire Department  
Scott Burton, Utilities General Manager  
Angela Magana, Community Improvement Manager

#### **PUBLIC COMMENTS**

*Citizens wishing to address the Development Advisory Board on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.*

*Please note that while the Development Advisory Board values your comments, the members cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.*

#### **AGENDA ITEMS**

*For each of the items listed below the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. The Development Advisory Board may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.*

## **CONSENT CALENDAR ITEMS**

### **A. MINUTES APPROVAL**

Development Advisory Board Minutes of May 1, 2023, approved as written.

## **PUBLIC HEARING ITEMS**

- B. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV21-044:** A public hearing to consider a Development Plan to construct a commercial restaurant building (Panera) with a drive-thru facility totaling 4,290 square feet on 1.26 acres of land located at the southwest corner of Fourth Street and Ontario Mills Drive within Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan. (APN: 238-014-05) **submitted by Panera LLC.**

#### **1. CEQA Determination**

No action necessary – Exempt: CEQA Guidelines Section § 15332

#### **2. File No. PDEV21-044 (Development Plan)**

Motion to Approve / Deny

- C. ENVIRONMENTAL ASSESSMENT AND TENTATIVE PARCEL MAP REVIEW FOR FILE NO. PMTT22-020:** A public hearing to consider Tentative Parcel Map No. 20587, subdividing 0.423 acres of land into two lots located at 121 East 6<sup>th</sup> Street within the LDR-5 (Low-Density - 2.1 to 5.0 du/ac) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15, Minor Land Divisions) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1011-521-30) **submitted by Samson Mulugeta. Planning Commission action is required.**

#### **1. CEQA Determination**

No action necessary – Exempt: CEQA Guidelines Section § 15315

#### **2. File No. PMTT22-020 (TPM 20587) (Tentative Parcel Map)**

Motion to recommend Approval/Denial

**D. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV22-038:** A public hearing to consider a Development Plan to construct 36 single-family 4-pack dwellings units with related on-site improvements for a subdivision previously approved with Tentative Tract Map No. 18916 (File No. PMTT21-011), on approximately 11.05 acres of land located on the west side of Archibald Avenue and approximately 575 feet south of Chino Avenue within Planning Area 2 (Neighborhood 7) of the Countryside Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with the Countryside Specific Plan (File No. PSP04-001) Environmental Impact Report (State Clearinghouse No. 2004071001) certified by the City Council on April 18, 2006. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 0218-131-34); **submitted by KB Homes Coastal Inc. Planning Commission action is required.**

**1. CEQA Determination**

No action necessary – use of previous EIR

**2. File No. PDEV22-038 (Development Plan)**

Motion to recommend Approval/Denial

If you wish to appeal a decision of the **Development Advisory Board**, you must do so within ten (10) days of the **Development Advisory Board** action. Please contact the **Planning Department** for information regarding the appeal process.

If you challenge any action of the **Development Advisory Board** in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the **Development Advisory Board** at, or prior to, the public hearing.

The next **Development Advisory Board** meets on **June 5, 2023**.

I, Gwen Berendsen, Administrative Assistant of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on or before **May 11, 2023**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East “B” Street, Ontario.



Administrative Assistant

**CITY OF ONTARIO**

**Development Advisory Board**

**Minutes**

**May 1, 2023**

**BOARD MEMBERS PRESENT**

Rudy Zeledon, Chairman, Planning Department  
James Caro, Building Department  
Charity Hernandez, Economic Development Agency  
Khoi Do, Engineering Department  
Michelle Starkey, Fire Department  
Christy Stevens, Municipal Utilities Company  
Heather Lugo, Police Department

**BOARD MEMBERS ABSENT**

Elda Zavala, Community Improvement

**STAFF MEMBERS PRESENT**

Gwen Berendsen, Planning Department  
Kim Ruddins, Planning Department  
Edmelynne Hutter, Planning Department  
Jocelyn Torres, Planning Department  
Raymond Lee, Engineering Department  
Michael Bhatanawin, Engineering Department

**PUBLIC COMMENTS**

No person from the public wished to speak.

**CONSENT CALENDAR ITEMS**

- A. **APPROVAL OF MINUTES:** Motion to approve the minutes of the April 17, 2023 meeting of the Development Advisory Board was made by Mr. Do; seconded by Ms. Stevens; and approved unanimously by those present (6-0). Mr. Caro recused himself as he was not at this meeting.

**PUBLIC HEARING ITEMS**

- B. **ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT22-005 AND PDEV22-008:** A public hearing to consider Parcel Map No. 20517 (File No. PMTT22-005) to subdivide 80 acres of land into six parcels to facilitate a Development Plan (File No. PDEV22-008) to construct six industrial buildings totaling 1,559,204 square feet. The Project site is bordered by Eucalyptus, Avenue on the north, Campus Avenue on the east, Merrill Avenue on the south, and Sultana Avenue on the west, and is located within the BP (Business Park) and IG (Industrial General) land use

districts of the Ontario Ranch Business Park Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with The Ontario Ranch Business Park Specific Plan Amendment (File No. PSPA21-002), for which a Final Subsequent Environmental Impact Report (State Clearinghouse No. 2019050018) was certified by the City Council on October 18, 2022. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan. The project site is also located within the Airport Influence area of Chino Airport and was evaluated and found to be consistent with the policies and criteria of the Chino Airport Land Use Compatibility Plan; (APNs: 1054-041-01, 1054-041-02, 1054-031-01, 1054-031-02, 1054-261-01, 1054-261-02, 1054-291-01, 1054-291-02) **submitted by Euclid Land Ventures, LLC. Planning Commission action is required for File No. PMTT22-005.**

Mr. Zeledon opened the public hearing.

Jeff Johnston, was present and thanked staff, and stated he agreed to all the Conditions of Approval.

As there was no one else wishing to speak on this item, Mr. Zeledon closed the public hearing.

Motion to recommend approval of **File No. PMTT22-002**, was made by Mr. Do; seconded by Mr. Caro; and approved unanimously by those present (7-0).

Motion to approve **File No. PDEV22-007**, was made by Mr. Do; seconded by Ms. Stevens; and approved unanimously by those present (7-0).

**C. ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP REVIEW FOR FILE NO. PMTT22-021:** A public hearing to consider Tentative Tract Map No. 20536, subdividing 23.2 acres of land for condominium purposes, into 141 numbered lots and 27 lettered lots to facilitate the development of 265 dwellings, located approximately 875 feet south of the intersection of Riverside Drive and Archibald Avenue, within the Planning Area 1 Neighborhood 2 of the Countryside Specific Plan. An Addendum to the Countryside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004071001), which was certified by the City Council on April 18, 2006, was prepared to be consistent with The Ontario Plan 2050 and associated Supplemental Environmental Impact Report (SEIR) (State Clearinghouse No. 2021070364), which was certified by the City Council on August 16, 2022. This application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0218-111-60, 0218-111-61) **submitted by RB Ontario LLC. Planning Commission action is required.**

Mr. Zeledon opened the public hearing.

Jeff Ragland, was present and thanked staff.

Mr. Zeledon asked if he had reviewed the updated Engineering and OMUC Conditions of Approval.

Mr. Ragland stated he agreed with all the revised COA's.

As there was no one else wishing to speak on this item, Mr. Zeledon closed the public hearing.

Motion to recommend approval of **File No. PMTT22-021**, was made by Mr. Do; seconded by Ms. Starkey; and approved unanimously by those present (7-0).

- D. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV22-022**: A public hearing to consider a Development Plan to construct a monopine wireless telecommunications facility (AT&T) and a 660 square foot ground-mounted equipment enclosure on 4.46 acres of land, located at 648 West D Street (James R. Bryant Park), within the OS-R (Open Space-Recreation) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15303 (Class 3, New Construction or Conversion of Small Structures) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 1048-331-13 and 1048-331-14) **submitted by New Cingular Wireless PCS, LLC dba AT&T Mobility. Planning Commission action is required.**

Mr. Zeledon opened the public hearing.

Eli Acevedo, representing AT&T Mobility, was present.

Mr. Zeledon asked if he had reviewed the Conditions of Approval.

Mr. Acevedo stated his team had reviewed them and had no questions regarding the COA's.

As there was no one else wishing to speak on this item, Mr. Zeledon closed the public hearing.

Motion to recommend approval of **File No. PDEV22-022**, was made by Ms. Lugo; seconded by Mr. Caro; and approved unanimously by those present (7-0).

There being no further business, the meeting was adjourned to the next meeting on May 15, 2023.

Respectfully submitted,



Gwen Berendsen  
Recording Secretary



# DEVELOPMENT ADVISORY BOARD DECISION

May 15, 2023

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

**DECISION NO.:** [insert #]

**FILE NO.:** PDEV21-044

**DESCRIPTION:** A hearing to consider a Development Plan to construct a 4,290 drive-thru restaurant (Panera) and 500-square-foot outdoor patio on 1.26 acres of land located at the southwest corner of Fourth Street and Ontario Mills Drive within Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan. (APN: 238-014-05); **submitted by Panera LLC.**

## PART 1: BACKGROUND & ANALYSIS

PANERA LLC, (herein after referred to as "Applicant") has filed an application requesting approval of a Development Plan, File No. PDEV21-044, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

**PROJECT SETTING:** The Project site is comprised of 1.26 acres of land located at southwest corner of Fourth Street and Ontario Mills Drive, which is depicted in Exhibit A: Project Location Map, attached. The proposed Panera drive-thru building will be located on the northeast corner of an existing parking lot for Regal Edwards Ontario Palace. The Regal Edwards Ontario Palace property is 17.2 acres in size and was constructed in 1997 with a 124,600-square foot building and parking lot. The properties to the south, east, and west are part of the Ontario Mills Mall and the properties to the north are located within the City of Rancho Cucamonga and are developed with a Costco Wholesale and furniture store. Existing land uses, Policy Plan (general plan) and zoning designations, and specific plan land designations on and surrounding the project site are as follows:

	<b>Existing Land Use</b>	<b>Policy Plan Land Use Designation</b>	<b>Zoning Designation</b>	<b>Specific Plan Land Use Designation</b>
Site:	Regal Edwards Ontario Palace parking lot	MU (Mixed Use)	Ontario Mills Specific Plan	Commercial / Office
North:	City of Rancho Cucamonga Costco and Furniture Store	21 <sup>st</sup> Century Employment District	ME2 Mixed Employment 2 Zone	
South:	Regal Edwards Ontario Palace	MU (Mixed Use)	Ontario Mills Specific Plan	Commercial / Office
East:	Multi-Tenant Retail Buildings	MU (Mixed Use)	Ontario Mills Specific Plan	Commercial / Office

	<b>Existing Land Use</b>	<b>Policy Plan Land Use Designation</b>	<b>Zoning Designation</b>	<b>Specific Plan Land Use Designation</b>
West:	Gas Station and Ontario Mills Mall	MU (Mixed Use)	Ontario Mills Specific Plan	Commercial / Office and Regional Commercial

(1) Background — On July 23, 2019, the Planning Commission approved Tentative Parcel Map No. 20087 (File No. PMTT18-010) to subdivide 17.92 acres (Regal Edwards Ontario Palace property) of land into two parcels located at the southwest corner of Fourth Street and Ontario Mills Drive. The approval required Covenants, Conditions and Restrictions (CC&Rs) to be recorded with the final map to address common maintenance, reciprocal access, and reciprocal parking between the two parcels. The approved Tentative Parcel Map also included a Shared Parking Analysis that evaluated the loss of 30 off-street parking spaces to accommodate a future commercial use and analyzed the parking demand for the existing movie theater (Parcel 1) and any future commercial use (Parcel 2). The Shared Parking Analysis concluded that the demand for both uses during peak hours would be 1,420 off-street parking spaces. Since a total of 1,593 off-street parking spaces would be provided for both parcels there would be no parking availability issues for any future commercial land use.

On November 19, 2021, the Applicant submitted a Development Plan application (File No. PDEV21-044) to construct a 4,290 drive-thru restaurant (Panera) and 500-square-foot outdoor patio on 1.26 acre project site.

(2) Site Design/Building Layout —The Project occupies Parcel 2 of the previously approved Tentative Parcel Map No. 20087 and will be built-out to a FAR of 0.08. The fast-food restaurant building is centered on the property (Parcel 2), oriented in a north-south configuration, with the drive-thru window facing east towards Ontario Mills Drive. The proposed Project will occupy a portion of four rows of existing parking and will not encroach into the principal drive aisles of the movie theatre parking lot, located north and east of the proposed building (see Exhibit B — Site Plan, attached).

The building is setback approximately 88 feet from the north property line (Fourth Street), approximately 83 feet from the east (Ontario Mills Drive) property line, approximately 120 feet from the west property line, and approximately 64 feet from the south property line. Parking for employees and customers has been provided to the north, south, east, and west of the building. The entrance to the drive-thru is located at the southeast corner of the Project site and will circulate in a west/east direction turning north and terminating along the eastern building elevation. The Project provides drive-thru lane stacking for 12 vehicles (see Exhibit B — Site Plan, attached) and 59 parking stalls.

The Project includes off-site parking improvements to the west and south of Parcel 2, which includes re-aligning existing drive aisles, new landscape planters, striping and new asphalt. The Project will also require the removal of 127 parking spaces within Parcel 2 to accommodate the proposed restaurant and the construction of a new trash enclosure designed to meet current city standards.



The building's main entrance is located on the northwest corner of the building, with doors facing west, towards the parking lot and a second set of doors facing north towards Fourth street. Walkways will be located along the west, north and south portions of the building, providing pedestrian connections to the main entrance. The drive-thru order menu boards are located to the south of the building and the pick-up window is centrally located on the east elevation (see Exhibit B — Site Plan, attached). The floor plan has been designed with the dining area occupying the northern half of the building and the kitchen/employee/bathrooms occupying the southern half of the building (see Exhibit C — Floor Plan, attached). An outdoor patio (500 square feet) has also been provided on the north side of the building, facing Fourth Street.

(3) Site Access/Circulation — Regal Edwards Ontario Palace is presently served by a total of nine access points, two access points from Fourth Street, two access points from Ontario Mills Drive, four access points from Mills Circle and one access point from Franklin Avenue. The proposed Project will not alter the location of any vehicular access points or internal major drive aisles of the parking lot. The Project site can be served by all vehicular entries, however primary access would likely be taken from Fourth Street and Ontario Mills Drive entries which lead to internal shared drive aisles. The approval of Tentative Parcel Map No. 20087 required CC&Rs to be recorded with the final map to address common maintenance, reciprocal access, and reciprocal parking between the two parcels to ensure safe and legal access and circulation across property lines, to each business.

(4) Parking — The Project has provided off-street parking pursuant to the Fast-Food Restaurants parking standards specified in the Ontario Development Code. The number of off-street parking spaces provided exceeds the minimum parking requirement for the Project. The off-street parking calculations for the Project are summarized in the table below:

<b>Parking Table Summary</b>				
<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
<i>Fast Food Restaurants</i>	4,290 SF 288 LF of drive-thru lane	13.3 spaces per 1,000 SF of GFA. Restaurants with drive-thru may be credited one space for each 24 lineal feet of drive-thru lane behind the pickup window (12 drive-thru spaces credit)	57	
<b>TOTAL</b>			<b>45</b>	<b>59</b>

As stated in the Background portion of this report, the approved Tentative Parcel Map included a Shared Parking Analysis. The Shared Parking Analysis evaluated the loss of 30 off-street parking spaces for future commercial use, however the proposed Project will result in the loss of 68 off-street parking spaces. Although, the Project results in the loss of an additional 38 off-street parking spaces the Shared Parking Analysis concluded that there would be a surplus of 173 off-street parking spaces during peak times. The proposed Project would result in a surplus of 135 off-street parking spaces that would still provide

sufficient parking during peak times for both uses. Staff believes that the proposed use will not adversely affect the overall parking demand within the existing center and sufficient parking will continue to be provided to support the proposed use and existing tenants.

(5) Architecture — The proposed building has a contemporary architectural design. The exterior walls are treated with a combination of brick veneer (stack bond design), stucco, aluminum wood, and a contrasting color palette that includes green, light brown, and beige tones. The northwest corner of the building incorporates a tower element with metal canopies that project over the two main entrances. The building incorporates metal canopies over windows, varying parapet heights, and the drive-thru features a covered canopy design to match the rest of the building (see Exhibit D — Elevations, attached).

The mechanical equipment will be roof-mounted and obscured from public view by parapet walls and, if necessary, equipment screens, which will incorporate design features consistent with the building's architecture. Staff believes that the proposed project illustrates the type of high-quality architecture promoted by the Ontario Development Code. This is exemplified through the use of:

- Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas;
- Articulation in the building parapet/roof line, which serves to accentuate the building's entries and breaks up large expanses of building wall;
- A mix of exterior materials, finishes and fixtures; and
- Additionally, the building has been designed to ensure that its massing and proportion, along with its colors and architectural detailing, are consistent on all building walls, giving a four-sided (360-degree) appearance.

(6) Landscaping — The Project provides landscaping improvements that extend beyond the Project site, includes additional landscape planter areas to the west and south to provide safe transition between the building, drive-thru lanes, existing parking stalls and existing drive aisles. The California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan requires a minimum of 15 percent landscaping, and the Project site is proposing 25 percent landscape coverage, which exceeds the minimum requirement (see Exhibit E — Landscape Plan). The Project site improvements includes new landscape planters within the parking lot, replacing missing landscaping along Fourth Street and Ontario Mills Drive and the installation of new landscape planters surrounding the proposed building and trash enclosure. The proposed plant palette includes:

- Eleven Western Redbud Trees (15-gallon) planted north, south and east of the building;

- Three Chitalpa Trees (48-inch box standard) planted south of the building within parking planters;
- Twelve Holly Oak Trees (36-inch and 24-inch box standard) planted to the east and south of the building within the parking planters; and
- A combination of drought tolerant shrubs, groundcover and grasses throughout the Project site.

(7) Signage — All project signage is required to comply with sign regulations provided in Ontario Development Code Division 8.1. Prior to the issuance of a Building Permit for the installation of any new on-site signage, the Applicant is required to submit Sign Plans for Planning Department review and approval.

(8) Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes the use of underground chambers located along the southern portion of the site within the drive aisles. Any overflow drainage will be conveyed to the public street by way of parkway drains and culverts.

**PUBLIC NOTIFICATION:** The subject application was advertised as a hearing in at least one newspaper of general circulation in the City of Ontario (the Inland Valley Daily Bulletin newspaper).

**CORRESPONDENCE:** As of the preparation of this Decision, Planning Department staff has not received any written or verbal communications from the owners of properties surrounding the project site or from the public in general, regarding the subject application.

**AGENCY/DEPARTMENT REVIEWS:** Each City agency/department has been provided the opportunity to review and comment on the subject application and recommend conditions of approval to be imposed upon the application. At the time of the Decision preparation, recommended conditions of approval were provided and are included with this Decision.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan.

On April 19, 2011, the City Council of the City of Ontario approved and adopted the ONT ALUCP, establishing the Airport Influence Area for Ontario International Airport, which

encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making body for the Project, the Development Advisory Board has reviewed and considered the facts and information contained in the Application and supporting documentation against the ONT ALUCP compatibility factors, including [1] Safety Criteria (ONT ALUCP Table 2-2) and Safety Zones (ONT ALUCP Map 2-2), [2] Noise Criteria (ONT ALUCP Table 2-3) and Noise Impact Zones (ONT ALUCP Map 2-3), [3] Airspace protection Zones (ONT ALUCP Map 2-4), and [4] Overflight Notification Zones (ONT ALUCP Map 2-5). As a result, the Development Advisory Board, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ONT ALUCP.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (general plan), and City Council Priorities components of The Ontario Plan ("TOP"). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G 1-2. Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

**Land Use Element:**

➤ LU-1.6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

▪ Goal LU-2 Compatibility: Compatibility between a wide range of uses and a resultant urban patterns and forms.

➤ LU-2.6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.

**Community Economics Element:**

▪ Goal CE-1 Complete Community: A complete community that provides for all incomes and stages of life.

▪ Goal CE-2 Placemaking: A City of distinctive neighborhoods, districts, corridors, and centers where people choose to be.

➤ CE-2.1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE-2.2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional, and sustainable places that will compete well with their competition within the region.

➤ CE-2.4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE-2.5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

▪ Goal S-1 Seismic & Geologic Hazards: Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S-1.1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

▪ Goal CD-1 Image & Identity: A dynamic, progressive city containing distinct and complete places that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD-1.1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing, enhancing, and preserving the character of our existing viable neighborhoods.

➤ CD-1.2 Place Types. We establish Place Types in urban, mixed use, and transit-oriented areas to foster the City's identity as a premier community and require new development within each Place Type to incorporate prescribed urban patterns, forms, and placemaking priorities.

➤ CD-1.3 Existing Neighborhoods. We require the existing character of viable residential and non-residential neighborhoods be preserved, protected, and enhanced.

▪ Goal CD-2 Design Quality: A high level of design quality resulting in neighborhoods, public spaces, parks, and streetscapes that are attractive, safe, functional, human-scale, and distinct.

➤ CD-2.1 Quality Building Design and Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide context-appropriate scale and proportion;
- A true architectural style which is carried out in plan, section, and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are articulated, high quality, durable, and appropriate for the architectural style.

➤ CD-2.7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping, and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials, and construction techniques.

➤ CD-2.8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding

physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

➤ CD-2.9 Landscape Design. We encourage durable, sustainable, and drought-tolerant landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD-2.10 Parking Areas. We require all development, including single-family residential, to minimize the visual impact of surface, structured, and garage parking areas visible from the public realm in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include:

- Surface parking: Shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field;
- Structured parking: facade articulation, screening, appropriate lighting, and landscaping; and
- Garage parking: providing access to single-family residential garages through alley access, recessing garages from the frontage to emphasize front doors or active living spaces.

➤ CD-2.11 Entry Statements. We encourage the inclusion of amenities, signage, and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD-2.12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD-2.13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

➤ CD-3.3 Complete and Connected Network. We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks, commercial areas, and transit stops) and are designed to maximize safety, comfort, and aesthetics.

➤ CD-3.4 Context-Aware and Appropriate Design. We require appropriate building and site design that complements existing development, respects the intent and identity of the Place Type, and provides appropriate transitions and connections

between adjacent uses to ensure compatibility of scale, maintain an appropriate level of privacy for each use, and minimize potential conflicts.

➤ CD-3.6 Managed Infrastructure. We collaborate with developers and property owners to facilitate development that realizes the envisioned character and functionality of the Place Type through the use of green and shared infrastructure within each Place Type.

▪ Goal CD-5 Protection of Investment: A sustained level of maintenance and improvement of properties, buildings, and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD-5.1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD-5.2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (general plan) component of The Ontario Plan, as the project site is not one of the properties in the Housing Element Sites contained in Tables B-1 and B-2 (Housing Element Sites Inventory) of the Housing Element Technical Report.

## **PART 2: RECITALS**

WHEREAS, the Application is a Project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, the Ontario Development Code Table 2.02-1 (Review Matrix) grants the Development Advisory Board (hereinafter referred to as "DAB") the responsibility and authority to review and act on the subject Application; and

WHEREAS, all members of the DAB of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the proposed development; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as



prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ONT ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 15, 2023, the DAB of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

### **PART 3: THE DECISION**

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED AND DECIDED by the Development Advisory Board of the City of Ontario as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making body for the Project, the DAB has reviewed and considered the information contained in the administrative record for the Project, including all written and oral evidence provided during the comment period. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the DAB, the DAB finds as follows:

(1) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (In-fill Development Projects) of the CEQA Guidelines, and meets all of the following conditions:

(a) *The proposed Project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.* The proposed Project is located within the Mixed-Use land use designation of the Policy Plan (general plan) Land Use Map, and the Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan. The

proposed Project is consistent with all applicable Policy Plan policies, as well as with the requirements of the specific plan.

(b) *The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.* The Project is proposed within the established boundaries of the City of Ontario, on a Project site totaling 1.26-acres of land, which is surrounded by established development on all sides and consists of a mix of commercial/retail land uses.

(c) *The Project site has no value as habitat for endangered, rare, or threatened species.* The site is located in an urbanized area, is devoid of any flora or fauna, is currently paved and used as a parking lot, and as such not suitable habitat for any endangered, rare, or threatened species.

(d) *Approval of the Project would not result in any significant effects relating to traffic, noise, air quality, or water quality.* The proposed commercial restaurant development is similar to, and of no greater impact than other allowed uses and development projects within the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan.

(e) *The site is adequately served by all required utilities and public services.* All necessary wet and dry utilities are within the public street (Fourth Street and Ontario Mills Drive) and are readily available for connection.

(2) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

(3) The determination of CEQA exemption reflects the independent judgment of the DAB.

SECTION 2: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the DAB finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Housing Element Sites contained in Tables B-1 and B-2 (Housing Element Sites Inventory) of the Housing Element Technical Report.

SECTION 3: Concluding Facts and Reasons. Based upon the substantial evidence presented to the DAB during the above-referenced hearing and upon the facts and information set forth in Parts I (Background and Analysis) and II (Recitals), above, and the determinations set forth in Sections 1 and 2, above, the DAB hereby concludes as follows:

(1) *The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.* The proposed Project is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

(2) *The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.* The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan, including standards relative to the particular land use proposed (commercial restaurant), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) *The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the Project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed Project.* The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan; and

(4) *The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.* The proposed Project has been reviewed for consistency with the general development standards and guidelines of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (commercial restaurant). As a result of this review, the Development Advisory Board has determined that the Project, when implemented

in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan.

SECTION 4: Development Advisory Board Action. Based on the findings and conclusions set forth in Sections 1 through 3, above, the DAB hereby APPROVES the Application subject to each and every condition set forth in the Conditions of Approval included as Attachment A of this Decision, and incorporated herein by this reference.

SECTION 5: Indemnification. The Applicant shall agree to defend, indemnify, and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 6: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

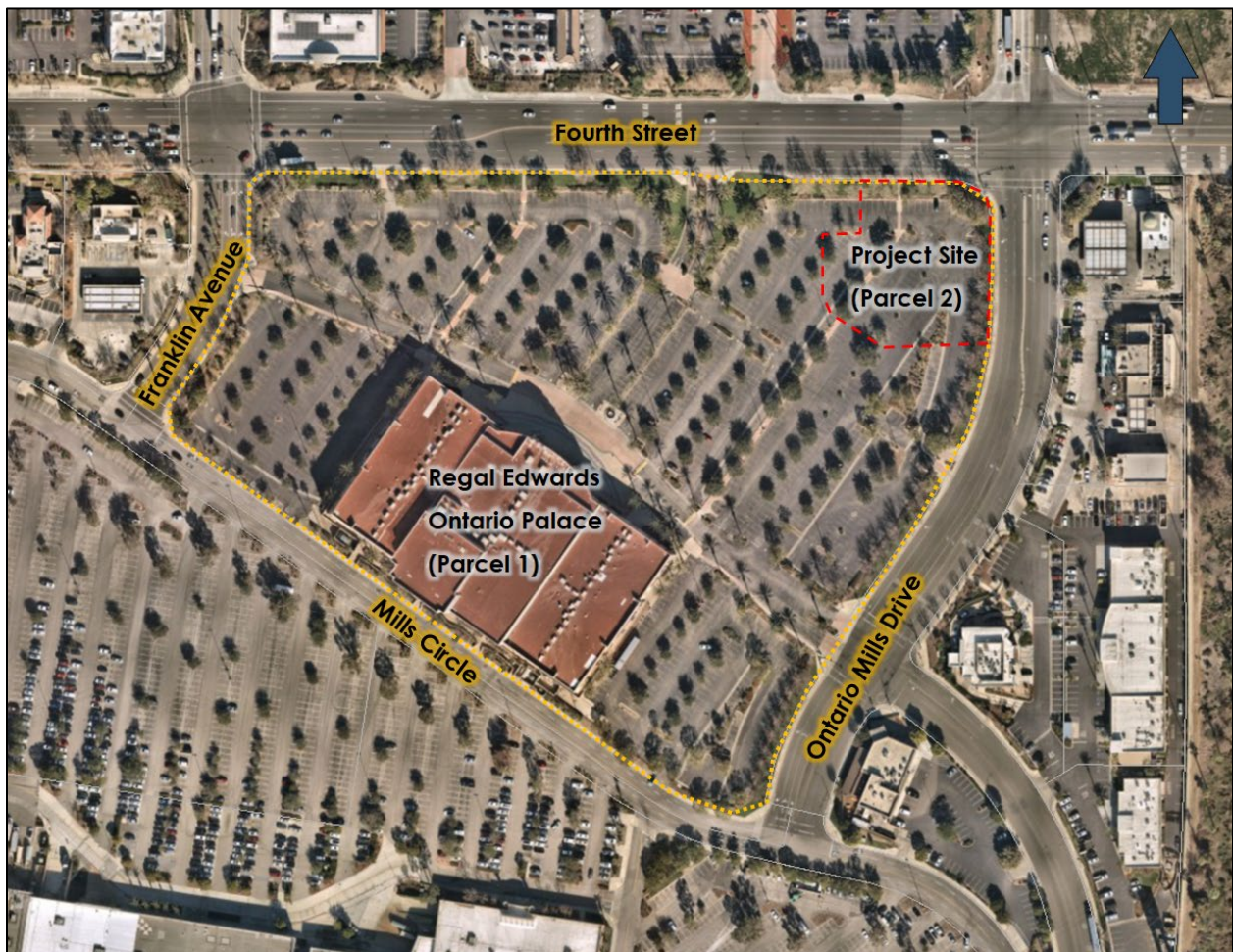
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APPROVED AND ADOPTED this 15<sup>th</sup> day of May, 2023.

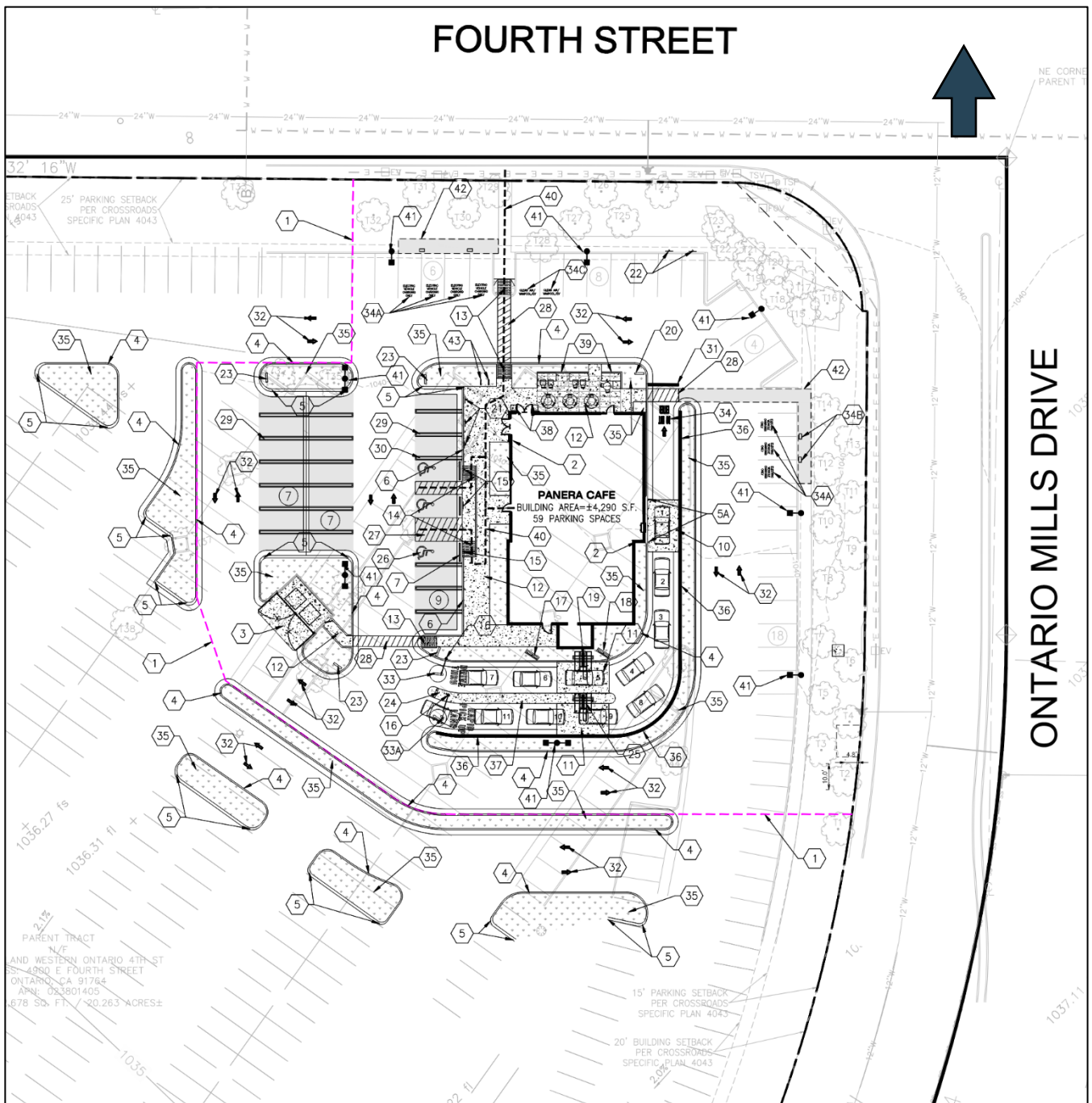
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Development Advisory Board Chairman

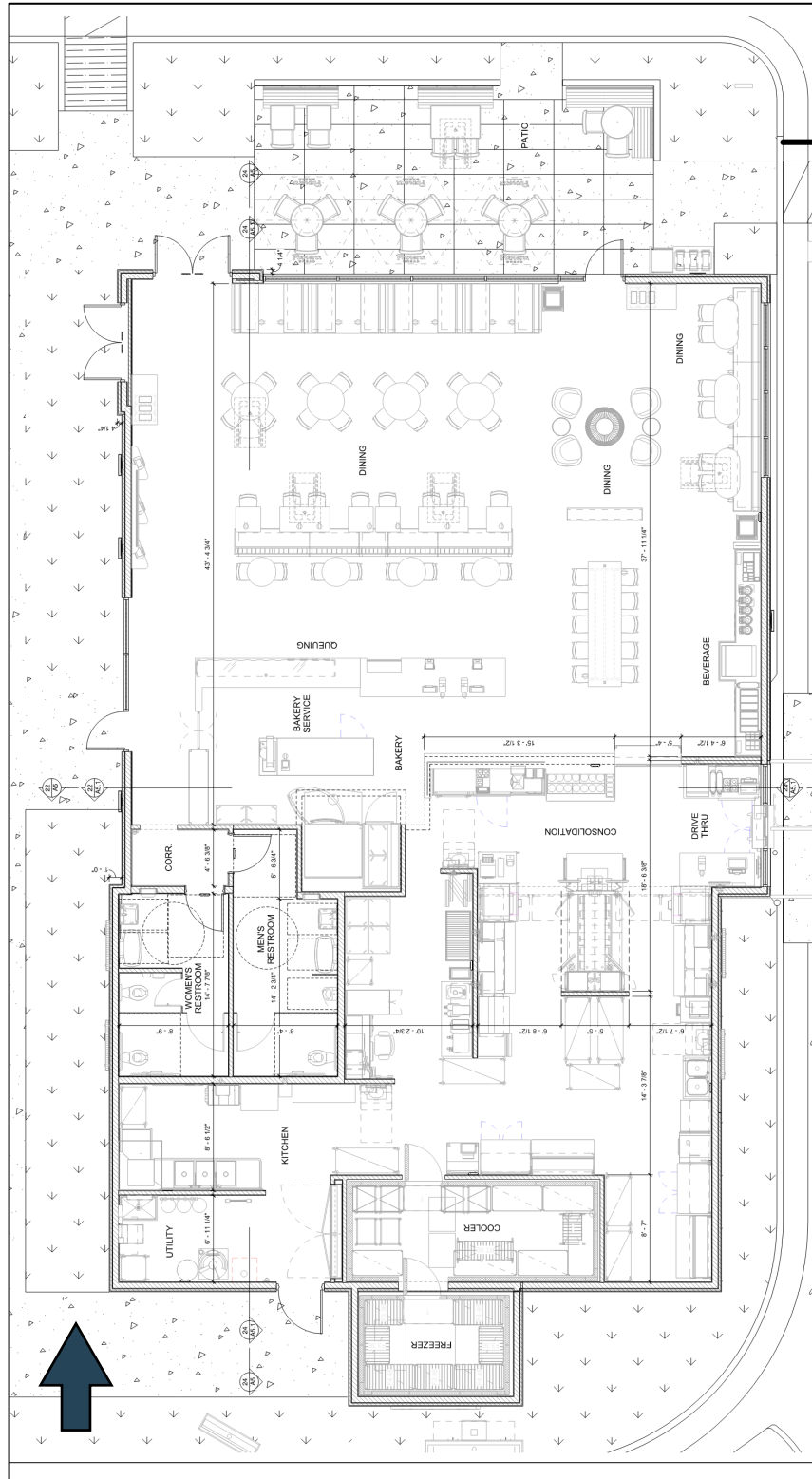
**Exhibit A: PROJECT LOCATION MAP**



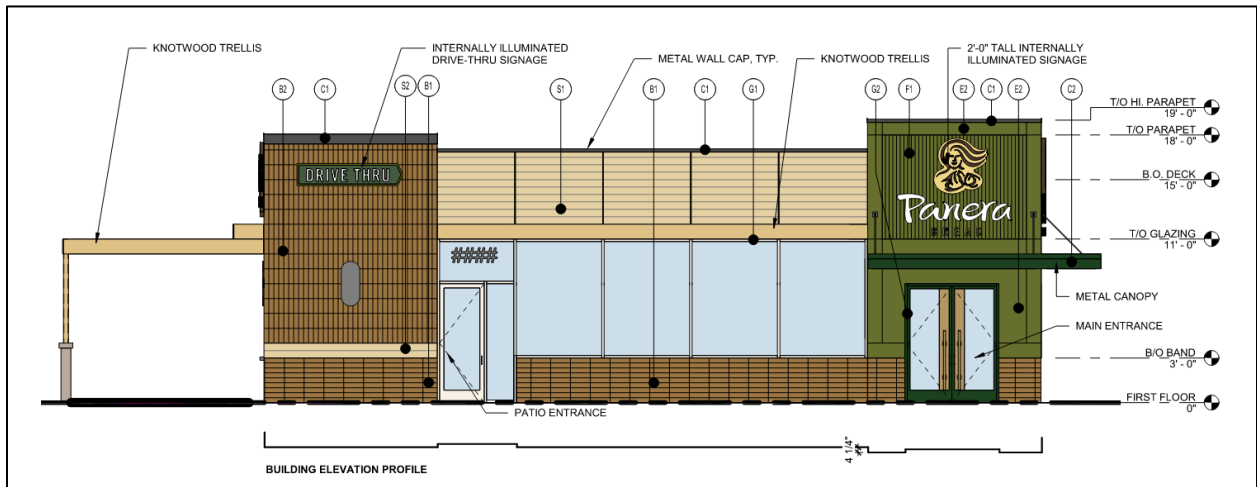
**Exhibit B: SITE PLAN**



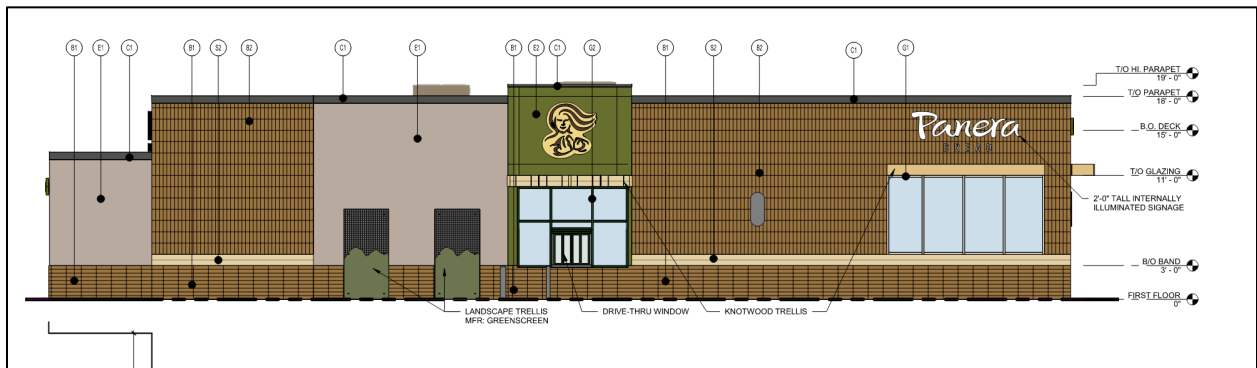
**Exhibit C: FLOOR PLAN**



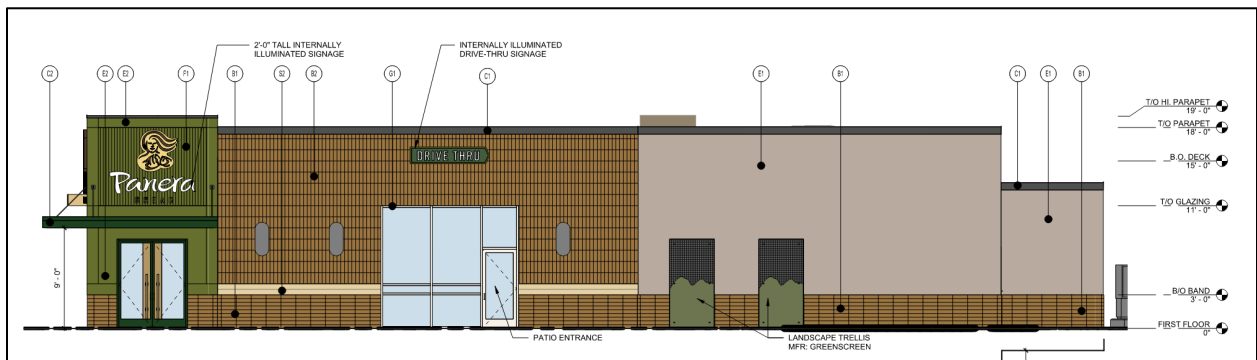
**Exhibit D: ELEVATIONS**



North Elevation



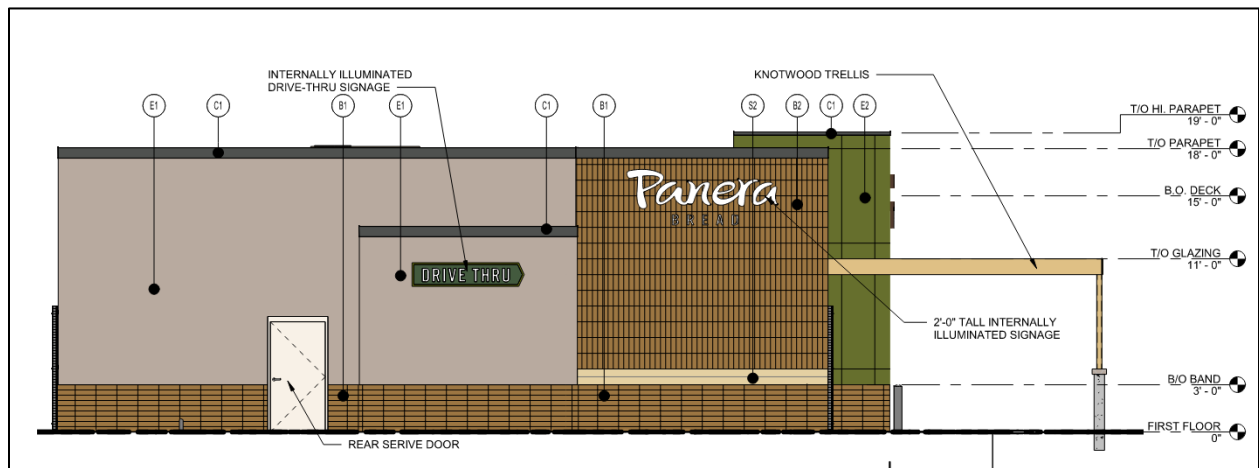
East Elevation



West Elevation











**Exhibit D: ELEVATIONS CONTINUED**

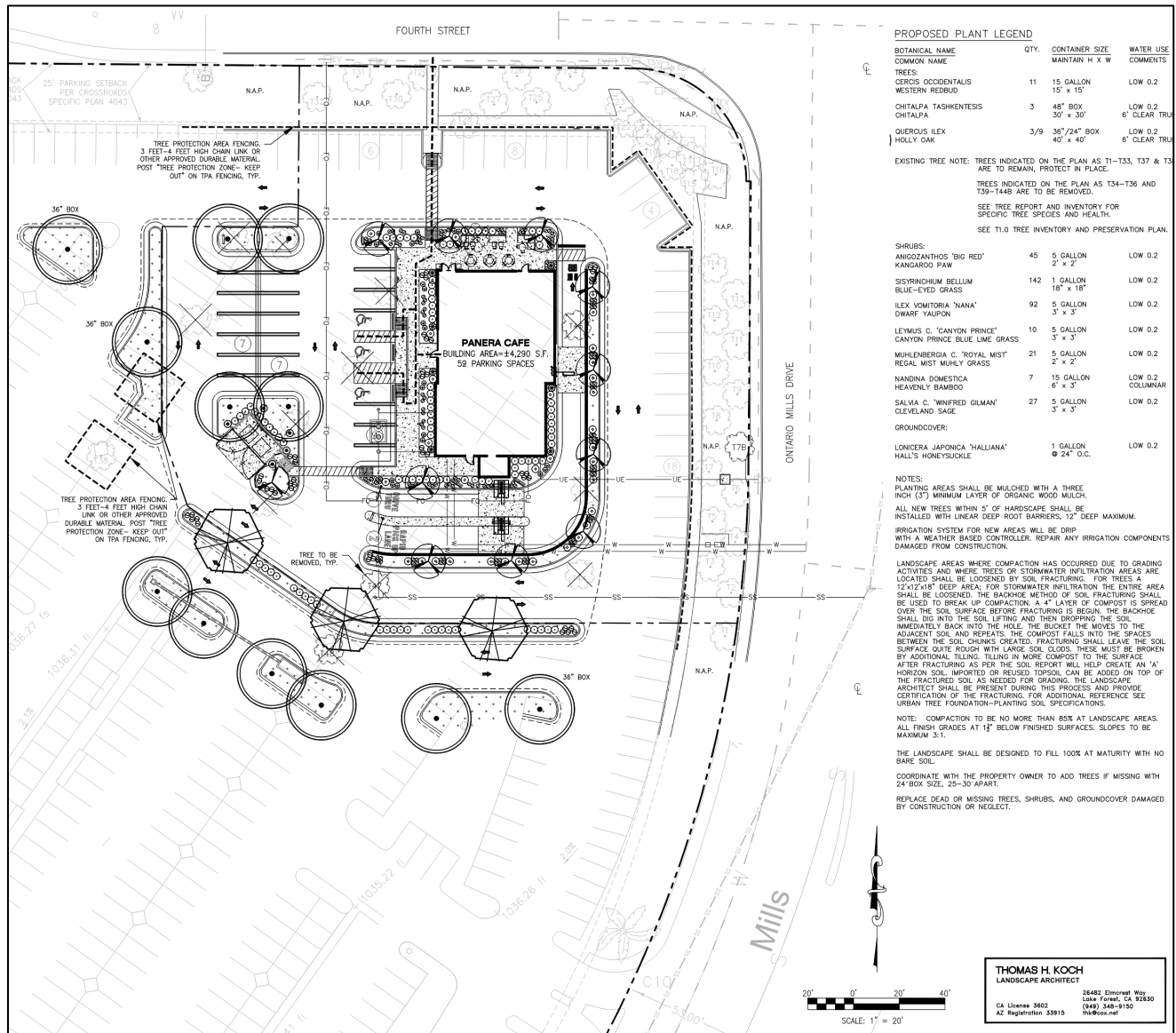


South Elevation

**EXTERIOR MATERIALS:**

- 
(B1) THIN BRICK SYSTEM: MCNEAR BRICK & BLOCK  
3/4" X 3 5/8" X 11 5/8", HORIZONTAL STACK BOND  
COLOR: SANTIAGO CREATIVE MATERIALS BLEND
- 
(B2) THIN BRICK SYSTEM: MCNEAR BRICK & BLOCK  
3/4" X 3 5/8" X 11 5/8", VERTICAL STACK BOND  
COLOR: SANTIAGO CREATIVE MATERIALS BLEND
- 
(C1) PRE-FINISHED METAL COPING  
COLOR: MATCH RAL 7043
- 
(C3) PRE-FINISHED ALUMINUM SCREENING  
COLOR: MATCH RAL 7043
- 
(C2) PRE-FINISHED ALUMINUM CANOPY  
COLOR: MATCH PMS 2411C
- 
(G2) ALUMINUM STOREFRONT  
KAWNEER, 2" X 4-1/2" PROFILE (OR APPROVED EQUAL)  
COLOR: MATCH PMS 2411C  
GLAZING: 1" INSULATED CLEAR GLASS
- 
(E1) EXTERIOR INSULATION AND FINISH SYSTEM (EIFS)  
TEXTURE TO MATCH DRYVIT LIMESTONE  
COLOR: DRYVIT #105 SUEDE
- 
(E2) EXTERIOR INSULATION AND FINISH SYSTEM (EIFS)  
FLUTED PROFILE, STO FINISH, FREEDOM SEAL COAT  
COLOR: MATCH PMS 2307C
- 
(F1) FIBERGLASS REINFORCED PANEL  
MFR: FORMGLAS, FORMGLAS FRP  
FINISH: BASF UNO, COLOR: MATCH PMS 2307C
- 
(G1) ALUMINUM STOREFRONT  
KAWNEER, 2" X 4-1/2" PROFILE (OR APPROVED EQUAL)  
FINISH: POWDERCOAT, COLOR: COLONIAL WHITE  
GLAZING: 1" INSULATED CLEAR GLASS
- 
(S1) ALUMINUM WOOD LOOK  
KNOTWOOD, CLADDING SYSTEM  
COLOR: WHITE ASH
- 
(S2) ALUMINUM WOOD LOOK  
KNOTWOOD, SELF MATING CLADDING SYSTEM  
COLOR: WHITE ASH

**Exhibit E: LANDSCAPE PLANS**



**Attachment A: Conditions of Approval**

*(Conditions of Approval follow this page)*



# LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

**Date Prepared:** 5/15/2023

**File No:** PDEV21-044

**Related Files:** PMTT18-010

**Project Description:** A Development Plan to construct a 4,290 drive-thru restaurant (Panera) and 500-square-foot outdoor patio on 1.26 acres of land located at the southwest corner of Fourth Street and Ontario Mills Drive within Commercial/Office land use designation of the California Commerce Center North/Ontario Gateway Plaza/Wagner Properties (Ontario Mills) Specific Plan. (APN: 238-014-05); **submitted by Panera LLC.**

**Prepared By:** Lorena Mejia, Senior Planner  
Phone: 909.395.2276 (direct)  
Email: lmejia@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1** Time Limits.

**(a)** Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.2** General Requirements. The Project shall comply with the following general requirements:

**(a)** All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape

and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

**(b)** The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

**(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

### **2.3** Landscaping.

**(a)** The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

**(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

**(c)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

**(d)** Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

**2.4** Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

### **2.5** Parking, Circulation and Access.

**(a)** The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

**(b)** Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

**(c)** The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

**(d)** Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

**(e)** Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11). Final design and placement of bicycle parking facilities shall be subject to Planning Department review and approval.

**2.6** Site Lighting.

**(a)** All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

**(b)** Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

**2.7** Mechanical and Rooftop Equipment.

**(a)** All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture. The building parapet walls shall be increased in height to completely screen the rooftop mechanical equipment, all rooftop mechanical equipment shall fall below parapet line.

**(b)** All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

**2.8** Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**2.9** Signs.

**(a)** All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

**2.10** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noise levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**(a)**

**2.11** Environmental Requirements.

**(a)** If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

**(b)** If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.12 Indemnification.** The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**2.13 Additional Fees.**

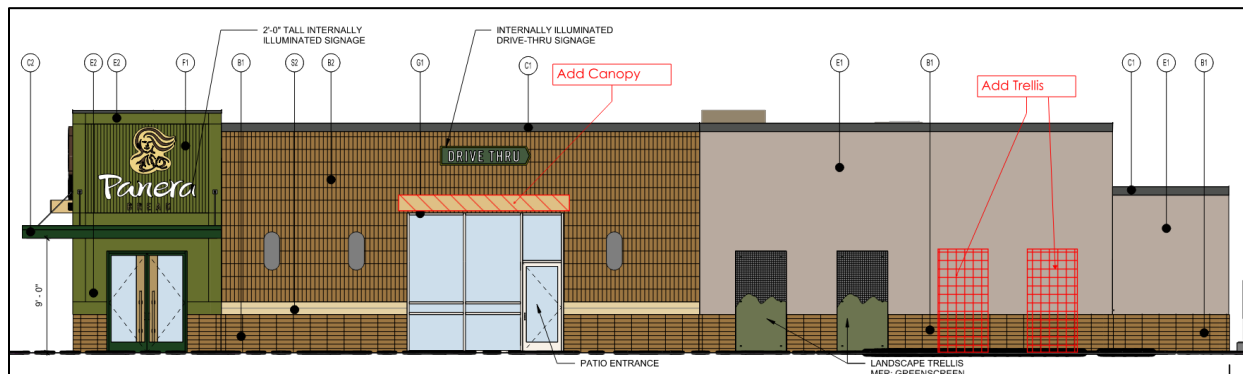
**(a)** Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of a NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

**(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**2.14 Additional Requirements.**

**(a)** The drive-thru screen wall shall be designed to match the architecture and building materials of the building and incorporate pilasters with a decorative cap.

**(b)** The west facing entrance shall incorporate a canopy over the entrance and incorporate 2 additional wall trellises (as shown below).



# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV21-044  
 Address: SWC 4th St & Ontario Mills Drive  
 APN: 0238-014-05  
 Existing Land Use: Parking Lot  
 Proposed Land Use: A Development Plan to construct a 4,290 SF drive-thru restaurant (Panera)  
 Site Acreage: 0.79 Proposed Structure Height: 20 FT  
 ONT-IAC Project Review: N/A  
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia  
 Contact Info: 909-395-2276  
 Project Planner: Lorena Mejia  
 Date: 1/25/2023  
 CD No.: 2021-074  
 PALU No.: N/A

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="checkbox"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="checkbox"/> Zone 4		Allowable Height: 190 FT	
<input type="checkbox"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6  
 Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP   
 Consistent   
 Consistent with Conditions   
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: \_\_\_\_\_





**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions incorporated)

<input checked="" type="checkbox"/> <b>DEVELOPMENT PLAN</b> <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
<b>PROJECT FILE NO. PDEV21-044</b> <b>RELATED FILE NO(S).</b> _____	
<input checked="" type="checkbox"/> <b>ORIGINAL</b> <input type="checkbox"/> REVISED: __/__/__	

**CITY PROJECT ENGINEER & PHONE NO:** David Zurita (909) 395-2155

**CITY PROJECT PLANNER & PHONE NO:** Lorena Mejia (909) 395-2721

**DAB MEETING DATE:** May 15, 2023

**PROJECT NAME / DESCRIPTION:** A development plan to construct one commercial building totaling 4,290 sqft on 0.79 acres of land located within the Regional Commercial land use district of the Ontario Mills Specific Plan.

**LOCATION:** 4900 E Fourth St (APN: 0238-014-05)

**APPLICANT:** Inland Western Ontario 4<sup>th</sup> St

**REVIEWED BY:** Raymond Lee 4/27/23.  
 Raymond Lee, P.E. Date  
 Assistant City Engineer

**APPROVED BY:** Khoi Do 4-27-23  
 Khoi Do, P.E. Date  
 City Engineer



**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL:** **Check When Complete**

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:   
 \_\_\_\_\_ feet on \_\_\_\_\_  
 Property line corner 'cut-back' required at the intersection of \_\_\_\_\_  
 and \_\_\_\_\_.
- 1.02 Dedicate to the City of Ontario, the following easement(s): \_\_\_\_\_   
 \_\_\_\_\_
- 1.03 Restrict vehicular access to the site as follows: \_\_\_\_\_
- 1.04 Vacate the following street(s) and/or easement(s):   
 a. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.   
 (1) \_\_\_\_\_  
 (2) \_\_\_\_\_
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.



- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ontarioca.gov](http://www.ontarioca.gov)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments: 
  - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
  - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
  - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: \_\_\_\_\_

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL  
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map/Tract Map No. \_\_\_\_\_ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 **Submit a PDF of the recorded Parcel Map 20087 to the City Engineer's office.**
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario per LLA96-015 (1997-092217).**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 Apply for a: 
  - Certificate of Compliance with a Record of Survey;
  - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);
  - Make a Dedication of Easement.



- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.
  
- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
  
- 2.08 Submit a soils/geology report.
  
- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies: 
  - State of California Department of Transportation (Caltrans)
  - San Bernardino County Road Department (SBCRD)
  - San Bernardino County Flood Control District (SBCFCD)
  - Federal Emergency Management Agency (FEMA)
  - Cucamonga Valley Water District (CVWD) for sewer/water service
  - United States Army Corps of Engineers (USACE)
  - California Department of Fish & Game
  - Inland Empire Utilities Agency (IEUA)
  - Other: \_\_\_\_\_
  
- 2.10 Dedicate to the City of Ontario the right-of-way described below: 

\_\_\_\_\_ feet on \_\_\_\_\_

Property line corner 'cut-back' required at the intersection of \_\_\_\_\_ and \_\_\_\_\_.
  
- 2.11 Dedicate to the City of Ontario the following easement(s): \_\_\_\_\_ 

\_\_\_\_\_
  
- 2.12 **Vacate the following street(s) and/or easement(s):** 
  - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.**
  
- 2.13 Ontario Ranch Developments: 
  - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a



maximum 3-foot high retaining wall.

- 2.14** Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
- 2.15** The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.
- 2.16** Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$21,746.34 shall be paid to the Building Department. Final fee shall be determined based on the approved site plan and the DIF rate at the time of payment.
- 2.17** Other conditions: 
  - A.** The project is subject to all the requirements and conditions of approval from the Ontario Mills Specific Plan and PMTT18-010 (PM-20087) as amended. For any conditions of approval that conflict, these conditions shall supersede those conflicting conditions.
  - B.** Submit a Final Utilities System Map (USM) as part of the precise grading plan submittal that meets all the City's USM requirements. These requirements include to show and label all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, etc.), sizes, points of connection and any easements. The final utility design shall comply with all Division of Drinking Water (CCR 64572) Separation Requirements. See Utility Systems Map (USM) Requirements document for details.



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

**2.18 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):**

Improvement	Fourth Street	Ontario Mills Drive	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



<b>Sewer</b> (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Water</b> (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Street Light</b> (see Sec. 2.F)	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
<b>Fiber Optics</b> (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above: \_\_\_\_\_

- 2.19 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s):**   
**A. Ontario Mills Drive along the entire frontage from curb to median.**
- 2.20 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.**



- 2.21 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.22 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \_\_\_\_\_, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.
- 2.23 Other conditions: \_\_\_\_\_

### C. SEWER

- 2.24 **An 8-inch sewer main is available for connection by this project in Ontario Mills Drive. (Ref: Sewer Drawing Number: S10505)**
- 2.25 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.26 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.27 **Other conditions:** 
  - A. Sewer Lateral:**
    - a. **Each building and its onsite private sewer system shall discharge wastewater to the City of Ontario Public Sanitary Sewer System through a Public Sewer Lateral per Standard #2003.**
    - b. **The existing sewer main being connected to shall be CCTV Inspected between the upstream and downstream manholes of the connection once before and once after the Sewer Lateral connections is made and any damage to the sewer main resulting from the installation of the Sewer Lateral shall be repaired to meet City Standards and Requirements prior to placing the Sewer Lateral in service.**
  - B. Onsite Sewer:**
    - a. **Onsite sewer shall be privately owned and maintained.**
    - b. **Private Onsite Sewer and Storm Water Quality Improvements: No storm water quality improvements (infiltration, detention, retention, bioswale, etc) shall be installed above or within 5 feet of any Private Onsite Sewer pipes.**
    - c. **The plumbing of the onsite sewer system shall be designed and constructed in such a way that:**
      - i. **The building's domestic wastewater leaves the building separate from the building's food service wastewater.**
      - ii. **A grease interceptor and sample box shall be installed for each food service establishment discharging a greasy wastestream as required by Ontario Municipal Code Section 6-7.404.**
  - C. Wastewater Discharge Requirements: For Wastewater Sewer Discharge, the project site shall meet the following requirements:**
    - a. **Each food service establishment occupant shall individually apply for a Wastewater Discharge Permit for their Establishment, and shall comply will all the requirements of the Wastewater Discharge Permit ([https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/fse\\_wastewater\\_permit\\_application\\_fillable.pdf](https://www.ontarioca.gov/sites/default/files/Ontario-Files/Municipal-Utilities-Company/fse_wastewater_permit_application_fillable.pdf)).Requirements of the Wastewater Discharge Permit may include, but not limited to: Installation of wastewater pretreatment equipment, such as grease interceptors.**

### D. WATER

- 2.28 **A 12-inch water main is available for connection by this project in Ontario Mills Drive. (Ref: Water Drawing Number: W10693)**





2.29 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.

2.30 **Other conditions:**

**A. Potable Water Service:**

- a. **Domestic Service:** The building shall have its own domestic water service and meter with backflow prevention device connected to the City of Ontario Public Potable Water System.
- b. **Irrigation Service:** For landscape irrigation uses that are not served by Recycled Water, the landscape irrigation uses shall have a separate irrigation water service and meter with backflow prevention device connected to the City of Ontario Public Potable Water System separate from the domestic water uses and the onsite plumbing systems shall be also separate from each other.
- c. **Fire Water Service:** For onsite private Fire System uses:
  - i. Where the domestic water service and meters connected to the Public Potable Water System that serves any use that is more than one (1) single family detached residential unit, or any non-residential use: if an onsite private fire system is required, then a separate Fire Service with Double Check Detector Assembly (DCDA) per City Standard #4208 connected to the City of Ontario Public Potable Water System is required, to serve the onsite private fire system. The onsite fire system and onsite domestic water plumbing system shall be separate.

**B. Fire Hydrants:** Fire hydrants fronting the Project shall be upgraded to meet current City standards.

**E. RECYCLED WATER**

2.31 A \_\_\_\_\_ inch recycled water main is available for connection by this project in \_\_\_\_\_.   
(Ref: Recycled Water Drawing Number:\_\_\_\_\_)

2.32 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.

2.33 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.

2.34 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

2.35 Other conditions: \_\_\_\_\_

**F. TRAFFIC / TRANSPORTATION**

2.36 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 

- 1. On-site and off-site circulation
- 2. Traffic level of service (LOS) at 'build-out' and future years
- 3. Impact at specific intersections as selected by the City Engineer

2.37 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.

2.38 **Other conditions:**

**A. The Applicant/Developer shall provide reciprocal access between the project site and all other adjacent properties.**

**B. The Applicant/Developer shall be responsible to replace any existing street light fixtures along its project frontage of 4<sup>th</sup> Street and Ontario Mills Drive with the current City approved**



**LED equivalent fixture per the Traffic and Transportation Design Guidelines.**

**C. Property frontage along 4<sup>th</sup> Street and Ontario Mills Drive shall be signed “No Stopping Anytime”.**

**G. DRAINAGE / HYDROLOGY**

- 2.39 An 18-inch storm drain main is available to accept flows from this project in Ontario Mills Drive. (Ref: Storm Drain Drawing Number: D10514)**
- 2.40 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.41 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100-year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.42 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project; unless it's covered in the CC&R's.**
- 2.43 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100-year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.44 Other conditions: \_\_\_\_\_

**H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)**

- 2.45 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.  
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.  
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.46 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.47 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.48 Other conditions:** 
  - A. Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally**



responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>

**J. SPECIAL DISTRICTS**

- 2.49 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 2.50 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

- 2.51 **A fiber optic line is available for connection by this project in Fourth Street. (Ref: Fiber Optic Drawing Number: O10158)**
- 2.52 **Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole. Generally located in Fourth Street, see Fiber Optic Exhibit herein.**
- 2.53 **Refer to the City’s Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.**

**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**



- 3.06** Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01** Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02** Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
- 4.03** The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.
- 4.04** Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.



## **EXHIBIT 'A'**

### **ENGINEERING DEPARTMENT First Plan Check Submittal Checklist**

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Project Number: PDEV21-044

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6.  Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
7.  Three (3) sets of Public Street improvement plan with street cross-sections
8.  Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9.  Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10.  Four (4) sets of Public Sewer improvement plan
11.  Five (5) sets of Public Storm Drain improvement plan
12.  Three (3) sets of Public Street Light improvement plan
13.  Three (3) sets of Signing and Striping improvement plan
14.  Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15.  Three (3) sets of HOA Landscape improvement plans. Show corner sight line distance per engineering standard drawing 1309.
16.  Five (5) sets of CFD Landscape improvement plans. Show corner sight line distance per engineering standard drawing 1309.
17.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
18.  Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
19.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**



- 20.  One (1) copy of Hydrology/Drainage study
- 21.  One (1) copy of Soils/Geology report
- 22.  Payment for Final Map/Parcel Map processing fee
- 23.  Three (3) copies of Final Map/Parcel Map
- 24.  One (1) copy of approved Tentative Map
- 25.  One (1) copy of Preliminary Title Report (current within 30 days)
- 26.  One (1) copy of Traverse Closure Calculations
- 27.  **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 28.  Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 29.  **Other:**
  - a. **Covenants, Conditions, and Restrictions (CC&R's) or agreements showing reciprocal access and cross lot drainage agreement between project site and all other adjacent properties**



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Lorena Mejia, Senior Planner  
Planning Department

**FROM:** Paul Ehrman, Sr. Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** December 20, 2021

**SUBJECT:** PDEV21-044 - A Development Plan to construct one (1) commercial buildings totaling 4,290 square feet on 0.79 acres of land located at the southwest corner of 4th Street and Ontario Mills Drive, within the Regional Commercial land use district of the Ontario Mills Specific Plan (APN(s): 0238-014-05). Related File(s): PMTT18-010.

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2019 CBC Type of Construction: Type VB
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): 4,290 Sq. Ft.
- D. Number of Stories: 1
- E. Total Square Footage: 4,290 Sq. Ft.
- F. 2019 CBC Occupancy Classification(s): A2

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov/Fire/Prevention](http://www.ontarioca.gov/Fire/Prevention).
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

### **3.0 WATER SUPPLY**

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.



- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

#### **4.0 FIRE PROTECTION SYSTEMS**

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.
- ☒ 4.8 A fixed fire extinguishing system is required for the protection of hood, duct, plenum and cooking surfaces. This system must comply with National Fire Protection Association (NFPA) Standards 17A and 96. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

## 5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Lorena Mejia, Senior Planner

**FROM:** Officer Tony Galban, Police Department

**DATE:** December 14, 2021

**SUBJECT:** PDEV21-044 - A DEVELOPMENT PLAN TO CONSTRUCT ONE COMMERCIAL BUILDING TOTALING 4,290 SQUARE FEET LOCATED AT THE SOUTHWEST CORNER OF 4TH STREET AND ONTARIO MILLS DRIVE. RELATED FILE PMTT18-010.

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, driveways, doorways, parking areas, and other areas used by the public shall be provided and operate on photosensor. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the building as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions.
- The Applicant shall install a video surveillance system on the site. Cameras shall cover at a minimum all entry doors, all cash registers, and at least one camera shall capture any vehicle utilizing the drive-thru. Cameras shall be positioned to maximize the coverage of patrons and vehicles in these areas. Cameras shall record at least 15 frames per second and at a minimum of 720p of resolution. Recordings shall be stored for a minimum of 30 days and made available upon request to any member of the Ontario Police Department.
- The applicant will be responsible for keeping the grounds of the business clean from debris and litter.
- Graffiti abatement by the business owner/licensee, or management shall be immediate and on-going on the premises, but in no event shall graffiti be allowed unabated on the premises for more than 72 hours. Abatement shall take the form of removal or shall be covered/painted over with a color reasonably matching the color of the existing building,

structure, or other surface being abated. Additionally, the business owner/licensee, or management shall notify the City within 24 hours at (909) 395-2626 (graffiti hotline) of any graffiti elsewhere on the property not under the business owner/licensee's or management control so that it may be abated by the property owner and/or the City's graffiti team.

- Maintain all landscaping on property to a standard that all ground covering shrubbery and hedges are no taller than 2 feet (24") and the lower canopy of all trees is no lower than 6 feet (72").
- Trash enclosure shall be fully secured by locks and screens/gates in order to reduce crime and encampment opportunities for homeless persons.

The Applicant is invited to call Tony Galban at (909) 408-1006 regarding any questions or concerns.

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

Sign Off



10/13/2022

Jamie Richardson, Sr. Landscape Planner

Date

Reviewer's Name:

**Jamie Richardson, Sr. Landscape Planner**

Phone:

**(909) 395-2615**

D.A.B. File No.:

PDEV21-044

Case Planner:

Lorena Mejia

Project Name and Location:

Panera Bread  
 SW Corner of 4<sup>th</sup> Street and Ontario Mills Drive

Applicant/Representative:

RSI Group, Inc./Nina Raey  
 3187 – A Airway Avenue  
 Costa Mesa, CA 92626



**Preliminary Plans (dated 09/14/2022) meet the Standard Conditions for New Development and have been approved considering that the following conditions below be met upon submittal of the landscape construction documents.**



**Preliminary Plans (dated) have not been approved. Corrections noted below are required before Preliminary Landscape Plan approval.**

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.**

Landscape construction plans with plan check number may be emailed to:

[landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)

**PREVIOUS COMMENTS – 01/07/2022**

Civil/ Site Plans

1. Replace modular wetland; consider storm chambers, drywells, etc.
  2. Before permit issuance, stormwater infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division. Any stormwater devices in parkway areas shall not displace street trees.
  3. Show transformers set back 5' from paving all sides. Coordinate with landscape plans.
  4. Show backflow devices set back 4' from paving all sides. Locate on level grade
  5. Locate utilities including light standards, fire hydrants, water, drain, and sewer lines to not conflict with required tree locations—coordinate civil plans with landscape plans.
  6. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 1/2" below finished surfaces. Slopes to be maximum 3:1.
  7. Dimension all planters to have a minimum 5' wide inside dimension.
  8. Dimension, show and call out for step-outs at parking spaces adjacent to planters; a 12" wide monolithic concrete curb, DG paving or pavers with edging.
- Landscape Plans
9. During plan check, coordinate with Ontario Municipal Utilities Company (OMUC) to submit irrigation plans for recycled water systems to [omucwaterquality@ontarioca.gov](mailto:omucwaterquality@ontarioca.gov). OMUC shall review and approve irrigation systems utilizing recycled water prior to final landscape approval. Submit an electronic approval letter or memo from OMUC with resubmittal of the landscape package.
  10. Show backflow devices with 36" high strappy leaf shrub screening and trash enclosures and transformers, a 4'-5' high evergreen hedge screening. Do not encircle utility, show as

- masses and duplicate masses in other locations at regular intervals.
11. Dimension all planters to have a minimum 5' wide inside dimension with 6" curbs and 12" wide curbs where parking spaces are adjacent to planters.
  12. Show 8' diameter of mulch only at new trees, 12' min. at existing trees. Detail irrigation dripline outside of mulched root zone.
  13. Designer or developer to provide agronomical soil testing and include a report on landscape construction plans. A new report is required for phased projects for each phase or a minimum of every six homes in residential developments.
  14. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
  15. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Landscape construction plans with building permit number for plan check may be emailed to: [landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)



# DEVELOPMENT ADVISORY BOARD DECISION

May 15, 2023

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

**DECISION NO.:** [insert #]

**FILE NO.:** PMTT22-020 (TPM 20587)

**DESCRIPTION:** A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land into 2 parcels located at 121 East Sixth Street within the LDR-5 (Low-Density Residential - 2.1 to 5.0 du/ac) zoning district. APN: 1047-241-05; **submitted by Samson Mulugeta. Planning Commission action is required.**

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## PART 1: BACKGROUND & ANALYSIS

SAMSON MULUGETA, (hereinafter referred to as "Applicant") has filed an application requesting approval of a Tentative Parcel Map, File No. PMTT22-020, as described in the subject of this Decision (hereinafter referred to as "Application" or "Project").

**PROJECT BACKGROUND:** The Project site is comprised of 0.423 acres of land located at 121 East Sixth Street, which is depicted in Exhibit A: Project Location Map, attached. The existing rectangular lot is developed with a 2,460 square-foot single-family residence built in 1950 (est.). The single-family one-story structure resembles the Mid-Century-California Ranch style, is L-shaped in plan with a design that includes a low gabled composition shingle roof, boxed eaves, two brick chimneys, and vertical wood and batten style siding and concrete block walls. The front elevation has fixed windows and the rear elevation has large floor-to-ceiling wooden windows. A 487-square-foot carport is attached to the west side of the house.

Between 1950 and 1956, the previous property owner extended the house to the west, converted the previous one-car garage to living space, added a second fireplace and chimney, and built a 487-square-foot two-car carport. In 1963, the kitchen was expanded to the south connecting it to the residence's main entrance and enclosed a portion of the front elevation with a concrete block wall. The structure is no longer occupied and the Applicant is proposing to restore the house to its previous condition and reestablish its architectural integrity (Exhibit D—Conceptual Elevations, attached).

A Cultural Resource Assessment was prepared (Charles J. Fisher, August 29, 2022) and concluded that the existing single-family residence is not architecturally significant and is not significantly associated with important events or people related to the founding or development of the region. In addition, the single-family residence does not embody the

distinctive characteristics of a type, period, region, or construction method, represent the work of an important creative individual, or possess high artistic values.

The Project site is located within LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district and is bounded by one- and two-story single-family residences on all sides. The existing land uses, Policy Plan (general plan) and zoning designations, and specific plan land designations on and surrounding the project site are as follows:

	<b>Existing Land Use</b>	<b>Policy Plan Land Use Designation</b>	<b>Zoning Designation</b>	<b>Specific Plan Land Use Designation</b>
Site:	Single-Family Residence	Low-Density Residential	LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district	Not Applicable
North:	Single-Family Residence	Low-Density Residential	LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district	Not Applicable
South:	Single-Family Residence	Low-Density Residential	LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district	Not Applicable
East:	Single-Family Residence	Low-Density Residential	LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district	Not Applicable
West:	Single-Family Residence	Low-Density Residential	LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district	Not Applicable

Tentative Parcel Map (TPM 20587) — The proposed Tentative Parcel Map will subdivide the Project site into 2 rectangular-shaped lots (see Exhibit B: Tentative Parcel Map 20587). The proposed parcel sizes range from 7,950 to 10,400 square feet. The lot widths range from 60 to 79.16 feet, and both proposed parcels have a depth of 132.50 feet. The Development Code requires lots established by the Traditional Single-Family Subdivision Standards to have a minimum lot size of 7,200 square feet, minimum lot width of 60 feet, and minimum lot depth of 75 feet. The Project meets the minimum lot area and dimension requirements of the LDR-5 (Low-Density Residential—2.1 to 5.0 du/ac) zoning district and is physically suitable for the existing and proposed density of development.

Approval of the Tentative Parcel Map will facilitate the future construction of a new two-story single-family dwelling unit on lot 1. The infill project will front onto Sixth Street (Exhibit C—Conceptual Site Plan, attached) and will be conditioned to be compatible with the existing neighborhood. Lot 1 will have an attached garage with a side-on configuration to be compatible with the existing neighborhood. Lot 1 will provide vehicle access from Sixth Street and will utilize its existing driveway. The Applicant will be required to construct a new driveway for Lot 2 to meet City standards. The proposed driveway will lead to a newly constructed two-car garage located in the rear of the lot.



Conditions of Approval have been imposed on the Project to ensure future development of the lots will be designed to safeguard the existing neighborhood. The development of the lots will require separate approval through the City's Building Plan Check process.

**PUBLIC NOTIFICATION:** Public notification is not required, as the Development Advisory Board is acting in its capacity as an advisory body to the Planning Commission. Public notification is required prior to the Planning Commission hearing on the Project.

**CORRESPONDENCE:** As of the preparation of this Decision, Planning Department staff has not received any written or verbal communications from the owners of properties surrounding the project site or from the public in general, regarding the subject application.

**AGENCY/DEPARTMENT REVIEWS:** Each City agency/department has been provided the opportunity to review and comment on the subject application and recommend conditions of approval to be imposed upon the application. At the time of the Decision preparation, recommended conditions of approval were provided and are included with this Decision.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan.

On April 19, 2011, the City Council of the City of Ontario approved and adopted the ONT ALUCP, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Development Advisory Board has reviewed and considered the facts and information contained in the Application and supporting documentation against the ONT ALUCP compatibility factors, including [1] Safety Criteria (ONT ALUCP Table 2-2) and Safety Zones (ONT ALUCP Map 2-2), [2] Noise Criteria (ONT ALUCP Table 2-3) and Noise Impact Zones (ONT ALUCP Map 2-3), [3] Airspace protection Zones (ONT ALUCP Map 2-4), and [4] Overflight Notification Zones (ONT ALUCP Map 2-5). As a result, the Development Advisory Board, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ONT ALUCP.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (general plan), and City Council Priorities components of The Ontario Plan ("TOP"). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

(1) City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Pursue City's Goals and Objectives by Working with Other Governmental Agencies
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)

(2) Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G 1-2. Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

**Land Use Element:**

- Goal LU-1 Balance: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU-1.1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.
  - LU-1.6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

- Goal LU-2 Compatibility: Compatibility between a wide range of uses and a resultant urban patterns and forms.
  - LU-2.6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.

#### **Housing Element:**

- Goal H-2 Housing Supply & Diversity: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
  - H-2.5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.

#### **Community Economics Element:**

- Goal CE-1 Complete Community: A complete community that provides for all incomes and stages of life.
  - CE-1.6 Diversity of Housing. We collaborate with residents, housing providers, and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community while being respectful of existing viable uses.
- Goal CE-2 Placemaking: A City of distinctive neighborhoods, districts, corridors, and centers where people choose to be.
  - CE-2.1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
  - CE-2.2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional, and sustainable places that will compete well with their competition within the region.
  - CE-2.4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
  - CE-2.5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

- Goal S-1 Seismic & Geologic Hazards: Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.

- S-1.1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

- Goal CD-1 Image & Identity: A dynamic, progressive city containing distinct and complete places that foster a positive sense of identity and belonging among residents, visitors, and businesses.

- CD-1.1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing, enhancing, and preserving the character of our existing viable neighborhoods.

- CD-1.3 Existing Neighborhoods. We require the existing character of viable residential and non-residential neighborhoods be preserved, protected, and enhanced.

- Goal CD-2 Design Quality: A high level of design quality resulting in neighborhoods, public spaces, parks, and streetscapes that are attractive, safe, functional, human-scale, and distinct.

- CD-2.1 Quality Building Design and Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide context-appropriate scale and proportion;
- A true architectural style which is carried out in plan, section, and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are articulated, high quality, durable, and appropriate for the architectural style.

- CD-2.2 Neighborhood Design. We create distinct residential neighborhoods that promote a sense of community and identity by emphasizing access, connectivity, livability, and social interaction through such elements as:

- A pattern of smaller, walkable blocks that promote activity, safety, and access to nearby amenities and services;
- Varied parcel sizes and lot configurations to accommodate a diversity of housing types;

- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable traffic flows and emergency evacuation access;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb and designed to maximize safety, comfort, and aesthetics for all users.

➤ CD-2.7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping, and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials, and construction techniques.

➤ CD-2.8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

➤ CD-2.9 Landscape Design. We encourage durable, sustainable, and drought-tolerant landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD-2.10 Parking Areas. We require all development, including single-family residential, to minimize the visual impact of surface, structured, and garage parking areas visible from the public realm in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include:

- Surface parking: Shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field;
- Structured parking: facade articulation, screening, appropriate lighting, and landscaping; and
- Garage parking: providing access to single-family residential garages through alley access, recessing garages from the frontage to emphasize front doors or active living spaces.

➤ CD-2.11 Entry Statements. We encourage the inclusion of amenities, signage, and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD-2.13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

➤ CD-3.3 Complete and Connected Network. We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks, commercial areas, and transit stops) and are designed to maximize safety, comfort, and aesthetics.

➤ CD-3.4 Context-Aware and Appropriate Design. We require appropriate building and site design that complements existing development, respects the intent and identity of the Place Type, and provides appropriate transitions and connections between adjacent uses to ensure compatibility of scale, maintain an appropriate level of privacy for each use, and minimize potential conflicts.

➤ CD-3.5 Active Frontages. We create lively pedestrian streetscapes by requiring primary building, business, and residential entrances, outdoor dining, and storefronts be located on ground floors adjacent to sidewalks or public spaces and designed to maximize safety, comfort, aesthetics, and the intended functionality (as defined by the Place Type).

➤ CD-3.6 Managed Infrastructure. We collaborate with developers and property owners to facilitate development that realizes the envisioned character and functionality of the Place Type through the use of green and shared infrastructure within each Place Type.

▪ Goal CD-5 Protection of Investment: A sustained level of maintenance and improvement of properties, buildings, and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD-5.1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD-5.2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The Project is consistent with the Housing Element of the Policy Plan (general plan) component of The Ontario Plan, as the project site is not one of the properties in the Housing Element Sites contained in Tables B-1 and B-2 (Housing Element Sites Inventory) of the Housing Element Technical Report.

## **PART 2: RECITALS**

WHEREAS, the Project is exempt from CEQA pursuant to a categorical exemption (listed in CEQA Guidelines Article 19, commencing with Section 15300) and the

application of that categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Development Advisory Board (hereinafter referred to as "DAB") the responsibility and authority to review and make recommendation to the Planning Commission on the subject Application; and

WHEREAS, all members of the DAB of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the proposed development; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ONT ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 15, 2023, the DAB of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

### **PART 3: THE DECISION**

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED AND DECIDED by the Development Advisory Board of the City of Ontario as follows:

SECTION 1: Environmental Determination and Findings. As the recommending body for the Project, the DAB has reviewed and considered the information contained in the administrative record for the Project, including all written and oral evidence

provided during the comment period. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the DAB, the DAB finds as follows:

- (1) The Project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15315 (Class 15: Minor Land Divisions) of the CEQA Guidelines, which consists of division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division is in conformance with the General Plan and zoning, no variance or exceptions are required, all services and access to the proposed parcels to local standards are available, the parcel was not involved in a division of a larger parcel within 2 years, and the parcel does not have an average slope greater than 20 percent; and
- (2) The application of the categorical exemption is not barred by one of the exceptions set forth in CEQA Guidelines Section 15300.2; and
- (3) The determination of CEQA exemption reflects the independent judgment of the DAB.

SECTION 2: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending body for the Project, the DAB finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Housing Element Sites contained in Tables B-1 and B-2 (Housing Element Sites Inventory) of the Housing Element Technical Report.

SECTION 3: Concluding Facts and Reasons. Based upon the substantial evidence presented to the DAB during the above-referenced hearing and upon the facts and information set forth in Parts I (Background and Analysis) and II (Recitals), above, and the determinations set forth in Sections 1 and 2, above, the DAB hereby concludes as follows:

- (1) *The proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.* The proposed Tentative Tract/Parcel Map is located within the Low-Density Residential land use district of the Policy Plan Land Use Map, and the LDR-5 (Low Density Residential—2.1 to 5.0 du/ac) zoning district. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing "a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life" (Goal LU-1). Furthermore, the Project will promote the City's policy to "incorporate a variety of land uses and building types in our land use planning



efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario" (Policy LU-1.6 *Complete Community*).

(2) *The design or improvement of the proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.* The proposed Tentative Tract/Parcel Map is located within the Low-Density Residential land use district of the Policy Plan Land Use Map, and the LDR-5 (Low Density Residential—2.1 to 5.0 du/ac) zoning district. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the Project will contribute to providing "[a] high level of design quality resulting in neighborhoods, commercial areas, public spaces, parks, and streetscapes that are attractive, safe, functional, human-scale, and distinct" (Goal CD-2). Furthermore, the Project will promote the City's policy to "create distinct residential neighborhoods that promote a sense of community and identity by emphasizing access, connectivity, livability, and social interaction through such elements as:

- A pattern of smaller, walkable blocks that promote activity, safety, and access to nearby amenities and services;
- Varied parcel sizes and lot configurations to accommodate a diversity of housing types;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb and designed to maximize safety, comfort, and aesthetics for all users." (Policy CD-2.2 *Neighborhood Design*).

(3) *The site is physically suitable for the type of development proposed.* The Project site meets the minimum lot area and dimensions of the LDR-5 (Low Density Residential—2.1 to 5.0 du/ac) zoning district and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions.

(4) *The site is physically suitable for the density/intensity of development proposed.* The Project site is proposed for residential development at a density of 2.1 -5.0 DUs/acre. The Project site meets the minimum lot area and dimensions of the LDR-5 (Low Density Residential—2.1 to 5.0 du/ac) zoning district and is physically suitable for this proposed density/ intensity of development.

(5) *The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.* The Project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in

local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.

(6) *The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.* The design of the proposed subdivision, and the infrastructure improvements existing or proposed on the Project site, are not likely to cause serious public health problems, as the Project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or Project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the Project site.

(7) *The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.* The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 4: Development Advisory Board Action. Based on the findings and conclusions set forth in Sections 1 through 3, above, the DAB hereby recommends the Planning Commission APPROVES the Application subject to each and every condition set forth in the Conditions of Approval included as Attachment A of this Decision and incorporated herein by this reference.

SECTION 5: Indemnification. The Applicant shall agree to defend, indemnify, and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the Applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 6: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

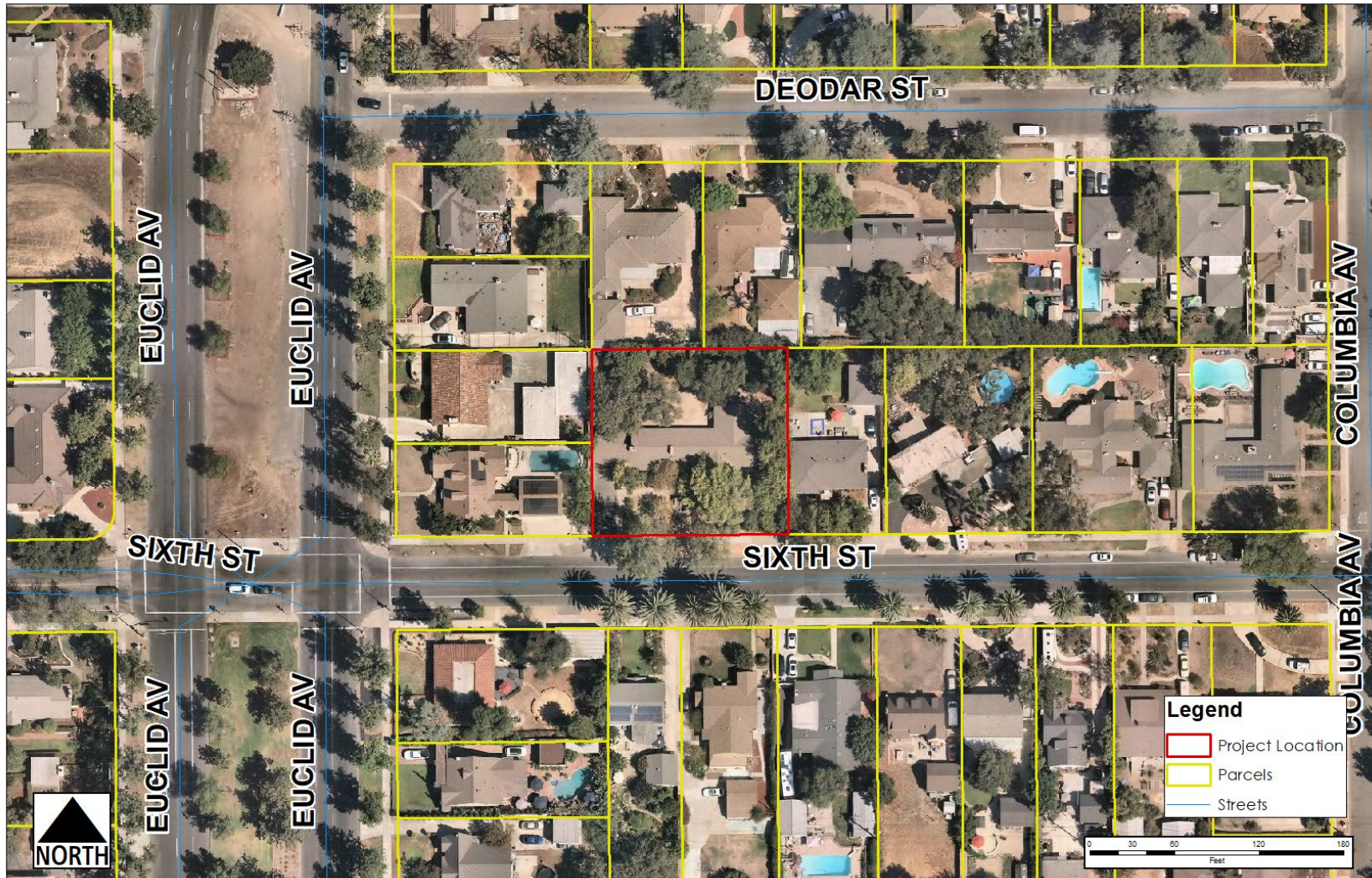
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APPROVED AND ADOPTED this 15th day of May 2023.

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Development Advisory Board Chairman

**Exhibit A: PROJECT LOCATION MAP**





**Exhibit C: CONCEPTUAL SITE PLAN**



**Exhibit D: CONCEPTUAL ELEVATIONS**



**Figure 1: Conceptual Elevation for Lot 1 (Street View)**



**Figure 2: Conceptual Elevation for Lot 1 (Northeast View)**





**Figure 3: Conceptual Elevation for Lot 2 (Street View)**



**Figure 4: Conceptual Elevation for Lot 2 (Northeast Elevation)**

**Exhibit E: SITE PHOTOS**



**View from 6<sup>th</sup> Street (Street View)**



**View of Main Entrance**



**View of the front façade  
(Northeastern view)**



**Rear View of Property**

**Attachment A: Conditions of Approval**

*(Conditions of Approval follow this page)*

**Date Prepared:** 5/15/2023

**File No:** PMTT22-020

**Project Description:** A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land into 2 parcels located at 121 East Sixth Street within the LDR-5 (Low-Density Residential - 2.1 to 5.0 du/ac) zoning district APN: 1047-241-05; **submitted by Samson Mulugeta.**

**Prepared By:** Robert Morales, Assistant Planner  
Phone: 909.395.2432 (direct)  
Email: Rmorales@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The Project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the Project shall comply with the following special conditions of approval:

**2.1 Time Limits.** Tentative Parcel Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

**2.2 Subdivision Map.**

**(a)** The Final Parcel Map shall be in conformance with the approved Tentative Parcel Map on file with the City. Variations from the approved Tentative Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

**(b)** Tentative Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

**(c)** Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

**2.3** General Requirements. The Project shall comply with the following general requirements:

**(a)** All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

**(b)** The Project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

**(c)** The herein-listed conditions of approval from all City departments shall be included in the construction plan set for the Project, which shall be maintained on-site during Project construction.

**2.4** Landscaping.

**(a)** The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

**(b)** Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

**2.5** Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

**2.6** Parking, Circulation and Access.

**(a)** The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

**(b)** All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

**(c)** Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the

outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

**(d)** The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

**(e)** Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

**(f)** Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

**2.7** Mechanical and Rooftop Equipment.

**(a)** All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

**2.8** Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

**2.9** Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

**2.10** Disclosure Statements. A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that is tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

**2.11** Environmental Requirements.

**(a)** The proposed Project is categorically exempt from the requirements of the California Environmental Quality Act of 1970 (CEQA), as amended, and the Guidelines promulgated thereunder, pursuant to Section 15315 (Class 15, Minor Land Division) of the CEQA Guidelines, which consists of the division of property in urbanized areas zoned for residential, commercial, or industrial use into four or fewer parcels when the division is in conformance with the General Plan and zoning, no variances or exceptions are required, all services and access to the proposed parcels to local standards are available, the parcel was not involved in a division of a larger parcel within 2 years, and the parcel does not have an average slope greater than 20 percent.

**(b)** If human remains are found during Project grading/excavation/construction activities, the area shall not be disturbed until any required



investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

**(c)** If any archeological or paleontological resources are found during Project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.12** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**2.13** Additional Fees.

**(a)** Within 5 days following final application approval, the Notice of Exemption ("NOE") filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act ("CEQA"). The filing of a NOE is voluntary; however, failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

**(b)** After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**2.14** Final Occupancy. The Project Architect of record will certify that construction of each building site and the exterior elevations of each structure shall be completed in compliance with the approved plans. Any deviation to approved plans shall require a resubmittal to the Planning Department for review and approval prior to construction. The Occupancy Release Request Form/Architect Certificate of Compliance shall be provided prior to final occupancy. After the receipt of this Certification, the Planning Department will conduct a final site and exterior elevations inspection. The Owner's Representative and Contractor shall be present.

**2.15** Additional Requirements.

**(a)** The future development of Lots 1 and 2 shall be designed in a manner compatible with the surrounding single-family residences and neighborhood. The Applicant/Owner shall submit Plans to the City's Building Department for City review.

**(i)** The Applicant shall obtain a building permit prior to any demolition, relocation, or construction.

**(b)** Infill construction on Lot 1 and 2 shall be oriented towards Sixth Street, have a greater front yard setback than existing residence in Lot 2. Infill construction shall be setback a minimum of 30-feet from south property line (Sixth Street).

**(c)** A new, 2-car garage shall be constructed on Lot 2. The garage shall be constructed to match the residence and shall match the residence dominant roof slope, horizontal wood siding and wood trim around windows and doors.

**(d)** The Applicant shall submit Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

**(i)** Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

**(e)** All buildings or structures located within the setback area along the newly created interior property line shall be demolished or removed prior to final map recordation.

**(f)** All new walls shall be constructed of split face block, slump stone, or plastered, textured, and painted to match the main structures.

**(g)** Prior to Occupancy the Planning Department shall inspect the premises to ensure the Conditions of Approval have been met and that the addition has been constructed per the approved plans.



# CITY OF ONTARIO MEMORANDUM

## ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Management Services Department Conditions incorporated)

**PROJECT ENGINEER:** Trevor Rivero, Administrative Intern (909) 395-2292


**PROJECT PLANNER:** Robert Morales, Assistant Planner (909) 395-2432


**DAB MEETING DATE:** May 15, 2023

**PROJECT NAME/DESCRIPTION:** PMTT 22-020 Parcel Map 20587, A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land in to 2 parcels located at 121 E 6th Street, within the LDR-5 (Low-Density – 2.1 to 5.0 du/ac) zoning district.

**LOCATION:** 121 E. 6<sup>th</sup> Street (APN: 1047-241-05-0-000)

**APPLICANT:** Genet Negash

**REVIEWED BY:**   
Raymond Lee, P.E.  
Assistant City Engineer 4/19/23 -  
Date

**APPROVED BY:**   
Khoi Do, P.E.  
City Engineer 4-19-23  
Date

**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE LISTED BELOW. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT. SEE ATTACHED EXHIBIT 'A' FOR PLAN CHECK SUBMITTAL REQUIREMENTS.**

1. Provide a preliminary title report current to within 30 days.
2. Record Parcel Map/Tract Map No. PM-20587 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
3. Submit a PDF of the recorded map to the City Engineer's office.
4. The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

5. Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following:
  - A. Drive Approach:**
    - The Applicant/Developer shall design and construct all proposed driveways in accordance with City of Ontario Standard Drawing No. 1203 for Residential Driveways.
  - B. Sewer:**
    - An 8-inch sewer main is available for connection by this project in 121 E. 6th St. (Ref: Sewer plan bar code: S12754).
    - Reference OMUC's comments (5. Sanitary Sewer Service) sections a & b
      - a. Each lot and its onsite private sewer system shall discharge wastewater to the Public Sanitary Sewer System through a Public Sewer Lateral per Standard #2003. Each sewer lateral shall have a cleanout located at the back of the PUE outside of the PUE.
      - b. Public Sewer Laterals and Storm Water Quality Improvements: No storm water quality improvements (infiltration, detention, retention, bioswale, etc.) shall be installed above or within 5 feet of any Public Sewer Lateral.
  - C. Water:**
    - A 12-inch water main is available for connection by this project in 121 E. 6th St. (Ref: Water plan bar code: W10303)
    - Reference OMUC's Comments (6. Potable Water Service) sections a, b & c.
      - a. Domestic Service: Each lot shall have a its own domestic water service and meter connected to the Public Potable Water System. The meter shall be located a minimum of 5 feet away from wing of the drive approach.
      - b. Relocated Services: For any existing service with appurtenances to be relocated, the service shall be abandoned back to the main connection and the service and appurtenances shall be installed new per related City Standards.
      - c. Division of Drinking Water Separation Requirements: The final utility design shall comply with all. Division of Drinking Water (CCR §64572) Separation Requirements
6. Reference OMUC's (Ontario Municipal Utilities Company) Conditions of approval for additional items.
7. Reference Broadband Operations Section for Standard Conditions of approval.
8. Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
9. Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.
10. The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.

## **EXHIBIT 'A'**

### **ENGINEERING DEPARTMENT First Plan Check Submittal Checklist**

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**Project Number: PMTT22-020, PM20587**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp**
4.  **One (1) copy of project Conditions of Approval**
5.  **Include a PDF (electronic submittal) of each required improvement plan at every submittal**
6.  **Two (2) sets of Domestic and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
7.  Three (3) sets of Public Street improvement plan with street cross-sections
8.  Three (3) sets of Public Street Light improvement plan
9.  Three (3) sets of Signing and Striping improvement plan
10.  Two (2) sets of the Site plan with proposed public street and utility improvements
11.  Two (2) sets of Delta Revision Plans to Record Street, Street Light, and/or Signing & Striping Drawings which reflect the proposed public street improvements
12.  Two (2) sets of Delta Revision Plans to Record Domestic Water, Recycled Water, and/or Sewer Drawings which reflect the installation of the proposed service laterals, and/or abandonment of unused existing service laterals
13.  Three (3) sets of Wet Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
14.  Two (2) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include AutoCAD electronic submittal)
15.  Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP)
16.  One (1) copy of Hydrology/Drainage study
17.  One (1) copy of Soils/Geology report
18.  Other: \_\_\_\_\_



# CITY OF ONTARIO

## MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)  
Rudy Zeledon, Planning Director (Copy of memo only)  
Diane Ayala, Advanced Planning Division (Copy of memo only)  
Charity Hernandez, Economic Development  
James Caro, Building Department  
Raymond Lee, Engineering Department  
Jamie Richardson, Landscape Planning Division  
Heather Young, Municipal Utility Company  
Heather Lugo, Police Department  
Paul Erhman, Deputy Fire Chief/Fire Marshal  
Jay Bautista, Traffic/Transportation Manager  
Lorena Mejia, Airport Planning  
Jeff Tang, Engineering/NPDES  
Angela Magana, Community Improvement (Copy of memo only)  
Jimmy Chang, IPA Department  
Blaine Ishii, Integrated Waste

RECEIVED

FEB 09 2023

CITY OF ONTARIO

FROM: Robert Morales, Assistant Planner

DATE: February 08, 2023

SUBJECT: FILE #: PMTT22-020 Revision # 2 Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

**PROJECT DESCRIPTION:** A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land into 2 parcels located at 121 E 6th, Street within the LDR-5 (Low-Density - 2.1 to 5.0 du/ac) zoning district (APN: 1047-241-05).

The plan does adequately address the departmental concerns at this time.

#9029

- No comments
- See previous report for Conditions
- Report attached (1 copy and email 1 copy)
- Standard Conditions of Approval apply

The plan does not adequately address the departmental concerns.

- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

OMUC Utilities Engineering *RCJ*  
Department Signature

Assistant Engineer 03/02/23  
Title Date



# CITY OF ONTARIO MEMORANDUM



**DATE:** March 2, 2023  
**TO:** Trevor Rivero, Engineering Department  
Jeffrey Tang, Engineering Department  
**CC:** Robert Morales, Planning Department  
**FROM:** Heather Young, Utilities Engineering Division  
Christy Stevens, Utilities Engineering Division  
**SUBJECT:** DPR#3 - Utilities Conditions of Approval (COA) (#9029)  
**PROJECT NO.:** PM-20587 (PMTT22-020)

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**BRIEF DESCRIPTION:**

*A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land into 2 parcels located at 121 E 6th, Street within the LDR-5 (Low-Density - 2.1 to 5.0 du/ac) zoning district (APN: 1047-241-05).*

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## OMUC UTILITIES ENGINEERING DIVISION CONDITIONS OF APPROVAL

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**CONDITIONS OF APPROVAL:** *The Ontario Municipal Utilities Company (OMUC) Utilities Engineering Division recommends this application for approval subject to the Conditions of Approval outlined below and compliance with the City's Design Development Guidelines, Specifications Design Criteria, and City Standards. The Applicant shall be responsible for the compliance with and the completion of all the following applicable Conditions of Approval prior to the following milestones and subject to compliance with City's Design Development Guidelines, Specifications Design Criteria, and City Standards:*

**General Conditions:**

1. **Standard Conditions of Approval:** Project shall comply with the requirements set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017, or as amended or superseded by Council Resolution; as well as project-specific conditions/requirements as outlined below.

**Prior to Issuance of Any Permits (Grading, Building, Demolition and Encroachment), unless other timeline milestones are specified by individual conditions below, the Applicant Shall:**

**General Conditions (Section 2.A, Other conditions): The Applicant shall comply with the following:**

2. **Precise Grading Plans:** The precise grading plans shall include the construction of the utility improvements required from the conditions below. The precise grading plans shall be submitted separate from the architectural and plumbing plans and be reviewed under a separate building permit application/record number. Immediately after formally resubmitting the plan set for Plan Check to the Building Department, email flatten PDF copies of the plans to: [utilityengplanchecks@ontarioca.gov](mailto:utilityengplanchecks@ontarioca.gov) (if files are larger than 20MB, then send a link to download the files). Be sure to include the Project Number (such as Tract or Parcel Map, or PDEV), Building Permit Number (B#####), and the Encroachment Permit Number (E#####).
3. **Note the following definitions and concepts for Public Utility Improvements and Private Utility Improvements:** Public Improvements shall be designed per City Public Design Guidelines and City Standards and constructed through a City Encroachment Permit; and Private Onsite Improvements shall be designed per Building Code and Plumbing Code and constructed through a City Building Permit.
  - a. Public Utility Improvements include the following: water main pipelines and sewer main pipelines; sewer laterals connecting to a Public Sewer Main up to the Cleanout (or Manhole) at PL/RoW; water services and connected appurtenances (Meters/Meter Boxes, Fire Hydrants, Airvacs, Blowoffs, etc.) connecting to a Public Water Main per City Standards; and Fire Services connecting to a Public Water

Main from the Main up to the DCDA. Public Water Improvements and Public Sewer Improvements are required to be designed and constructed through Public Improvement Plans with Plan View and Profile View per City Standards, Guidelines, and Requirements.

- b. Private Utility Improvements include the following: onsite water plumbing lines after a Public Meter, or after the Fire DCDA and including the DCDA; Backflow Devices and other Cross-Connection Prevention; onsite sewer upstream of the Public Sewer Lateral, including the Cleanout (or Manhole) at PL/RoW/PUE Edge; Monitoring Manholes and other Wastewater Pretreatment Facilities. Private Onsite Utility Improvements are required to be designed and constructed per Building and Plumbing Plans with: the Backflows, DCDAs, Cleanout (or Manhole) at PL/RoW/PUE Edge, and Monitoring Manholes being designed and constructed through a Precise Grading Plan; and, the other Pretreatment Devices (Grease Interceptor, Sand, Oil Interceptors, etc.) and the connections to the buildings and structures through a building Plumbing Plan.
4. Public Utilities and Public Right-of-Way including Public Utility Easements (PUE): All City of Ontario Public Utilities shall be installed within a Public Right-of-Way (RoW), or within a Public Utility Easements (PUE), or within a combination of RoW and PUE. In this case, Public Utilities is referring to the mains and connected appurtenances of the following City of Ontario/OMUC Utilities: Public Potable Water; Public Recycled Water; and Public Sanitary Sewer. Public Utilities shall be subject to the Minimum RoW/PUE Area Requirements and PUE Restrictions:
- a. Minimum RoW Area Requirements: Public Utilities shall be installed within in existing RoW/PUE in alignments/locations that meet the following minimum RoW/PUE areas surrounding the Public Utilities, and/or additional RoW/PUE shall be dedicated/granted to the City to provide the following minimum RoW/PUE areas surrounding the Public Utilities:
    - i. For each main, the RoW/PUE Area shall be a minimum of 10 feet wide.
  - b. PUE Restrictions: The Minimum PUE Area required surrounding Public Utilities shall be subject to the following restrictions:
    - i. The Minimum PRow Area required surrounding Public Utilities shall not contain:
      - A. Any storm water quality improvements (infiltration, detention, retention, bioswale, etc);
      - B. Landscaping with thick or intrusive root structures,
      - C. Any trees;
      - D. Any private utilities, plumbing lines, private fire system, or irrigation lines; or,
      - E. Any permanent structures or overhangs of permanent structures.
    - ii. Minimum Separations: Within a PUE, all Department of Drinking Water (DDW) Water Main Separations per California Code of Regulations (CCR) §64572 shall be met for all Public Potable Water Mains and Services between: all Public City Utilities; Non-City Utilities; and Private Utilities. Additionally, the following Minimum Separations shall be met:
      - A. At minimum there shall be a 4 feet horizontal separation between each utility as measured between the outside walls of the utility pipelines, or in the case of a Joint Utility Trench, between the outside edge of the Joint Utility Trench and the outside wall of the Utility Pipeline.
      - B. Public Utility mains shall not be located behind curb or under curb & gutter and shall be located at minimum of 5 feet from curbface.

***Sanitary Sewer Conditions (Section 2.C): The Applicant shall comply with the following:***

5. Sanitary Sewer Service:
- a. Each lot and its onsite private sewer system shall discharge wastewater to the Public Sanitary Sewer System through a Public Sewer Lateral per Standard #2003. Each sewer lateral shall have a cleanout located at the back of the PUE outside of the PUE.
  - b. Public Sewer Laterals and Storm Water Quality Improvements: No storm water quality improvements (infiltration, detention, retention, bioswale, etc) shall be installed above or within 5 feet of any Public Sewer Lateral.

***Potable Water Conditions (Section 2.D): The Applicant shall comply with the following:***

6. Potable Water Service:
- a. Domestic Service: Each lot shall have a its own domestic water service and meter connected to the Public Potable Water System. The meter shall be located a minimum of 5 feet away from wing of the drive approach.



- b. Relocated Services: For any existing service with appurtenances to be relocated, the service shall be abandoned back to the main connection and the service and appurtenances shall be installed new per related City Standards.
- c. Division of Drinking Water Separation Requirements: The final utility design shall comply with all Division of Drinking Water (CCR §64572) Separation Requirements.



# CITY OF ONTARIO

## MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)  
Rudy Zeledon, Planning Director (Copy of memo only)  
Diane Ayala, Advanced Planning Division (Copy of memo only)  
Charity Hernandez, Economic Development  
James Caro, Building Department  
Raymond Lee, Engineering Department  
Jamie Richardson, Landscape Planning Division  
Dennis Mejia, Municipal Utility Company  
Jeremy Phillips, Police Department  
Paul Erhman, Deputy Fire Chief/Fire Marshal  
Jay Bautista, Traffic/Transportation Manager  
Lorena Mejia, Airport Planning  
Tricia Espinoza, Engineering/NPDES  
Angela Magana, Community Improvement (Copy of memo only)  
Jimmy Chang, IPA Department  
Ben Mayorga, Integrated Waste

FROM: Robert Morales, Assistant Planner

DATE: June 17, 2022

SUBJECT: FILE #: PMTT22-020 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
  - Both DAB and Planning Commission actions are required
  - Only Planning Commission action is required
  - DAB, Planning Commission and City Council actions are required
  - Only Zoning Administrator action is required

**PROJECT DESCRIPTION:** A Tentative Parcel Map (TPM 20375) to subdivide 0.423 acres of land into 2 parcels located at 121 E 6th, Street within the LDR-5 (Low-Density - 2.1 to 5.0 du/ac) zoning district (APN: 1047-241-05).

- The plan does adequately address the departmental concerns at this time.
  - No comments
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
  - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

**Broadband Operations**  
Department

*Cameron Chadwick*  
Signature

\_\_\_\_\_  
Title

07/19/2022  
Date



# CITY OF ONTARIO

## MEMORANDUM

### DEVELOPMENT PLAN REVIEW COMMENTS Broadband Operations Section

**DATE:** 07-19-22

**PROJECT:** PMTT22-020

**LOCATION:** 121 E 6th st

**PROJECT ENGINEER:** Jeff

**BROADBAND PLAN CHECKER:** Cameron Chadwick - CChadwick@ontarioca.gov

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#### **A. General Comments:**

1. The applicant/developer shall respond to these comments as well as the comments provided by the Environmental Section, Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations and address all of them prior to the next submittal.
2. The applicant/developer shall address all additional redlined comments on the plans attached.
3. Provide plans in digital format (PDF) for future submittals.
4. Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines

#### **B. The following items will be incorporated in the Conditions of Approval Report prior to the Development Advisory Board and/or Zoning Administrator Hearing upon all departments' comments being satisfactorily addressed:**

1. Project shall be designed and constructed to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole in the Right-of-Way (ROW) and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
2. Contractor is responsible for locating and connecting conduit to existing OntarioNet hand holes on adjacent properties within a reasonable distance. There should be no "Gaps" in conduit between the contractor's development and the adjacent property. OntarioNet hand holes are typically located in the ROW at the extreme edge of a property.
3. Where a joint telecom or street light street crossing is required, include (2) 2" HDPE SDR-11 conduits or (1) 4" schedule 80 conduit sleeve. Terminate the street crossing conduit(s) in a new HH-3/22 OntarioNet hand hole in the right of way
4. The City requires a public utility easement for fiber optics on all private aisles/alley ways.
5. Hand holes - Design and install OntarioNet fiber optic hand hole HH-FP (10x00x10), HH-1 (13x24x18), HH-2 (17x30x24), HH-2A (24x36x30), HH-3 (30x48x36) and/or HH-4 (36x60x36) as needed. Respectively, Newbasis Part # PLA100010T-00002, PCA132418-00006, PCA-173024-90116, PCA-243630-90064, PCA-304836-90244 and PCA-366036-90146 or equivalent as specified per City Standard 1316. Conduits sweeping into hand holes shall enter in flush with the cut-out mouse holes aligned parallel to the bottom of

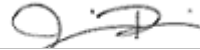
the box and come in perpendicular to the wall of the box. Conduits shall not enter at any angle other than parallel. Provide 5-foot minimum clearance from existing/proposed utilities. All hand holes will have ¼-inch galvanized wire between the hand holes and the gravel it is placed on.

6. ROW Conduit – Design and install fiber optic conduit at a minimum depth of 36-inch. Trenching shall be per City Standard 1306. Install (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct and (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange with Black Stripe) duct. Conduit(s) between ROW hand holes and hand holes on private property shall be 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct.
7. Building Entrance (Single Family) – Design and install 0.75-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct from hand holes on property or hand holes in the ROW. Consult City's Fiber Team for design assistance.
8. Building Entrance (Multi-family and Commercial) - From the nearest handhole to the building entrance, design and install fiber optic conduit at a minimum depth of 36-inches. Trenching shall be per City Standard for Commercial Buildings. (1) 2-inch HDPE SDR-11 (Smoothwall) roll pipe (Orange) duct. Install locate/tracer wires minimum 12AWG within conduit bank and fiber warning tape 18-inch above the uppermost duct
9. Multi-family and commercial properties shall terminate conduit in an electrical room adjacent to the wall no less than five inches above the finished floor. A 20" width X length 36" space shall be reserved on the plywood wall for OntarioNet equipment. This space shall be labeled "OntarioNet Only". Ontario Conduit shall be labeled "OntarioNet"
10. A minimum 1.5-inch joint use telecommunications conduit with pull-rope from the single-family, multi-family or commercial building communal telecom/electrical room/closet to each multi-family or commercial building unit shall be installed. See Structured Wiring Checklist on City's website for additional details.
11. Warning Tape - Contractor shall supply and install an approved non-detectable warning tape 18-inch above the uppermost conduit when backfilling trenches, pits or excavations greater than 10' in length. Warning Tape shall be non-detectable, Orange in color, 4-inch minimum width, 4 mil, 500% minimum elongation, with bold printed black letters "CAUTION - BURIED FIBER OPTIC CABLE BELOW" printed in bold black lettering no less than 2-inch high.
12. All hand holes, conduits, conduit banks, materials and installations are per the City's Fiber Optic Master Plan and City Fiber Optic Cable and Duct Standards. All hand holes, conduits and ducts shall be placed in the public right of way.
13. All unused conduits/ducts/microducts shall be protected with duct plugs that provide a positive seal. Ducts that are occupied shall be protected with industry accepted duct seal compound.
14. Locate/Tracer Wire - Conduit bank requires (1) 12AWG high strength (minimum break load 452#) copper-clad steel with 30mil HDPE orange insulation for locate/tracer wire. Contact City's Fiber Team for tracer wire specifications and see note 8.
15. Multi-family dwellings are considered commercial property.
16. Refer to the In-tract Fiber Network Design guideline on the City's website for additional in-tract conduit guidelines.

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

**Sign Off**



Jamie Richardson, Sr. Landscape Planner

10/28/2022

Date

Reviewer's Name:

**Jamie Richardson, Sr. Landscape Planner**

Phone:

**(909) 395-2615**

D.A.B. File No.:

PMTT22-020

Related Files:

Case Planner:

Robert Morales

Project Name and Location:

Tentative Parcel Map to subdivide 0.423 Acres  
 121 E 6<sup>th</sup> Street

Applicant/Representative:

Gary Andreasen [jaryand@aeicivil.com](mailto:jaryand@aeicivil.com) (909) 623-1595 Ext 23  
 195 N Euclid Avenue, Suite 101  
 Upland, CA 91786



**A Tentative Tract/Parcel Map (dated 10/6/2022) has been approved, considering that the following conditions below be met upon submittal of the landscape construction documents.**



**A Tentative Tract Map/Parcel (dated) has not been approved. Corrections noted below are required before DAB approval.**

**CORRECTIONS REQUIRED**

1. Locate utilities to minimum clearances to allow parkway trees. Show and note a 10' parkway tree space, 5' clearance on each side of the tree from any utility or hardscape including water, sewer, drain lines, and driveways; and min. 10' clear from street lights. Parkway trees are to be 30' apart.
2. Show and identify any on-site stormwater infiltration areas or stormwater infiltration devices proposed in parkways or other landscape areas.
3. 269 inches of trunk replacement is required; see below for mitigation measures:
  - a. 269 new 15-gallon trees min 1" diameter trunk, in addition to trees required.
  - b. 179 new 24" box trees min 1.5" diameter trunk, in addition to trees required.
  - c. Provide a matrix showing upsized trees on the plan one size larger such as 15 gallons to 24" box, or 24" to 36" box size.
  - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal," approved certified arborist plant appraiser, may be equal to the value of the installation cost of planting, fertilizing, staking, and irrigating 15-gallon trees (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items. A total monetary value of \$26,900.

**On Grading or Utility Construction Plans:**

4. Stormwater infiltration devices located in parkways or other landscape areas shall be routed to this department to be reviewed and approved before permit approval or installation.
5. Note for compaction to not be greater than 85% at landscape areas; all finished grades 1 1/2" below finished surfaces; landscaped slopes to be max 3:1.

6. Show or note transformers shall be located in planter areas and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformers greater than 4' high. Coordinate with landscape plans. Locate on level grade.
7. Show or note backflow devices shall be located in planter areas and set back min 3' from paving. Locate on level grade. Coordinate with landscape plans.
8. Provide a utility clear space 8' wide in parkways and 30' apart for street trees. Move water meters, drain lines, and light standards to the minimum spacing to allow space for street trees.
9. Show light standards 15' away from required tree locations.
10. Wall footings shall not restrict landscape; max 12" in front of footing with 12" of cover.
11. Stormwater infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division before installation.
12. After a project's entitlement approval, the applicant shall pay all applicable fees at a rate established by resolution of the City Council.

Once items are complete, you may email an electronic set to:

[landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PMTT22-020

Address: 121 East 6th Street

APN: 1047-241-05

Existing Land Use: Existing Single Family Home

Proposed Land Use: Tentative Parcel Map to subdivide 0.423 acres into 2 parcels

Site Acreage: 0.423 Proposed Structure Height: Existing Building

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Robert Morales

Date: 1/30/2023

CD No.: 2022-057

PALU No.: N/A

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="checkbox"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="checkbox"/> Zone 4		Allowable Height: <u>130 FT</u>	
<input type="checkbox"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6

Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  
  Exempt from the ALUCP  
  Consistent  
  Consistent with Conditions  
  Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: \_\_\_\_\_

*Lorena Mejia*

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2022-057  
PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

1. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

#### NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.





# CITY OF ONTARIO

## MEMORANDUM

**TO:** Robert Morales, Assistant Planner  
Planning Department

**FROM:** Paul Ehrman, Sr. Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** June 23, 2022

**SUBJECT:** PMTT22-020 - A Tentative Parcel Map (TPM 20587) to subdivide 0.423 acres of land into 2 parcels located at 121 E 6th, Street within the LDR-5 (Low-Density - 2.1 to 5.0 du/ac) zoning district (APN: 1047-241-05).

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The plan **does** adequately address Fire Department requirements at this time.

No comments.



# DEVELOPMENT ADVISORY BOARD DECISION

May 15, 2023

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

**DECISION NO.:** [insert #]

**FILE NO.:** PDEV22-038

**DESCRIPTION:** A public hearing to consider a Development Plan to construct 36 single-family detached 4-pack dwellings with related on-site improvements for a subdivision previously approved with Tentative Tract Map No. 18916 (File No. PMTT21-011), on approximately 11.05 acres of land located on the west side of Archibald Avenue and approximately 575 feet south of Chino Avenue within Planning Area 2 (Neighborhood 7) of the Countryside Specific Plan (APN: 0218-131-34); **submitted by KB Homes Coastal Inc. Planning Commission action is required.**

## PART 1: BACKGROUND & ANALYSIS

KB HOME COASTAL INC., (herein after referred to as "Applicant") has filed an application requesting Development Plan approval, File No. PDEV22-038, as described in the subject of this Decision (herein after referred to as "Application" or "Project").

**PROJECT SETTING:** The Project site is comprised of 11.05 gross acres of land located on the west side of Archibald Avenue and approximately 575 feet south of Chino Avenue, as depicted in Exhibit A: Project Location Map, attached. Existing land uses, General Plan and zoning designations, and specific plan land uses on and surrounding the project site are as follows:

	<i>Existing Land Use</i>	<i>Policy Plan Land Use Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use Designation</i>
Site:	Vacant	Low Density Residential (LDR): 2.1 - 5 DU/AC & Open Space – Non-Recreation (OS-NR)	Countryside Specific Plan	Neighborhood 7 (RD-Alley Loaded)
North:	Religious Place of Worship & Preschool	Low Density Residential (LDR): 2.1 - 5 DU/AC	Countryside Specific Plan	Neighborhood 7 (RD-Alley Loaded)
South:	Residential Subdivision & Deer Creek Channel	Low Density Residential (LDR): 2.1 - 5 DU/AC & Open Space – Non-Recreation (OS-NR)	Countryside Specific Plan	Neighborhood 6 (Cluster Court)
East:	Southern California Edison ("SCE")	Low Density Residential (LDR): 2.1 - 5 DU/AC &	LDR-5 (Low Density Residential—2.1 to 5.0	N/A

	<i>Existing Land Use</i>	<i>Policy Plan Land Use Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use Designation</i>
	easement & Residential Subdivision	Open Space – Non-Recreation (OS-NR)	DU/Acre)) & UC (Utilities Corridor)	
West:	Southern California Edison (“SCE”) easement & Residential Subdivision	Low Density Residential (LDR): 2.1 - 5 DU/AC & Open Space – Non-Recreation (OS-NR)	Countryside Specific Plan	Neighborhood 5 (Z-Lot)

**PROJECT ANALYSIS:**

(1) Background — The Countryside Specific Plan (File No. PSP04-001) and related Environmental Impact Report (State Clearinghouse No. 2004071001) was approved by the City Council on April 18, 2006. The Countryside Specific Plan established the land use designation, development standards, and design guidelines for the 178-acre Specific Plan area, which includes the potential development of 825 residential dwelling units and approximately 8.5 acres of neighborhood paseos and two centrally located parks (see Figure 1: Countryside Specific Plan Land Use Map).

On October 20, 2021, the Zoning Administrator approved a Specific Plan Minor Amendment (File No. PSPA19-001) to modify the Countryside Specific Plan to allow for: (1) the introduction of Neighborhood 3A (“Cluster Court 2”); (2) an increase in the number of allowed dwelling units from 819 to 825; (3) the use of Cluster Court product within the proposed Neighborhood 3A and existing Neighborhood 7 of the Countryside Specific Plan, (4) minor text amendments to the RD Z-Lot and RD Cluster Court Site Development Standards; and (5) accommodate related text and exhibit updates.

On November 23, 2021, the Planning Commission approved Tentative Tract Map No. 18916 (File No. PMTT21-011) for Planning Area 2 (Neighborhood 7), which subdivided the 11.05-acre Project site into 36 numbered lots and two lettered lots to facilitate future residential development. The Final Map application is currently under review and is expected to be considered by the City Council after June 2023.

On August 15, 2022, the Applicant submitted the subject Development Plan (File No. PDEV22-038) to facilitate the development of Tract Map No. 18916 with 36 single-family detached 4-pack dwellings on 11.05 acres of land within Planning Area 2 (Neighborhood 7, RD-Alley Loaded) of the Countryside Specific Plan.

Site Design/Building Layout —The proposed 4-Pack Courtyard cluster product provides single-family detached homes with a driveway, two-car garage, and private yard area.

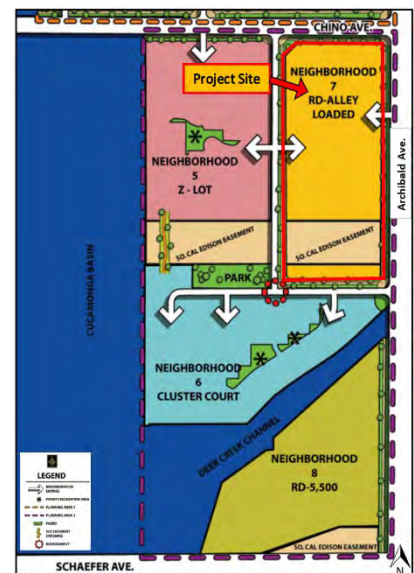


Figure 1: Countryside Specific Plan Land Use Map

The site design is characterized by motor court drives constructed with enhanced paving consisting of integral-colored concrete with a medium release finish and color concrete crosswalks that provides access to each unit’s two-car garage and driveways, for a total of four parking spaces for each unit. The residences that are located within the interior of the cluster will have front entryways facing the motor court drives, and the residences with frontage along East Towhee Street and Dolomite Street will have their front entrance doors facing the street (see Exhibit B—Site Plan, attached).

The Project proposes two-story floor plans, with three elevations per plan. The Countryside Specific Plan requires a minimum lot size of 2,925 square feet and a maximum 60 percent lot coverage. The Project typical lot sizes range from 4,564 to 5,833 square feet, with a lot coverage ranging from 27 percent to 38 percent (without the optional patio) and 30 percent to 42 percent (with the optional patio). Homebuyers have the option of selecting an optional attached patio that is designed with the same architectural characteristics as the main residence. All plans incorporate various design features, such as a mix of one- and two-story massing, varied entries, second floor laundry facilities, loft, kitchen, open dining, and great room/living areas (see Attachment A- Full set of Elevations and Floor Plans). The proposed floor plans consist of the following:

**Table 1: Floor Plan Summary**

<b>Plan Type (No. of Units)</b>	<b>Gross Floor Area</b>	<b>Key Features</b>
1 (9 DUs)	2,380 SF	3 bedrooms, 3 bathrooms, loft, 4th bedroom/optional den
2 (4 DUs)	2,524 SF	4 bedrooms, 3 bathrooms, den/optional 5th bedroom, loft/optional 6th bedroom
3 (9 DUs)	2,609 SF	4 bedrooms, 3 bathrooms, den/optional 5th bedroom, loft/optional 6th bedroom
4 (14 DUs)	2,791 SF	5 bedrooms, 3 bathrooms, loft, den/optional office

The project site contains an approximately three-acre Southern California Edison (“SCE”) easement, transecting the project site in an east-west direction, and contains two transmission towers and high voltage power lines. The 4-pack clusters will be constructed to the north and south of the SCE easement. A minor recreational facility, proposed along the north boundary of the SCE easement, consists of a 30-foot-wide walking paseo that contains a meandering sidewalk, landscaping, a dog park, enhanced entryways, and seating areas. A Homeowner’s Association (HOA) is proposed to maintain the common and private open space areas.

(2) Site Access/Circulation — Primary Access to the project site will be provided from Kinglet Avenue to the west and Archibald Avenue to the east. The interior streets, East Towhee Street and Dolomite Street, are parallel streets that travel in an east-west direction and intersect with both Kinglet Avenue and Archibald Avenue. The proposed 4-pack clusters will contain motor court drives that provide access to the residential unit’s

garage and the driveway. Pedestrian circulation is provided via the new sidewalks, which connect to the active walking trails located within and surrounding the community.

(3) Parking — The Countryside Specific Plan requires a minimum two-car garage (20 ft. by 20 ft.) for single-family residential units. The Project provides the required 2-car garage in addition to two driveway spaces and 44 on-street parking spaces along Towhee Street, Kinglet Avenue, and Dolomite Street (see Exhibit C - Parking Exhibit). The Project requires a total of 72 parking spaces, and a total of 188 parking spaces will be provided, exceeding the minimum requirements by 116 parking spaces, as summarized in Table 2, below.

**Table 2: Parking Summary**

<b><i>Parking Space</i></b>	<b><i>Required</i></b>	<b><i>Provided</i></b>
Garage Spaces (2/unit)	72	72
Driveway Spaces (9'×18')	0	72
Street Spaces (8'×24')	0	44
<b>Total</b>	<b>72</b>	<b>188</b>

(4) Architecture — A total of three architectural styles, Spanish Colonial, Craftsman, and American Farmhouse, are proposed for the Project. The architectural styles embody a variety of building material and designs among the different plan types (see Exhibit D – Elevations/Floor Plans and Exhibit E – Street Scene).

As proposed, the building elevations are compatible with the surrounding residential development in that the architectural style of the homes includes a variety of design elements and articulated massing such as vertical and horizontal offsets on the exterior wall planes, varied roof planes and roof pitches, and recessed doorways, as well as other features. A total of four (4) different floor plans are provided with all the units two (2) stories high and a height that ranges from 23-feet 11-inches to 28-feet 7-inches. The architectural styles include the following features:

- Spanish Colonial: Varying gable roof with concrete “S” tile roofing, arched entry ways, deep recessed openings, wood fascia board, trim, decorative shutters, decorative ceramic tiles, and decorative gables and end details.
- Craftsman: Varying gable and shed roofs with concrete flat roof tiles; stucco exteriors with horizontal wood siding details, trim along the windows, decorative shutters, stone veneer at the base of the building, and fiber cement shake shingle siding as well as a stucco finish.
- American Farmhouses: shed roofs with concrete flat roof tiles; wood posts, wood trim and a stucco finish, fiber cement siding with vertical battens, wood trim along the windows, and decorative gabled end details.

The architectural design provides a variety of material based on the architectural style and unit type with additional enhancement in areas that are visible from public vantage points (see Exhibit F – Material & Color Sample Board). Enhanced architectural treatment is provided on the elevations that are visible from public vantage points, including the public right-of-way, the elevations facing the SCE easement, and the paseo through the incorporation of features such as additional shutters, trim, siding, and pop outs.

Landscaping/Open Space — Within the Project site, a combination of trees, shrubs and groundcover is proposed (see Exhibit G – Landscaping Plans). The Landscaping Plan includes a variety of 24-inch box, 36-inch box, and 48-inch box trees. The proposed trees include the Western Redbud, Raywood Ash, Australian Willow, Muskogee Crape Myrtle Standard, Muskogee Crape Myrtle Multi-Trunk, Saratoga Hybrid Laurel – Standard Form, Afghan Pine, California Sycamore, Fern Pine, Coast Live Oak, and the Bay Laurel. The trees located along the paseo will include a mix of Lagerstroemia and Cercis trees with a maximum height of 15 feet per SCE requirements. Front yard landscaping will be provided and maintained by the Homeowner’s Association (HOA) as indicated in the Countryside Specific Plan. Each residential lot will have a private rear yard area that ranges from 897 square feet to 2,158 square feet.

The landscaping located on the west side of Archibald Avenue will be constructed in accordance with the Archibald Streetscape Master Plan, which requires a 27-foot-wide landscaped parkway with a single row of street trees, an 8-foot-wide bikeway/pedestrian trail, and a 5-foot-wide meandering sidewalk. Trees and shrubs will also be planted in a series of layers to add texture and color.

TOP 2050 Policy PR-1.6 requires new development to provide a minimum of two acres of developed private park space per 1,000 residents. The park area requirement for this Project is 0.274 acres, based on 36 dwelling units and the rate of 3.806 occupants/dwelling for single-family units less than 25 du/acre, and multiplied by 0.002. A total of 0.88 acres of open space is provided within Lots A, B, C, and D, which include open space areas and the paseo. The paseo is approximately 12,450 square feet (or 0.28 acres) in size. The paseo is designed with a dog park with low fencing, a 5-foot-wide decomposed granite multi-use trail, rest areas with seating boulders and trees, and two pedestrian entry portals (see Exhibit G – Paseo Enlargement). The entry portals will have field/flagstone veneer pilasters with an architectural concrete cap that is designed to match the neighborhood entry monumentation.

The Countryside Specific Plan area will have a paseo network that will provide access to neighborhood parks and all neighborhoods within the Specific Plan. The paseo will provide connectivity throughout the community with the use of the multi-purpose pedestrian trail and the bicycle paths that are located within the community. The paseo system begins at Riverside Drive (within Planning Area 1) and travels south crossing Chino Avenue into Planning Area 2. The Project site will have access to neighborhood parks via the multi-purpose trail and street networks.

Walls and fences are proposed along the perimeter of the Project site and along the interior residential property lines, consistent with the Countryside Specific Plan. A 6-foot-high split face block wall with a decorative cap is proposed in areas that are visible from the public right-of-way. Along interior residential property lines, the developer will install a 6-foot-high precision block wall with a precast precision cap. Along the segment of Archibald Avenue, the wall height will increase from 6 feet to 8 feet to provide a noise barrier for the residential lots that are adjacent to Archibald Avenue. A neighborhood entry monumentation is proposed along the eastern portion of Dolomite Street, which includes a 5-foot-tall pedestrian entry pilaster with field/flagstone veneer pilasters and an architectural concrete cap with accent pilasters that is designed to match the paseo entry pilasters (see Exhibit H –Wall Plan).

(5) Noise Study – A Noise Impact Study was prepared for the proposed Project to evaluate the future exterior and interior long-term noise impacts along Archibald Avenue (see Attachment B – Noise Impact Analysis). The Noise Impact Analysis, prepared by Urban Crossroads dated March 7, 2023, indicates that an 8-feet high noise barrier will be required for the outdoor living areas (rear yards) on all lots adjacent to Archibald Avenue. The Applicant will be required to construct an 8-foot-tall split face masonry block wall on all lots adjacent to Archibald Avenue to be consistent with the project design features indicated in the Noise Study. Through the incorporation of the 8-foot-high block wall the future exterior noise levels will range from 64.6 to 64.8 dBA CNEL and satisfy the City of Ontario 65 dBA CNEL exterior noise levels standard for residential land uses.

To meet the City of Ontario 45 dBA CNEL interior noise level criteria, first-floor receivers on lots adjacent to Archibald Avenue will require a Noise Reduction of up to 19.8 dBA and second story receivers on lots adjacent to Archibald Avenue will require a Noise Reduction of up to 29.7 dBA. The following noise abatement measures will be incorporated and verified during the plan check process:

- **Windows & Glass Doors:** First story facades on lots adjacent to Archibald Avenue require windows and glass doors with well-fitted, well-weather-stripped assemblies with minimum sound transmission class (STC) ratings of 27 and a means of mechanical ventilation (e.g., air conditioning). Second Story facades on lots adjacent to Archibald Avenue require windows and glass doors with well-fitted, well-weather-stripped assemblies with minimum STC ratings of 32 and a means of mechanical ventilation (e.g., air conditioning).
- **Doors (Non-Glass):** All exterior doors shall be well weather-stripped. Well-sealed perimeter gaps around the doors are essential to achieve the optimal STC rating.
- **Walls:** At any penetrations of exterior walls by pipes, ducts, or conduits, the space between the wall and pipes, ducts, or conduits shall be caulked or filled with mortar to form an airtight seal.

- Roof: Roof sheathing of wood construction shall be per manufacturer's specification or caulked plywood of at least one-half inch thick. Ceilings shall be per manufacturer's specification or well-sealed gypsum board of at least one-half inch thick. Insulation with at least a rating of R-19 shall be used in the attic space.
- Ventilation: Arrangements for any habitable room shall be such that any exterior door or window can be kept closed when the room is in use and still receive circulated air. A forced air circulation system (e.g., air conditioning) or active ventilation system (e.g., fresh air supply) shall be provided which satisfies the requirements of the Uniform Building Code.

(6) Signage — Any proposed signage shall comply with the sign regulations provided in the Countryside Specific Plan and the Ontario Development Code Division 8.01.

(7) Utilities (drainage, sewer) — All major backbone improvements and interior site improvements will be constructed in congruence with the related Tract Map. The Applicant will also obtain an encroachment permit to conduct work within the public rights-of-way. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. Additionally, the Project is consistent with the previously approved Development Agreement (File No. PDA21-004) that required all major backbone infrastructure improvements within the Countryside Specific Plan.

(8) Covenants, Conditions and Restrictions ("CC&Rs") — In November of 2021, Planning Commission approved the related Tentative Tract Map No. 18916 (File No. PMTT21-011) for Planning Area 2 (Neighborhood 7). As part of the Tentative Tract Map Conditions of Approval, CC&Rs were required to be prepared and recorded with the Final Tentative Tract Map. The CC&Rs will outline the maintenance responsibilities for the open space areas, recreation amenities, drive aisles, utilities, and upkeep of the entire site, to ensure the on-going maintenance of the common areas and facilities

**PUBLIC NOTIFICATION:** Public notification is not required, as the Development Advisory Board is acting in its capacity as an advisory body to the Planning Commission. Public notification is required prior to the Planning Commission hearing on the Project.

**CORRESPONDENCE:** As of the preparation of this Decision, Planning Department staff has not received any written or verbal communications from the owners of properties surrounding the project site or from the public in general, regarding the subject application.



**AGENCY/DEPARTMENT REVIEWS:** Each City agency/department has been provided the opportunity to review and comment on the subject application and recommend conditions of approval to be imposed upon the application. At the time of the Decision preparation, recommended conditions of approval were provided and are included with this Decision.

**AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE:** The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan.

On April 19, 2011, the City Council of the City of Ontario approved and adopted the ONT ALUCP, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the Development Advisory Board has reviewed and considered the facts and information contained in the Application and supporting documentation against the ONT ALUCP compatibility factors, including [1] Safety Criteria (ONT ALUCP Table 2-2) and Safety Zones (ONT ALUCP Map 2-2), [2] Noise Criteria (ONT ALUCP Table 2-3) and Noise Impact Zones (ONT ALUCP Map 2-3), [3] Airspace protection Zones (ONT ALUCP Map 2-4), and [4] Overflight Notification Zones (ONT ALUCP Map 2-5). As a result, the Development Advisory Board, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ONT ALUCP.

**COMPLIANCE WITH THE ONTARIO PLAN:** The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (general plan), and City Council Priorities components of The Ontario Plan ("TOP"). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

(1) City Council Goals.

- (a) Invest in the Growth and Evolution of the City's Economy
- (b) Operate in a Businesslike Manner
- (c) Focus Resources in Ontario's Commercial and Residential Neighborhoods
- (d) Invest in the City's Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- (e) Ensure the Development of a Well Planned, Balanced, and Self-Sustaining Community in the New Model Colony

(2) Vision.

**Distinctive Development:**

- Commercial and Residential Development
  - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

(3) Governance.

**Decision Making:**

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
  - G 1-2. Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

(4) Policy Plan (General Plan)

**Land Use Element:**

- Goal LU-1 Balance: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
  - LU-1.1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, foster the development of transit, and support the expansion of the active and multimodal transportation networks throughout the City.
  - LU-1.6 Complete Community. We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.
- Goal LU-2 Compatibility: Compatibility between a wide range of uses and a resultant urban patterns and forms.
  - LU-2.6 Infrastructure Compatibility. We require infrastructure to be aesthetically pleasing and in context with the community character.

### **Housing Element:**

- Goal H-2 Housing Supply & Diversity: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H-2.4 Ontario Ranch. We support a premier lifestyle community in the Ontario Ranch, distinguished by diverse housing, highest design quality, and cohesive and highly amenitized neighborhoods.

- H-2.5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices, and other best practices.

- Goal H-5 Special Needs: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age, or other status.

- H-5.2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation, and other amenities.

### **Community Economics Element:**

- Goal CE-1 Complete Community: A complete community that provides for all incomes and stages of life.

- CE-1.6 Diversity of Housing. We collaborate with residents, housing providers, and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to encourage the development of housing supportive of our efforts to attract business in growing sectors of the community while being respectful of existing viable uses.

- Goal CE-2 Placemaking: A City of distinctive neighborhoods, districts, corridors, and centers where people choose to be.

- CE-2.1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

- CE-2.2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional, and sustainable places that will compete well with their competition within the region.

➤ CE-2.4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE-2.5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

**Safety Element:**

▪ Goal S-1 Seismic & Geologic Hazards: Minimized risk of injury, loss of life, property damage, and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S-1.1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

**Community Design Element:**

▪ Goal CD-1 Image & Identity: A dynamic, progressive city containing distinct and complete places that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD-1.1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing, enhancing, and preserving the character of our existing viable neighborhoods.

➤ CD-1.3 Existing Neighborhoods. We require the existing character of viable residential and non-residential neighborhoods be preserved, protected, and enhanced.

▪ Goal CD-2 Design Quality: A high level of design quality resulting in neighborhoods, public spaces, parks, and streetscapes that are attractive, safe, functional, human-scale, and distinct.

➤ CD-2.1 Quality Building Design and Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide context-appropriate scale and proportion;
- A true architectural style which is carried out in plan, section, and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are articulated, high quality, durable, and appropriate for the architectural style.

➤ CD-2.2 Neighborhood Design. We create distinct residential neighborhoods that promote a sense of community and identity by emphasizing access, connectivity, livability, and social interaction through such elements as:

- A pattern of smaller, walkable blocks that promote activity, safety, and access to nearby amenities and services;
- Varied parcel sizes and lot configurations to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable traffic flows and emergency evacuation access;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the "outdoor living room"), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb and designed to maximize safety, comfort, and aesthetics for all users.

➤ CD-2.7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping, and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials, and construction techniques.

➤ CD-2.8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintaining visibility and accessibility, and using lighting.

➤ CD-2.9 Landscape Design. We encourage durable, sustainable, and drought-tolerant landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD-2.10 Parking Areas. We require all development, including single-family residential, to minimize the visual impact of surface, structured, and garage parking areas visible from the public realm in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include:

- Surface parking: Shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field;
- Garage parking: providing access to single-family residential garages through alley access, recessing garages from the frontage to emphasize front doors or active living spaces.

➤ CD-2.13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD-3 Urban, Mixed Use, and Transit-Oriented Place Types: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD-3.3 Complete and Connected Network. We require that pedestrian, vehicular, and bicycle circulation on both public and private property be coordinated to provide connections internally and externally to adjacent neighborhoods and properties (existing and planned) through a system of local roads and trails that promote walking and biking to nearby destinations (including existing and planned parks, commercial areas, and transit stops) and are designed to maximize safety, comfort, and aesthetics.

➤ CD-3.4 Context-Aware and Appropriate Design. We require appropriate building and site design that complements existing development, respects the intent and identity of the Place Type, and provides appropriate transitions and connections between adjacent uses to ensure compatibility of scale, maintain an appropriate level of privacy for each use, and minimize potential conflicts.

➤ CD-3.5 Active Frontages. We create lively pedestrian streetscapes by requiring primary building, business, and residential entrances, outdoor dining, and storefronts be located on ground floors adjacent to sidewalks or public spaces and designed to maximize safety, comfort, aesthetics, and the intended functionality (as defined by the Place Type).

▪ Goal CD-5 Protection of Investment: A sustained level of maintenance and improvement of properties, buildings, and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD-5.1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD-5.2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

**HOUSING ELEMENT COMPLIANCE:** The project is consistent with the Housing Element of the Policy Plan (general plan) component of The Ontario Plan, as the project site is not one of the properties in the Housing Element Sites contained in Tables B-1 and B-2 (Housing Element Sites Inventory) of the Housing Element Technical Report.

***PART 2: RECITALS***

WHEREAS, the Application is a Project pursuant to the California Environmental Quality Act (Public Resources Code Section 21000 et seq.) ("CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the Countryside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004071001) was certified on April 18, 2006 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Countryside Specific Plan Environmental Impact Report (State Clearinghouse No. 2004071001) was certified by the City Council on April 18, 2006 ("Certified EIR") in conjunction with File No. PSP04-001, in which development and use of the Project site was discussed; and

WHEREAS, the environmental impacts of this Project were thoroughly analyzed in the Certified EIR, which concluded that implementation of the Project could result in a number of significant effects on the environment and identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Development Advisory Board (hereinafter referred to as "DAB") the responsibility and authority to review and make recommendation to the Planning Commission on the subject Application; and

WHEREAS, all members of the DAB of the City of Ontario were provided the opportunity to review and comment on the Application, and no comments were received opposing the proposed development; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan

(hereinafter referred to as "ONT ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on May 15, 2023, the DAB of the City of Ontario conducted a hearing on the Application and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Decision have occurred.

### ***PART 3: THE DECISION***

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED AND DECIDED by the Development Advisory Board of the City of Ontario as follows:

SECTION 1: Environmental Determination and Findings. As the recommending body for the Project, the DAB has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the DAB finds as follows:

(1) The environmental impacts of this Project were previously reviewed in conjunction with File No. PSP04-001, a Specific Plan for which an Environmental Impact Report (State Clearinghouse No. 2004071001) ("Certified EIR") was adopted by the City Council on April 18, 2006; and

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA, and the Guidelines promulgated thereunder, and the City of Ontario Local CEQA Guidelines; and

(4) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(5) The proposed Project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.



SECTION 2: Subsequent or Supplemental Environmental Review Not Required.

Based on the information presented to the DAB, and the specific findings set forth in Section 1, above, the DAB finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

- (1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and
- (2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and
- (3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:
  - (a) The Project will have one or more significant effects not discussed in the Certified EIR; or
  - (b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or
  - (c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or
  - (d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: ALUCP Compliance. The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending body for the Project, the DAB has reviewed and considered the facts and information contained in the

Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the DAB, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: Concluding Facts and Reasons. Based upon the substantial evidence presented to the DAB during the above-referenced hearing and upon the facts and information set forth in Parts I (Background and Analysis) and II (Recitals), above, and the determinations set forth in Sections 1 through 3, above, the DAB hereby concludes as follows:

(1) *The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.* The proposed Project is located within the (LDR) Low Density Residential (2.1-5 DU/AC) and (OS-NR) Open Space – Non-Recreation land use district of the TOP 2050 Policy Plan Land Use Map, and Neighborhood 7 (RD-Alley Loaded) land use district of the Countryside Specific Plan. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan; and

(2) *The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.* The Project has been designed consistent with the requirements of the City of Ontario Development Code and Neighborhood 7 (RD-Alley Loaded) land use district of the Countryside Specific Plan, including standards relative to the particular land use proposed (residential), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions; and

(3) *The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the Project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed Project.* The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Countryside Specific Plan are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Countryside Specific Plan; and

(4) *The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.* The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Countryside Specific Plan that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (residential). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Countryside Specific Plan.

SECTION 5: Development Advisory Board Action. Based on the findings and conclusions set forth in Sections 1 through 4, above, the DAB hereby recommends the Planning Commission APPROVES the Application subject to each and every condition set forth in the Conditions of Approval included as Attachment A of this Decision, and incorporated herein by this reference.

SECTION 6: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario. The records are available for inspection by any interested person, upon request.

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APPROVED AND ADOPTED this 15<sup>th</sup> day of May 2023.

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Development Advisory Chairman

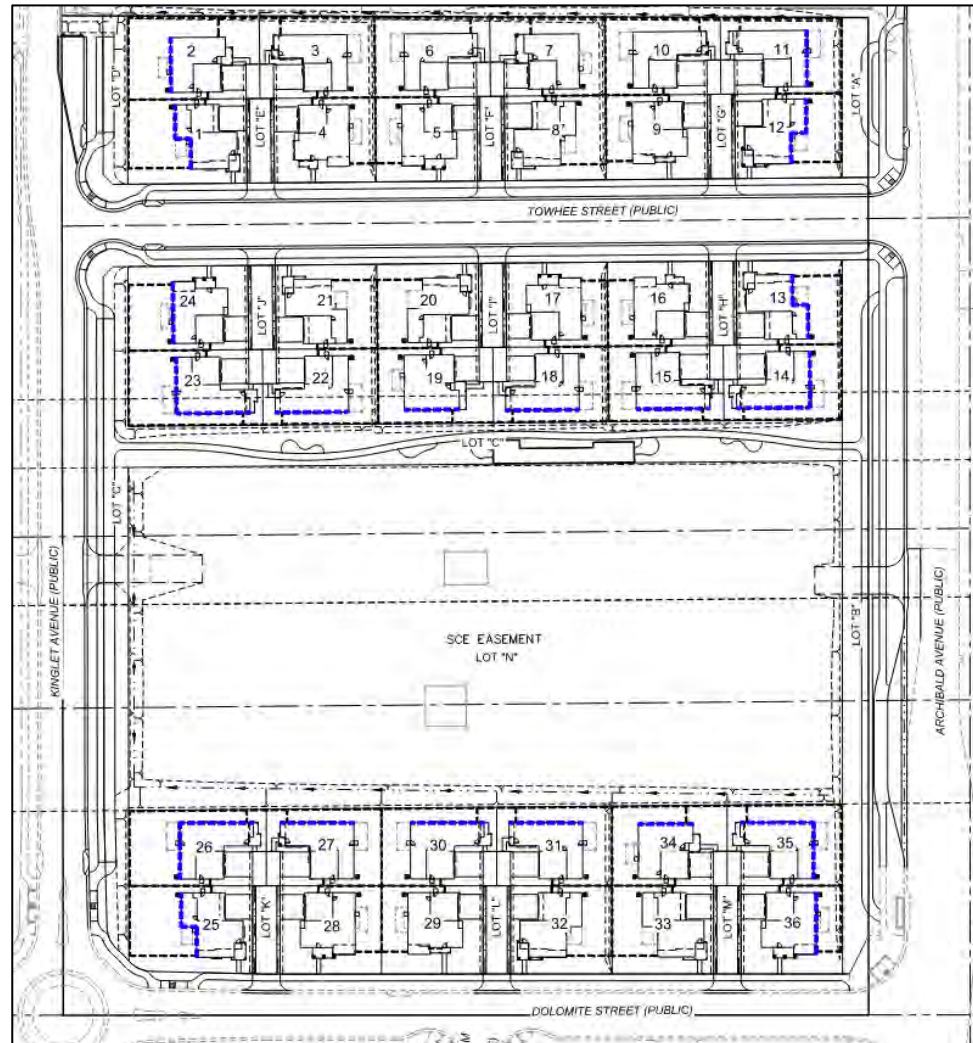
**Exhibit A: PROJECT LOCATION MAP**



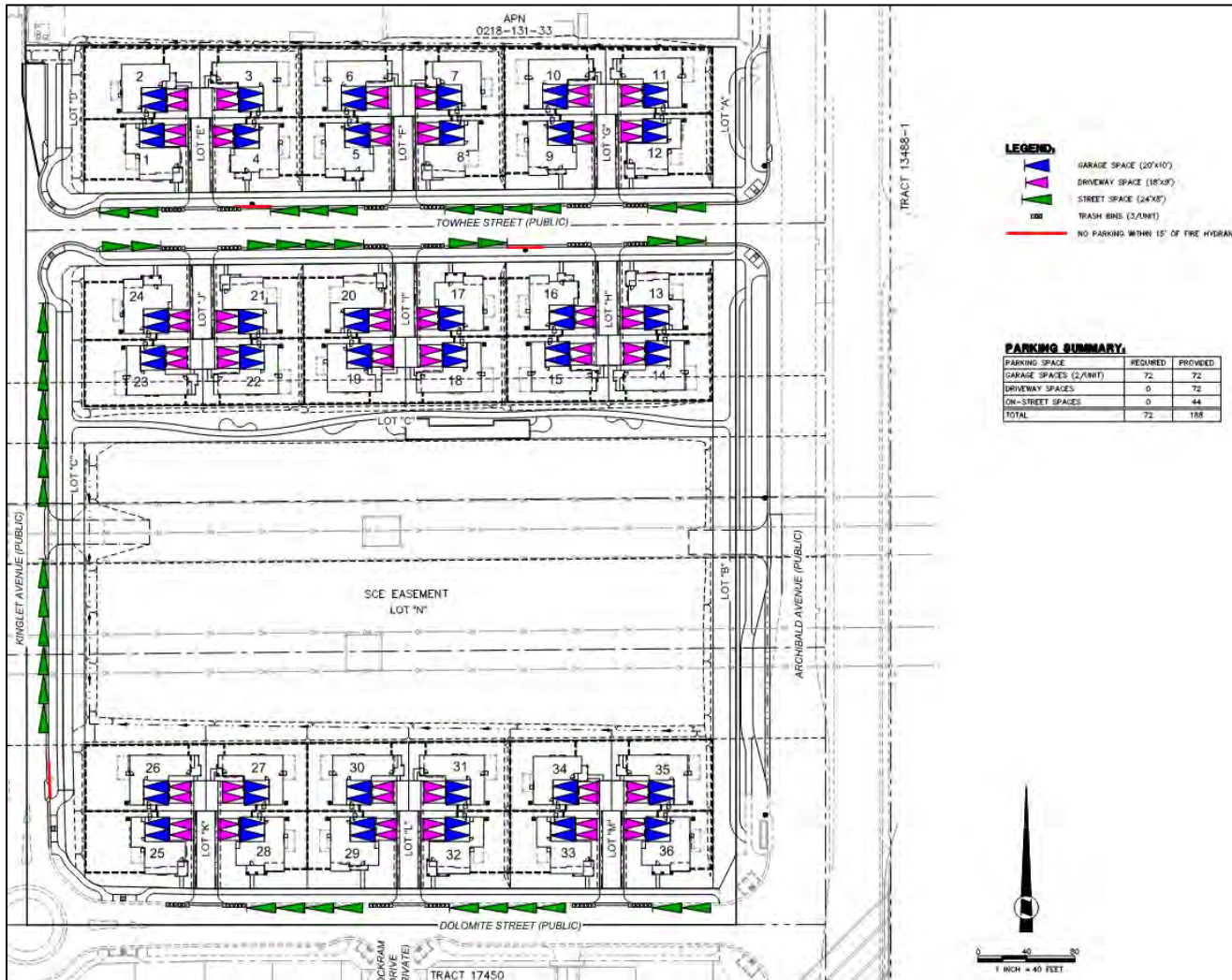
**Exhibit B-1: SITE PLAN**



**Exhibit B-2: ENHANCED ELEVATION SITE PLAN**



**Exhibit C: PARKING EXHIBIT**

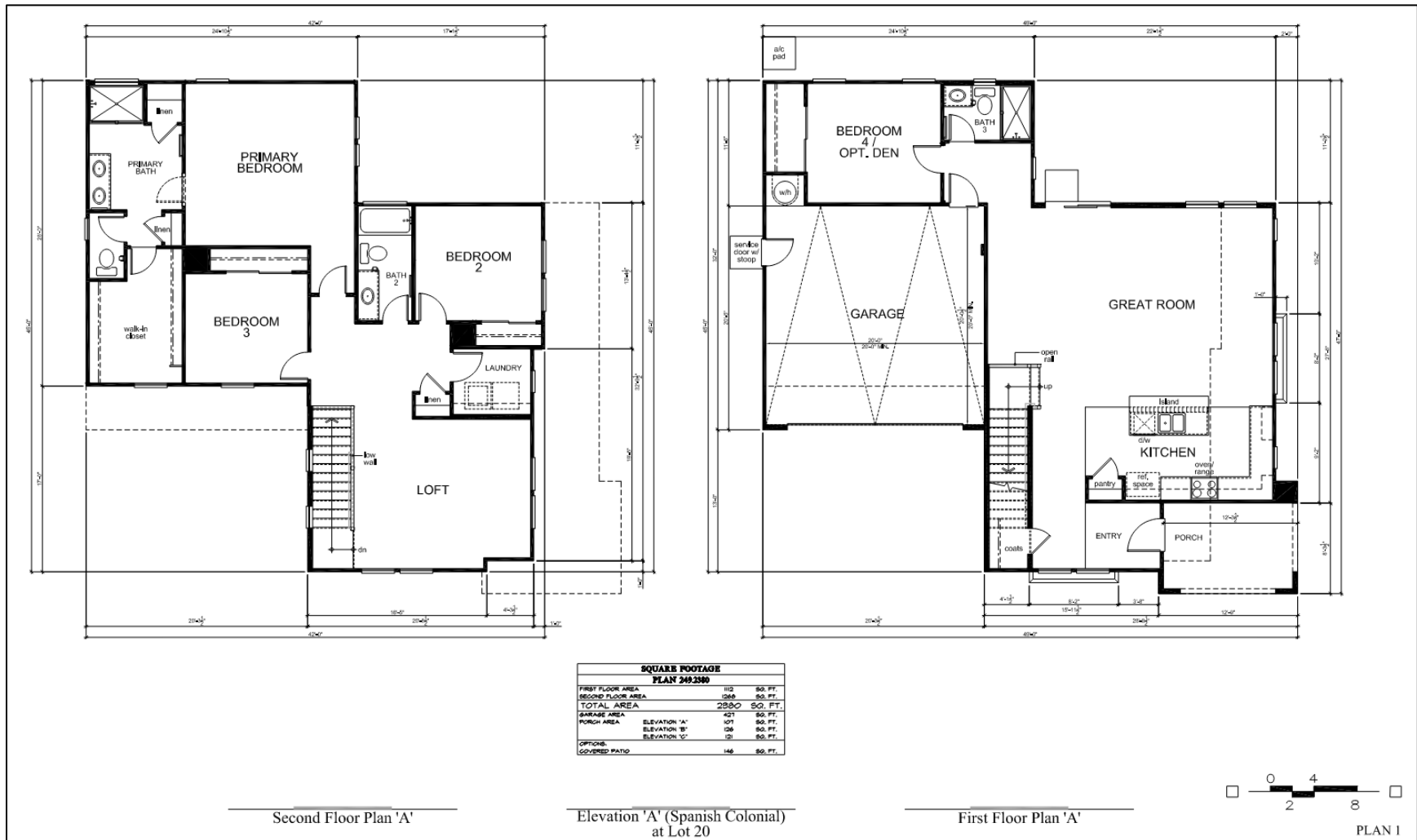


**Exhibit D-1: Front Elevations (Plan 1)**





**Exhibit D-1: Floor Plan (Plan 1)**



**Exhibit D-2: Front Elevations (Plan 2)**



**ELEVATION LEGEND**

1 CONCRETE TILE ROOFING	15 RESERVED
2 CONCRETE FLAT TILE ROOFING	16 ADDRESS PLAQUE
3 WOOD FASCIA BOARD	17 WOOD CORBEL
4 STUCCO FINISH (16/20 SAND)	18 RESERVED
5 STUCCO OVER FOAM TRIM	19 FIBER CEMENT SIDING WITH VERTICAL BATTENS
6 DECORATIVE STUCCO FINISH OVER FOAM CORBEL-SHAPED FOAM TRIM	20 PRE-FAB OUTLOOKER
7 STUCCO COLUMNS	21 WOOD POST
8 VINYL WINDOW SYSTEM	22 WOOD TRIM
9 COMPOSITE ENTRY DOOR	23 RESERVED
10 METAL SECTIONAL GARAGE DOOR	24 FIBER CEMENT 4" LAP SIDING, U.N.O.
11 DECORATIVE SHUTTERS	25 DECORATIVE CERAMIC TILES
12 DECORATIVE GABLE END DETAIL	26 RESERVED
13 COACH LIGHTS	27 STUCCO SHELF
14 STONE VENEER	28 FIBER CEMENT SHAKE SHINGLE SIDING

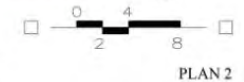
**Elevation 'A' (Spanish Colonial)**



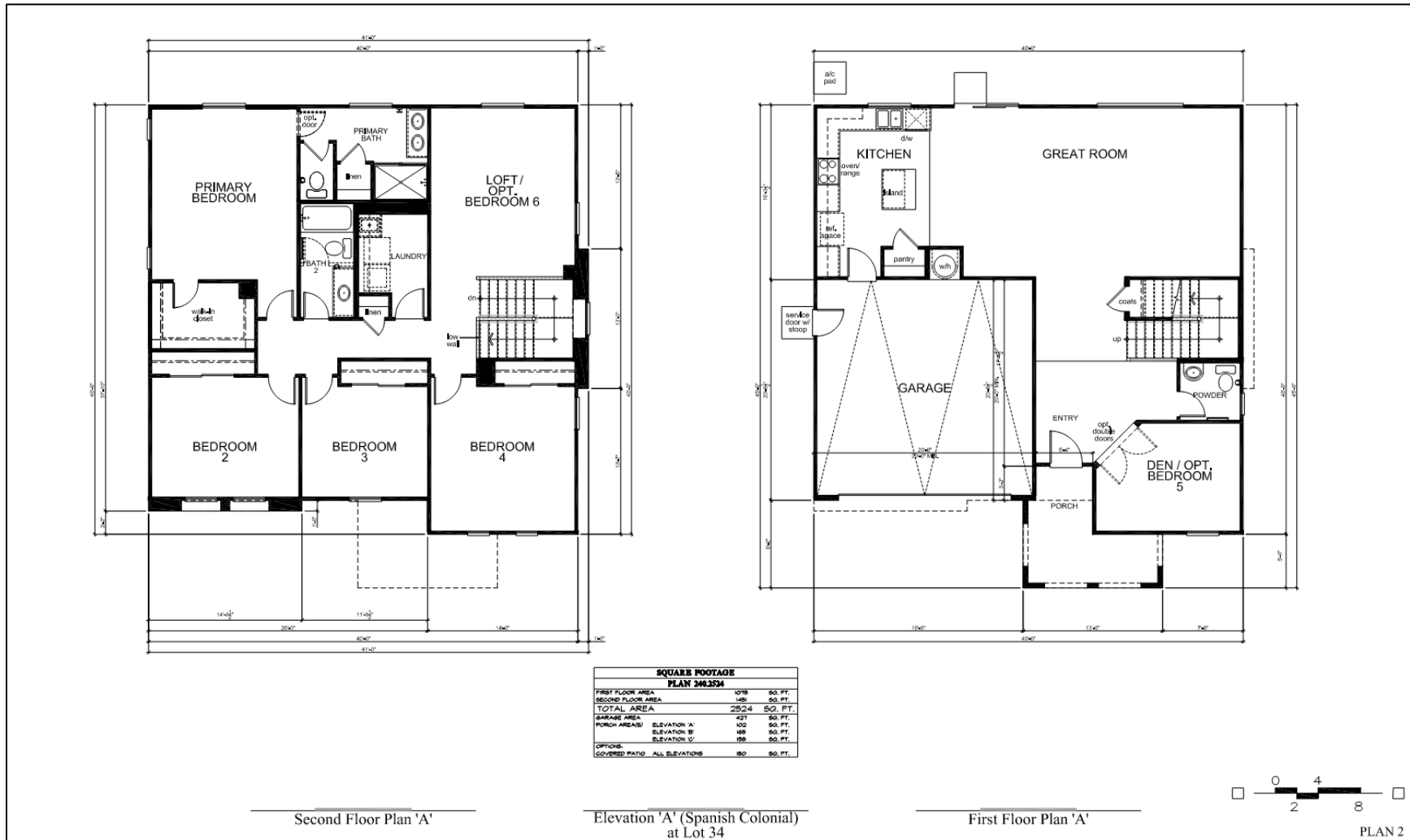
**Elevation 'B' (Craftsman)**



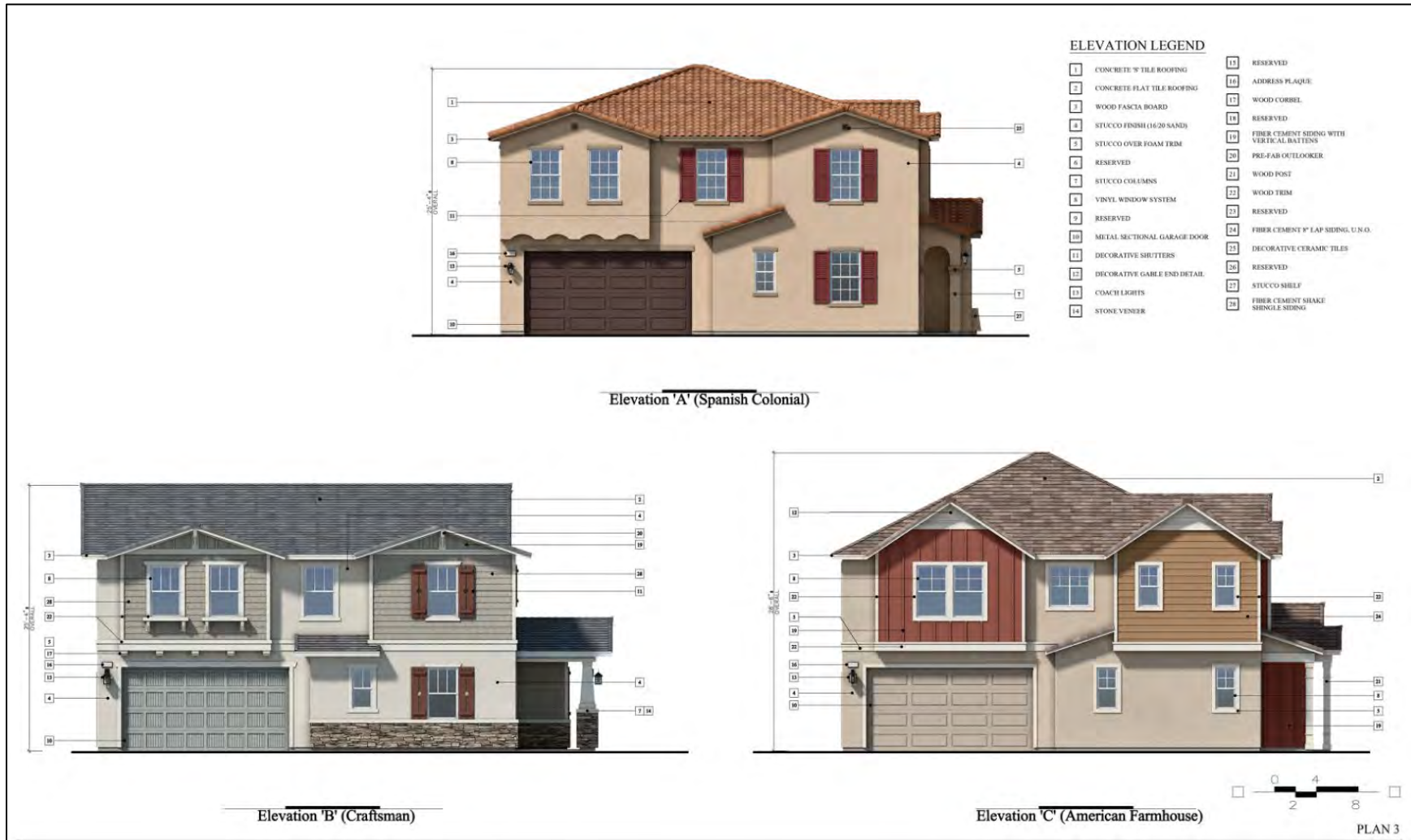
**Elevation 'C' (American Farmhouse)**



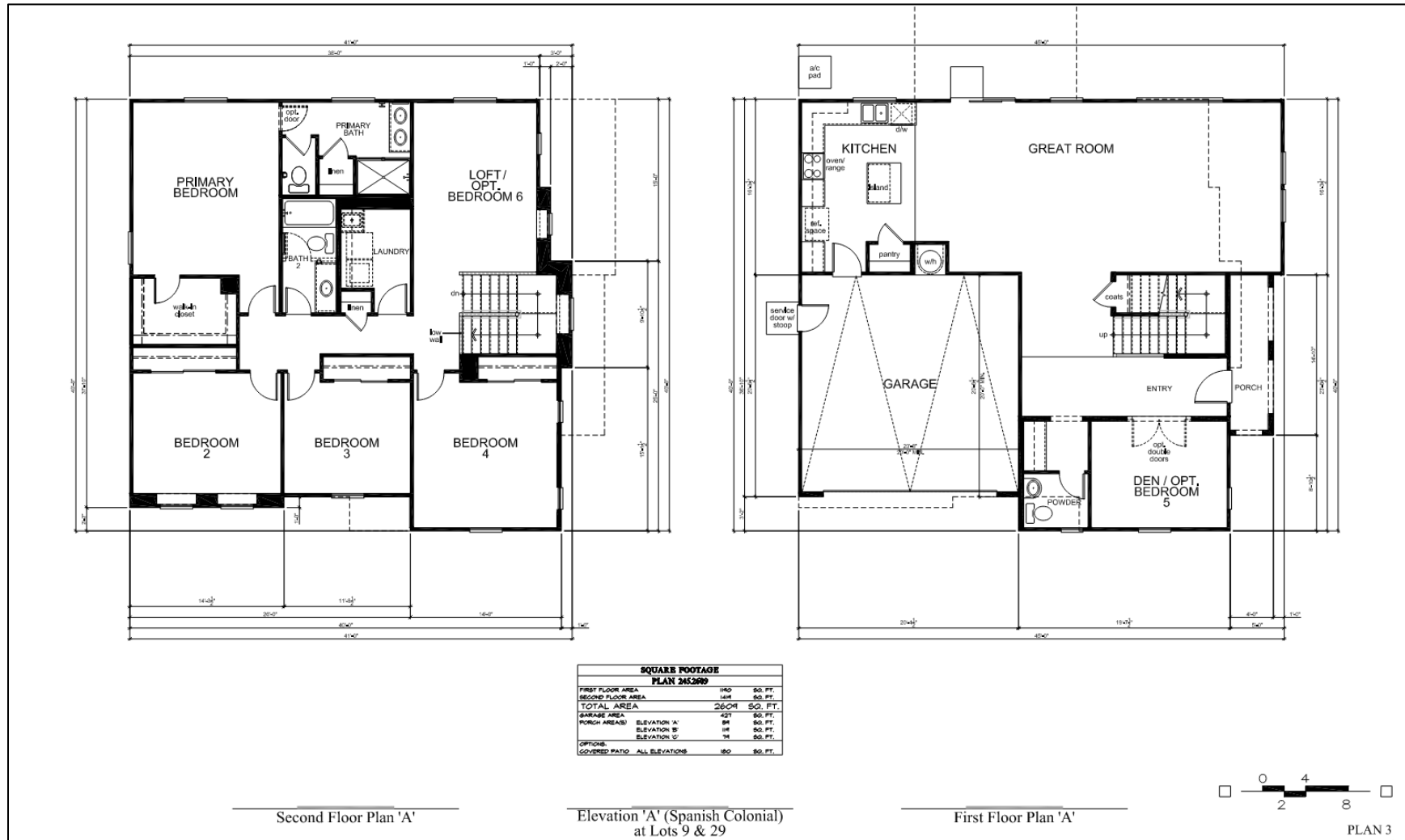
**Exhibit D-2: Floor Plan (Plan 2)**



**Exhibit D-3: Front Elevations (Plan 3)**



**Exhibit D-3: Floor Plan (Plan 3)**



**Exhibit D-4: Front Elevations (Plan 4)**



**Exhibit D-4: Floor Plan (Plan 4)**




**Exhibit E: STREET SCENE**






**Exhibit F-1: MATERIAL & COLOR SAMPLE BOARD – SPANISH ELEVATIONS**


**SCHEME 1**



**ROOF**  
BARCELONA California Mission Blend  
1 BCCS 6464




**DECO TILE**  
Santa Monica  
20001




**SOLID TILE**  
Merlot Gloss  
30619


**STUCCO**  
KB 1316  
White Flour (SW 710Z)




**FASCIA**  
Jute Brown (SW 6096)




**TRIM**  
Macadamia (SW 6142)




**WROUGHT IRON**  
Sealskin (SW 7675)



**FRONT ENTRY / SHUTTERS**  
Rookwood Dark Red (SW 2801)




**GARAGE**  
FACTORY TAUPE (SW 7038)




**CLAY PIPE**  
Aurora Brown (SW 2837)


**SCHEME 2**



**ROOF**  
BARCELONA Santa Catarina  
1 BCCS 6036




**DECO TILE**  
Jaen  
20039




**SOLID TILE**  
Charcoal Green Matte  
20107


**STUCCO**  
KB 2067  
Casa Blanca (SW 7571)




**FASCIA**  
Status Bronze (SW 7034)




**TRIM**  
Outerbanks (SW 7534)




**WROUGHT IRON**  
Sealskin (SW 7675)



**FRONT ENTRY / SHUTTERS**  
Lagoon (SW 6480)




**GARAGE**  
FACTORY TAUPE (SW 7038)




**CLAY PIPE**  
Aurora Brown (SW 2837)


**SCHEME 3**



**ROOF**  
BARCELONA Casa Grande Blend  
1 BCCS 6169




**DECO TILE**  
Capistrano 1  
20005




**SOLID TILE**  
Red Chestnut  
30617


**STUCCO**  
1230  
Townhall Tan (SW 7680)




**FASCIA**  
Smokehouse (SW 7040)




**TRIM**  
Resort Tan (SW 7550)




**WROUGHT IRON**  
Sealskin (SW 7675)



**FRONT ENTRY / SHUTTERS**  
Sun Dried Tomato (SW 7555)



**GARAGE**  
FACTORY BROWN (SW 6006)



**CLAY PIPE**  
Aurora Brown (SW 2837)

**Exhibit F-2: MATERIAL & COLOR SAMPLE BOARD – CRAFTSMAN ELEVATIONS**

**SCHEME 4**

**ROOF**  
SAXONY SHAKE Forest Green  
1 FBCJ 4598

**STUCCO**  
KB 2096  
San Antonio Sage (SW 7731)

**FASCIA**  
Umber (SW 6146)

**FRONT ENTRY / SHUTTERS**  
Red Barn (SW 7591)

**TRIM**  
Moderate White (SW 6140)

**GARAGE**  
FACTORY BROWN (SW 6006)

**GABLE END**  
Connected Gray (SW 6165)

**SIDING**  
Cocoon (SW 6173)

**STONE (ELDORADO)**  
CLIFFSTONE  
Montecito

**SCHEME 5**

**ROOF**  
SAXONY SHAKE Charcoal Blend  
1 FBCJ 1430

**STUCCO**  
KB 1339  
Useful Gray (SW 7050)

**FASCIA / TRIM**  
Nuanetsi (SW 7049)

**FRONT ENTRY / SHUTTERS**  
Terra Brun (SW 6048)

**GABLE END**  
Anonymous (SW 7046)

**GARAGE**  
FACTORY GRAY (SW 7660)

**SIDING**  
Rushing River (SW 7746)

**STONE**  
CLIFFSTONE  
Manzanita

**SCHEME 6**

**ROOF**  
SAXONY SHAKE Charcoal Brown Blend  
1 FBCJ 1132

**STUCCO**  
KB 1285  
China Doll (SW 7517)

**FASCIA / TRIM**  
Rookwood Dark Brown  
(SW 2808)

**FRONT ENTRY / SHUTTERS**  
Rosemary (SW 6187)

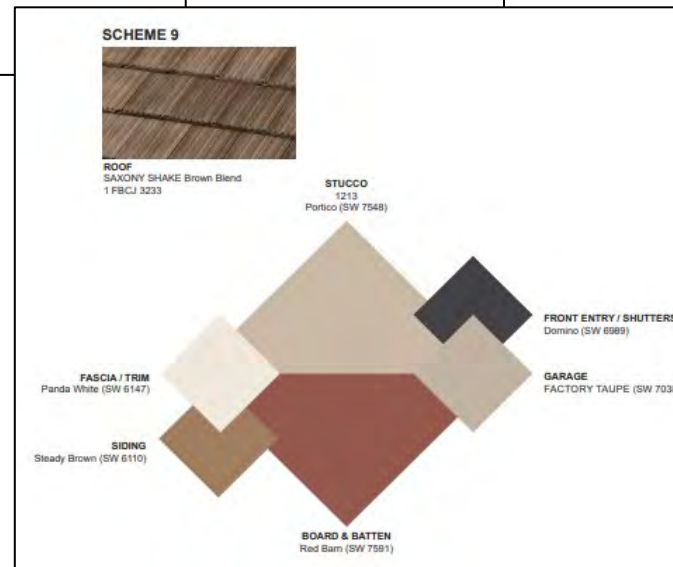
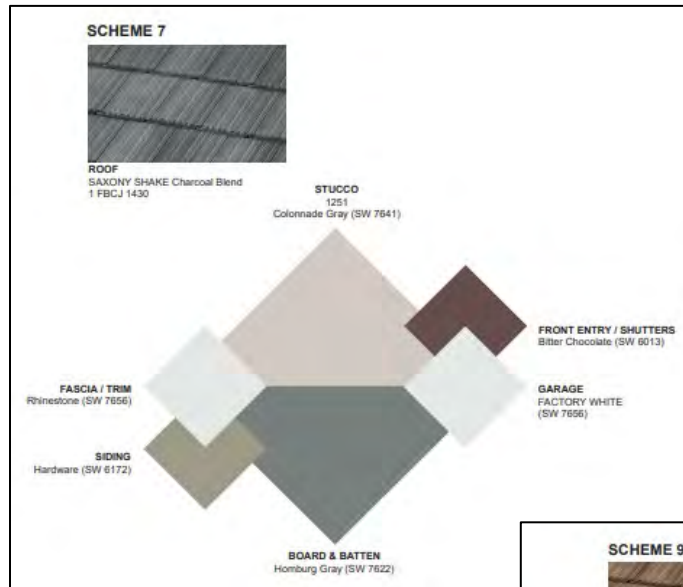
**GABLE END**  
Virtual Taupe (SW 7039)

**GARAGE**  
FACTORY BROWN (SW 6006)

**SIDING**  
Tony Taupe (SW 7038)

**STONE**  
CLIFFSTONE  
Lantern

**Exhibit F-3: MATERIAL & COLOR SAMPLE BOARD – AMERICAN FARMHOUSE ELEVATIONS**



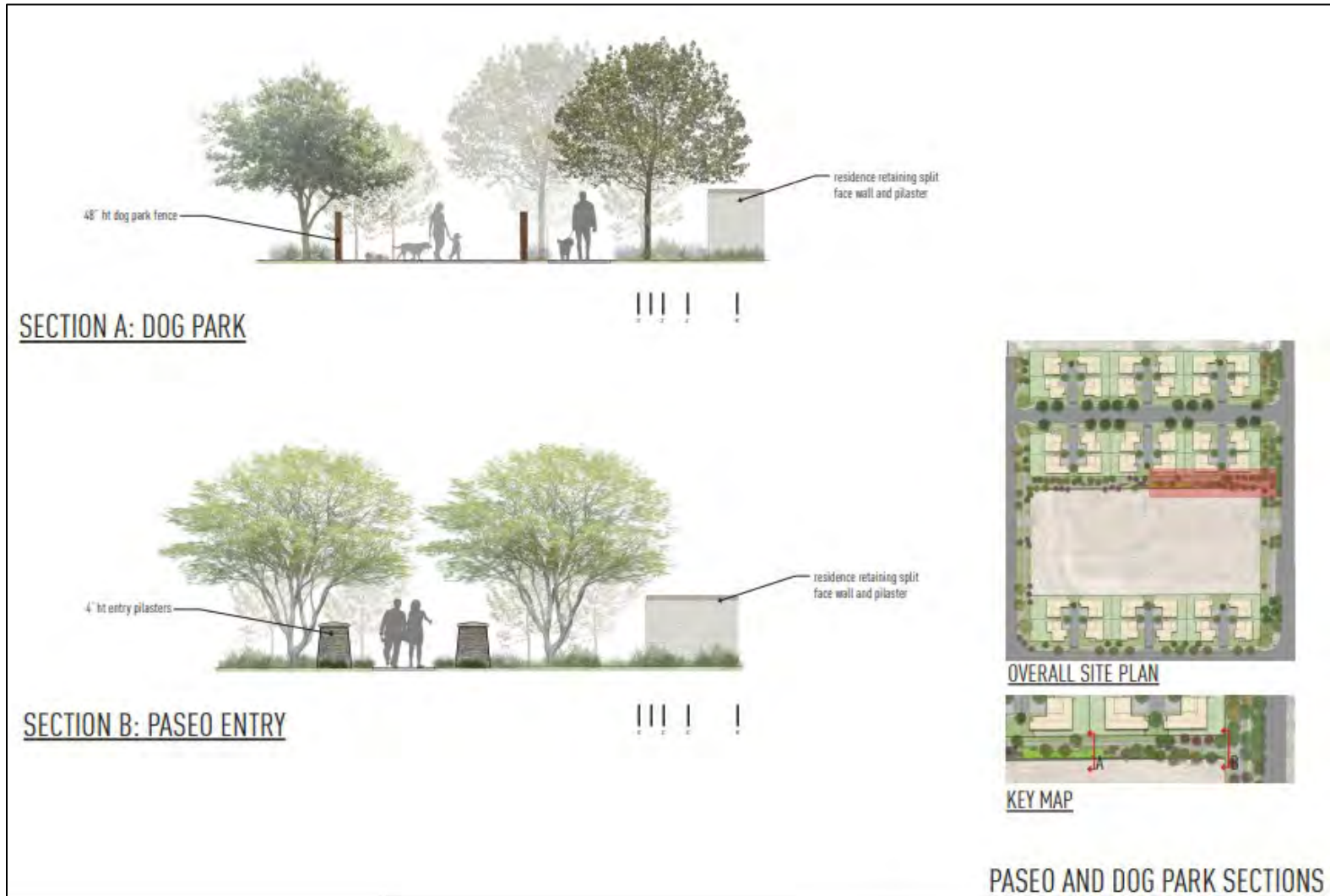
**Exhibit G-1: LANDSCAPING PLANS**



**Exhibit G-2: PASEO ENLARGEMENT**

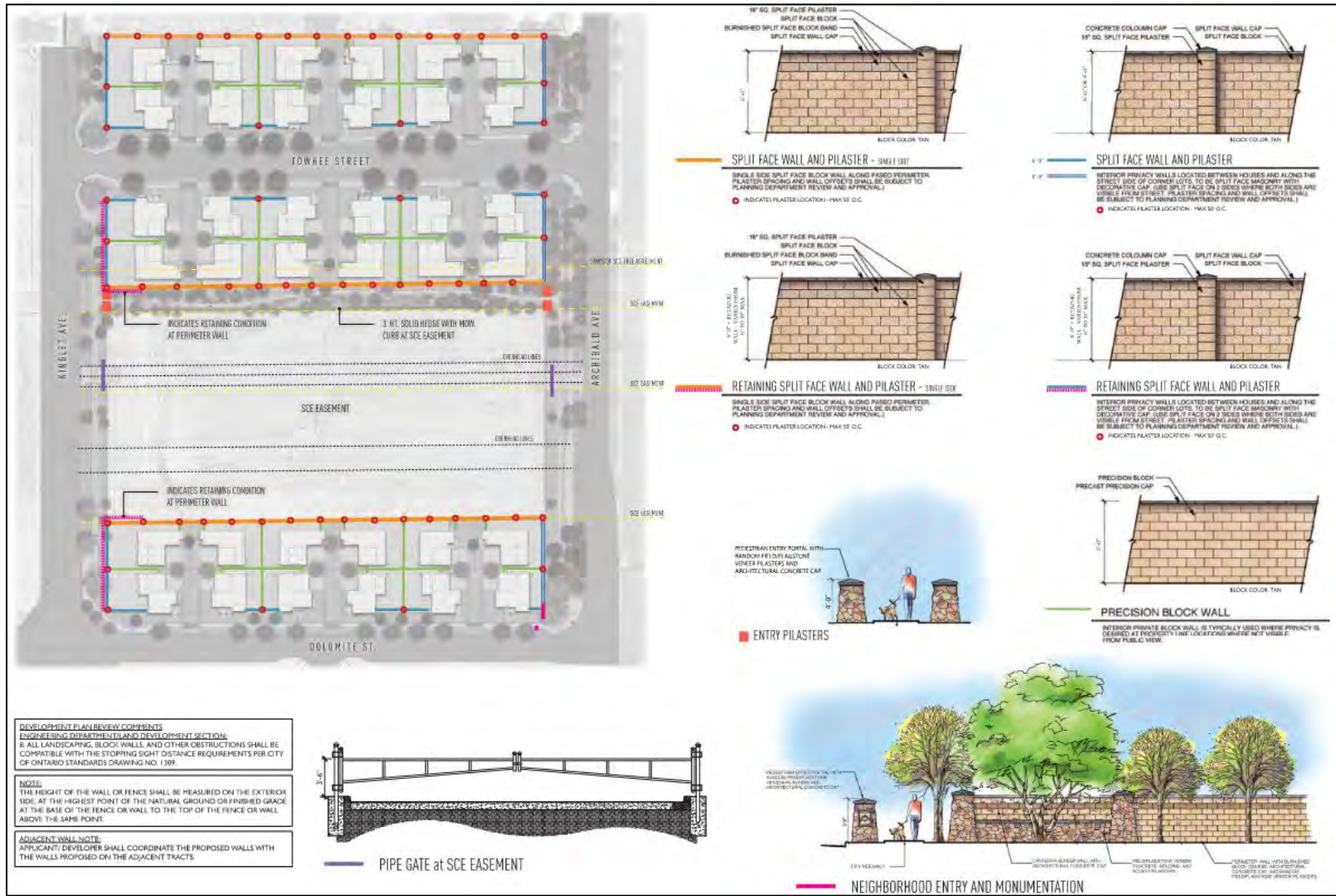


**Exhibit G-3: PASEO AND DOG PARK SECTIONS**





**Exhibit H: WALL PLAN**



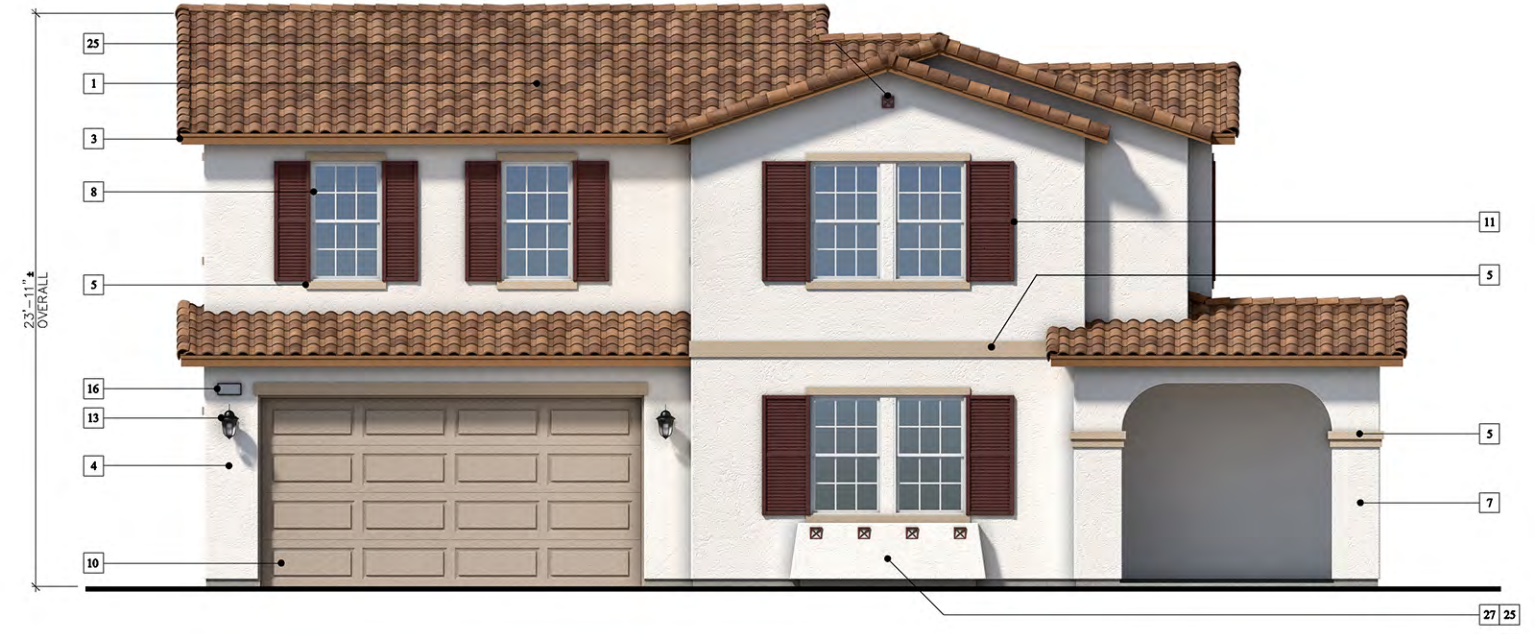


**Attachment A: Elevations and Floor Plans**

*(Full set of Elevations and Floor Plans follow this page)*

**ELEVATION LEGEND**

- |    |                             |    |   |
|----|-----------------------------|----|---|
| 1  | CONCRETE 'S' TILE ROOFING   | 15 | RESERVED                                  |
| 2  | CONCRETE FLAT TILE ROOFING  | 16 | ADDRESS PLAQUE                            |
| 3  | WOOD FASCIA BOARD           | 17 | RESERVED                                  |
| 4  | STUCCO FINISH (16/20 SAND)  | 18 | RESERVED                                  |
| 5  | STUCCO OVER FOAM TRIM       | 19 | FIBER CEMENT SIDING WITH VERTICAL BATTENS |
| 6  | RESERVED                    | 20 | PRE-FAB OUTLOOKER                         |
| 7  | STUCCO COLUMNS              | 21 | WOOD POST                                 |
| 8  | VINYL WINDOW SYSTEM         | 22 | WOOD TRIM                                 |
| 9  | RESERVED                    | 23 | RESERVED                                  |
| 10 | METAL SECTIONAL GARAGE DOOR | 24 | FIBER CEMENT 8" LAP SIDING, U.N.O.        |
| 11 | DECORATIVE SHUTTERS         | 25 | DECORATIVE CERAMIC TILES                  |
| 12 | DECORATIVE GABLE END DETAIL | 26 | RESERVED                                  |
| 13 | COACH LIGHTS                | 27 | STUCCO SHELF                              |
| 14 | STONE VENEER                | 28 | FIBER CEMENT SHAKE SHINGLE SIDING         |



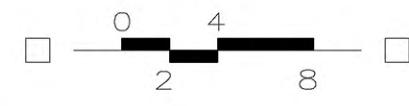
**Elevation 'A' (Spanish Colonial)**

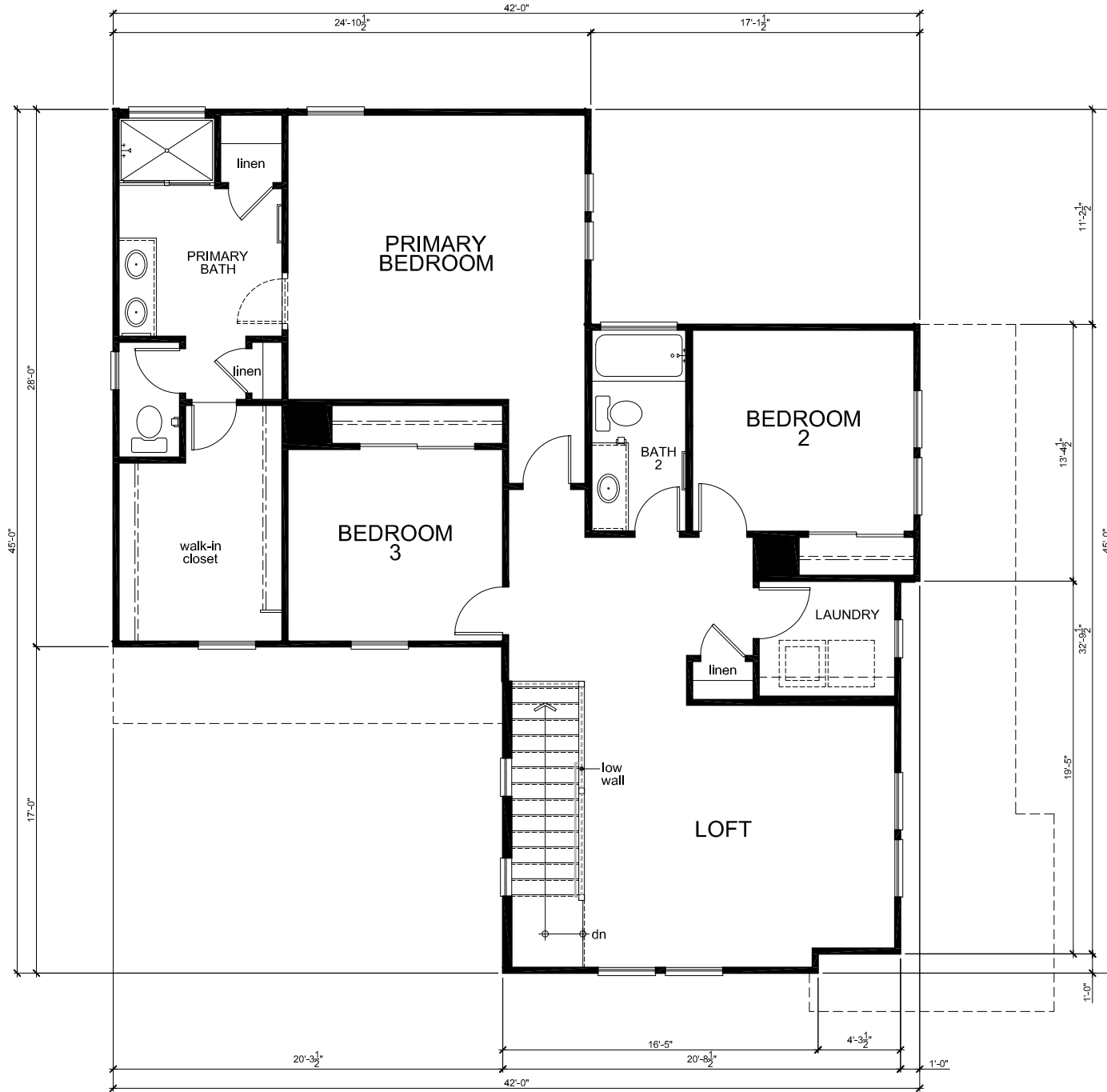


**Elevation 'B' (Craftsman)**

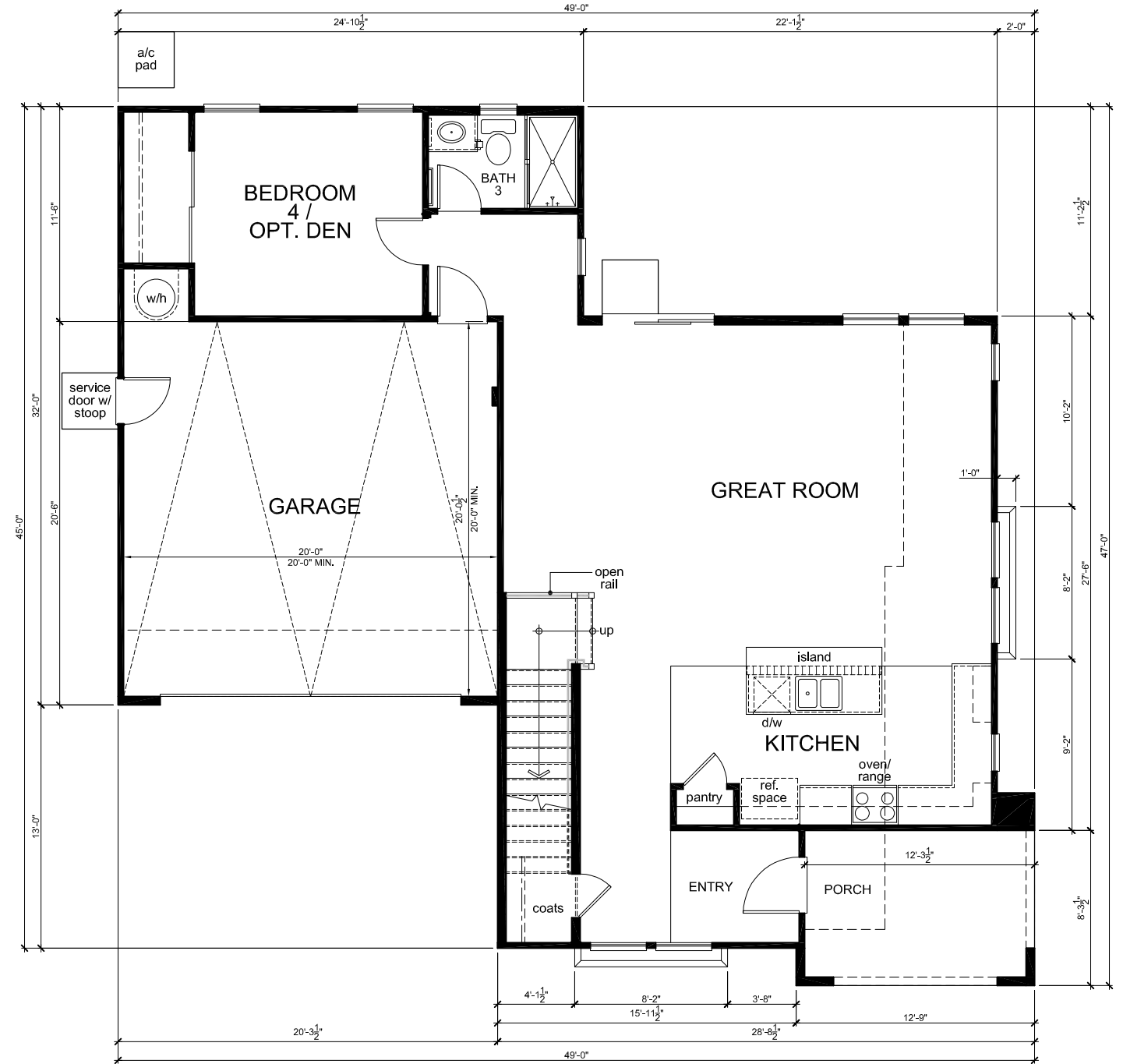


**Elevation 'C' (American Farmhouse)**





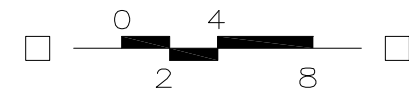
Second Floor Plan 'A'



First Floor Plan 'A'

SQUARE FOOTAGE			
PLAN 249.2380			
FIRST FLOOR AREA	1112	SQ. FT.	
SECOND FLOOR AREA	1268	SQ. FT.	
TOTAL AREA	2380	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA	ELEVATION "A"	107	SQ. FT.
	ELEVATION "B"	126	SQ. FT.
	ELEVATION "C"	121	SQ. FT.
OPTIONS:			
COVERED PATIO	146	SQ. FT.	

Elevation 'A' (Spanish Colonial)  
at Lot 20



PLAN 1



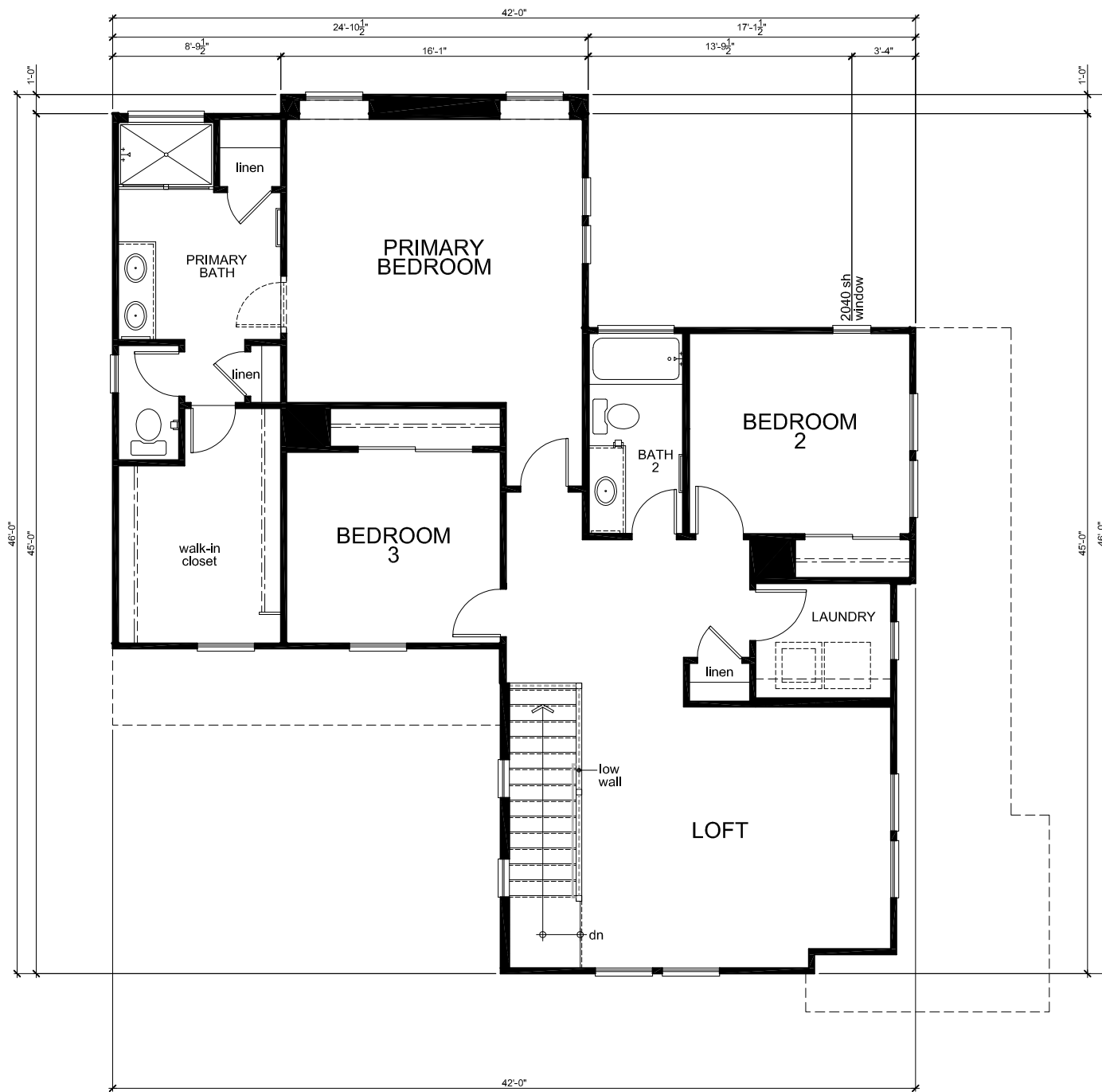
KB Home Southern California/Inland Empire  
36310 Inland Valley Drive  
Wildomar, CA 92595  
(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

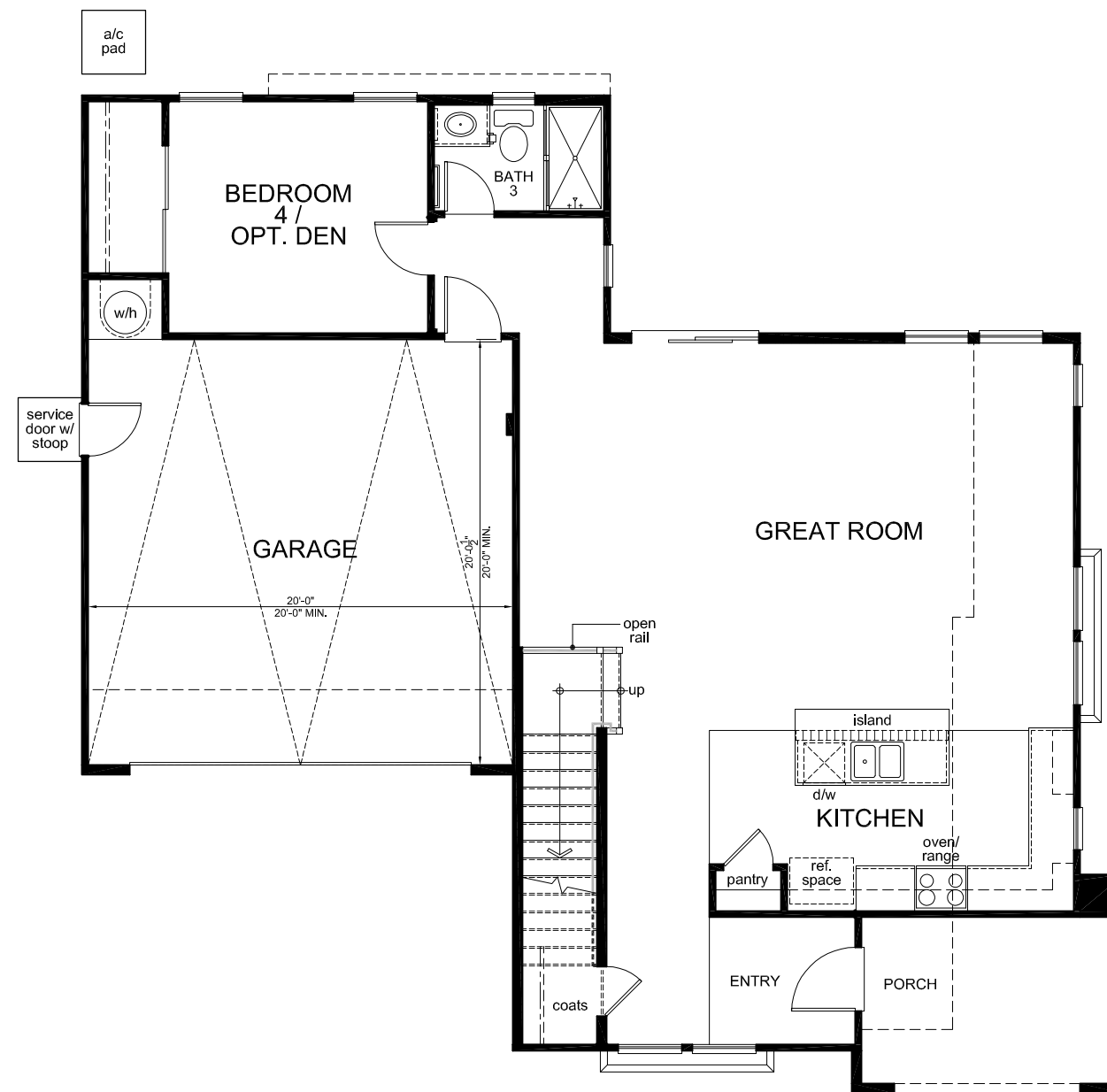
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JOB No.: 350999  
STORY: Two  
REV: December 12, 2022

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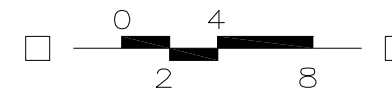
Second Floor Plan 'A'



First Floor Plan 'A'

SQUARE FOOTAGE			
PLAN 249.2380			
FIRST FLOOR AREA	1112	SQ. FT.	
SECOND FLOOR AREA	1268	SQ. FT.	
TOTAL AREA	2380	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA	ELEVATION "A"	107	SQ. FT.
	ELEVATION "B"	126	SQ. FT.
	ELEVATION "C"	121	SQ. FT.
OPTIONS:			
COVERED PATIO	146	SQ. FT.	

Elevation 'A' (Spanish Colonial)  
at Lots 1, 13 & 25



PLAN 1



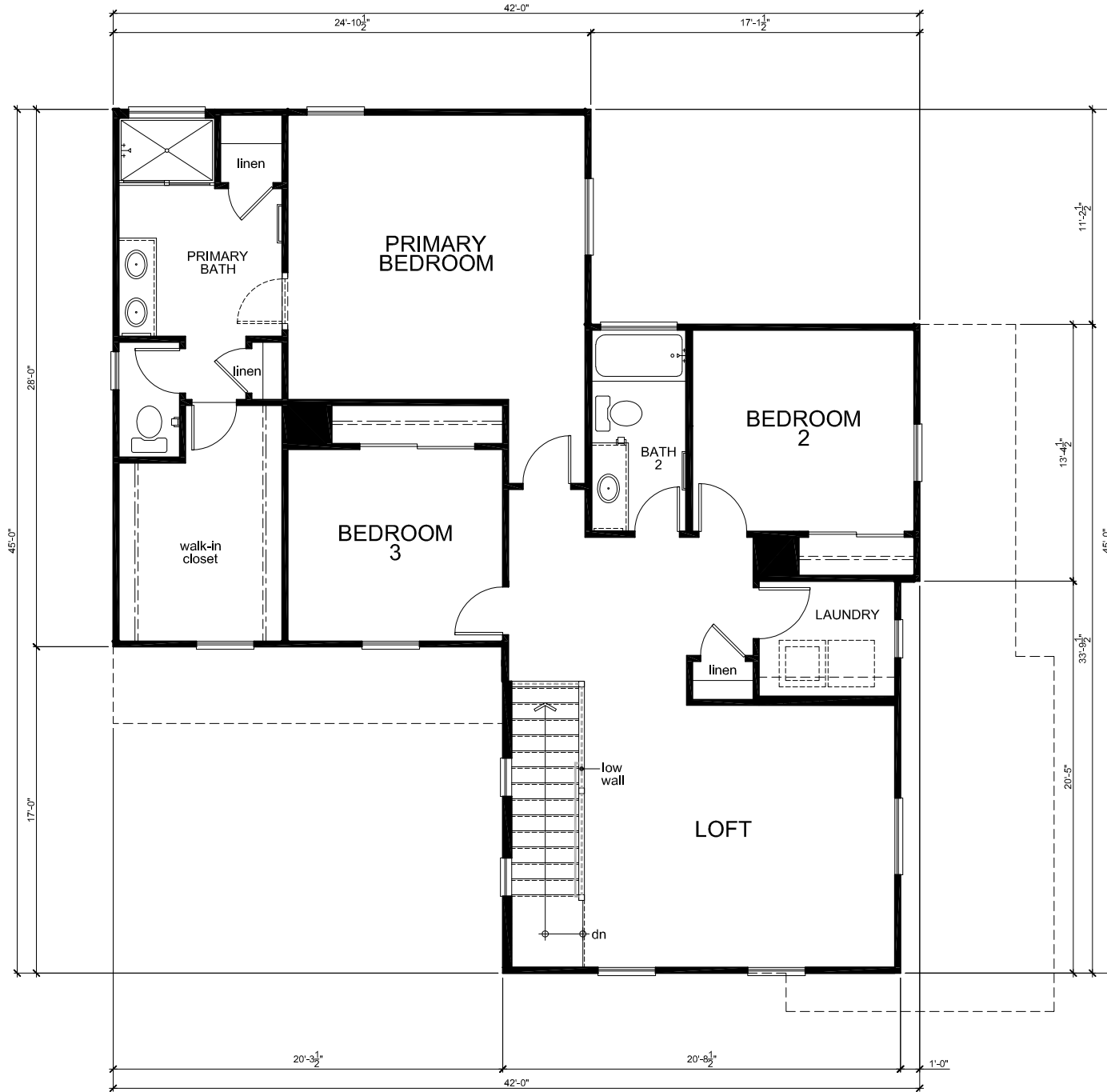
KB Home Southern California/Inland Empire  
36310 Inland Valley Drive  
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(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

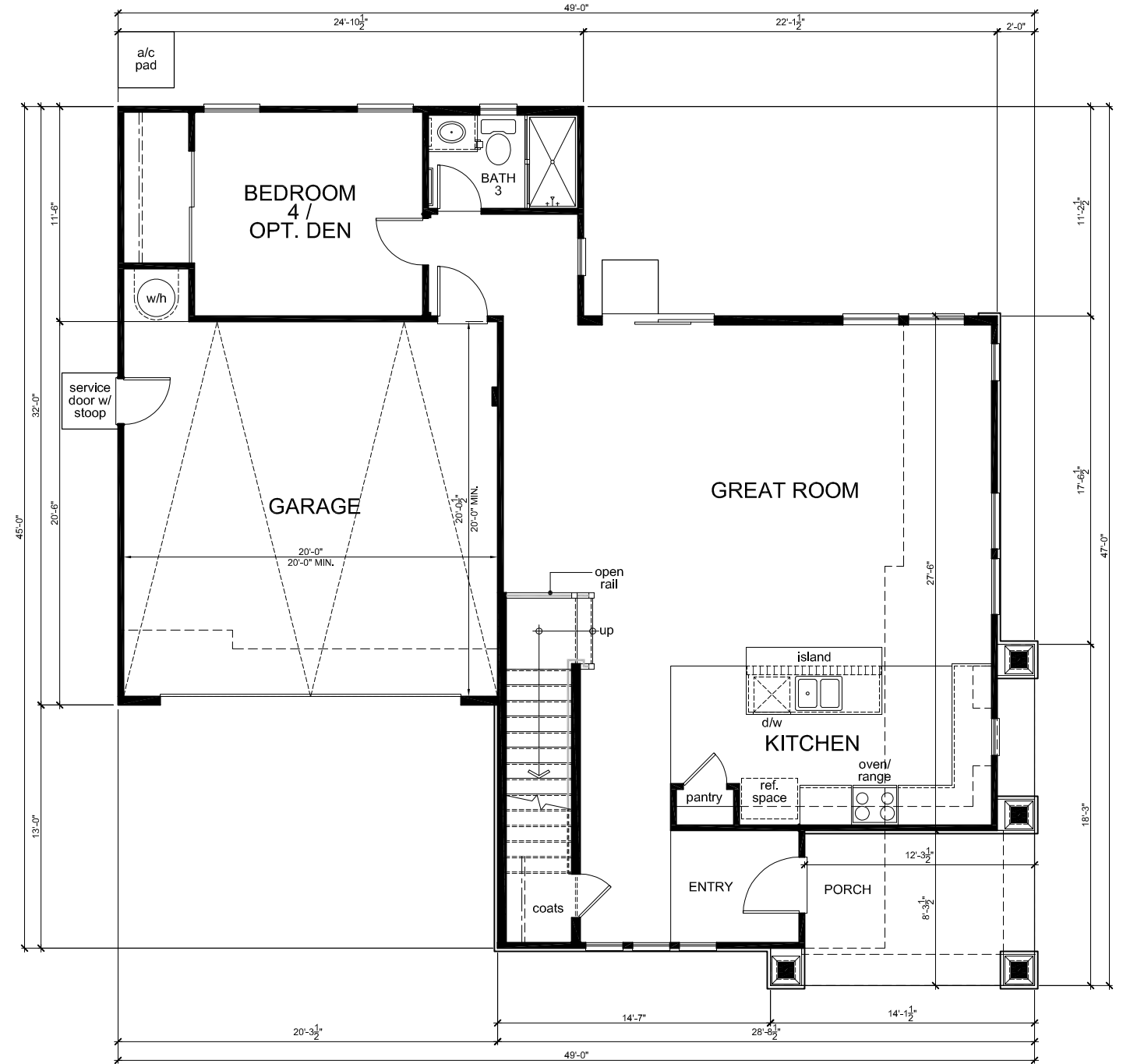
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JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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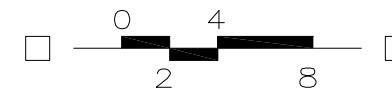
Second Floor Plan 'B'



First Floor Plan 'B'

SQUARE FOOTAGE			
PLAN 249.2380			
FIRST FLOOR AREA	1112	SQ. FT.	
SECOND FLOOR AREA	1268	SQ. FT.	
TOTAL AREA	2380	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA	ELEVATION "A"	107	SQ. FT.
	ELEVATION "B"	126	SQ. FT.
	ELEVATION "C"	121	SQ. FT.
OPTIONS:			
COVERED PATIO	146	SQ. FT.	

Elevation 'B' (Craftsman)  
at Lot 32



PLAN 1



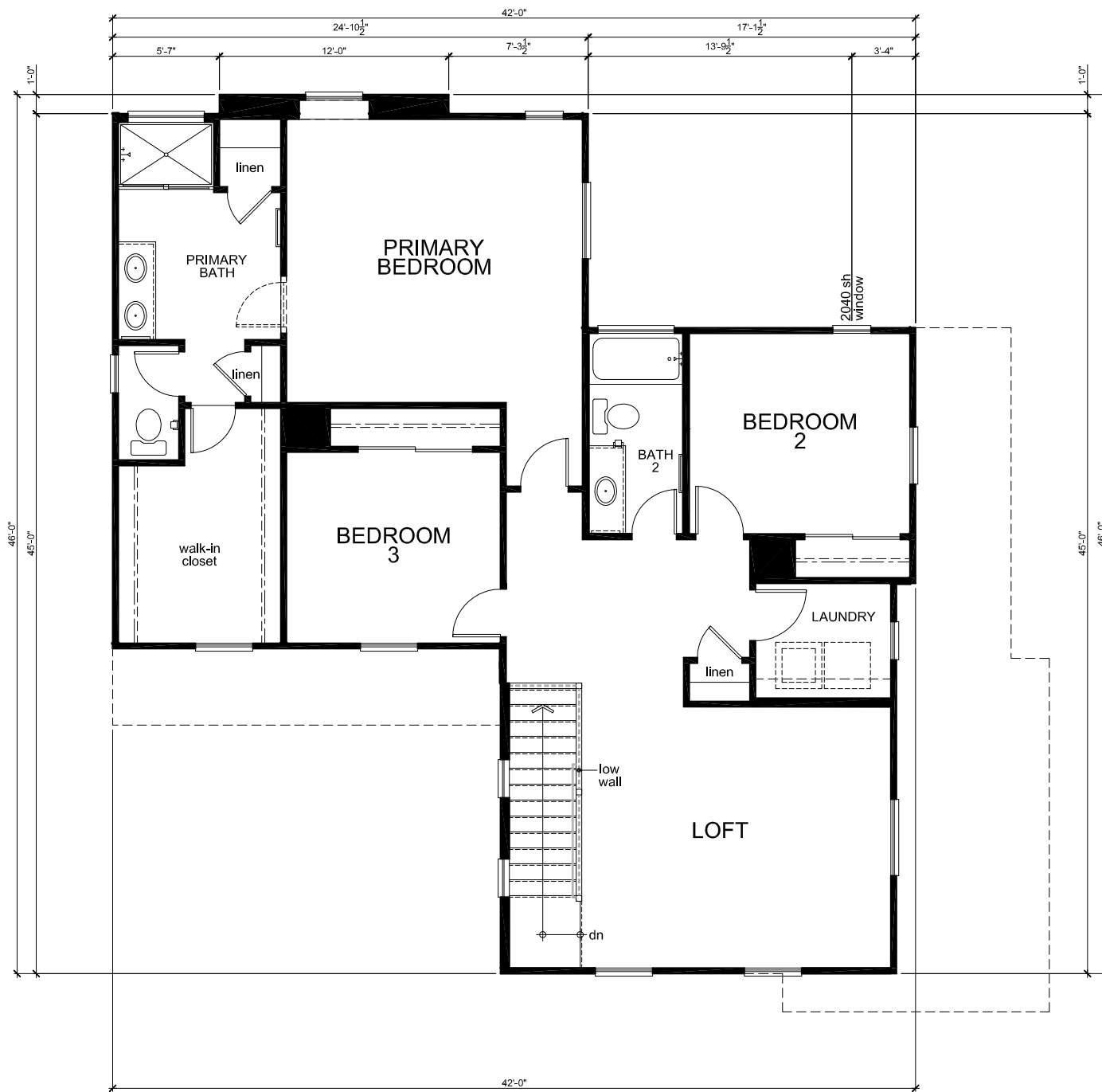
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Wildomar, CA 92595  
(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

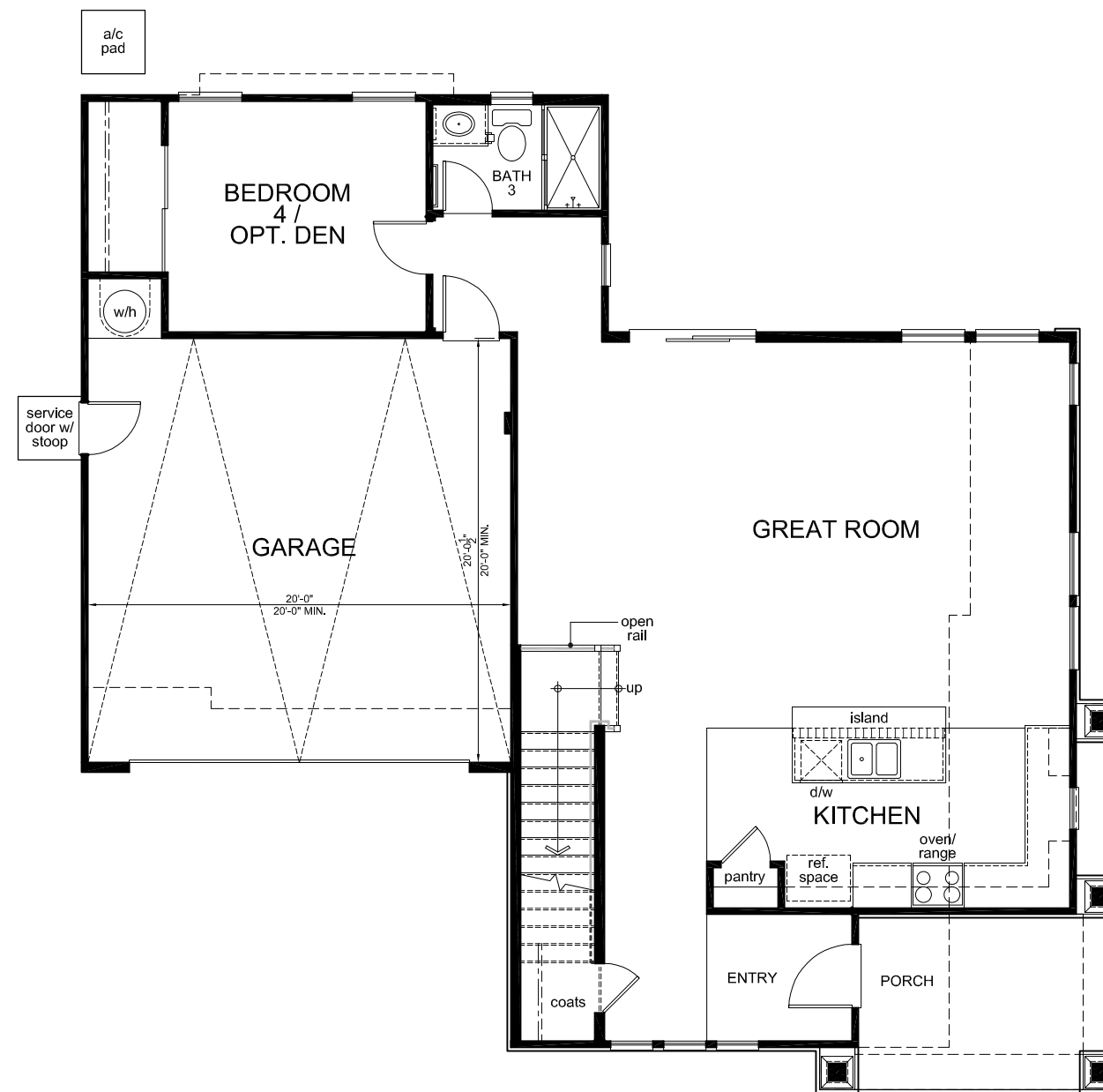
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JOB No.: 350999  
STORY: Two  
REV: December 12, 2022

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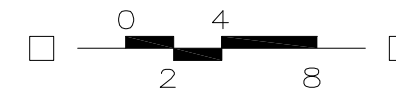
Second Floor Plan 'B'



First Floor Plan 'B'

SQUARE FOOTAGE			
PLAN 249.2380			
FIRST FLOOR AREA	1112	SQ. FT.	
SECOND FLOOR AREA	1268	SQ. FT.	
TOTAL AREA	2380	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA			
	ELEVATION "A"	107	SQ. FT.
	ELEVATION "B"	126	SQ. FT.
	ELEVATION "C"	121	SQ. FT.
OPTIONS:			
COVERED PATIO	146	SQ. FT.	

Elevation 'B' (Craftsman)  
at Lot 12



PLAN 1

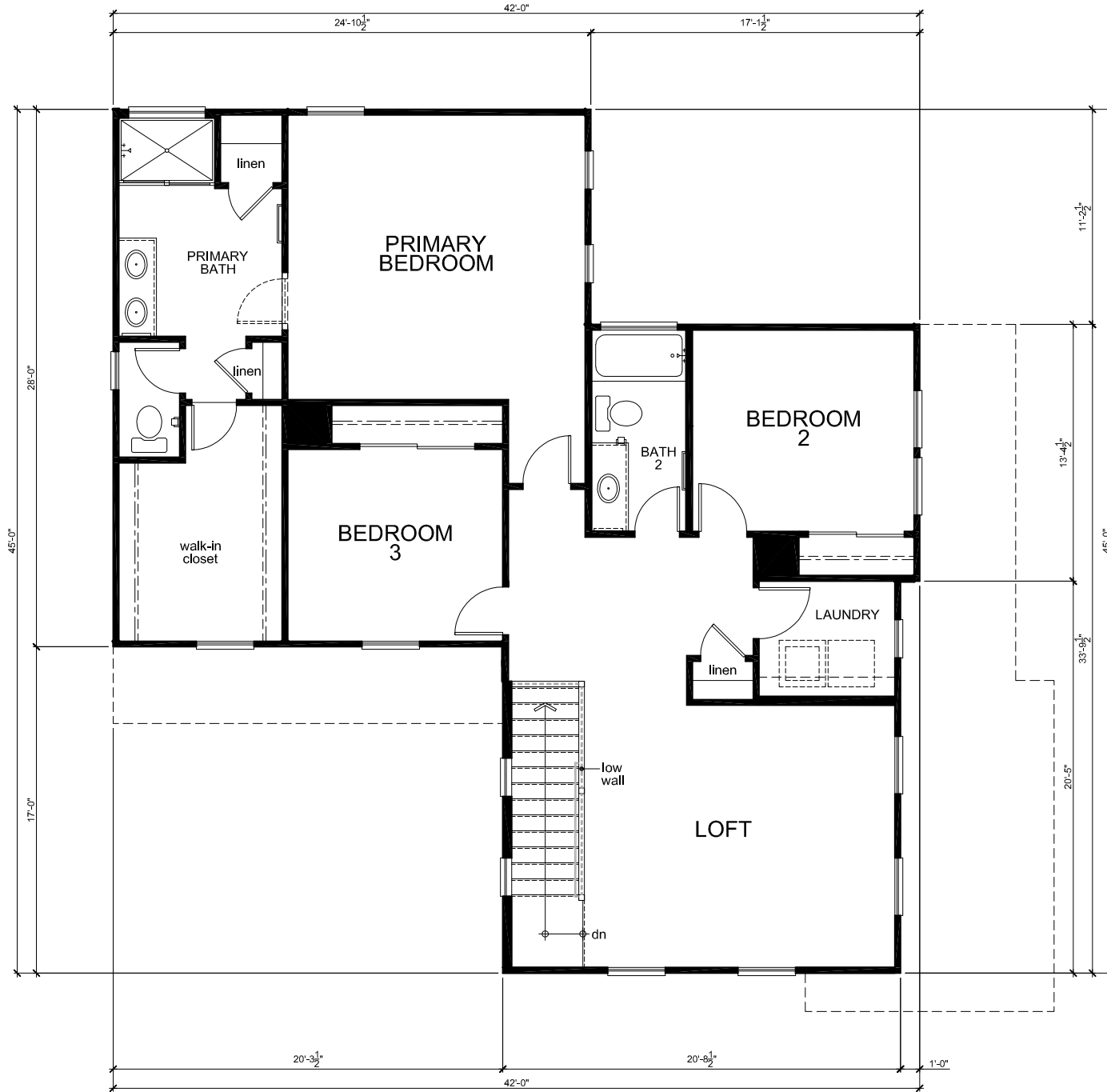


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ONTARIO, CA

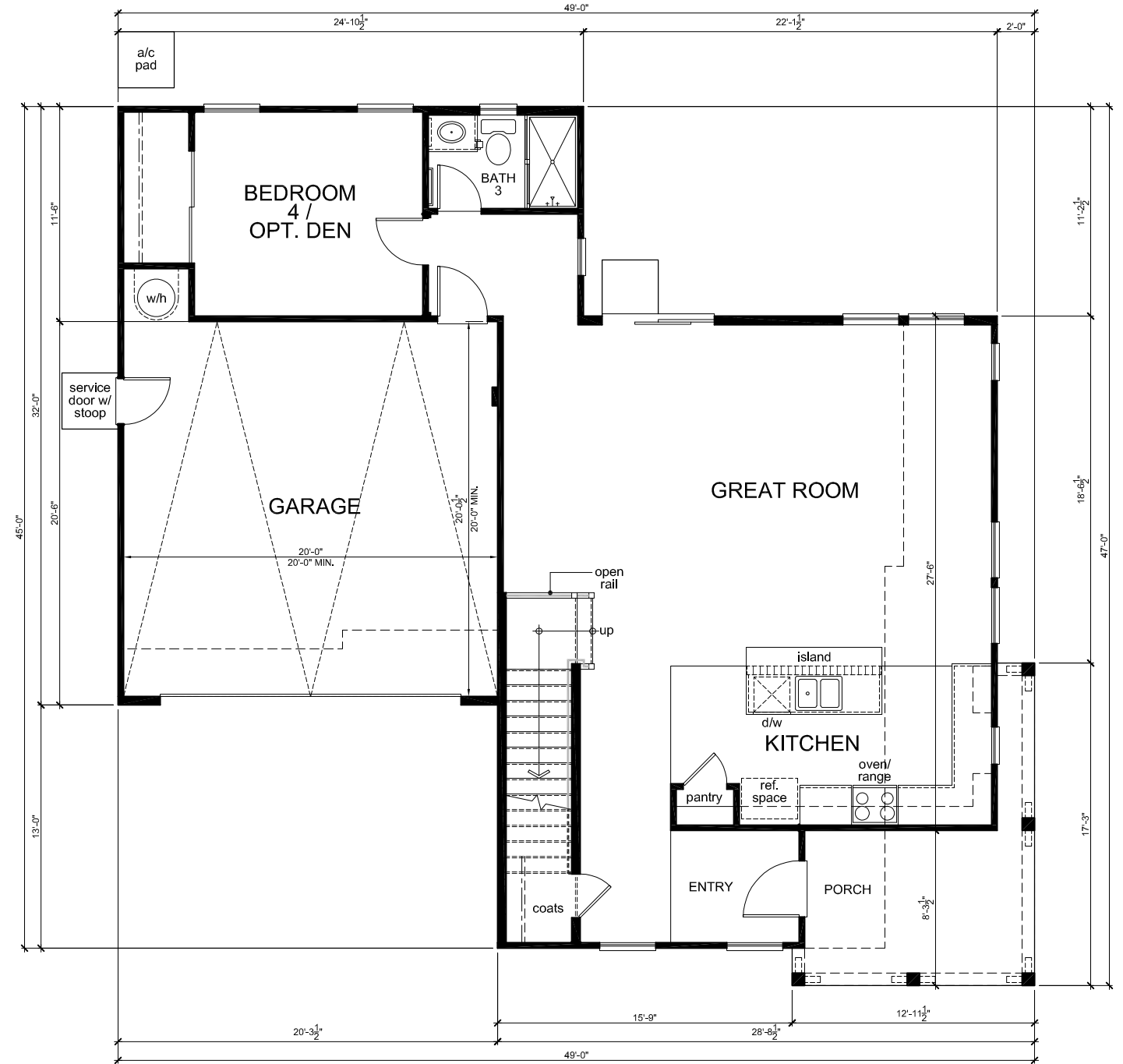
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

249.2380

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022



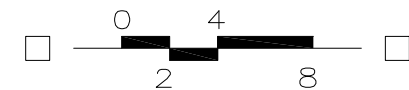
Second Floor Plan 'C'



First Floor Plan 'C'

SQUARE FOOTAGE			
PLAN 249.2380			
FIRST FLOOR AREA	1112	SQ. FT.	
SECOND FLOOR AREA	1268	SQ. FT.	
TOTAL AREA	2380	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA	ELEVATION "A"	107	SQ. FT.
	ELEVATION "B"	126	SQ. FT.
	ELEVATION "C"	121	SQ. FT.
OPTIONS:			
COVERED PATIO	146	SQ. FT.	

Elevation 'C' (American Farmhouse)  
at Lots 8, 21 & 33



PLAN 1



KB Home Southern California/Inland Empire  
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Wildomar, CA 92595  
(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

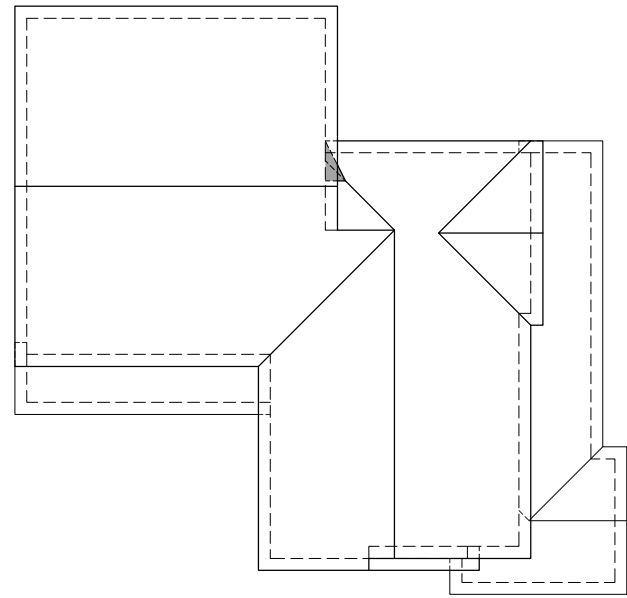
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JOB No. : 350999

STORY: Two

REV: December 12, 2022

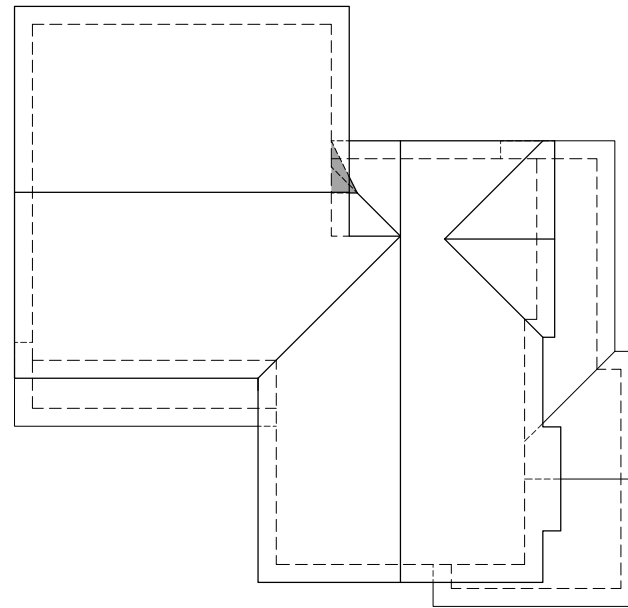
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**'A' Spanish Colonial**

at Lot 20

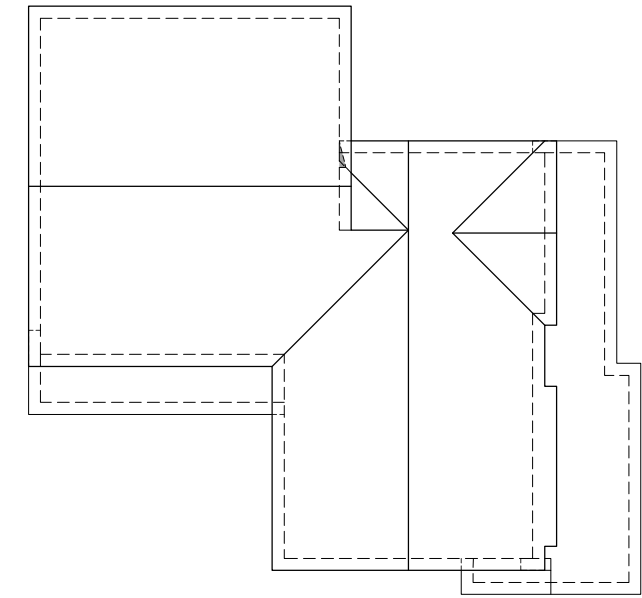
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**'B' Craftsman**

at Lot 32

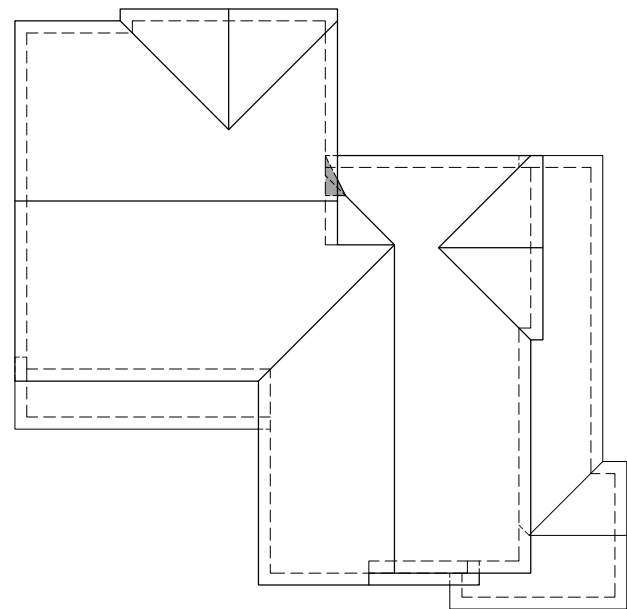
4:12



**'C' American Farmhouse**

at Lots 8, 21 & 33

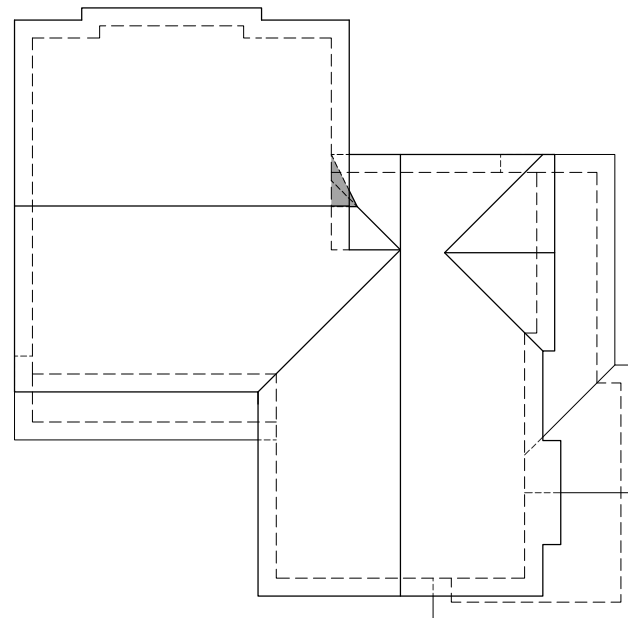
8:12



**'A' Spanish Colonial**

at Lots 1, 13 & 25

4:12



**'B' Craftsman**

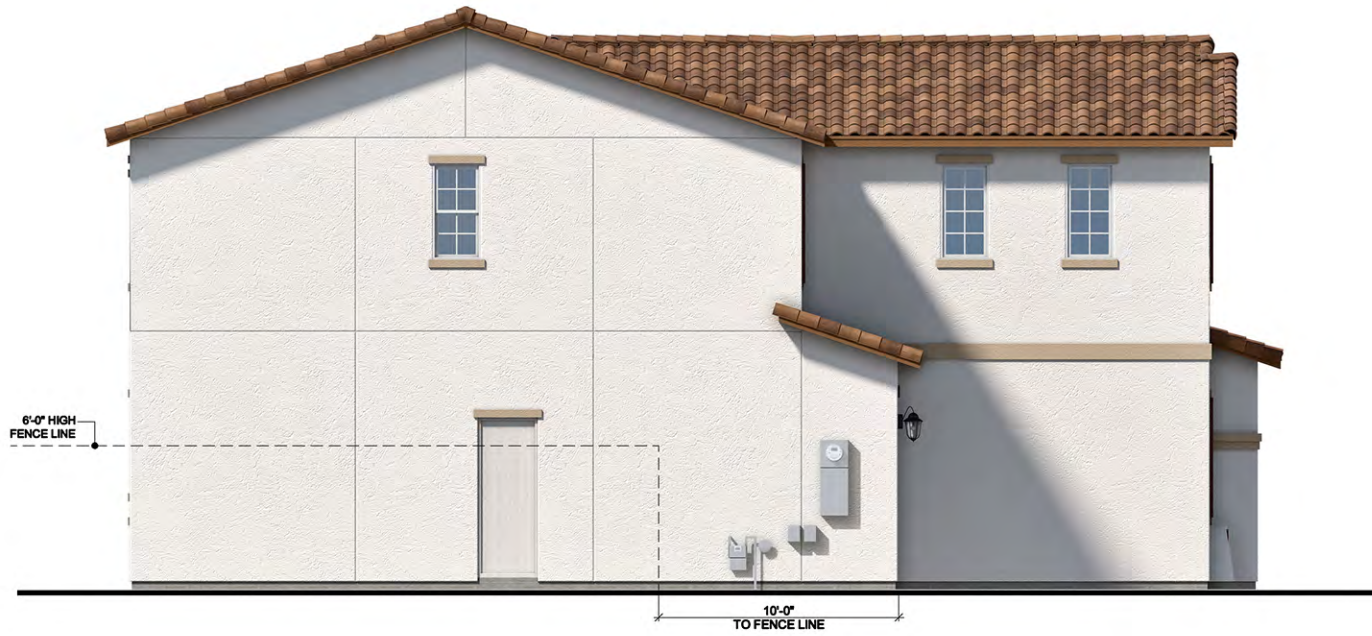
at Lot 12

4:12

Roof Plans







Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

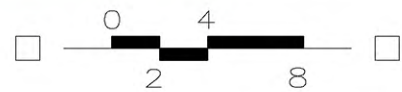


Right Elevation 'A' (Spanish Colonial)  
Visible from Towhee Street



Rear Elevation 'A' (Spanish Colonial)

Elevation 'A' (Spanish Colonial)  
at Lot 20



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COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

PLAN 1

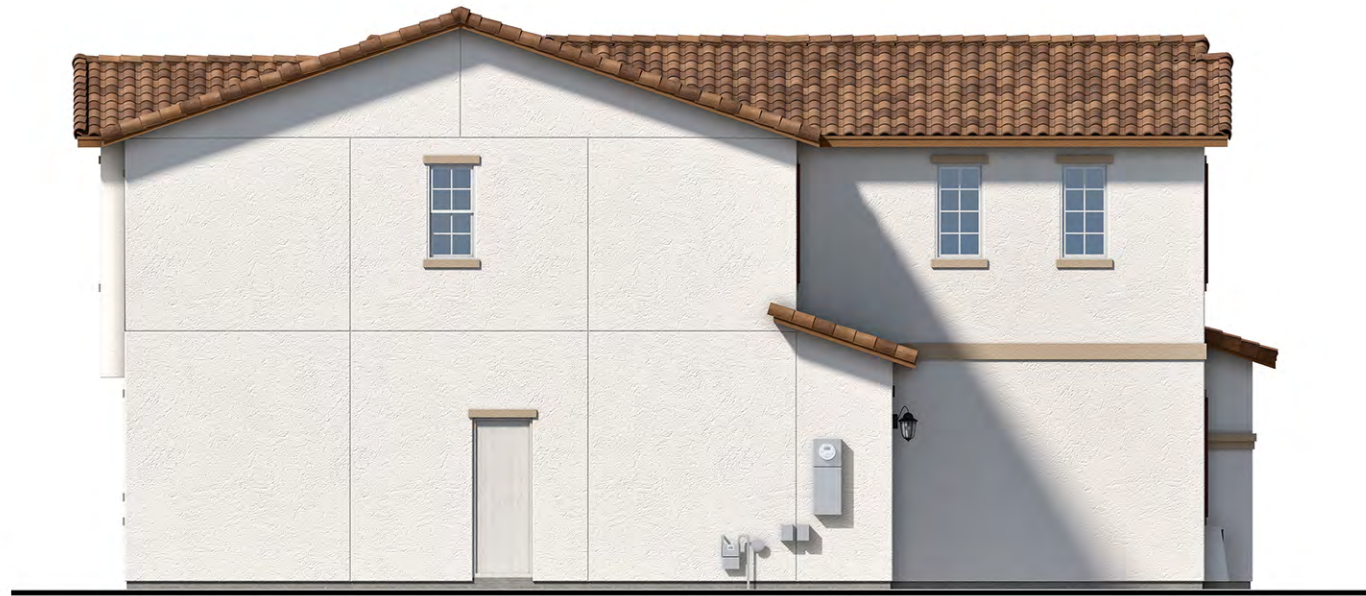
249.2380

JOB No. : 350999

STORY: Two

REV: December 12, 2022

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Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

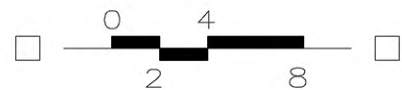


Right Elevation 'A' (Spanish Colonial)  
Visible from Towhee Street & Dolomite Street



Rear Elevation 'A' (Spanish Colonial)  
Visible from Kinglet Avenue & Archibald Avenue

Elevation 'A' (Spanish Colonial)  
at Lots 1, 13 & 25



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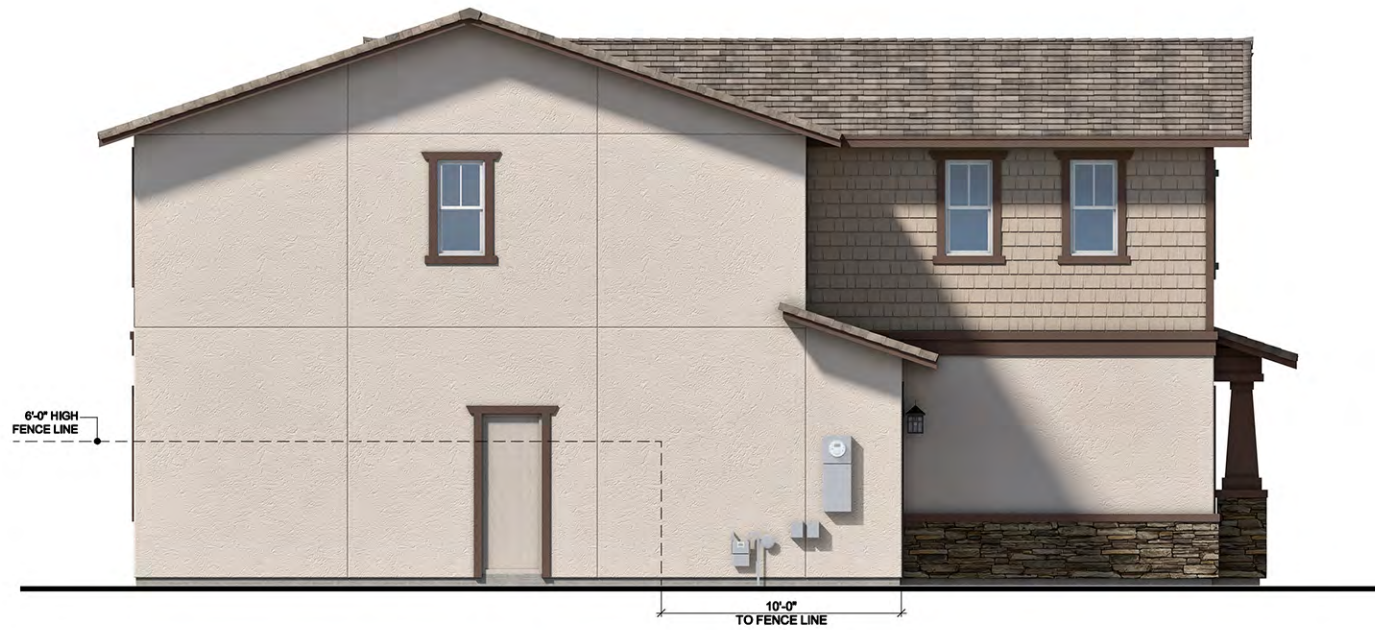
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

PLAN 1

249.2380

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

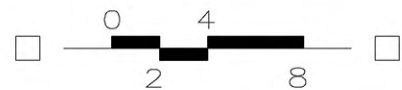


Right Elevation 'B' (Craftsman)  
Visible from Dolomite Street



Rear Elevation 'B' (Craftsman)

Elevation 'B' (Craftsman)  
at Lot 32





Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

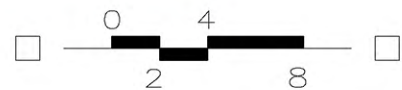


Right Elevation 'B' (Craftsman)  
Visible from Towhee Street



Rear Elevation 'B' (Craftsman)  
Visible from Archibald Avenue

Elevation 'B' (Craftsman)  
at Lot 12



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COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

PLAN 1

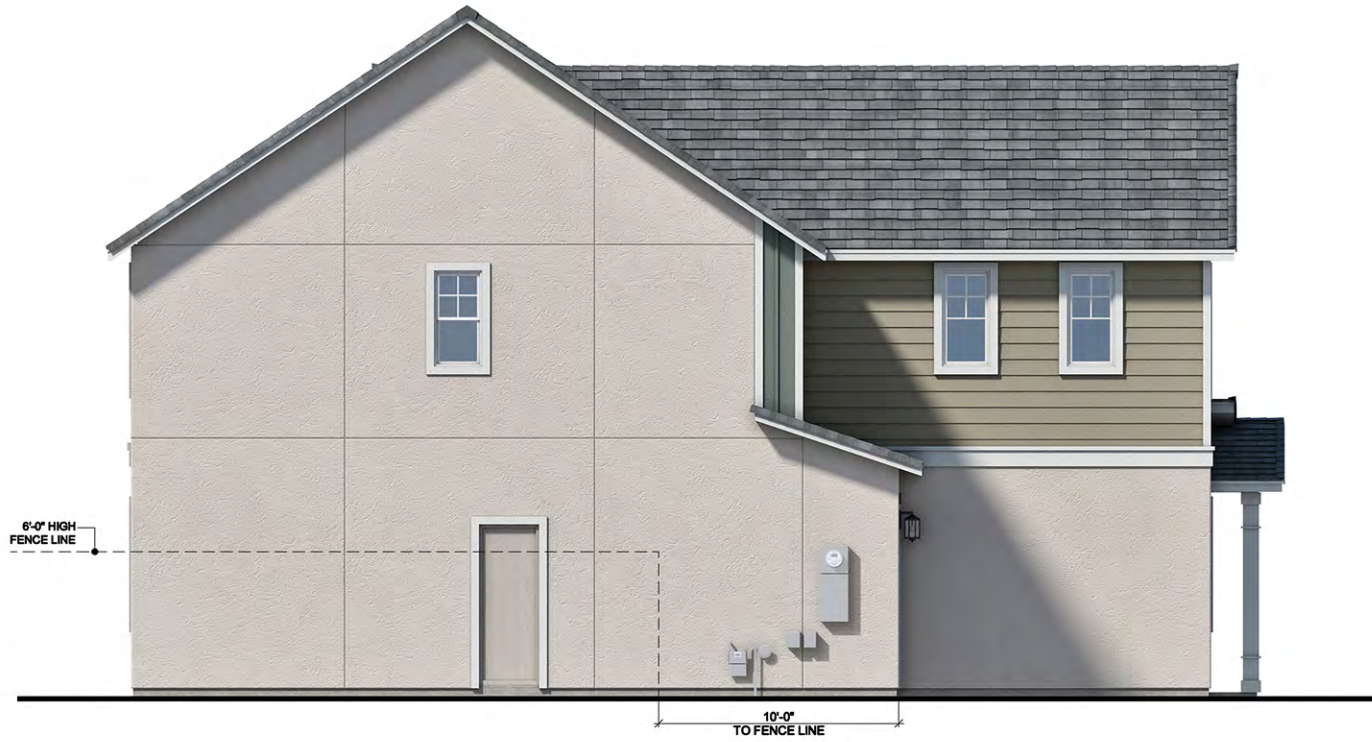
249.2380

JOB No. : 350999

STORY: Two

REV: December 12, 2022

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Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

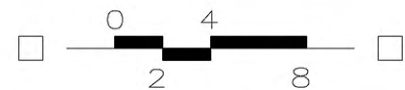


Right Elevation 'C' (American Farmhouse)  
Visible from Towhee Street & Dolomite Street



Rear Elevation 'C' (American Farmhouse)

Elevation 'C' (American Farmhouse)  
at Lots 8, 21 & 33



PLAN 1



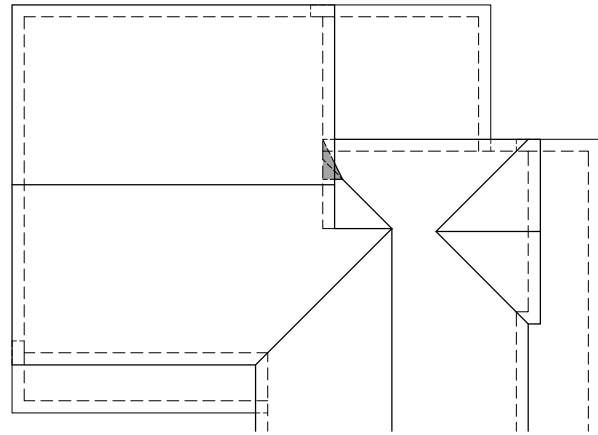
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ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

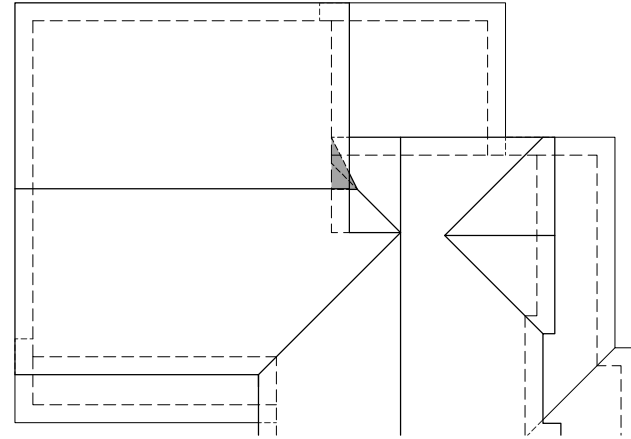
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JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

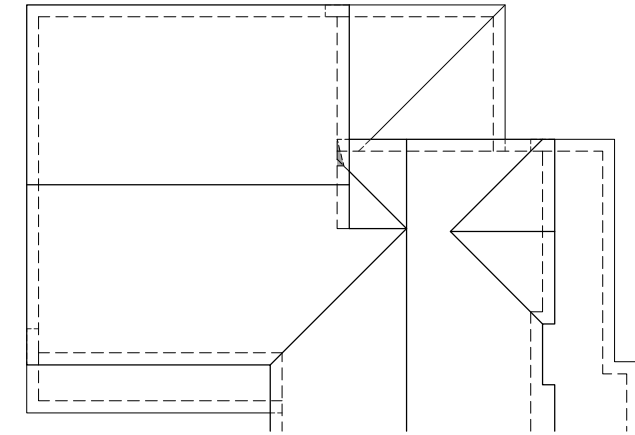
Item D - 52 of 201



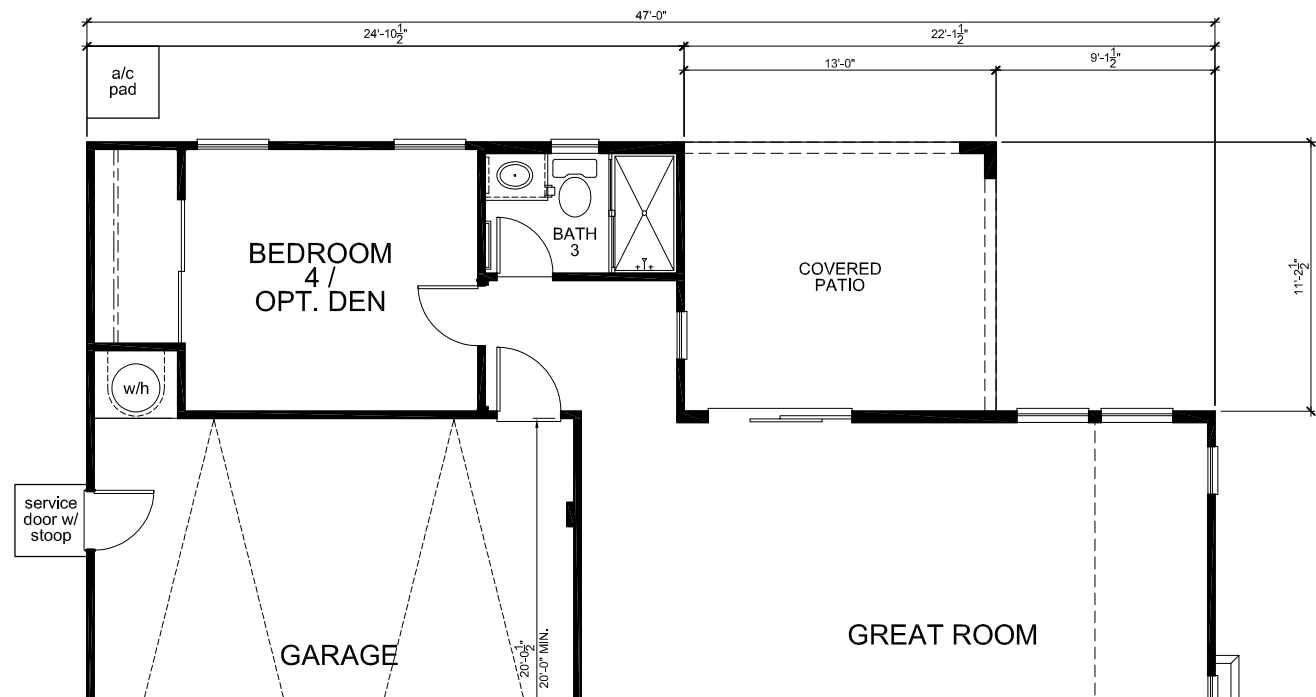
Partial Roof Plan 'A' (Spanish Colonial)  
at Covered Patio



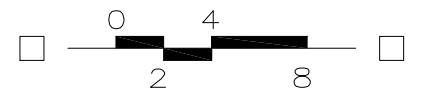
Partial Roof Plan 'B' (Craftsman)  
at Covered Patio



Partial Roof Plan 'C' (American Farmhouse)  
at Covered Patio



Partial First Floor Plan  
at Covered Patio



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249.2380

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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Partial Left Elevation 'B' (Craftsman)  
at Covered Patio



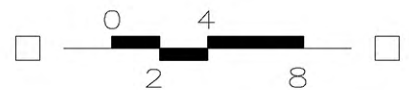
Rear Elevation 'B' (Craftsman)  
at Covered Patio



Partial Left Elevation 'A' (Spanish Colonial)  
at Covered Patio



Rear Elevation 'A' (Spanish Colonial)  
at Covered Patio

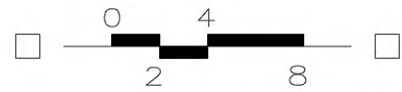




Partial Left Elevation 'C' (American Farmhouse)  
at Covered Patio



Rear Elevation 'C' (American Farmhouse)  
at Covered Patio



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JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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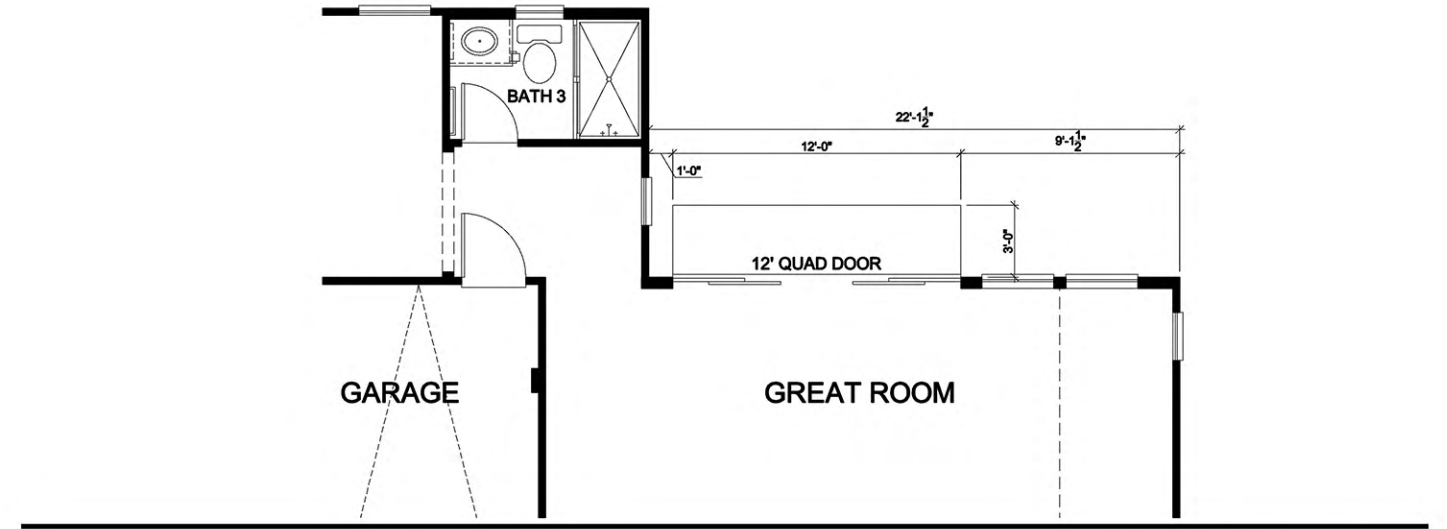
Rear Elevation 'A' at Quad Door Option



Rear Elevation 'B' at Quad Door Option



Rear Elevation 'C' at Quad Door Option

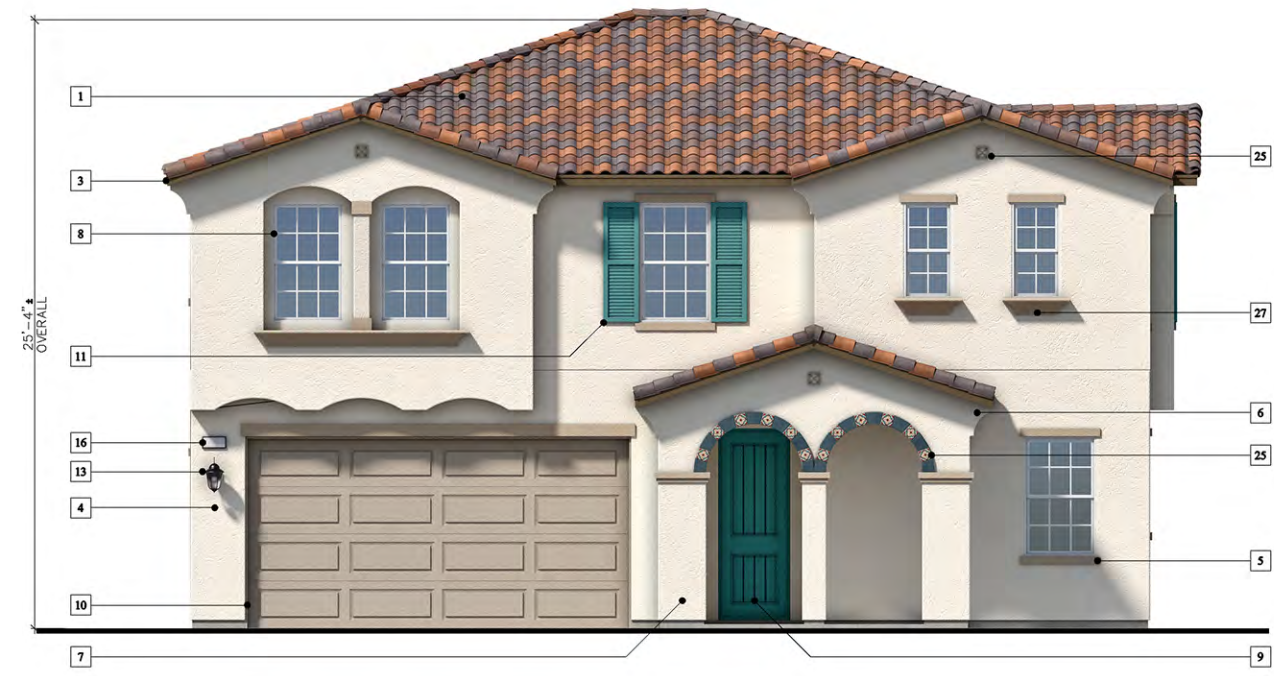


Quad Door Option



**ELEVATION LEGEND**

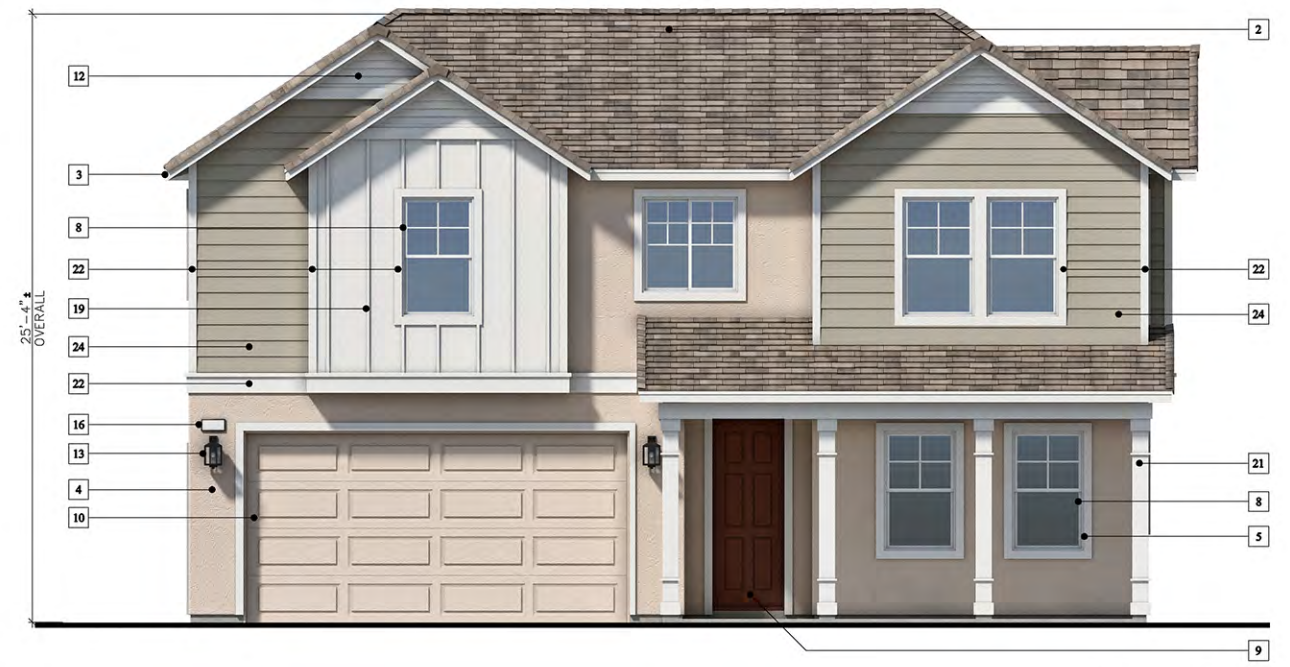
- |    |   |    |   |
|----|---|----|---|
| 1  | CONCRETE 'S' TILE ROOFING                                   | 15 | RESERVED                                  |
| 2  | CONCRETE FLAT TILE ROOFING                                  | 16 | ADDRESS PLAQUE                            |
| 3  | WOOD FASCIA BOARD   | 17 | WOOD CORBEL                               |
| 4  | STUCCO FINISH (16/20 SAND)                                  | 18 | RESERVED                                  |
| 5  | STUCCO OVER FOAM TRIM                                       | 19 | FIBER CEMENT SIDING WITH VERTICAL BATTENS |
| 6  | DECORATIVE STUCCO FINISH OVER FOAM CORBELS/SHAPED FOAM TRIM | 20 | PRE-FAB OUTLOOKER                         |
| 7  | STUCCO COLUMNS  | 21 | WOOD POST                                 |
| 8  | VINYL WINDOW SYSTEM   | 22 | WOOD TRIM                                 |
| 9  | COMPOSITE ENTRY DOOR  | 23 | RESERVED                                  |
| 10 | METAL SECTIONAL GARAGE DOOR                                 | 24 | FIBER CEMENT 8" LAP SIDING, U.N.O.        |
| 11 | DECORATIVE SHUTTERS   | 25 | DECORATIVE CERAMIC TILES                  |
| 12 | DECORATIVE GABLE END DETAIL                                 | 26 | RESERVED                                  |
| 13 | COACH LIGHTS  | 27 | STUCCO SHELF                              |
| 14 | STONE VENEER  | 28 | FIBER CEMENT SHAKE SHINGLE SIDING         |



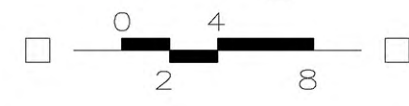
**Elevation 'A' (Spanish Colonial)**

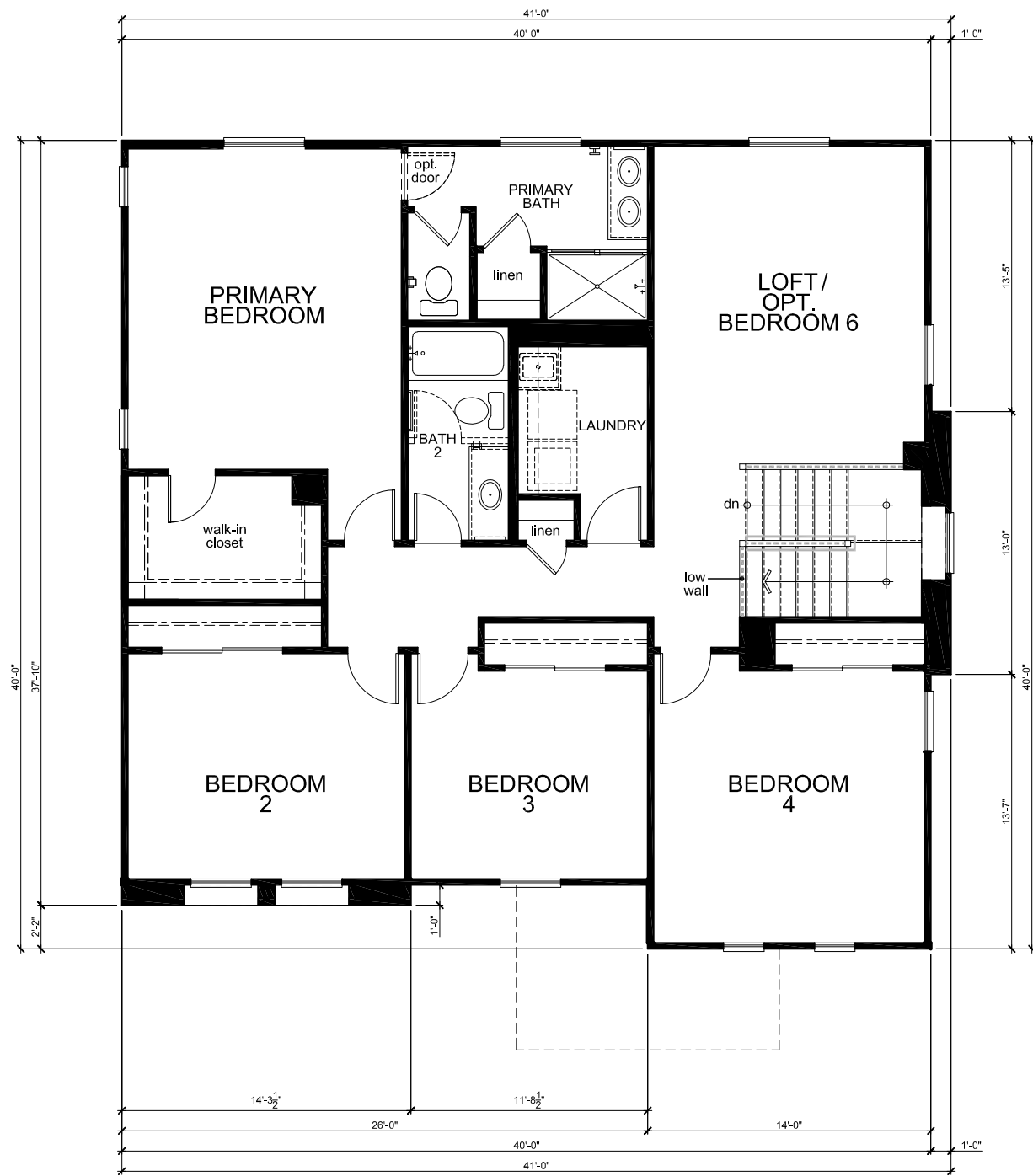


**Elevation 'B' (Craftsman)**

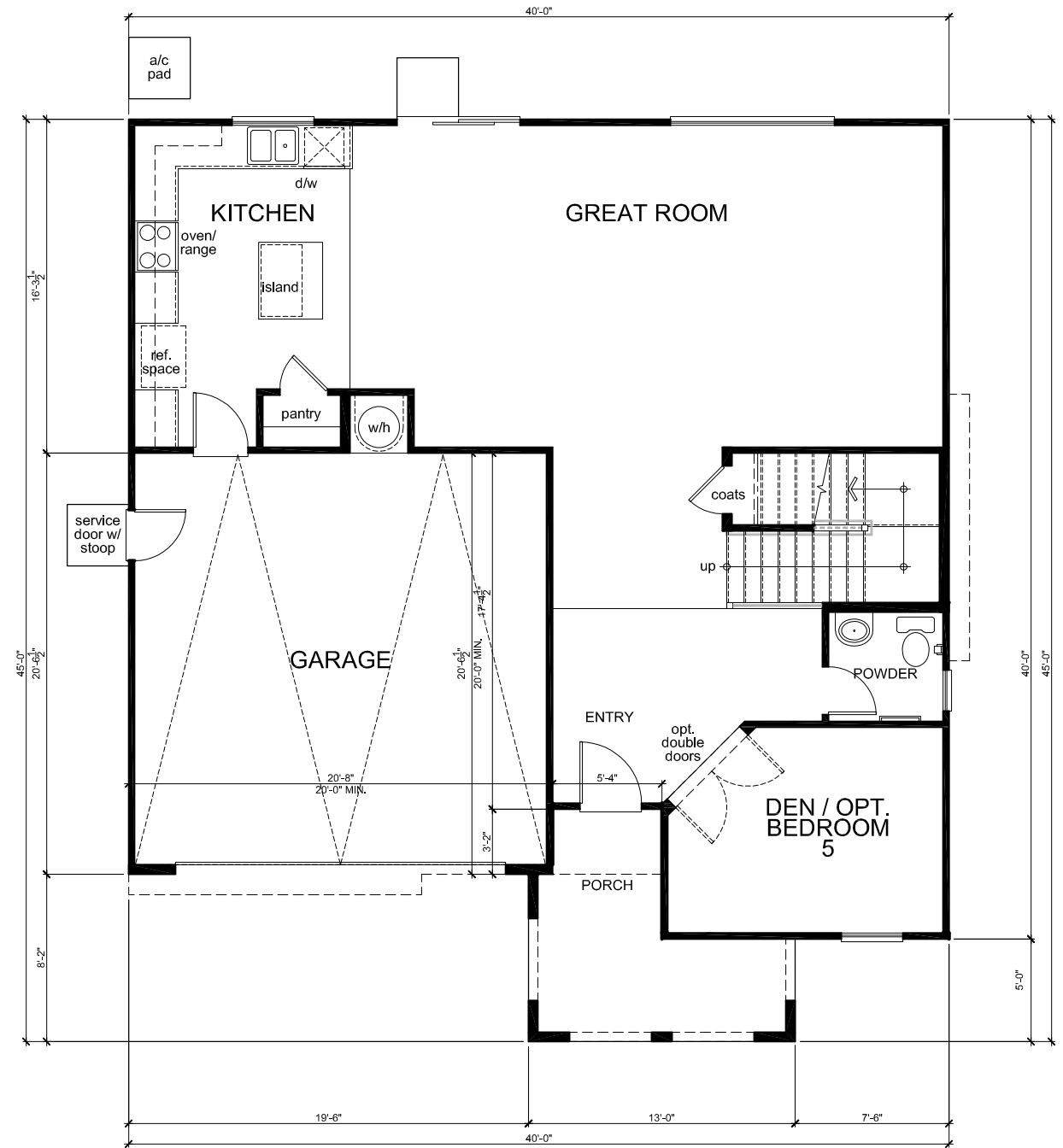


**Elevation 'C' (American Farmhouse)**





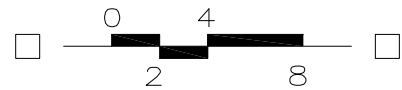
Second Floor Plan 'A'

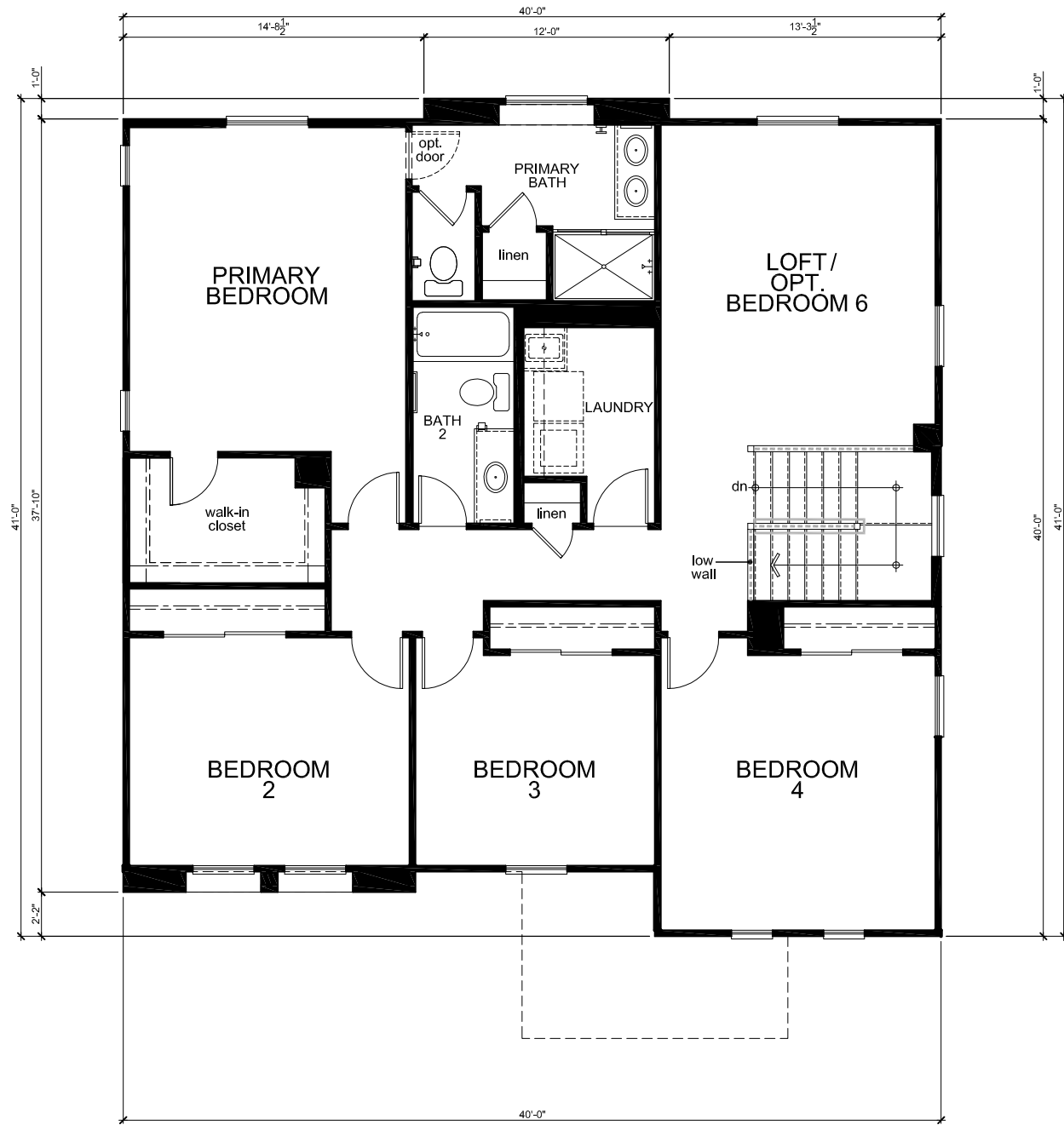


First Floor Plan 'A'

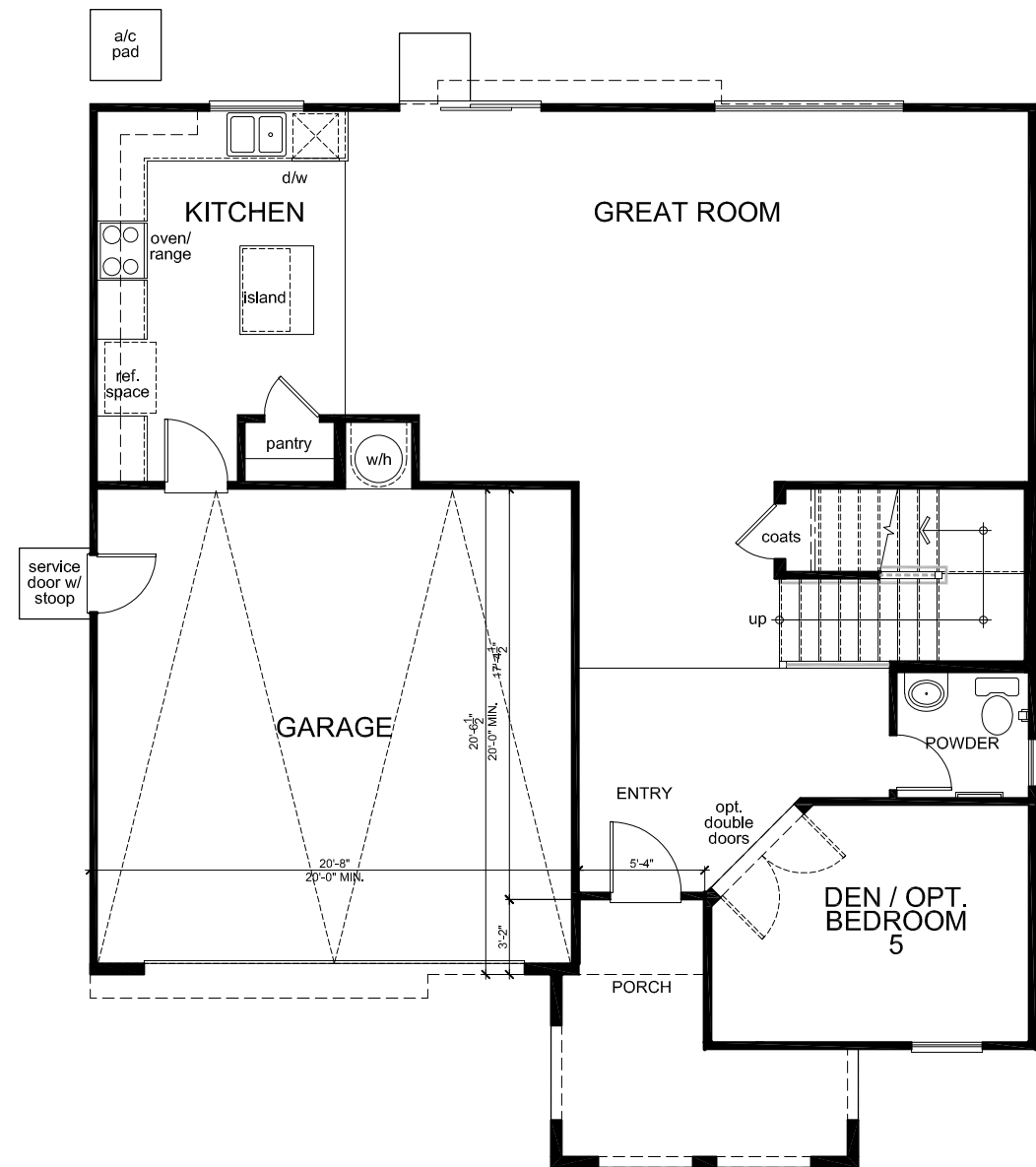
SQUARE FOOTAGE		
PLAN 240.2524		
FIRST FLOOR AREA	1079	SQ. FT.
SECOND FLOOR AREA	1451	SQ. FT.
TOTAL AREA	2524	SQ. FT.
GARAGE AREA	427	SQ. FT.
PORCH AREA(S)	ELEVATION 'A'	102 SQ. FT.
	ELEVATION 'B'	165 SQ. FT.
	ELEVATION 'C'	138 SQ. FT.
OPTIONS:		
COVERED PATIO	ALL ELEVATIONS	150 SQ. FT.

Elevation 'A' (Spanish Colonial)  
at Lot 34



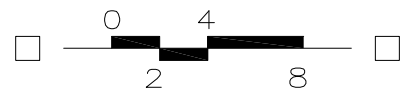


Second Floor Plan 'A'



First Floor Plan 'A'

Elevation 'A' (Spanish Colonial) at Lot 2



PLAN 2



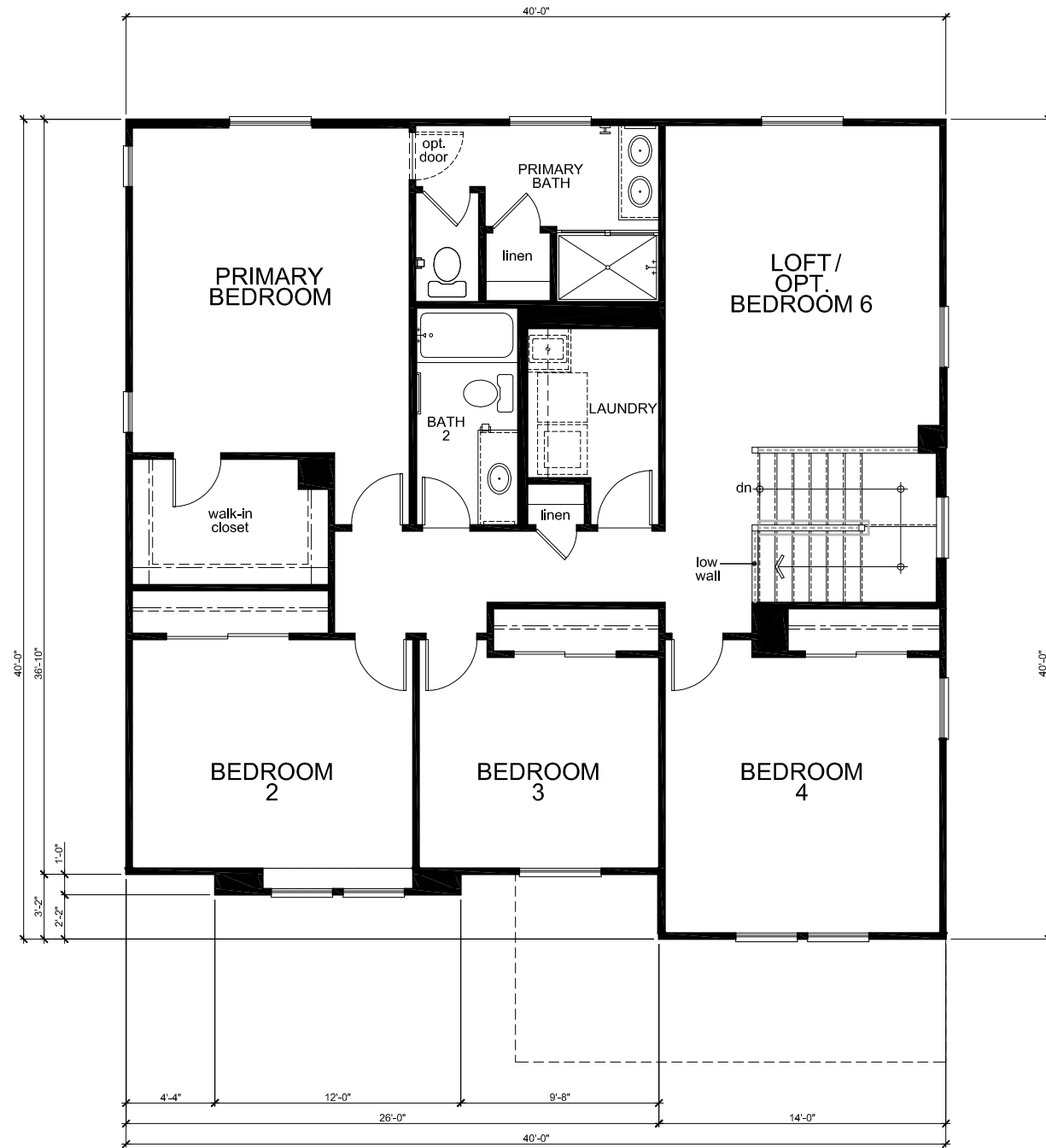
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 ONTARIO, CA

COUNTRYSIDE 36  
 TENTATIVE TRACT MAP 18916

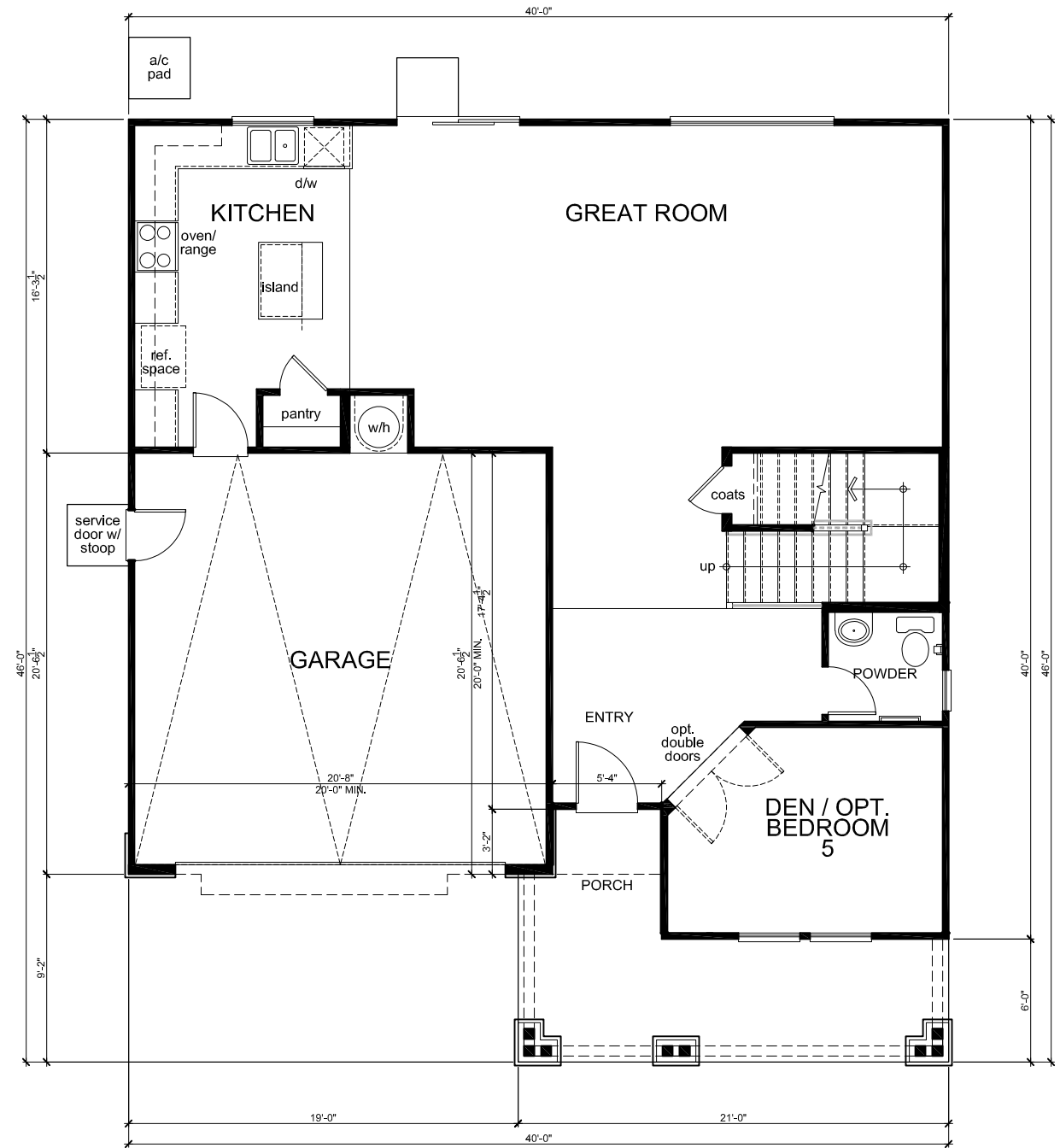
240.2524

JOB No. : 350999  
 STORY: Two  
 REV: December 12, 2022

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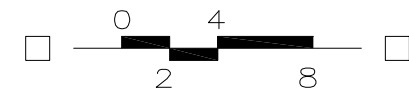
Second Floor Plan 'B'



First Floor Plan 'B'

SQUARE FOOTAGE			
PLAN 240.2524			
FIRST FLOOR AREA	1079	SQ. FT.	
SECOND FLOOR AREA	1451	SQ. FT.	
TOTAL AREA	2524	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA(S)	ELEVATION 'A'	102	SQ. FT.
	ELEVATION 'B'	165	SQ. FT.
	ELEVATION 'C'	138	SQ. FT.
OPTIONS:			
COVERED PATIO	ALL ELEVATIONS	150	SQ. FT.

Elevation 'B' (Craftsman)  
at Lot 7



PLAN 2

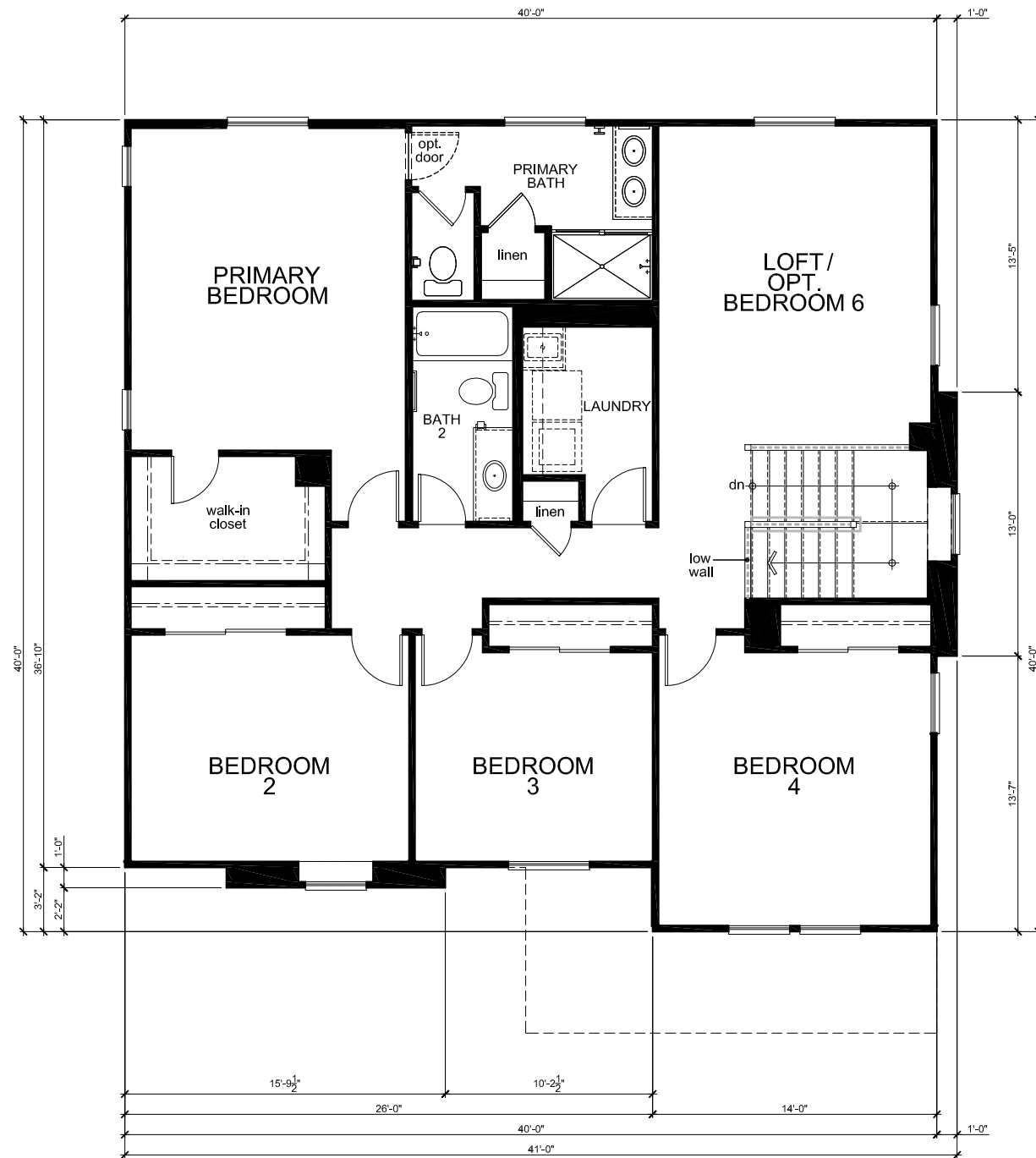


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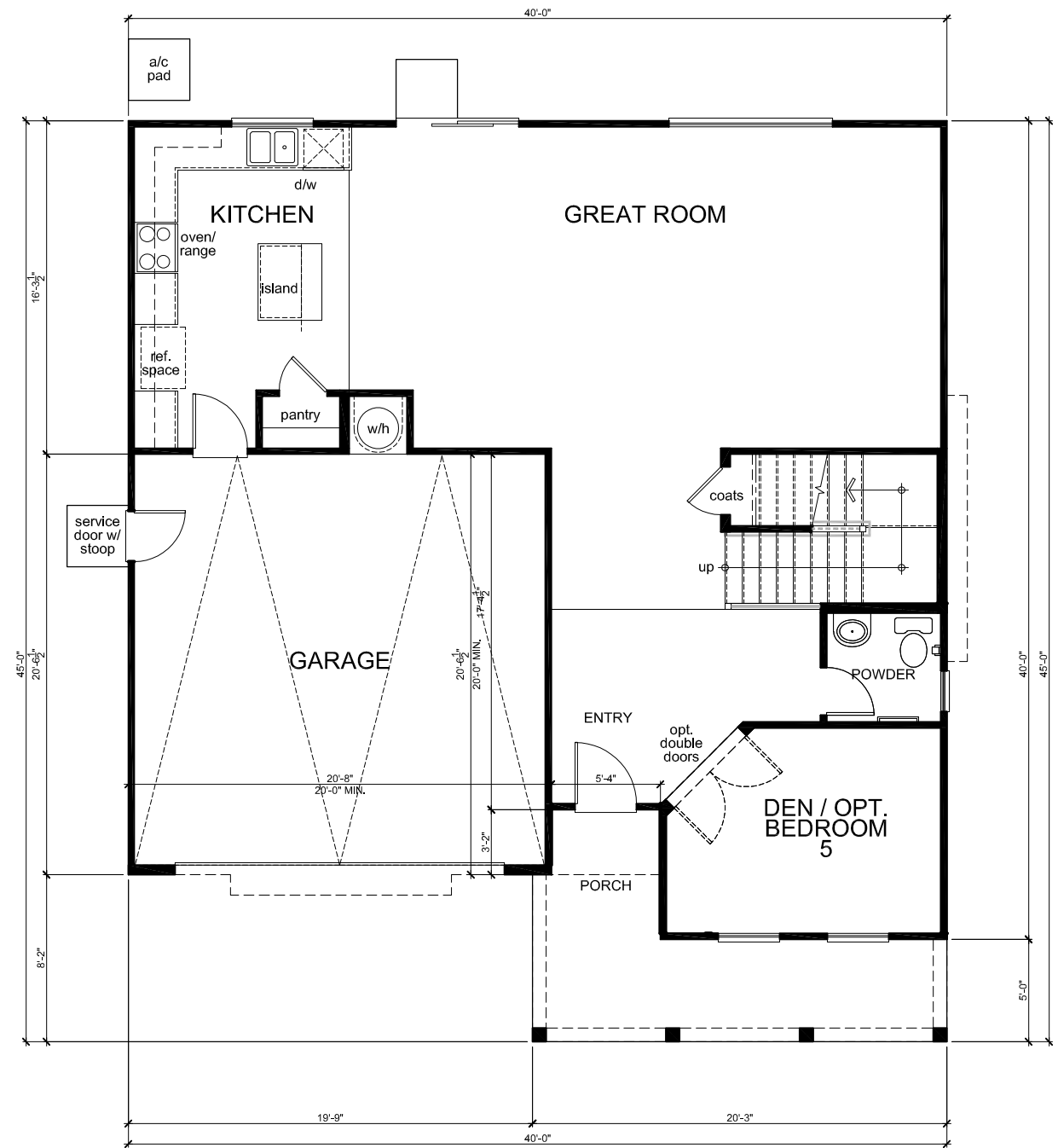
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

240.2524

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022



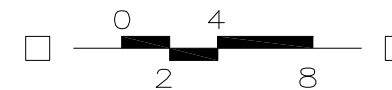
Second Floor Plan 'C'



First Floor Plan 'C'

SQUARE FOOTAGE			
PLAN 240.2524			
FIRST FLOOR AREA	1079	SQ. FT.	
SECOND FLOOR AREA	1451	SQ. FT.	
TOTAL AREA	2524	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA(S)	ELEVATION 'A'	102	SQ. FT.
	ELEVATION 'B'	165	SQ. FT.
	ELEVATION 'C'	138	SQ. FT.
OPTIONS:			
COVERED PATIO	ALL ELEVATIONS	150	SQ. FT.

Elevation 'C' (American Farmhouse)  
at Lot 19



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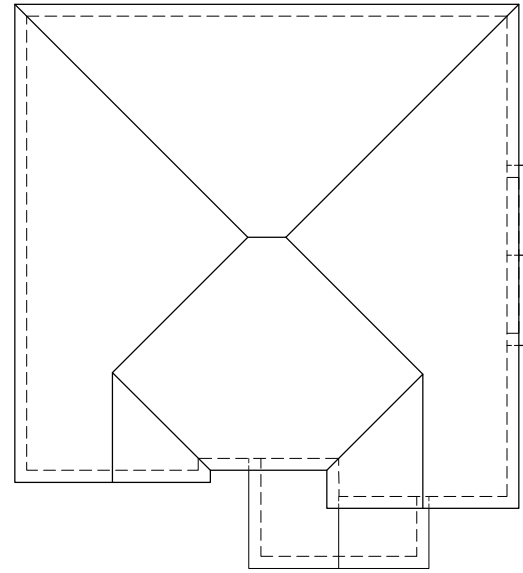
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

PLAN 2

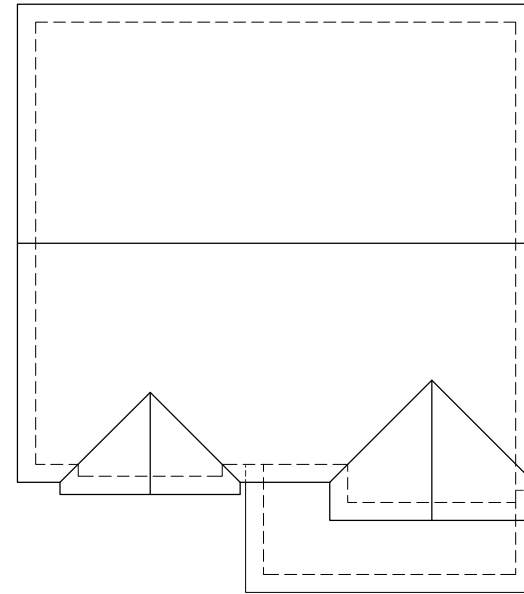
240.2524

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

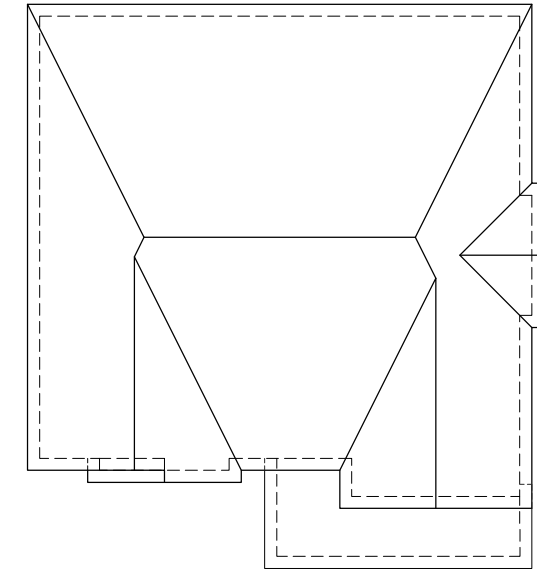
Item D - 61 of 201



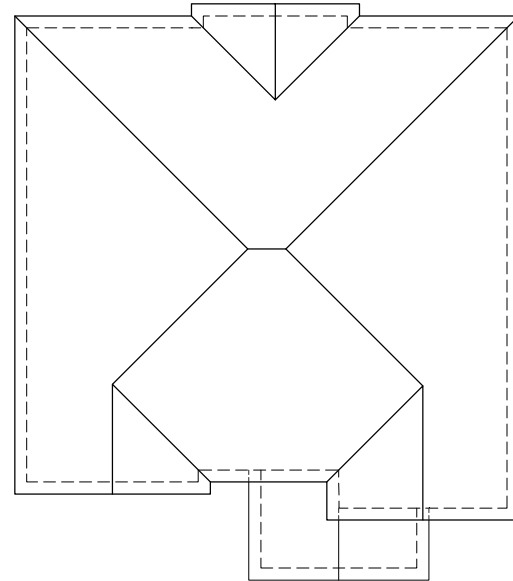
**'A' Spanish Colonial**  
at Lot 34  
 4:12



**'B' Craftsman**  
at Lot 7  
 4:12



**'C' American Farmhouse**  
at Lot 19  
 4:12 / 8:12



**'A' Spanish Colonial**  
at Lot 2  
 4:12

Roof Plans

COUNTRYSIDE 36  
 TENTATIVE TRACT MAP 18916

PLAN 2

240.2524

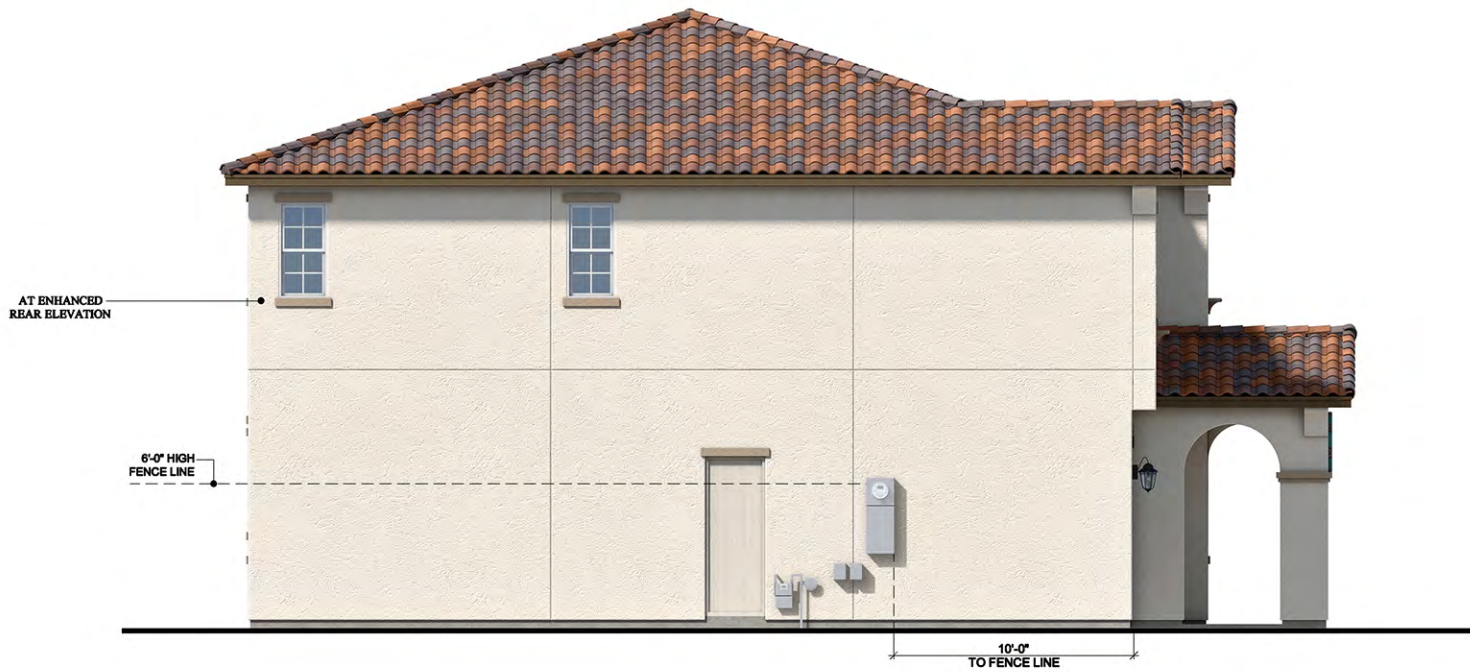
JOB No. : 350999  
 STORY: Two

REV: December 12, 2022

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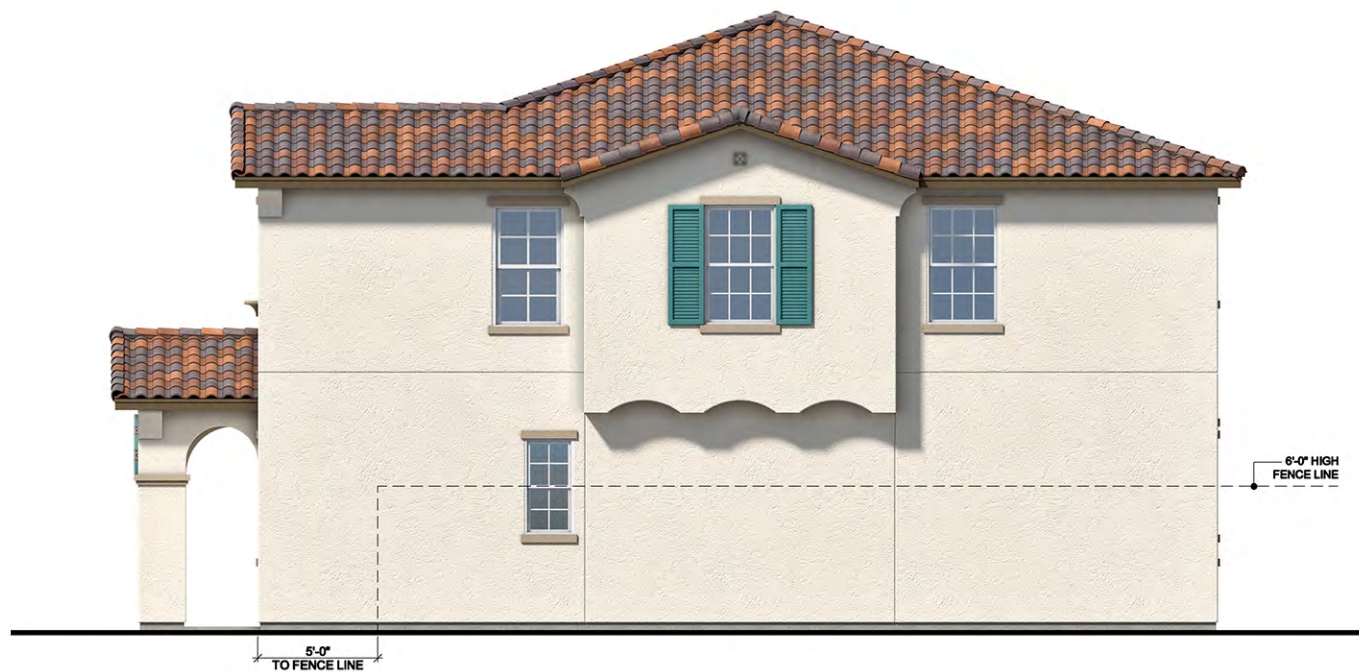
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Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

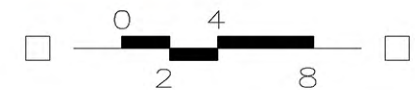


Right Elevation 'A' (Spanish Colonial)  
Visible from the Green Belt

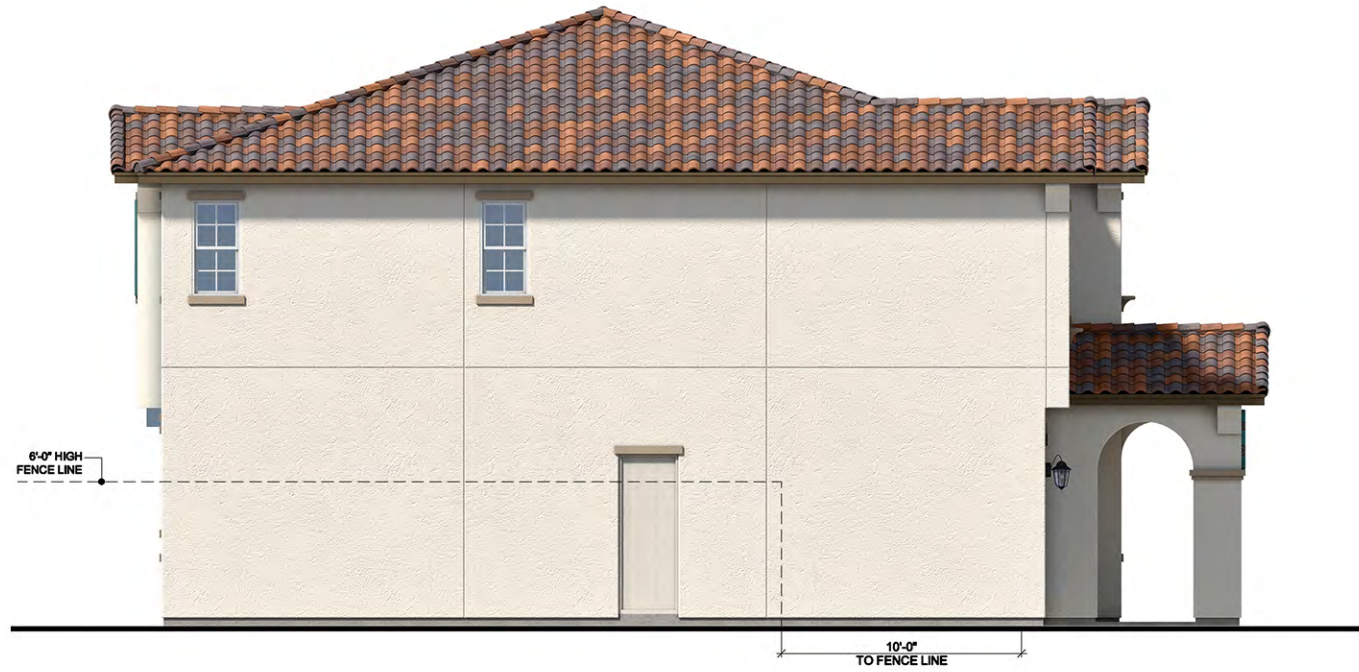


Rear Elevation 'A' (Spanish Colonial)

Elevation 'A' (Spanish Colonial)  
at Lot 34



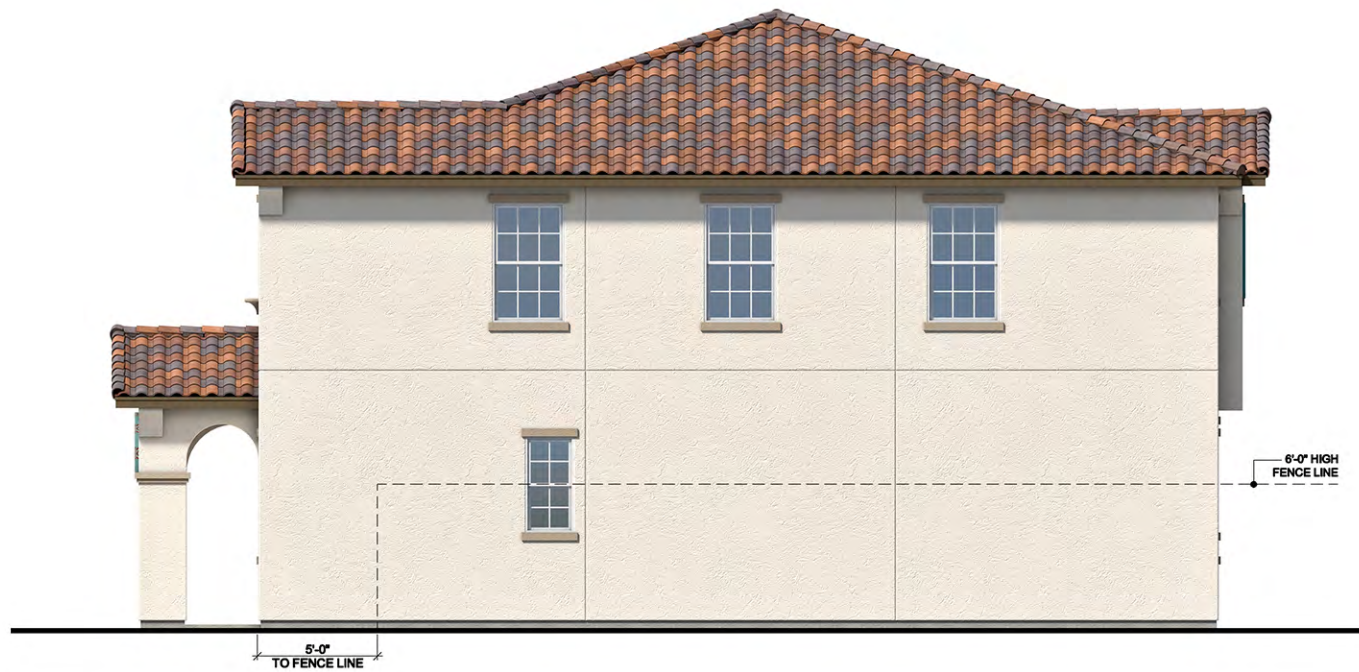




Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

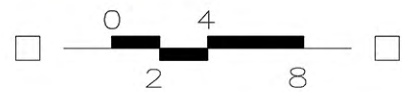


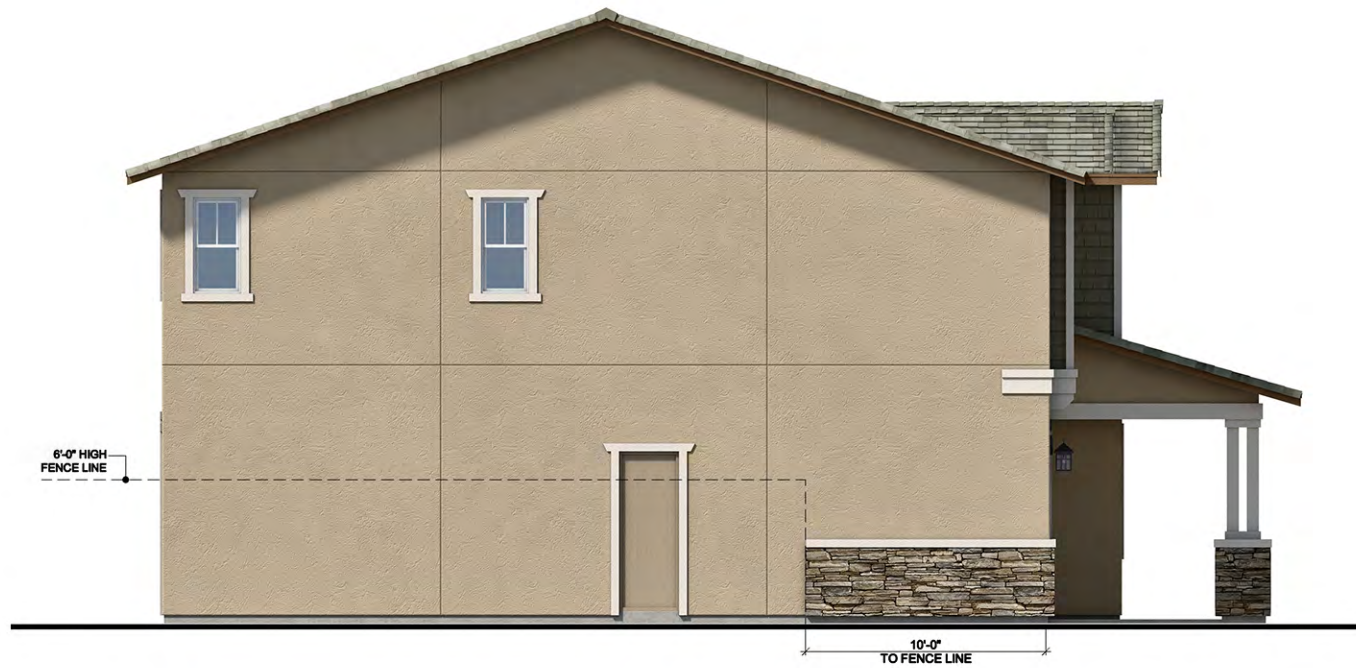
Right Elevation 'A' (Spanish Colonial)



Rear Elevation 'A' (Spanish Colonial)  
Visible from Kinglet Avenue

Elevation 'A' (Spanish Colonial)  
at Lot 2

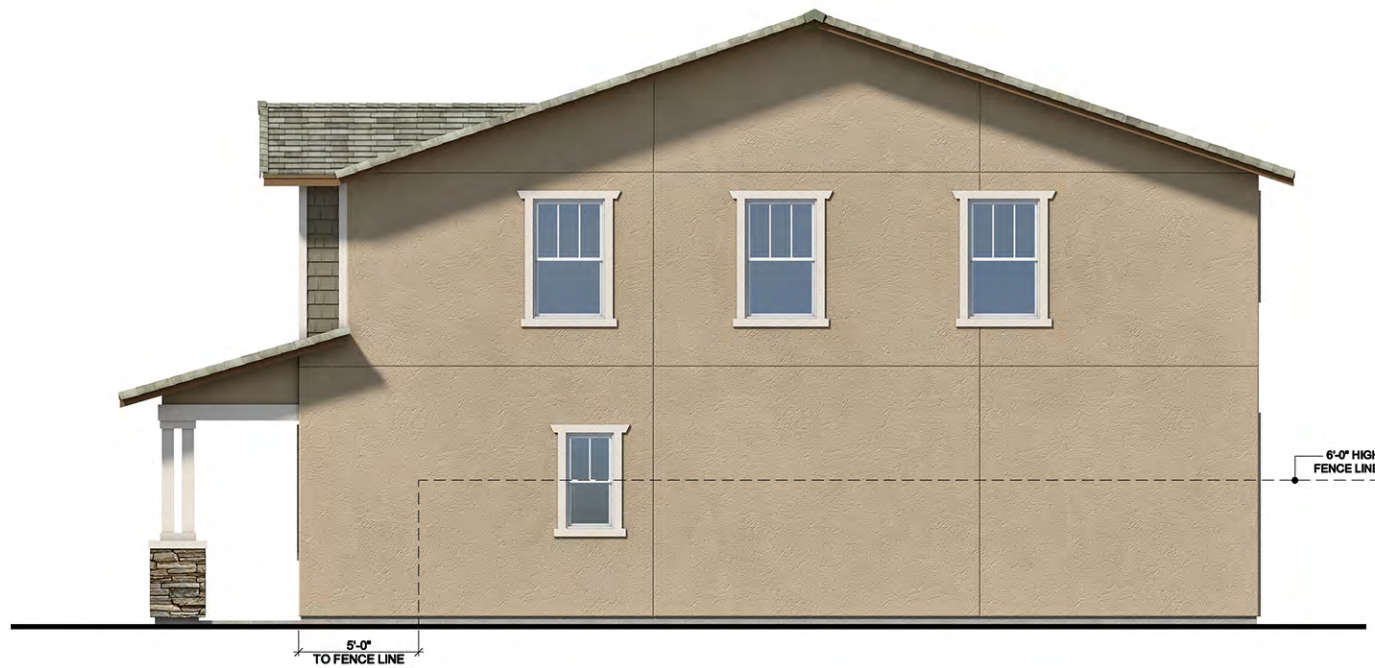




Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

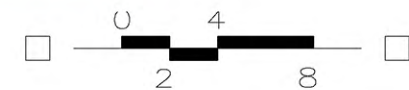


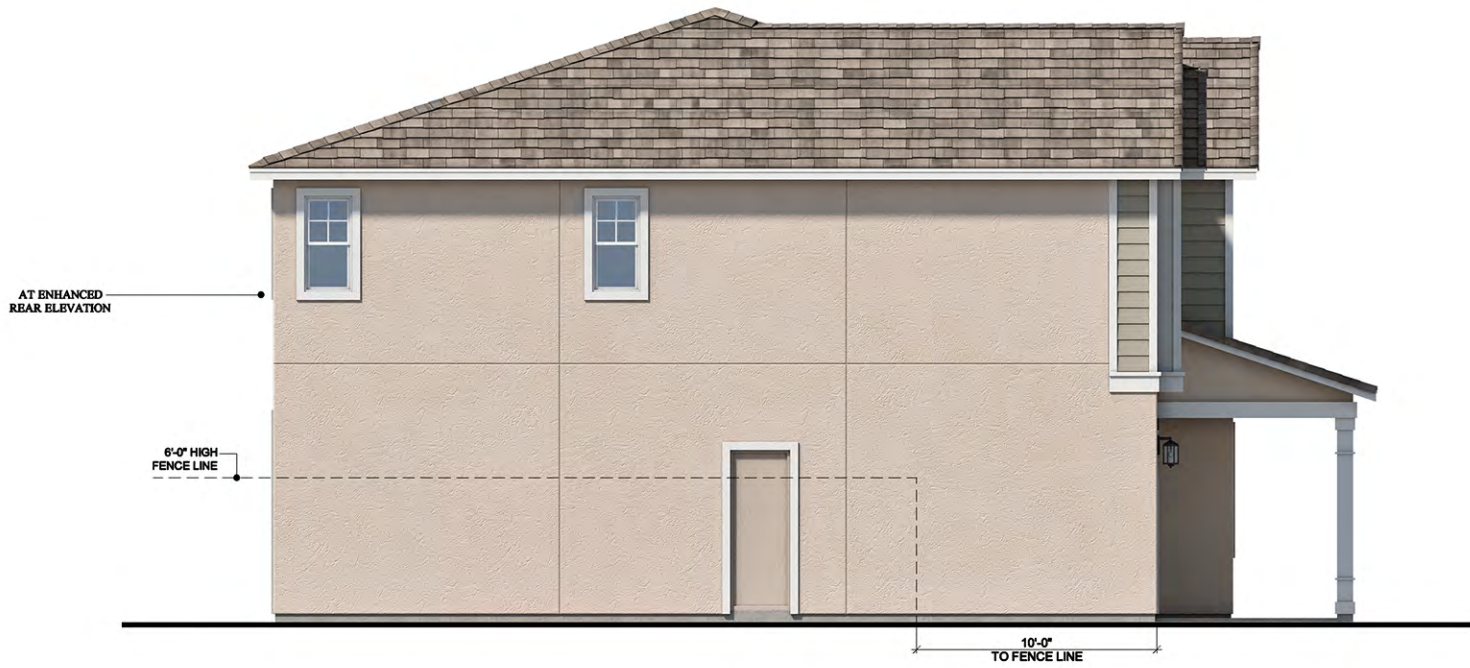
Right Elevation 'B' (Craftsman)



Rear Elevation 'B' (Craftsman)

Elevation 'B' (Craftsman)  
at Lot 7

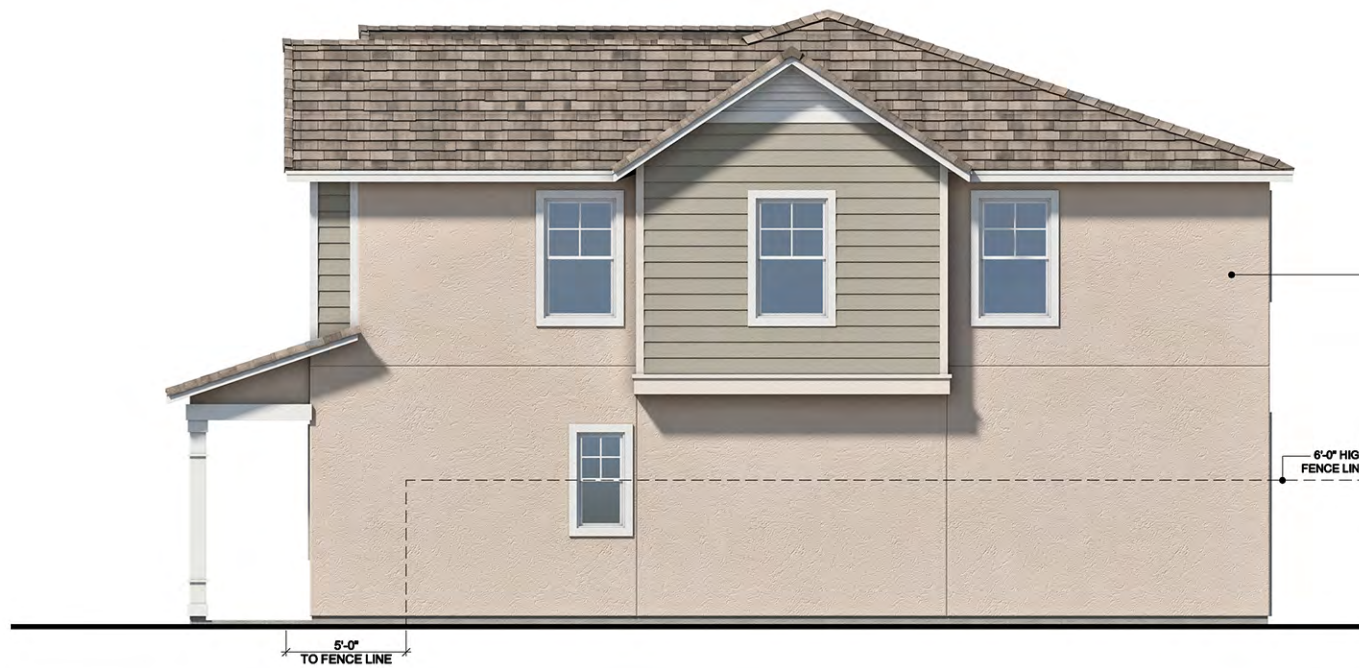




Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

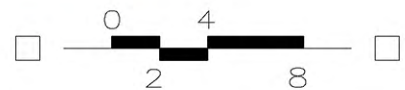


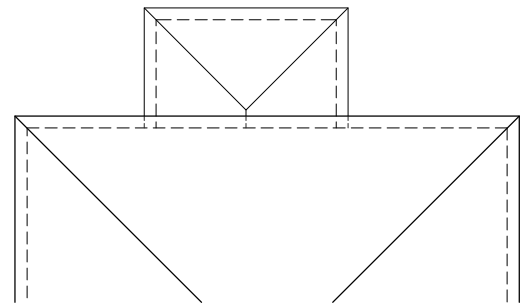
Right Elevation 'C' (American Farmhouse)  
Visible from the Green Belt

Elevation 'C' (American Farmhouse)  
at Lot 19

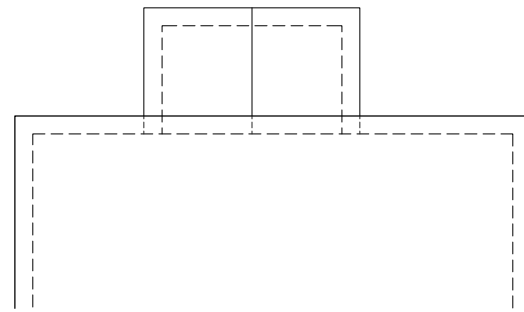


Rear Elevation 'C' (American Farmhouse)

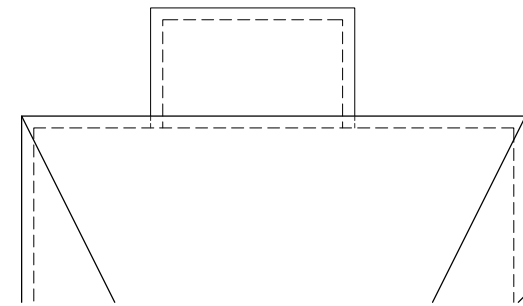




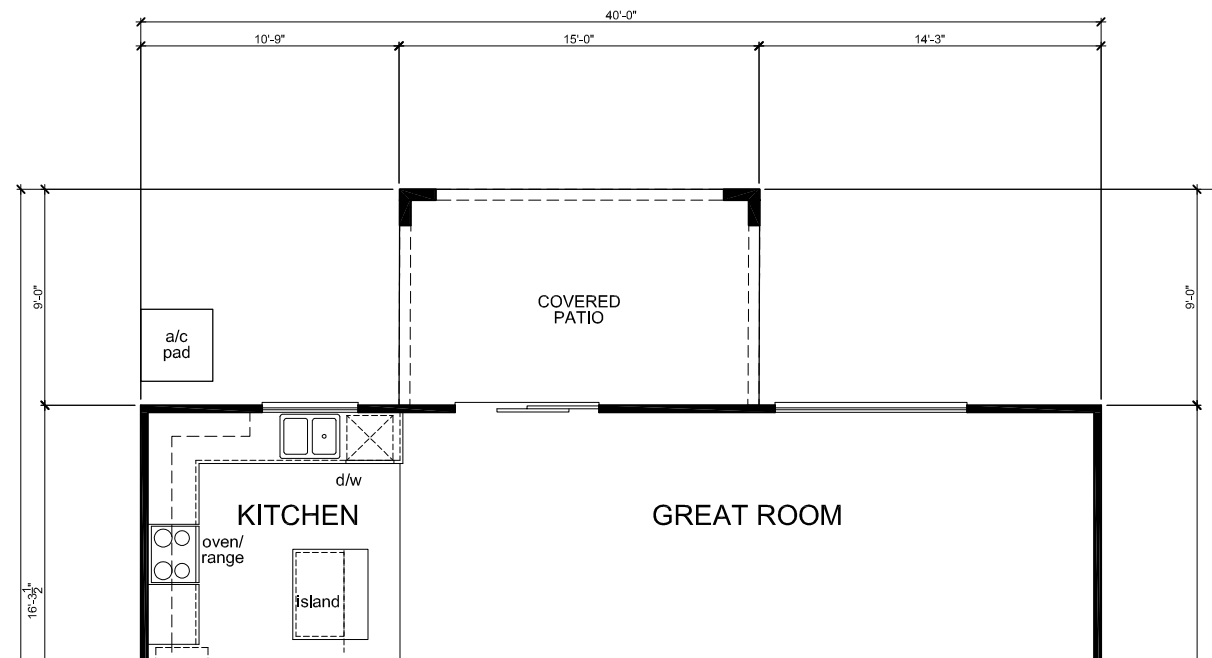
Partial Roof Plan 'A' (Spanish Colonial)  
at Covered Patio



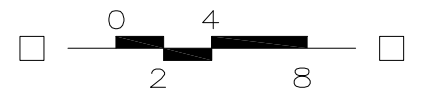
Partial Roof Plan 'B' (Craftsman)  
at Covered Patio



Partial Roof Plan 'C' (American Farmhouse)  
at Covered Patio



Partial First Floor Plan  
at Covered Patio



PLAN 2



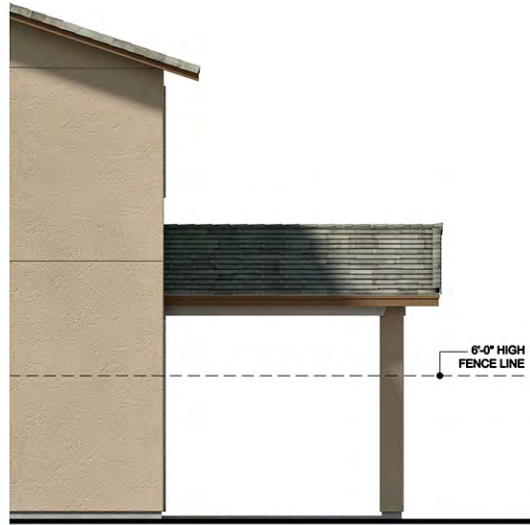
KB Home Southern California/Inland Empire  
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Wildomar, CA 92595  
(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

240.2524

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

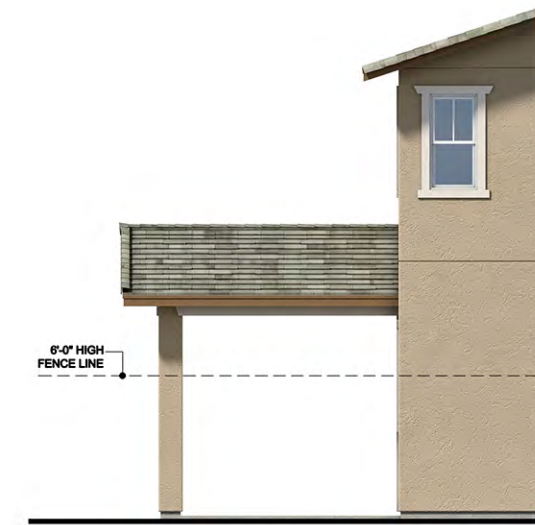
Item D - 67 of 201



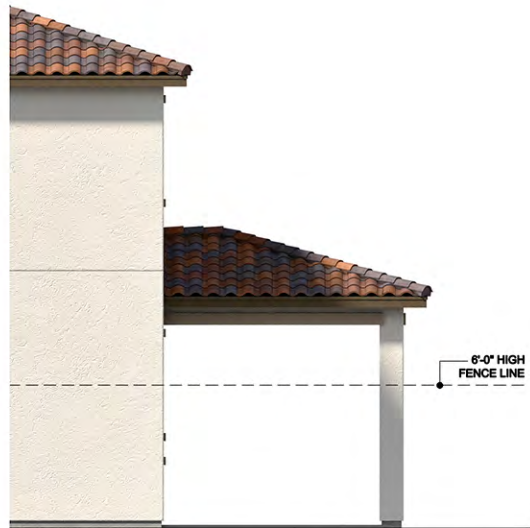
Partial Right Elevation 'B' (Craftsman)  
at Covered Patio



Rear Elevation 'B' (Craftsman)  
at Covered Patio



Partial Left Elevation 'B' (Craftsman)  
at Covered Patio



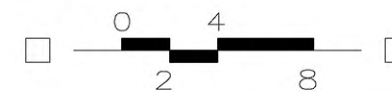
Partial Right Elevation 'A' (Spanish Colonial)  
at Covered Patio

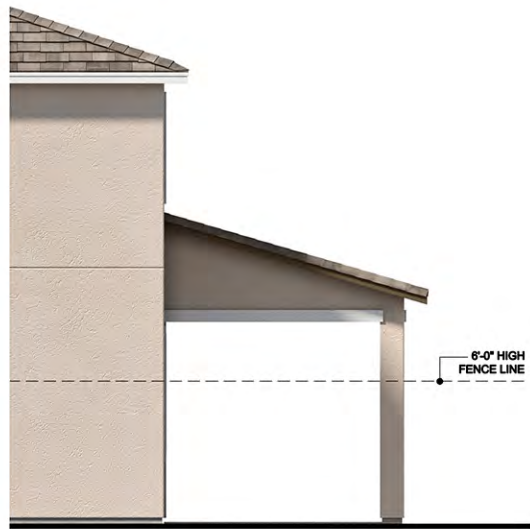


Rear Elevation 'A' (Spanish Colonial)  
at Covered Patio



Partial Left Elevation 'A' (Spanish Colonial)  
at Covered Patio

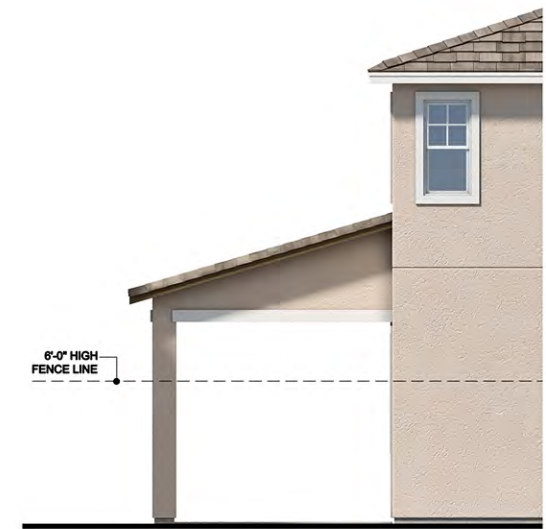




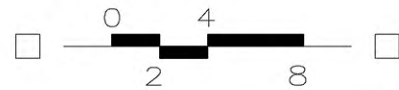
Partial Right Elevation 'C' (American Farmhouse)  
at Covered Patio



Rear Elevation 'C' (American Farmhouse)  
at Covered Patio



Partial Left Elevation 'C' (American Farmhouse)  
at Covered Patio



PLAN 2



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COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

240.2524

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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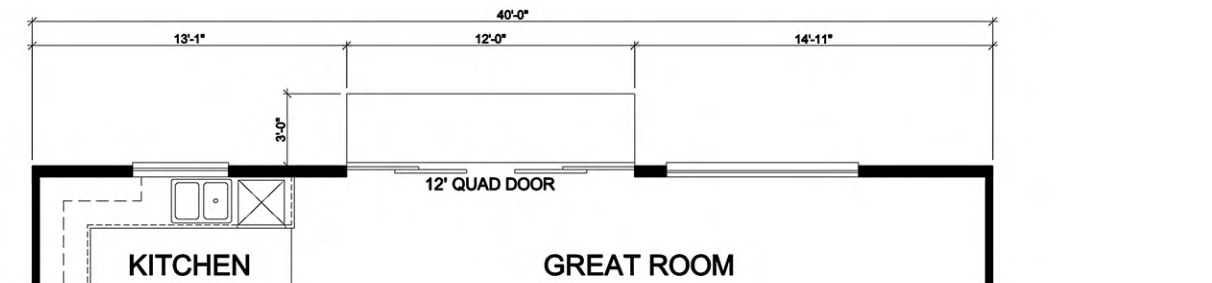
Right Elevation 'A' at Quad Door Option



Right Elevation 'B' at Quad Door Option



Right Elevation 'C' at Quad Door Option



Quad Door Option





**ELEVATION LEGEND**

- |                                |  |
|--------------------------------|--|
| 1 CONCRETE 'S' TILE ROOFING    | 15 RESERVED                                  |
| 2 CONCRETE FLAT TILE ROOFING   | 16 ADDRESS PLAQUE                            |
| 3 WOOD FASCIA BOARD            | 17 WOOD CORBEL                               |
| 4 STUCCO FINISH (16/20 SAND)   | 18 RESERVED                                  |
| 5 STUCCO OVER FOAM TRIM        | 19 FIBER CEMENT SIDING WITH VERTICAL BATTENS |
| 6 RESERVED                     | 20 PRE-FAB OUTLOOKER                         |
| 7 STUCCO COLUMNS               | 21 WOOD POST                                 |
| 8 VINYL WINDOW SYSTEM          | 22 WOOD TRIM                                 |
| 9 RESERVED                     | 23 RESERVED                                  |
| 10 METAL SECTIONAL GARAGE DOOR | 24 FIBER CEMENT 8" LAP SIDING, U.N.O.        |
| 11 DECORATIVE SHUTTERS         | 25 DECORATIVE CERAMIC TILES                  |
| 12 DECORATIVE GABLE END DETAIL | 26 RESERVED                                  |
| 13 COACH LIGHTS                | 27 STUCCO SHELF                              |
| 14 STONE VENEER                | 28 FIBER CEMENT SHAKE SHINGLE SIDING         |

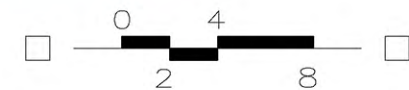
**Elevation 'A' (Spanish Colonial)**



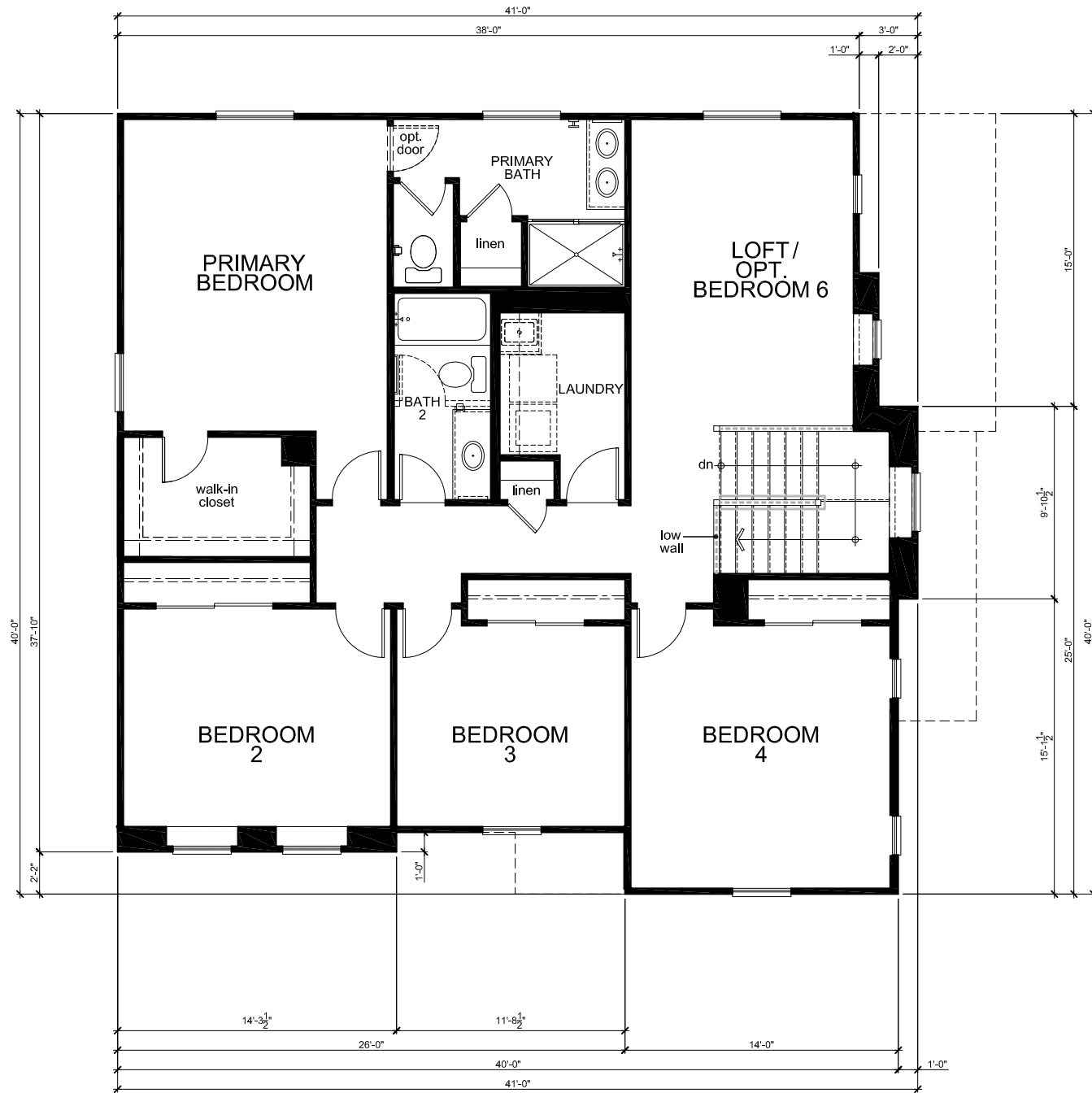
**Elevation 'B' (Craftsman)**



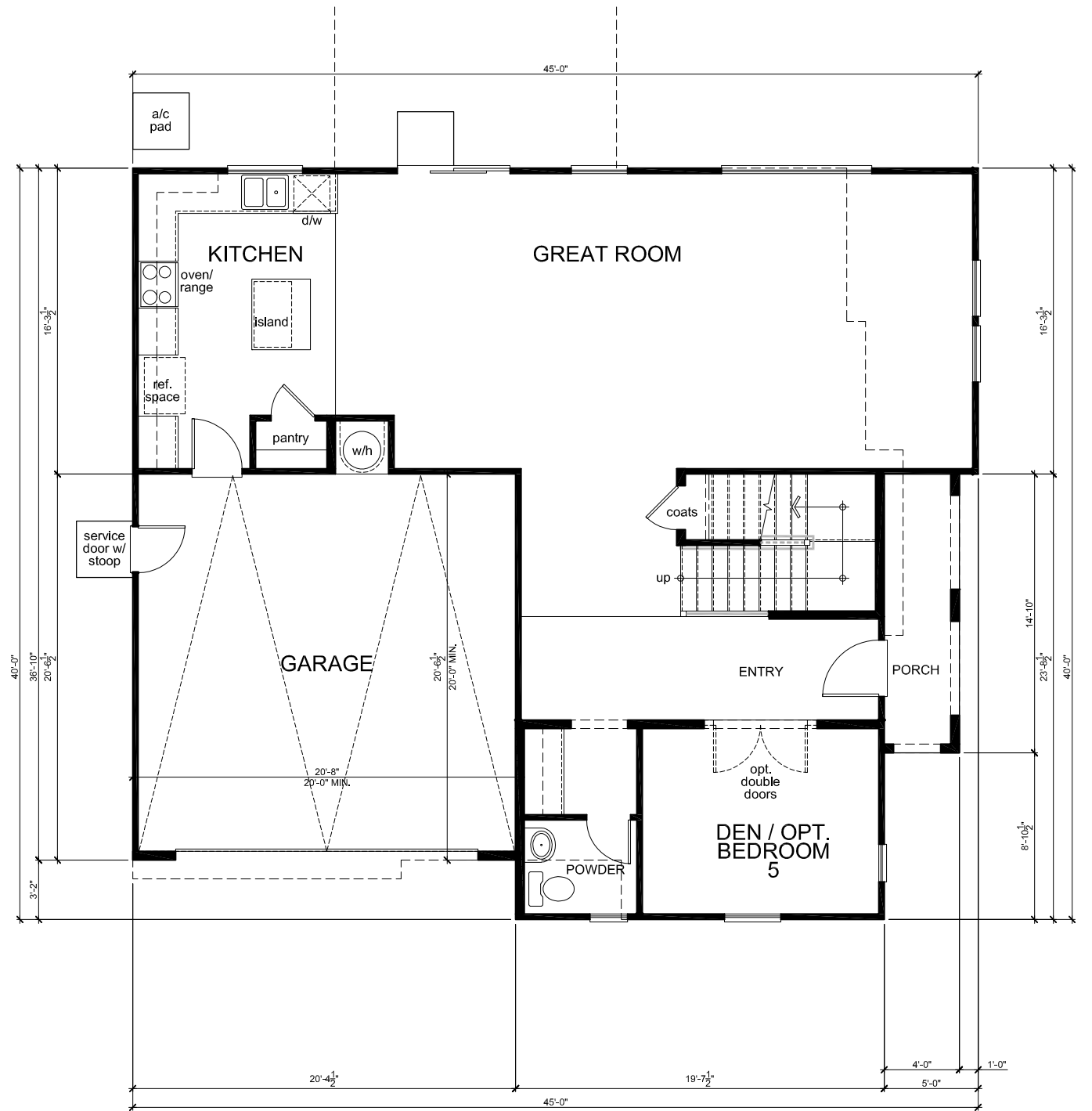
**Elevation 'C' (American Farmhouse)**







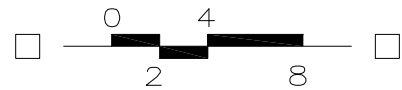
Second Floor Plan 'A'



First Floor Plan 'A'

SQUARE FOOTAGE			
PLAN 245.2609			
FIRST FLOOR AREA	1140	SQ. FT.	
SECOND FLOOR AREA	1419	SQ. FT.	
TOTAL AREA	2609	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA(S)			
ELEVATION 'A'	54	SQ. FT.	
ELEVATION 'B'	114	SQ. FT.	
ELEVATION 'C'	71	SQ. FT.	
OPTIONS:			
COVERED PATIO ALL ELEVATIONS	180	SQ. FT.	

Elevation 'A' (Spanish Colonial)  
at Lots 9 & 29



PLAN 3



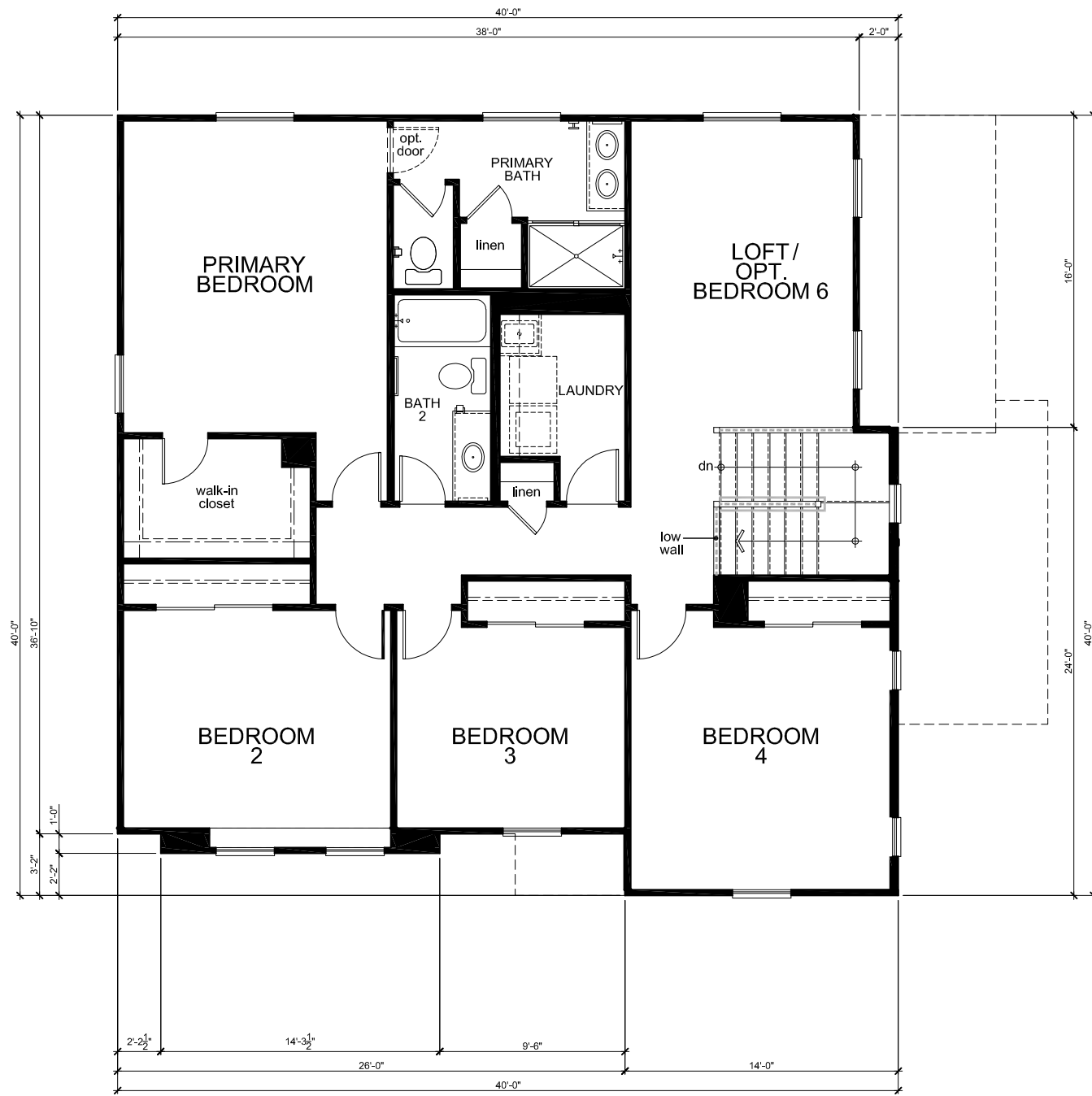
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COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

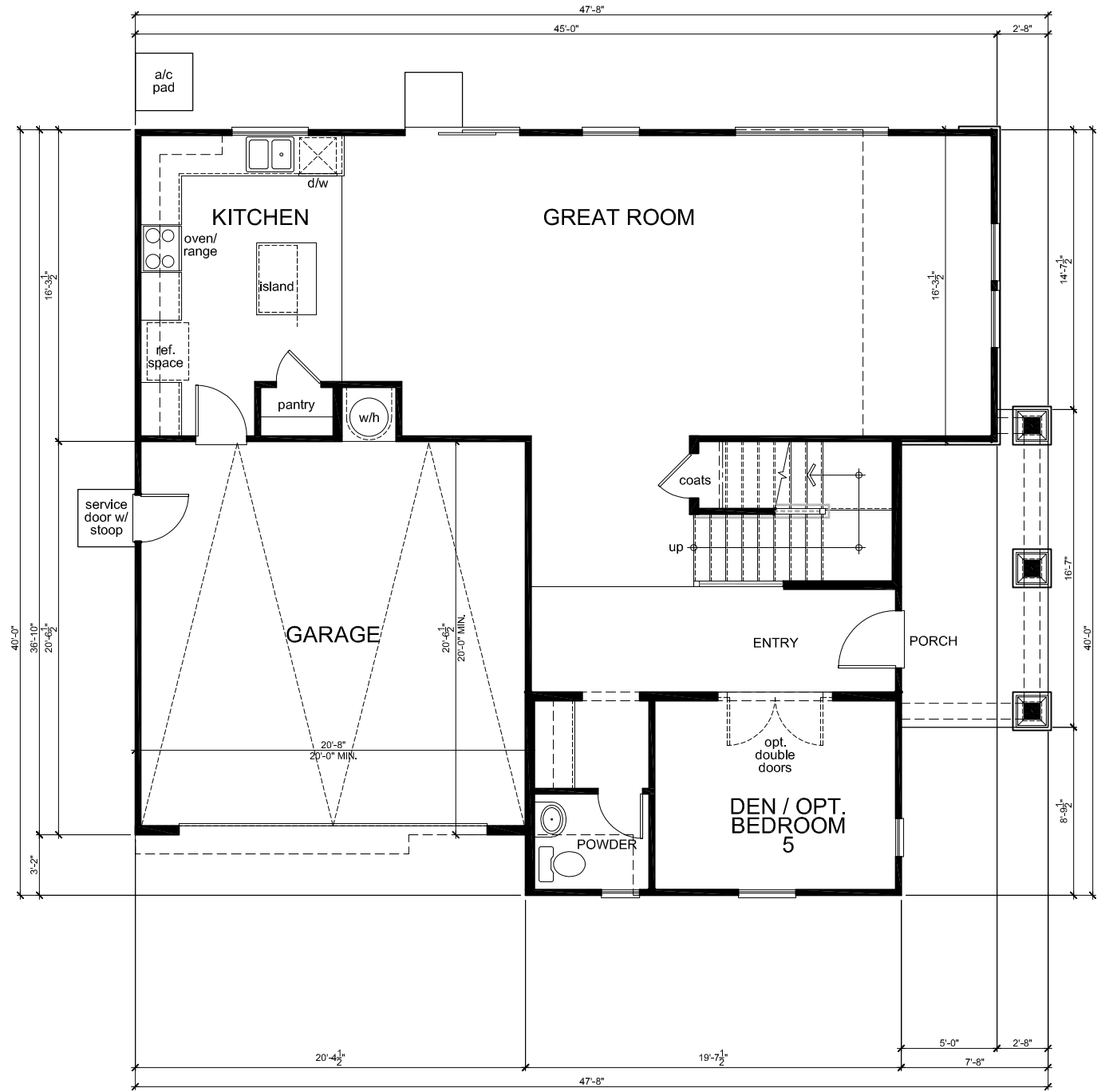
245.2609

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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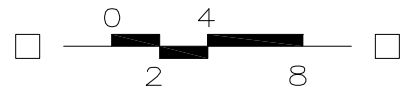
Second Floor Plan 'B'



First Floor Plan 'B'

SQUARE FOOTAGE			
PLAN 245.2609			
FIRST FLOOR AREA	1140	SQ. FT.	
SECOND FLOOR AREA	1419	SQ. FT.	
TOTAL AREA	2609	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA(S)			
ELEVATION 'A'	54	SQ. FT.	
ELEVATION 'B'	119	SQ. FT.	
ELEVATION 'C'	79	SQ. FT.	
OPTIONS:			
COVERED PATIO ALL ELEVATIONS	180	SQ. FT.	

Elevation 'B' (Craftsman)  
at Lots 5, 17 & 28



PLAN 3



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TENTATIVE TRACT MAP 18916

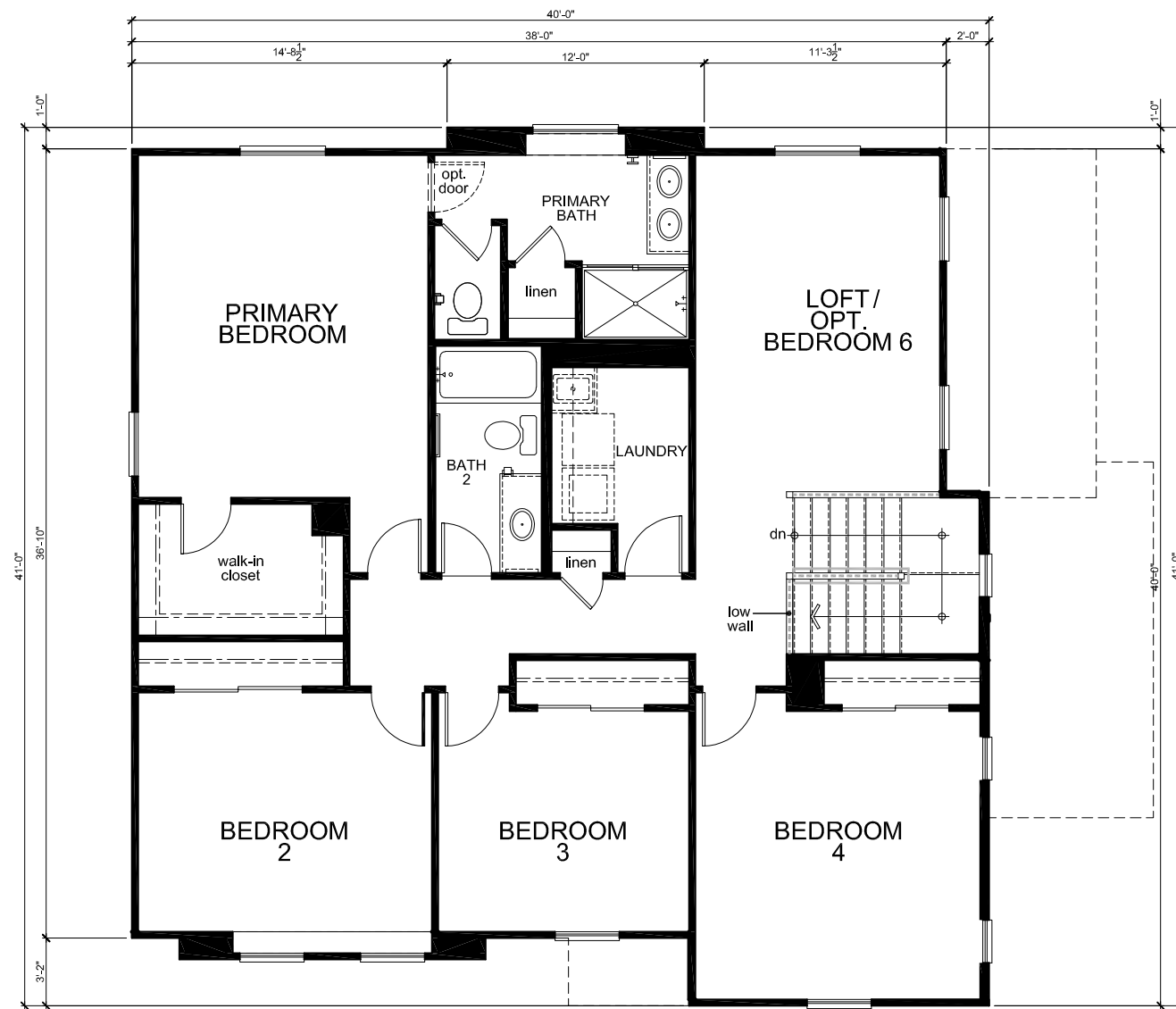
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JOB No. : 350999

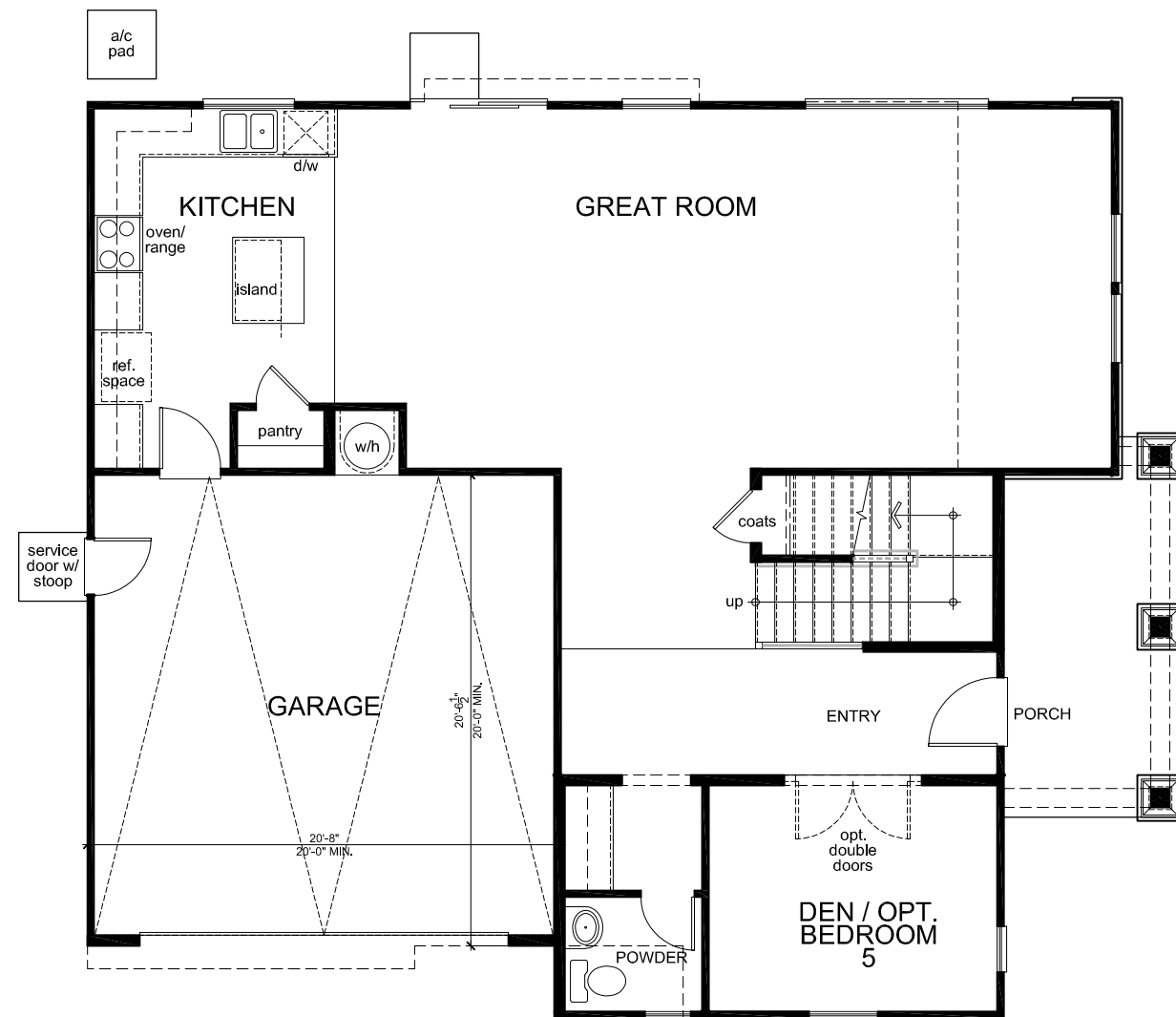
STORY: Two

REV: December 12, 2022

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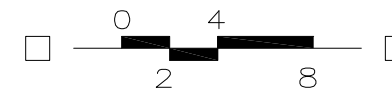
Second Floor Plan 'B'



First Floor Plan 'B'

SQUARE FOOTAGE		
PLAN 245.2609		
FIRST FLOOR AREA	1140	SQ. FT.
SECOND FLOOR AREA	1419	SQ. FT.
TOTAL AREA	2609	SQ. FT.
GARAGE AREA	427	SQ. FT.
PORCH AREA(S)		
ELEVATION 'A'	54	SQ. FT.
ELEVATION 'B'	119	SQ. FT.
ELEVATION 'C'	79	SQ. FT.
OPTIONS:		
COVERED PATIO ALL ELEVATIONS	180	SQ. FT.

Elevation 'B' (Craftsman)  
at Lots 24 & 36



PLAN 3

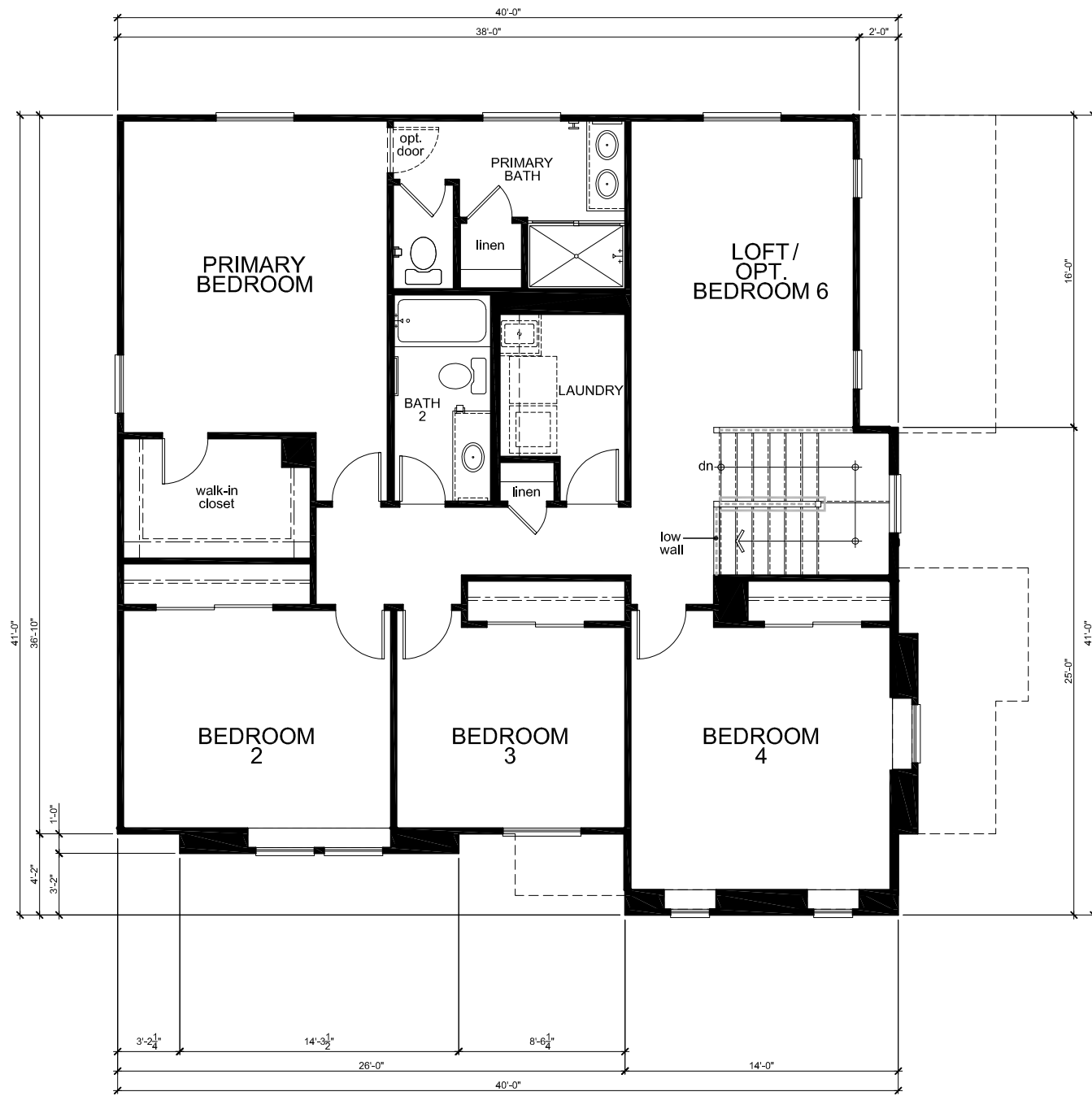


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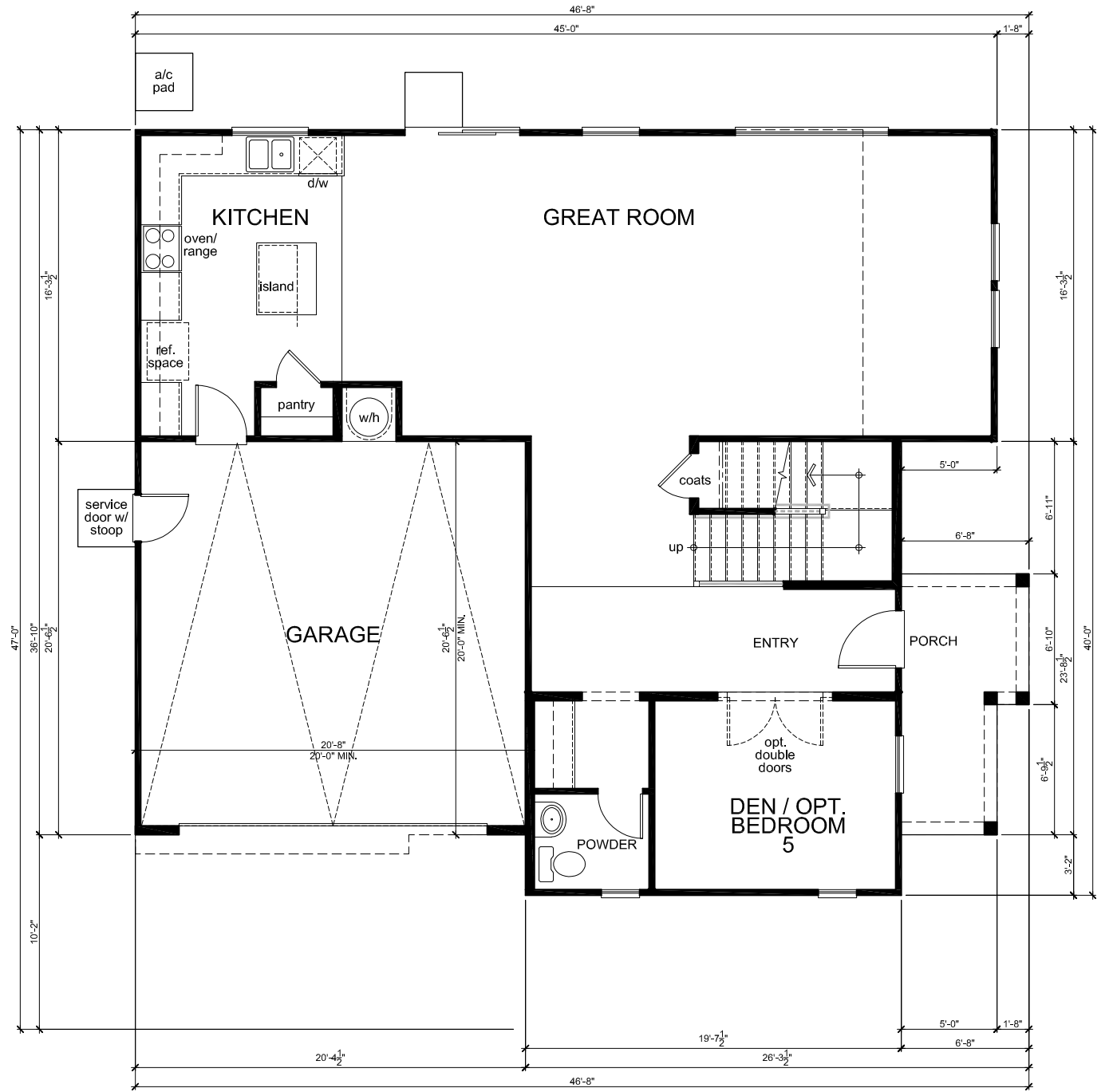
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

245.2609

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022



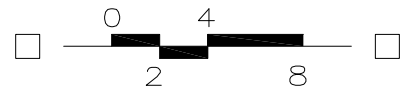
Second Floor Plan 'C'



First Floor Plan 'C'

SQUARE FOOTAGE			
PLAN 245.2609			
FIRST FLOOR AREA	1140	SQ. FT.	
SECOND FLOOR AREA	1419	SQ. FT.	
TOTAL AREA	2609	SQ. FT.	
GARAGE AREA	427	SQ. FT.	
PORCH AREA(S)			
ELEVATION 'A'	54	SQ. FT.	
ELEVATION 'B'	114	SQ. FT.	
ELEVATION 'C'	71	SQ. FT.	
OPTIONS:			
COVERED PATIO	180	SQ. FT.	

Elevation 'C' (American Farmhouse)  
at Lots 4 & 16



PLAN 3



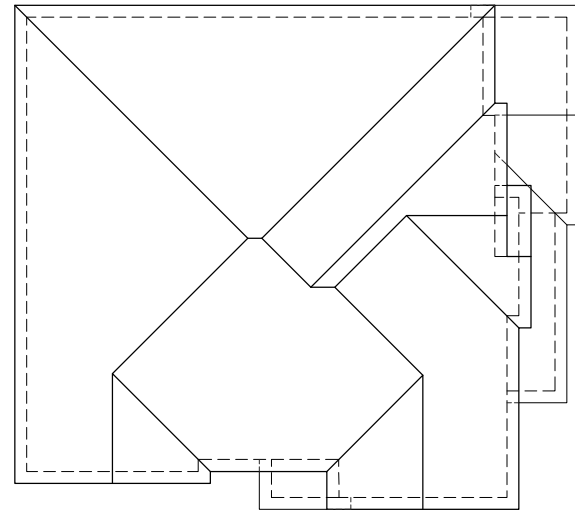
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COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

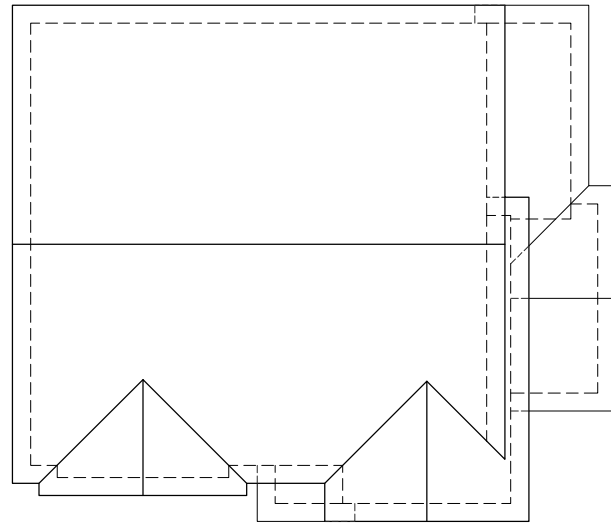
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JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

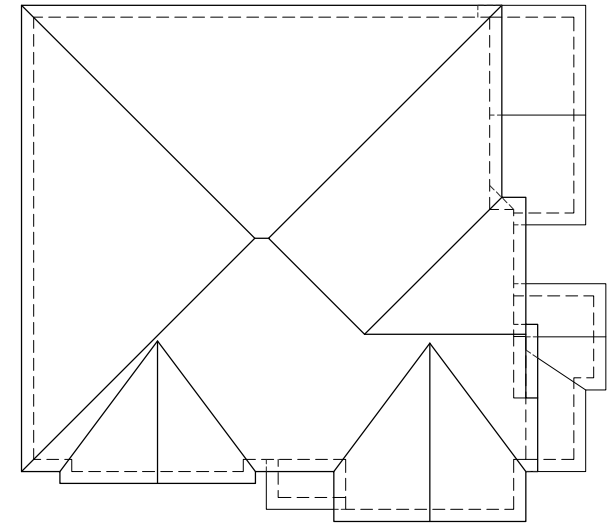
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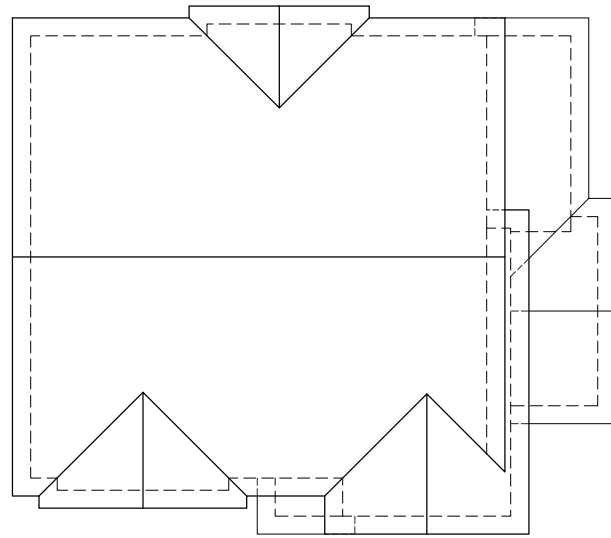
**'A' Spanish Colonial**  
at Lots 9 & 29  
 4:12



**'B' Craftsman**  
at Lots 5, 17 & 28  
 4:12



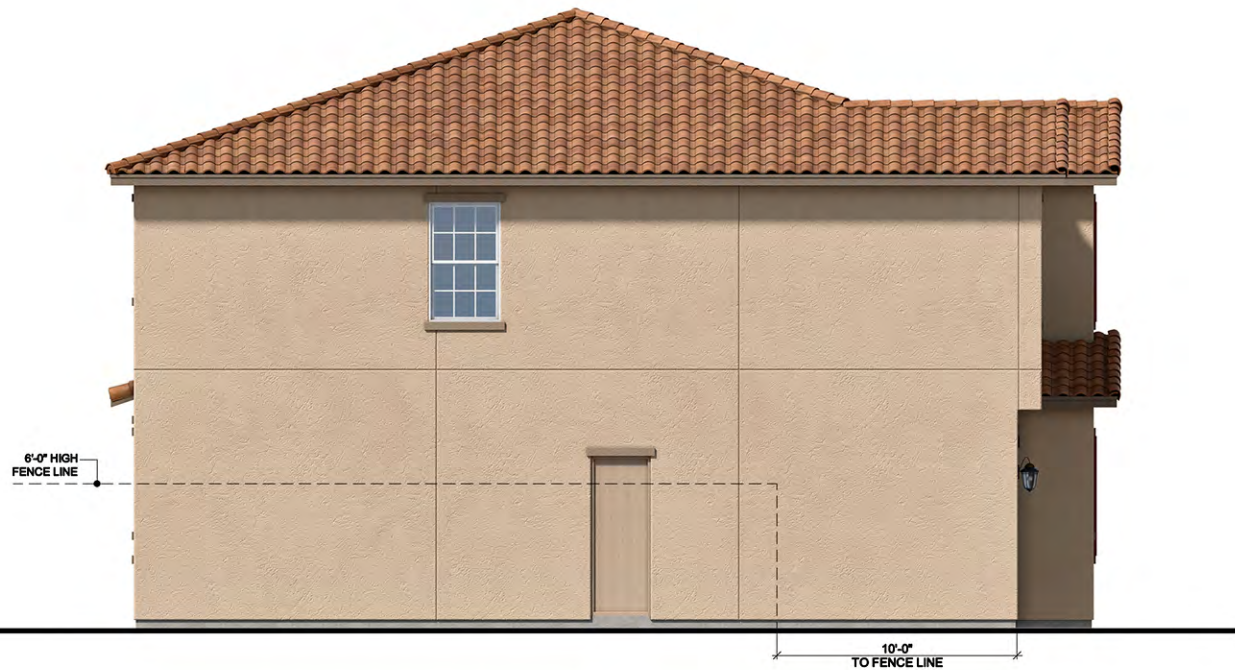
**'C' American Farmhouse**  
at Lots 4 & 16  
 6:12 / 8:12



**'B' Craftsman**  
at Lots 24 & 36  
 4:12

Roof Plans





Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

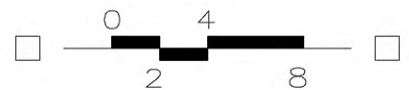


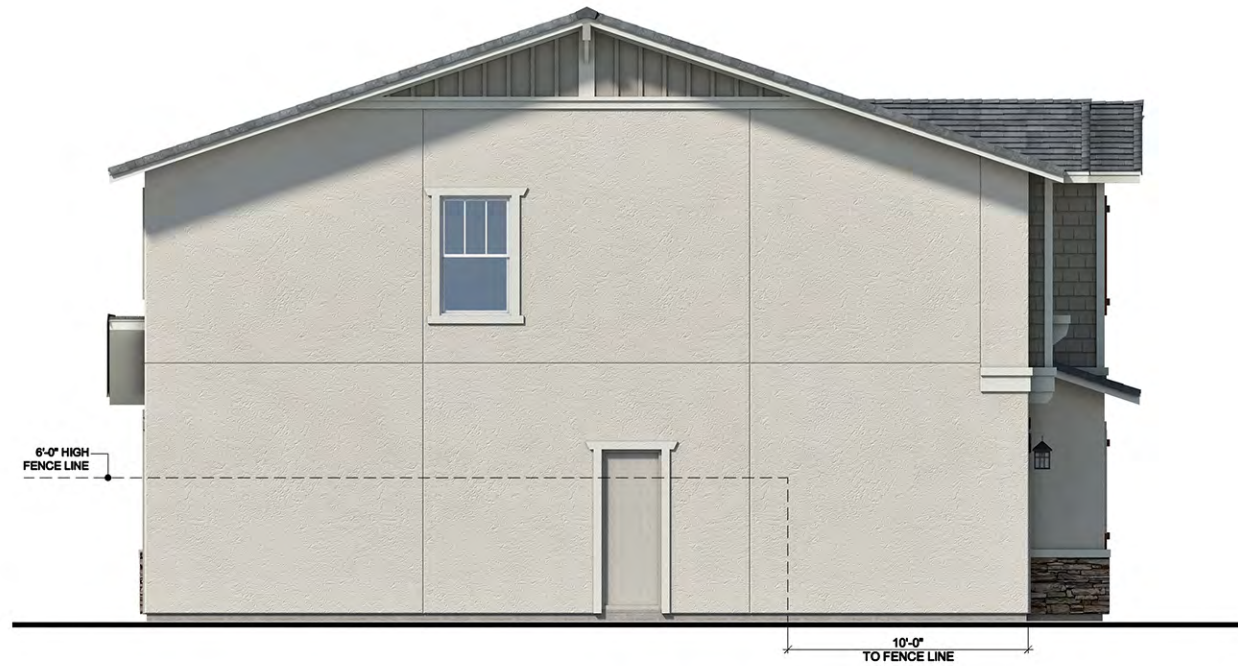
Right Elevation 'A' (Spanish Colonial)  
Visible from Towhee Street & Dolomite Street



Rear Elevation 'A' (Spanish Colonial)

Elevation 'A' (Spanish Colonial)  
at Lots 9 & 29





Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

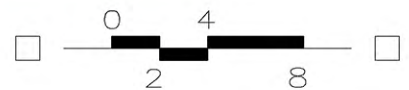


Right Elevation 'B' (Craftsman)  
Visible from Towhee Street & Dolomite Street



Rear Elevation 'B' (Craftsman)

Elevation 'B' (Craftsman)  
at Lots 5, 17 & 28



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PLAN 3

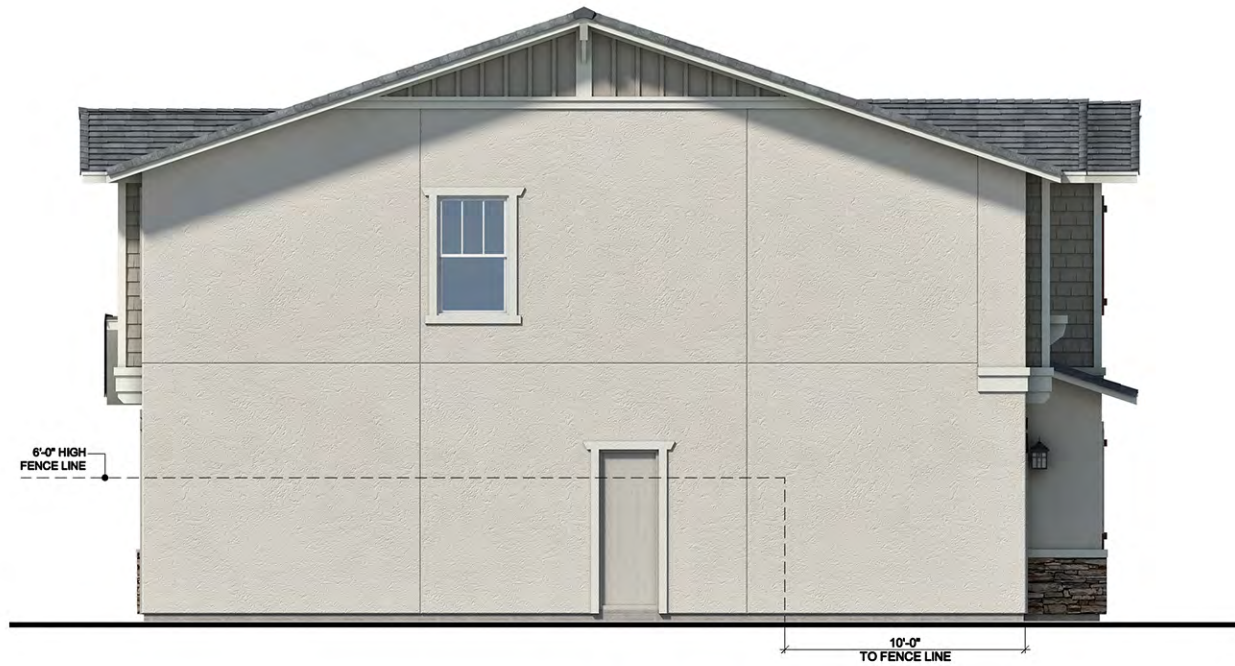
245.2609

JOB No. : 350999

STORY: Two

REV: December 12, 2022

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Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

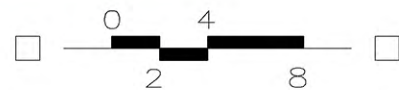


Right Elevation 'B' (Craftsman)  
Visible from Towhee Street & Dolomite Street

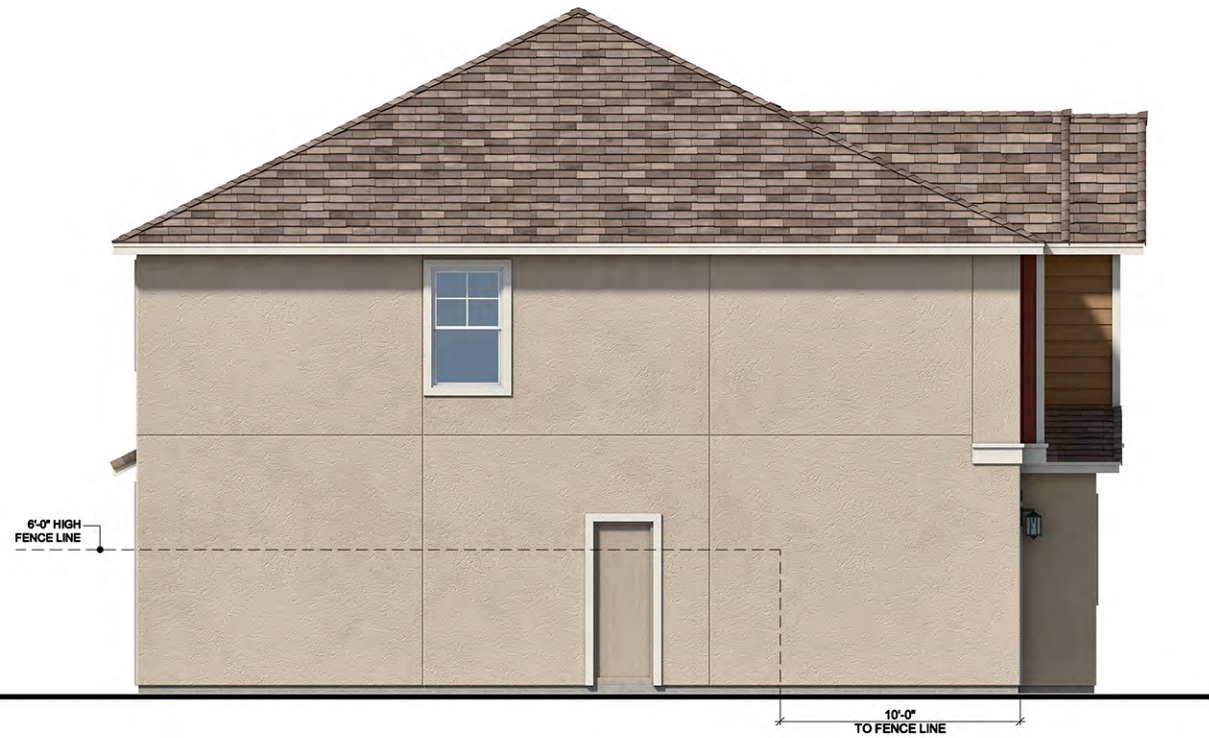


Rear Elevation 'B' (Craftsman)  
Visible from Archibald Avenue & Kinglet Avenue

Elevation 'B' (Craftsman)  
at Lots 24 & 36







Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

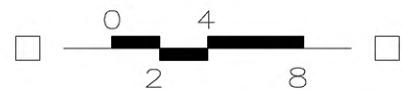


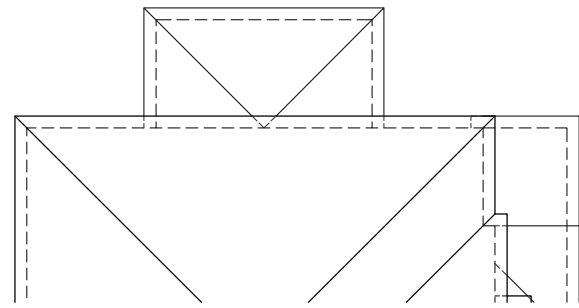
Right Elevation 'C' (American Farmhouse)  
Visible from Towhee Street & Dolomite Street



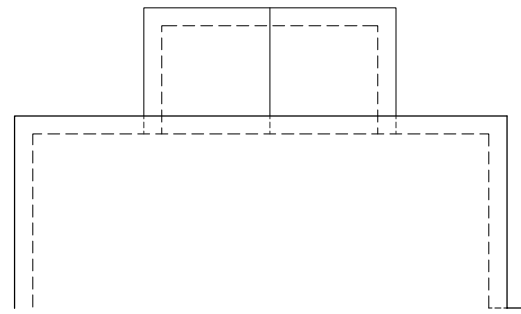
Rear Elevation 'C' (American Farmhouse)

Elevation 'C' (American Farmhouse)  
at Lots 4 & 16

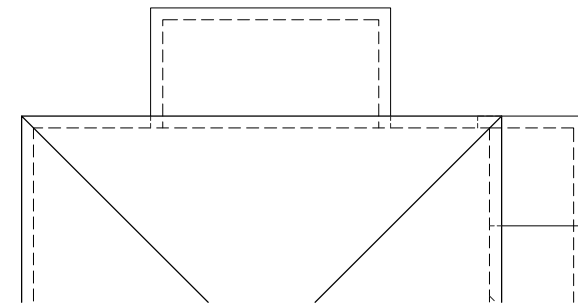




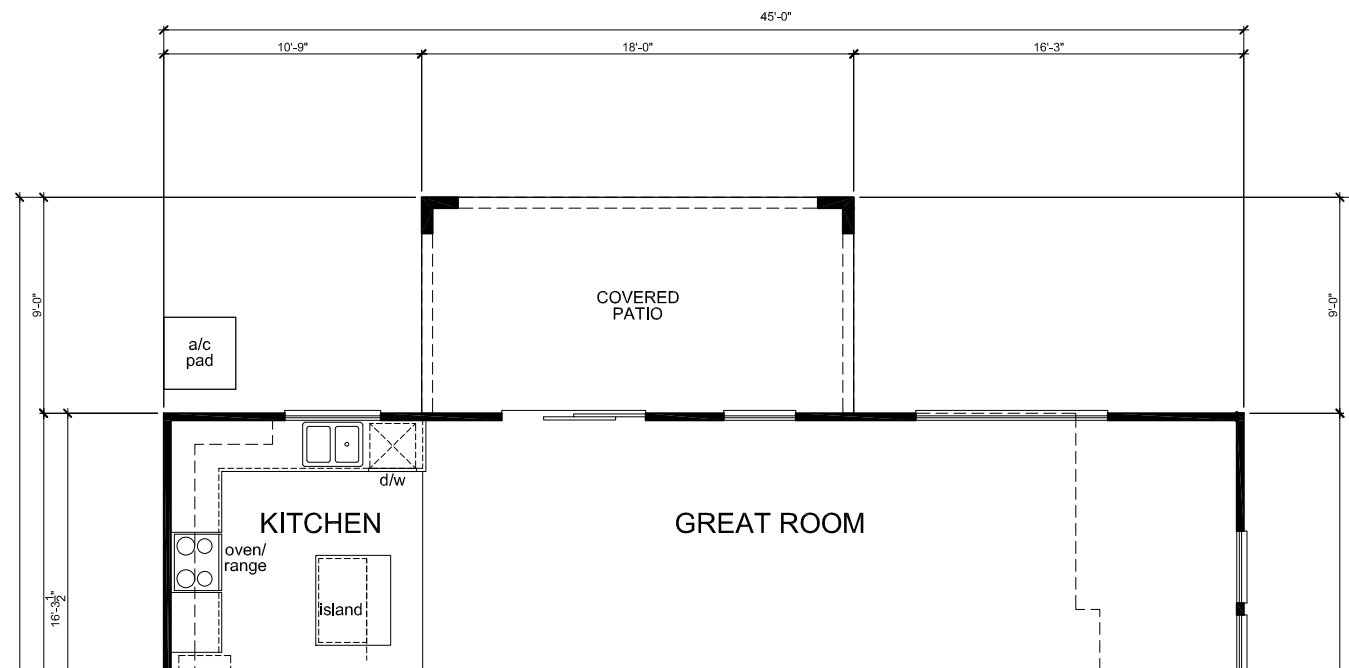
Partial Roof Plan 'A' (Spanish Colonial)  
at Covered Patio



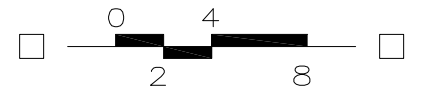
Partial Roof Plan 'B' (Craftsman)  
at Covered Patio



Partial Roof Plan 'C' (American Farmhouse)  
at Covered Patio



Partial First Floor Plan  
at Covered Patio



PLAN 3



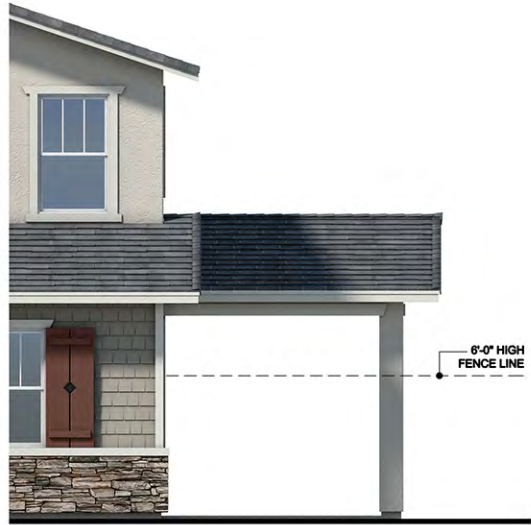
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ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

245.2609

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

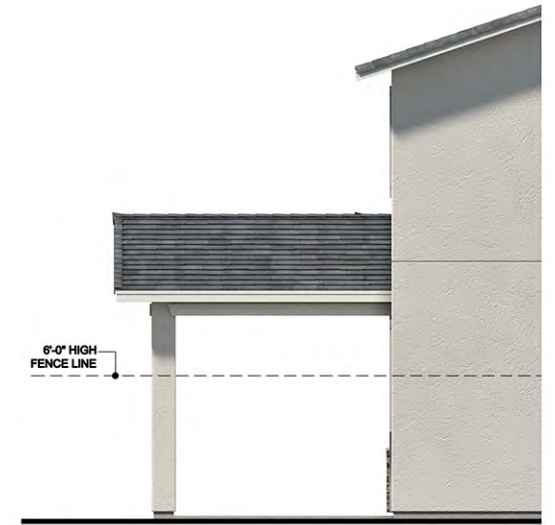
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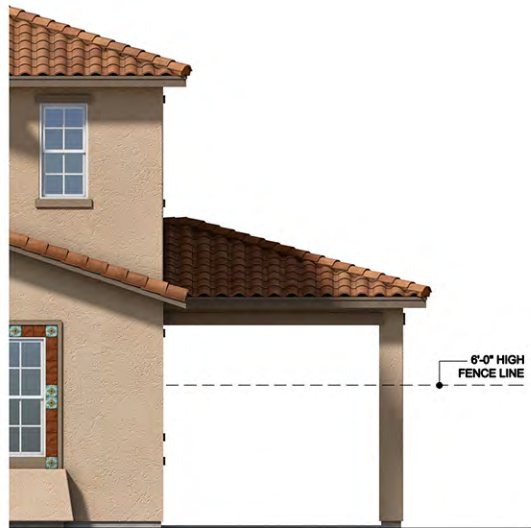
Partial Right Elevation 'B' (Craftsman)  
at Covered Patio



Rear Elevation 'B' (Craftsman)  
at Covered Patio



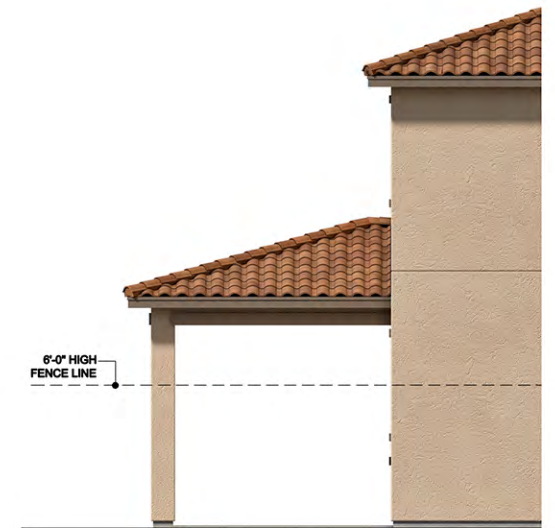
Partial Left Elevation 'B' (Craftsman)  
at Covered Patio



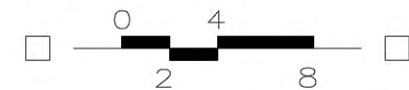
Partial Right Elevation 'A' (Spanish Colonial)  
at Covered Patio

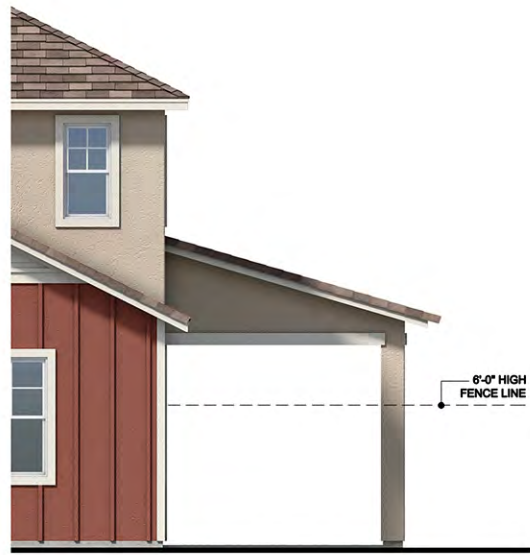


Rear Elevation 'A' (Spanish Colonial)  
at Covered Patio



Partial Left Elevation 'A' (Spanish Colonial)  
at Covered Patio

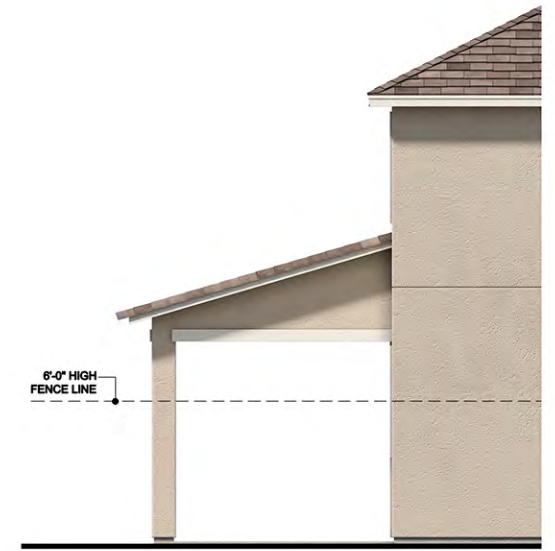




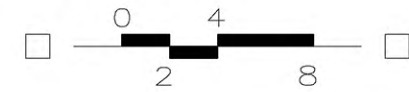
Partial Right Elevation 'C' (American Farmhouse)  
at Covered Patio



Rear Elevation 'C' (American Farmhouse)  
at Covered Patio



Partial Left Elevation 'C' (American Farmhouse)  
at Covered Patio



PLAN 3



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245.2609

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022



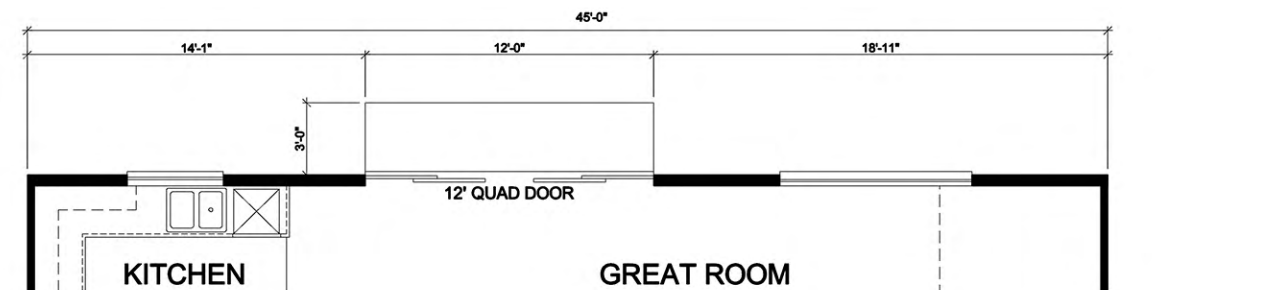
Rear Elevation 'A' at Quad Door Option



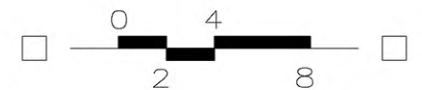
Rear Elevation 'B' at Quad Door Option



Rear Elevation 'C' at Quad Door Option



Quad Door Option



**ELEVATION LEGEND**

- |    |   |    |   |
|----|---|----|---|
| 1  | CONCRETE 'S' TILE ROOFING                                   | 15 | RESERVED                                  |
| 2  | CONCRETE FLAT TILE ROOFING                                  | 16 | ADDRESS PLAQUE                            |
| 3  | WOOD FASCIA BOARD   | 17 | WOOD CORBEL                               |
| 4  | STUCCO FINISH (16/20 SAND)                                  | 18 | RESERVED                                  |
| 5  | STUCCO OVER FOAM TRIM                                       | 19 | FIBER CEMENT SIDING WITH VERTICAL BATTENS |
| 6  | DECORATIVE STUCCO FINISH OVER FOAM CORBELS/SHAPED FOAM TRIM | 20 | PRE-FAB OUTLOOKER                         |
| 7  | STUCCO COLUMNS  | 21 | WOOD POST                                 |
| 8  | VINYL WINDOW SYSTEM   | 22 | WOOD TRIM                                 |
| 9  | COMPOSITE ENTRY DOOR  | 23 | RESERVED                                  |
| 10 | METAL SECTIONAL GARAGE DOOR                                 | 24 | FIBER CEMENT 8" LAP SIDING, U.N.O.        |
| 11 | DECORATIVE SHUTTERS   | 25 | DECORATIVE CERAMIC TILES                  |
| 12 | DECORATIVE GABLE END DETAIL                                 | 26 | RESERVED                                  |
| 13 | COACH LIGHTS  | 27 | RESERVED                                  |
| 14 | STONE VENEER  | 28 | FIBER CEMENT SHAKE SHINGLE SIDING         |



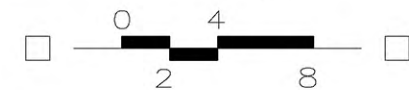
**Elevation 'A' (Spanish Colonial)**

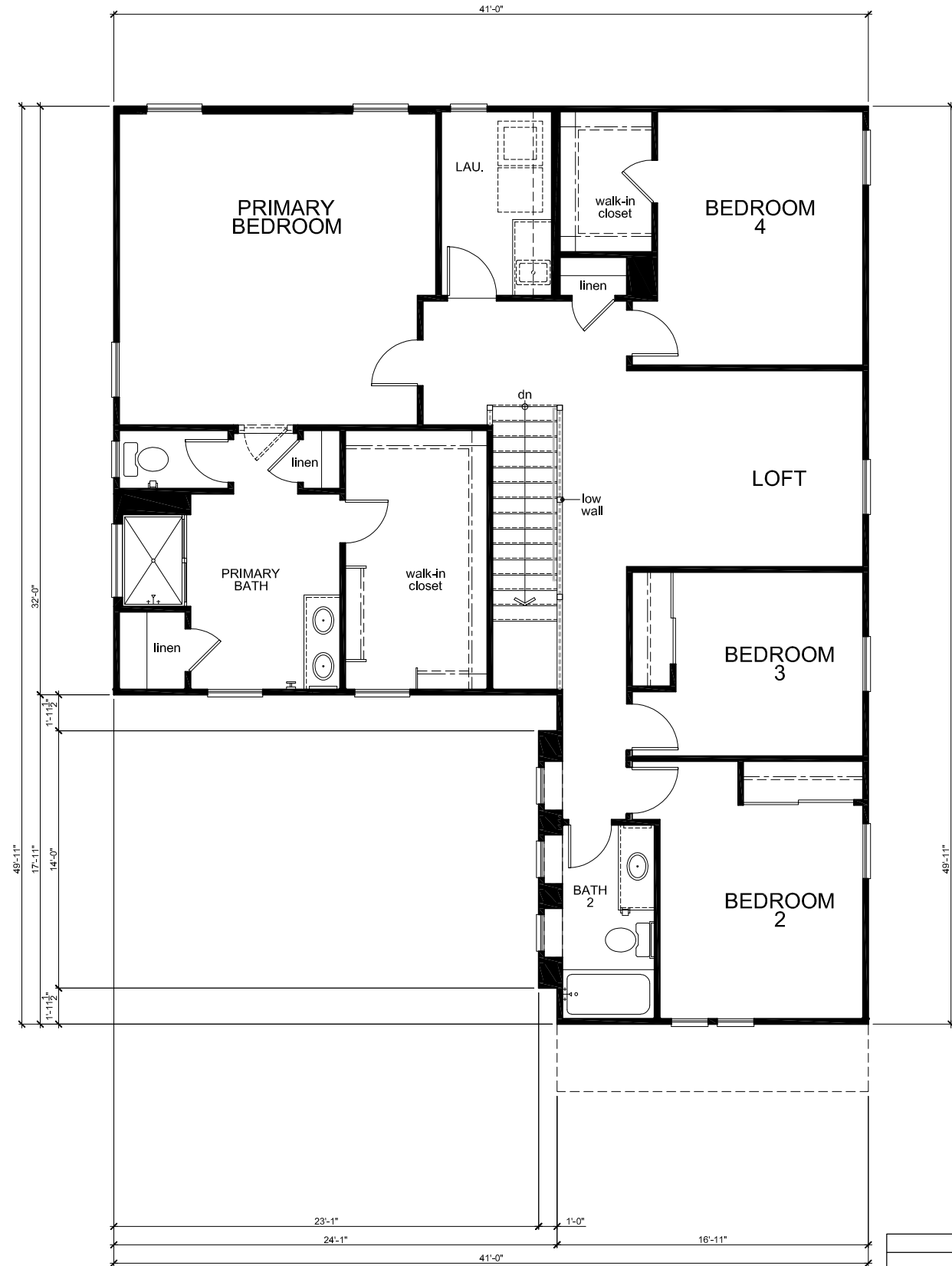


**Elevation 'B' (Craftsman)**

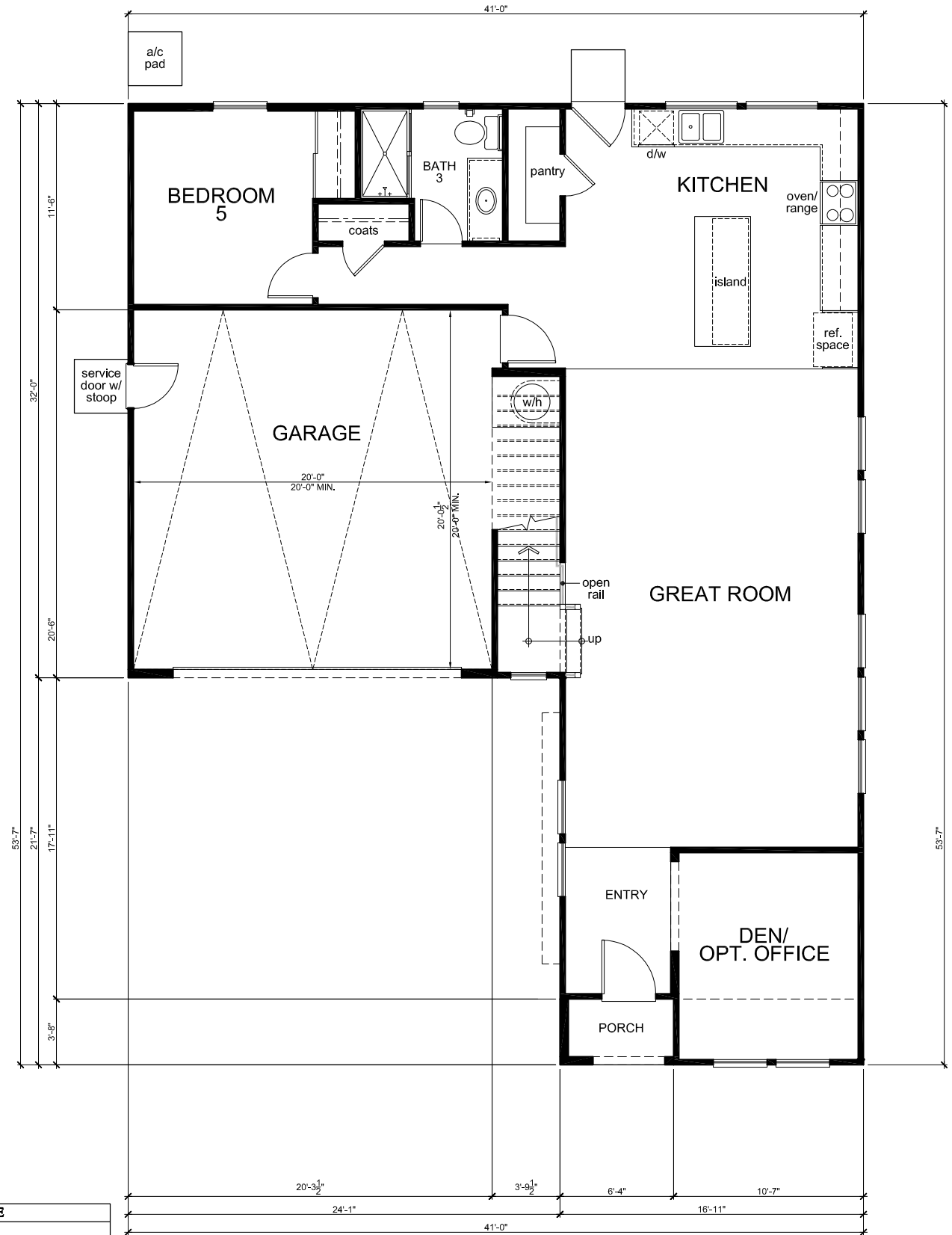


**Elevation 'C' (American Farmhouse)**





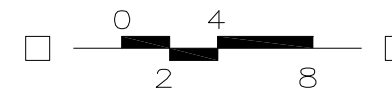
Second Floor Plan 'A'

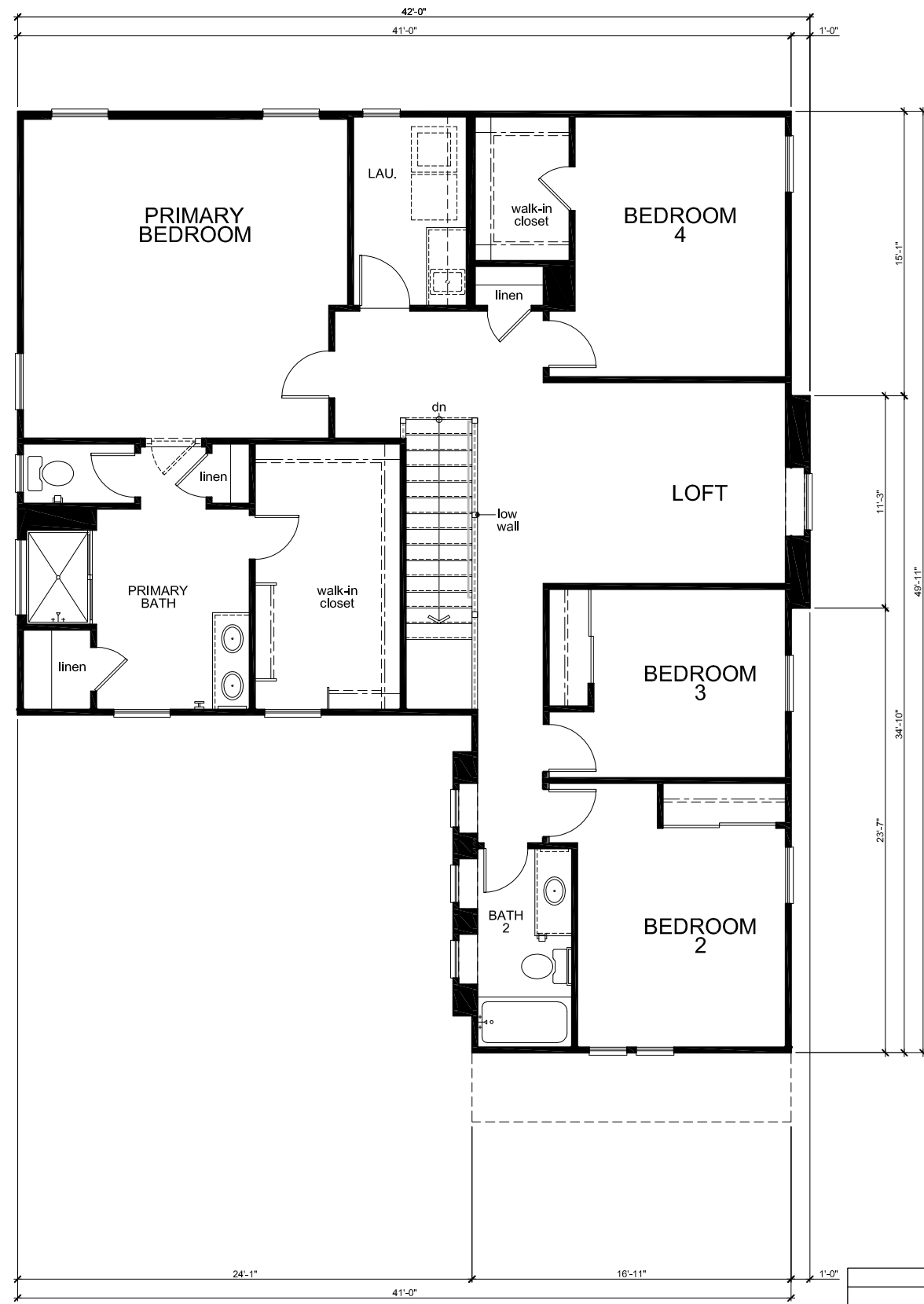


First Floor Plan 'A'

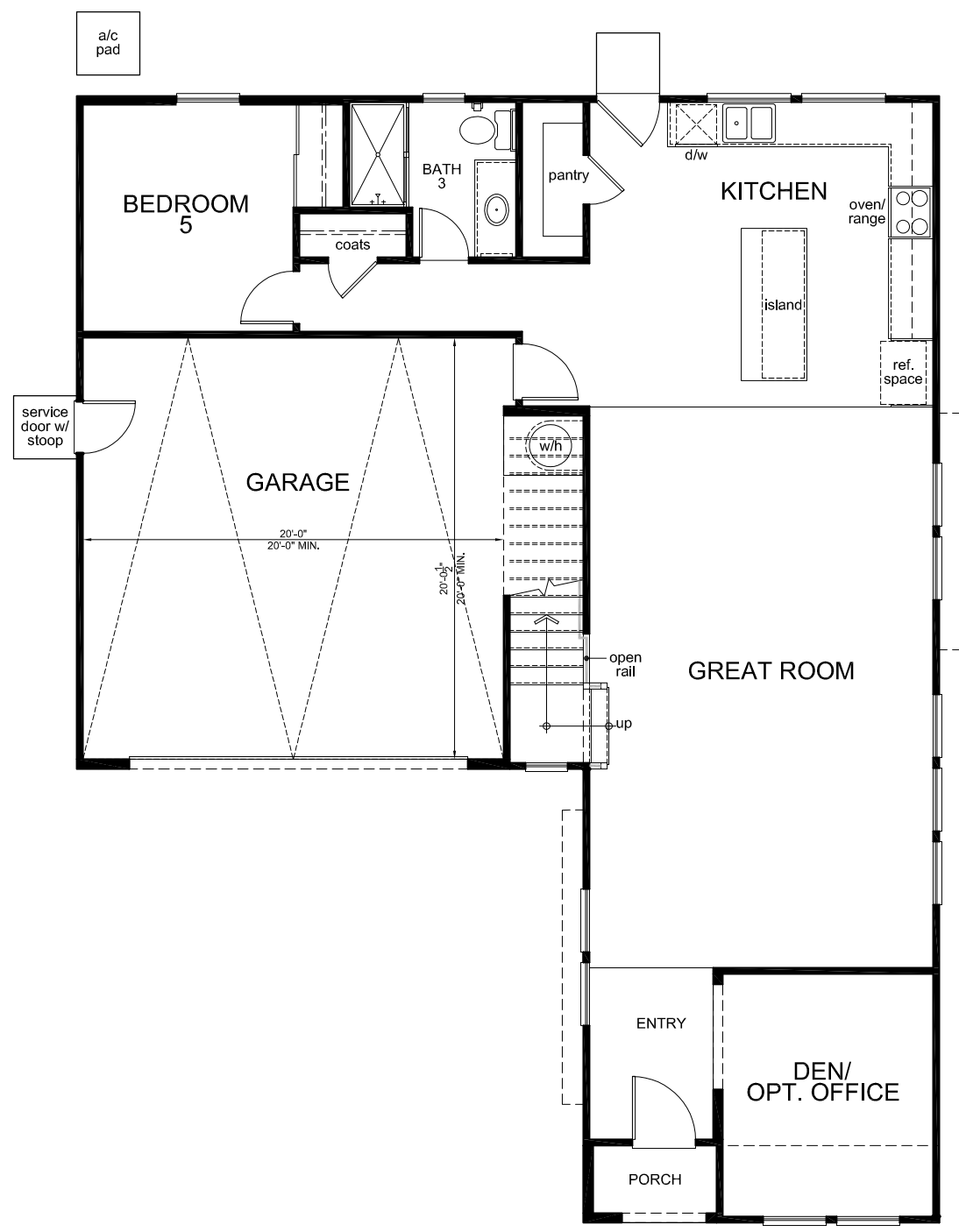
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SECOND FLOOR AREA	1560	SQ. FT.	
TOTAL AREA	2791	SQ. FT.	
GARAGE AREA	450	SQ. FT.	
PORCH AREA			
	ELEVATION 'A'	29	SQ. FT.
	ELEVATION 'B'	35	SQ. FT.
	ELEVATION 'C'	36	SQ. FT.

Elevation 'A' (Spanish Colonial) at Lot 6





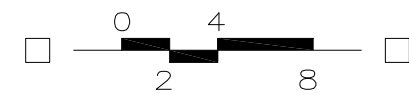
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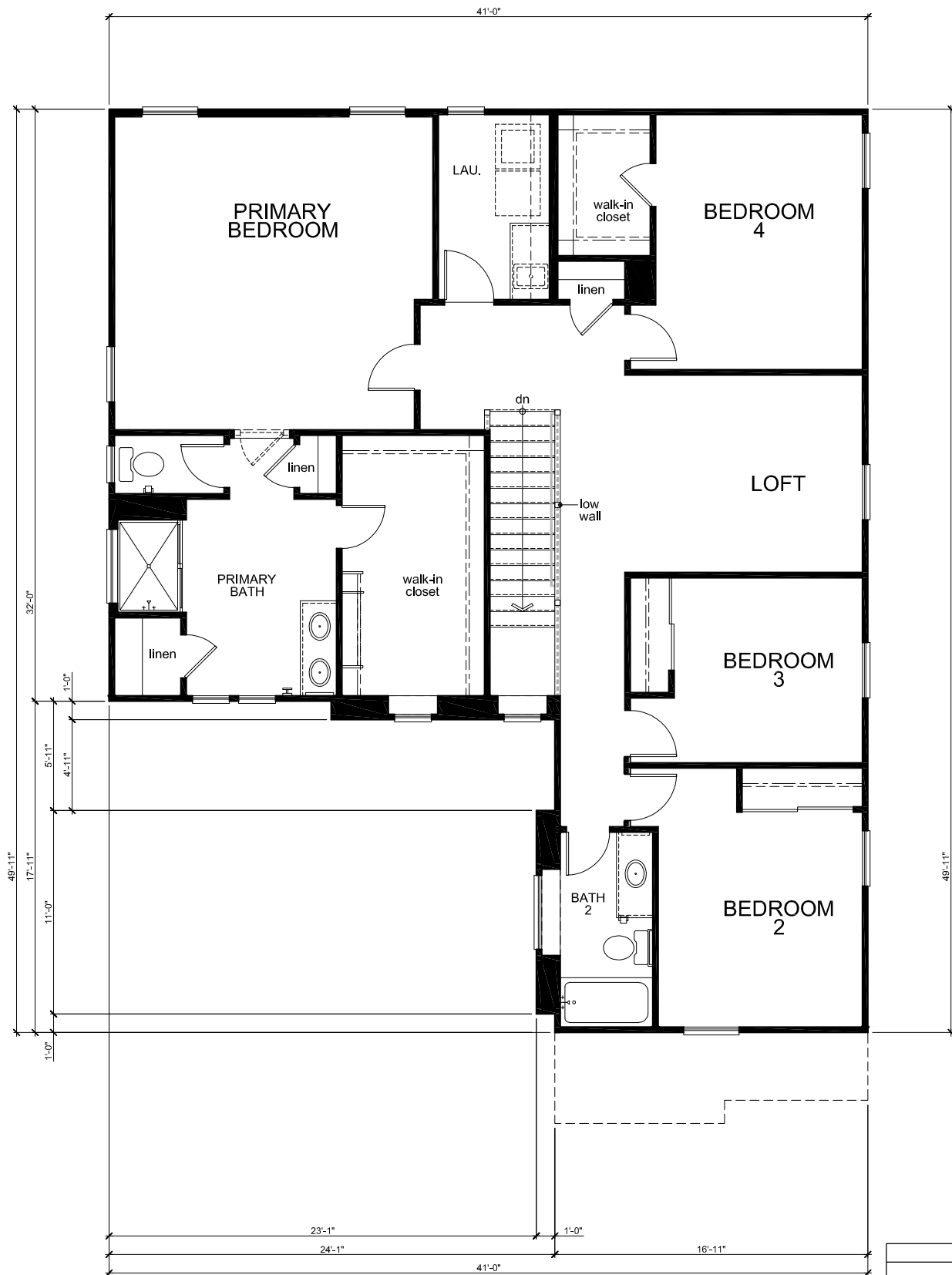
First Floor Plan 'A'

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TOTAL AREA	2791	SQ. FT.	
GARAGE AREA	450	SQ. FT.	
PORCH AREA			
	ELEVATION 'A'	29	SQ. FT.
	ELEVATION 'B'	35	SQ. FT.
	ELEVATION 'C'	36	SQ. FT.

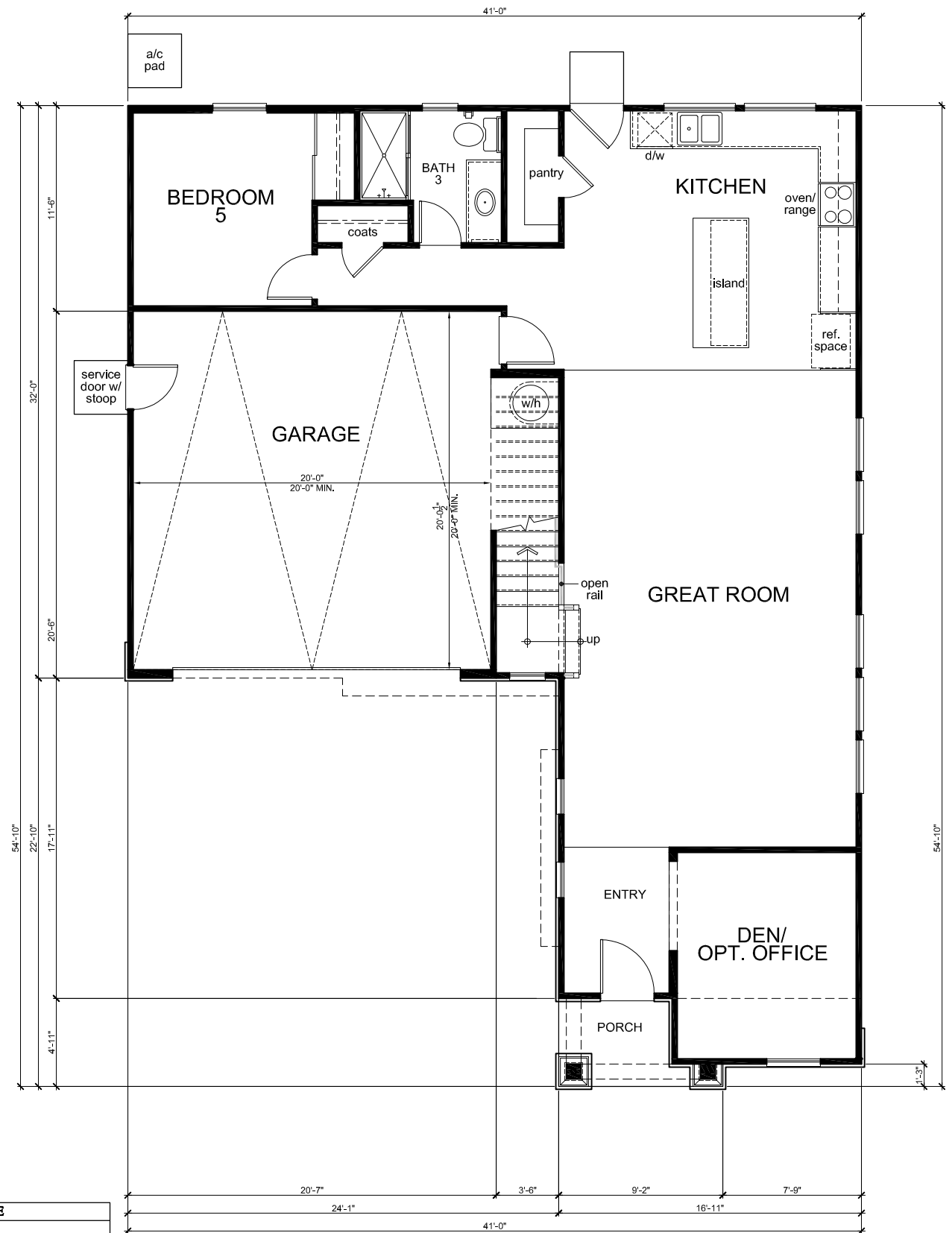
Elevation 'A' (Spanish Colonial)  
at Lots 18, 22 & 31







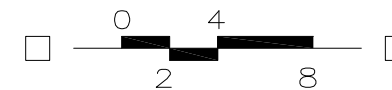
Second Floor Plan 'B'



First Floor Plan 'B'

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SECOND FLOOR AREA	1560	SQ. FT.
TOTAL AREA	2791	SQ. FT.
GARAGE AREA	455	SQ. FT.
PORCH AREA	23	SQ. FT.
ELEVATION "A"	35	SQ. FT.
ELEVATION "B"	36	SQ. FT.
ELEVATION "C"		

Elevation 'B' (Craftsman)  
at Lots 3 & 10



PLAN 4



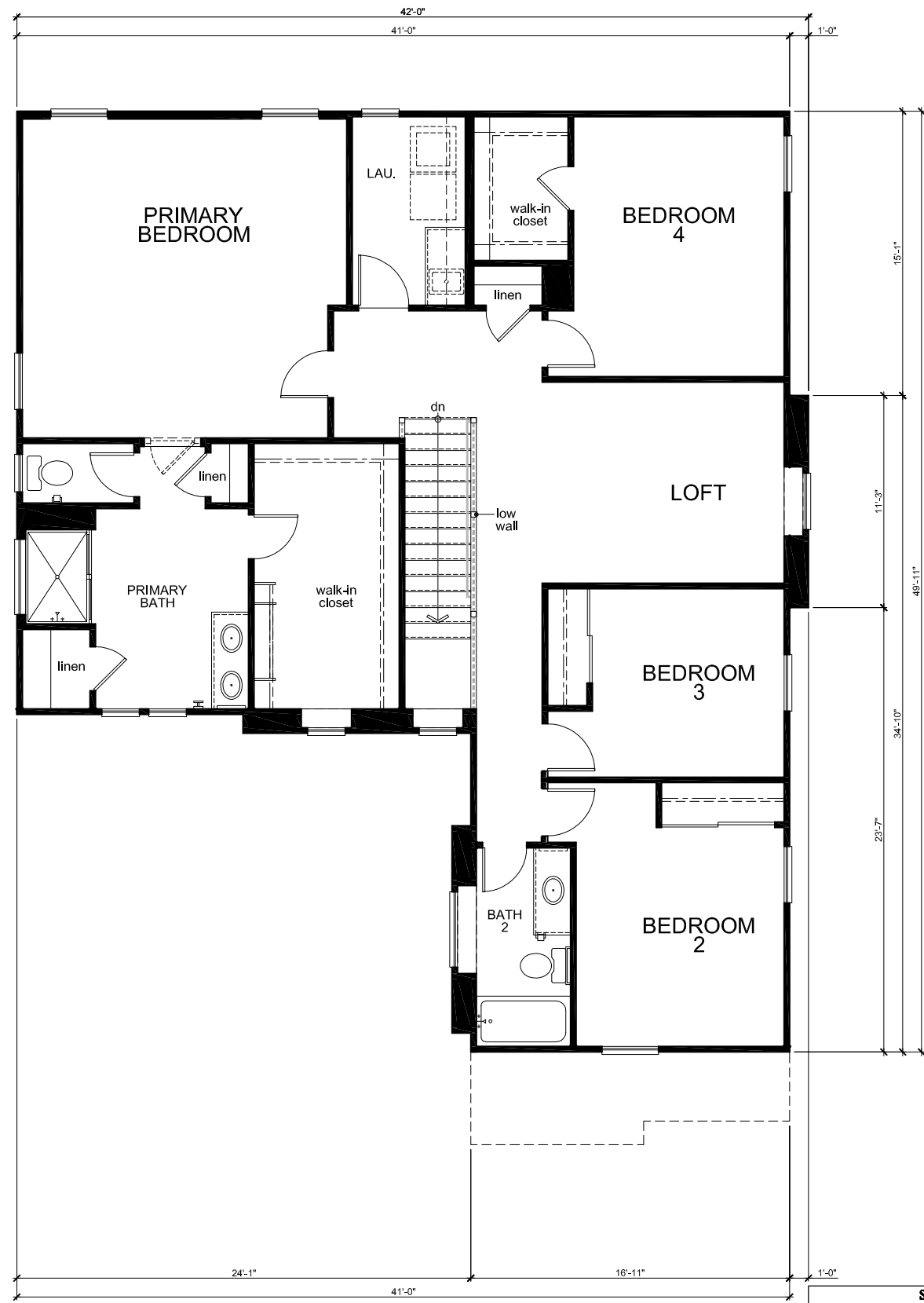
KB Home Southern California/Inland Empire  
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(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

241.2791

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

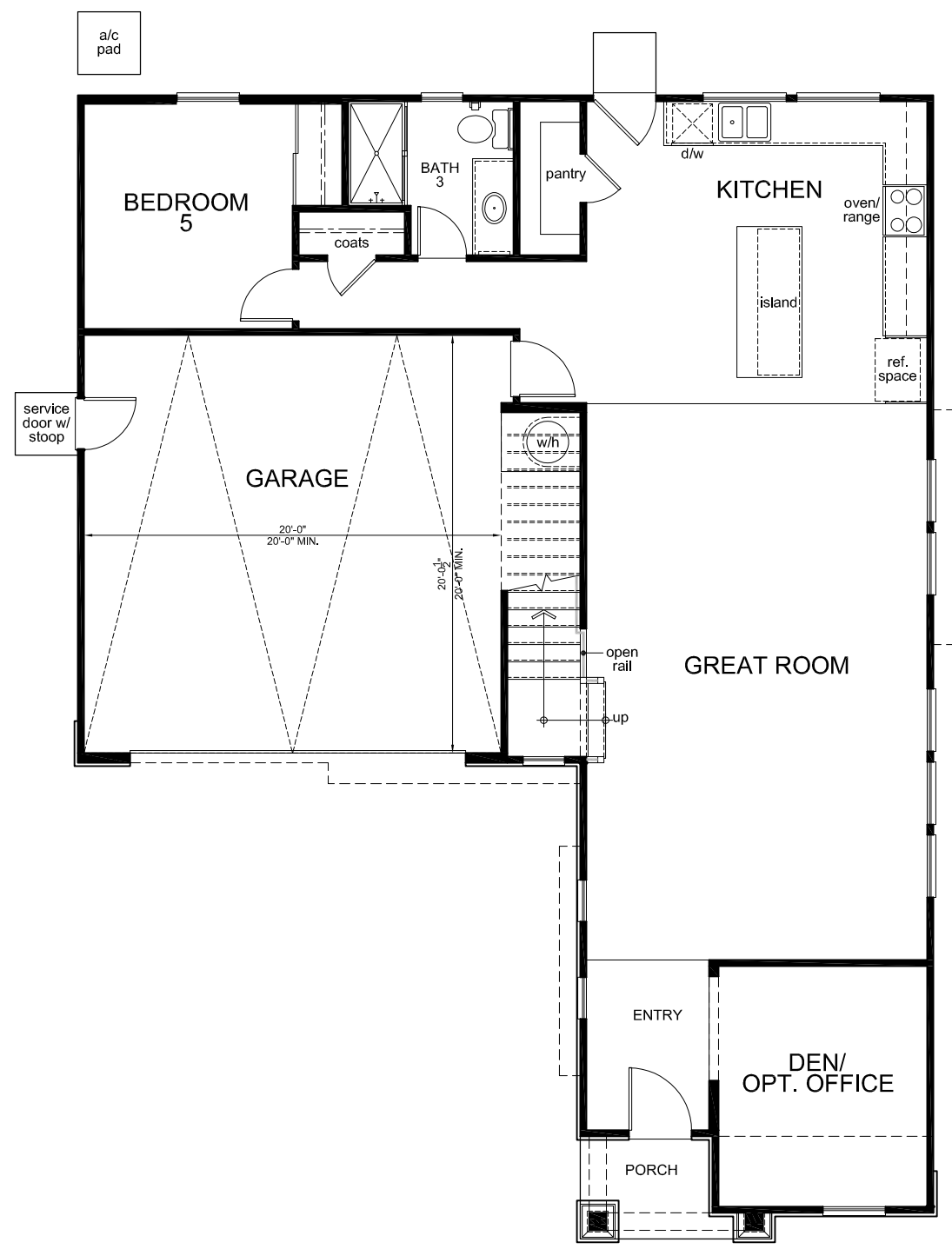
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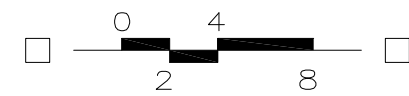
Second Floor Plan 'B'

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TOTAL AREA	2791	SQ. FT.
GARAGE AREA	459	SQ. FT.
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	ELEVATION "B"	35 SQ. FT.
	ELEVATION "C"	36 SQ. FT.

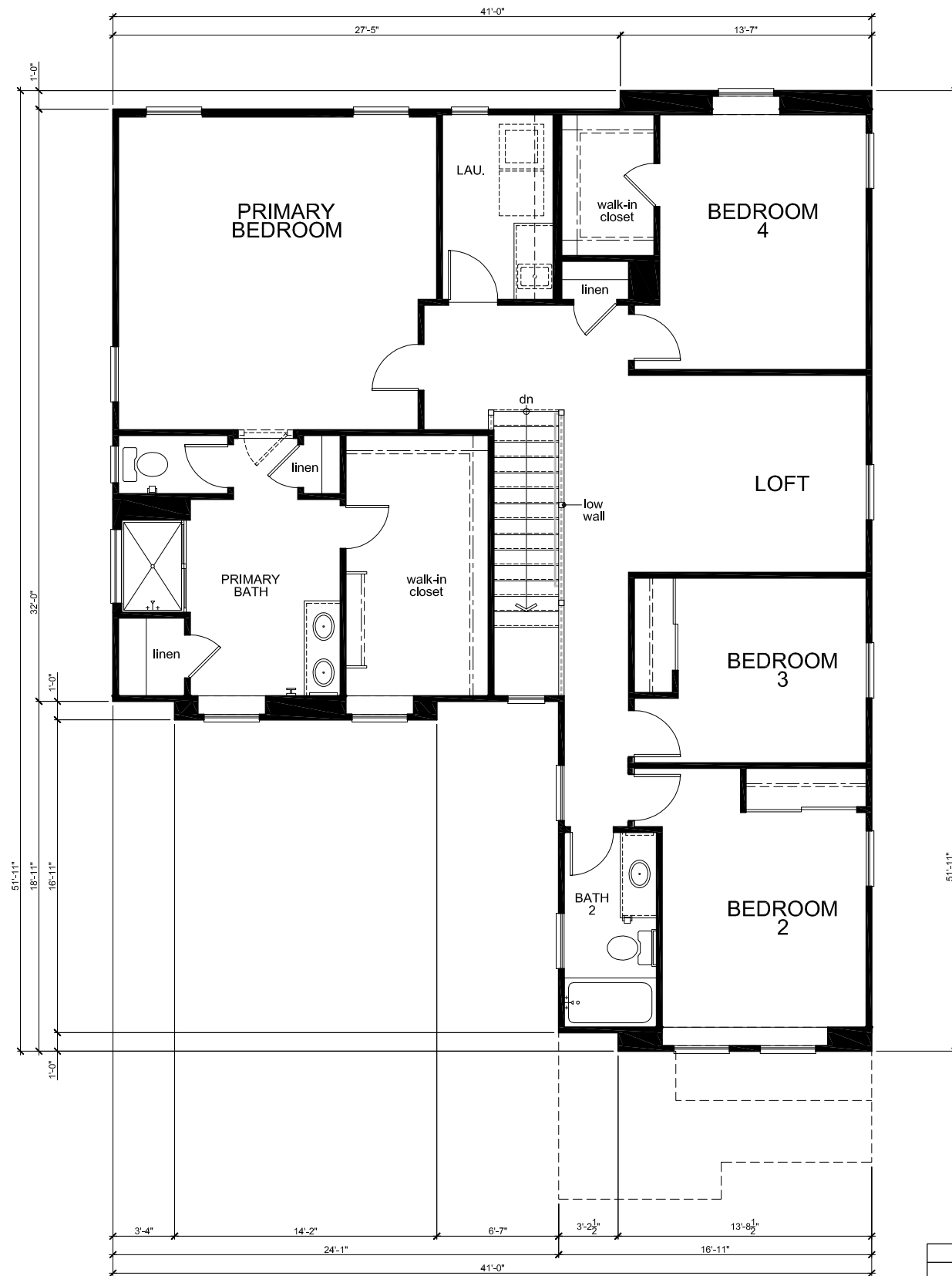
Elevation 'B' (Craftsman)  
at Lots 15 & 27



First Floor Plan 'B'



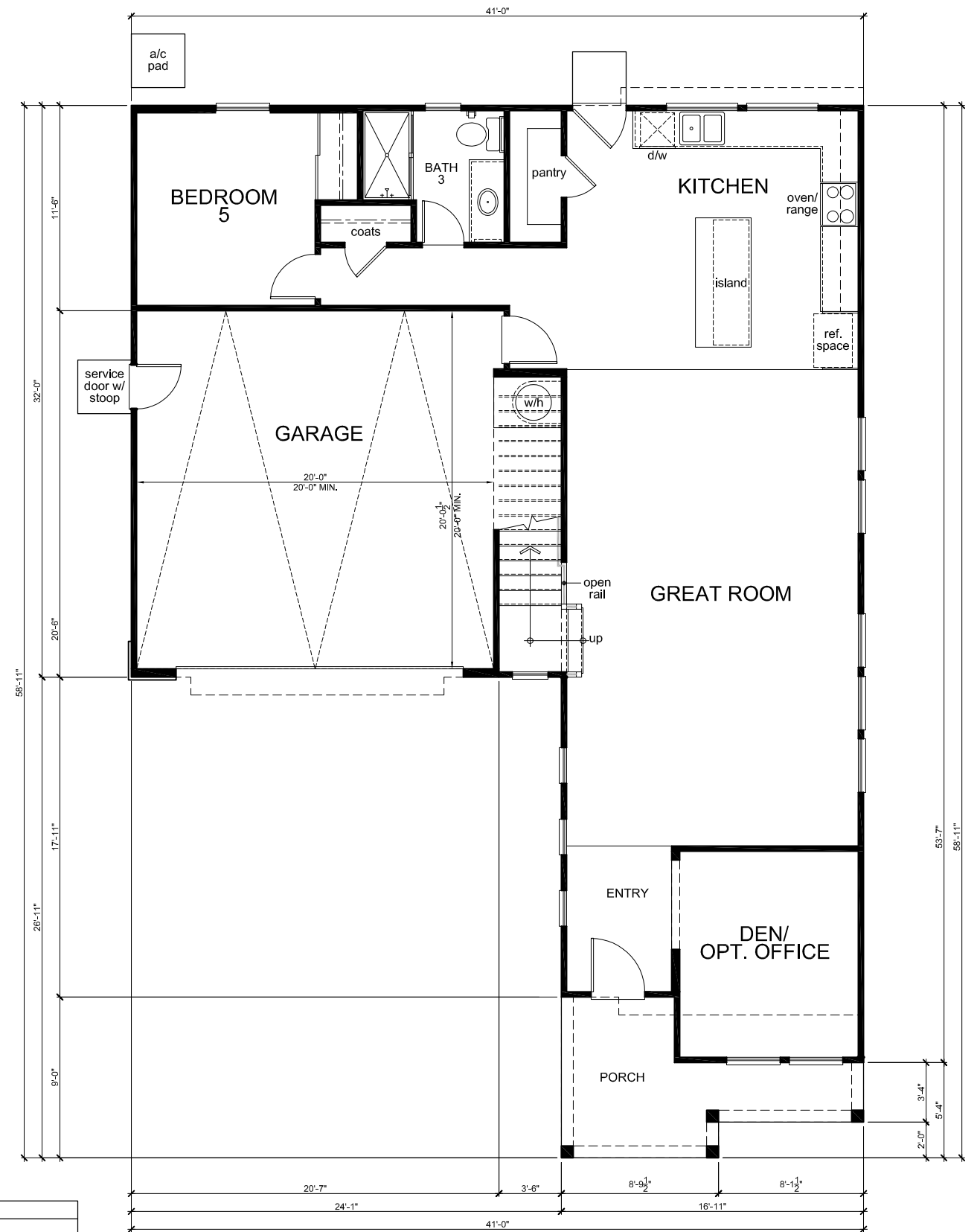
PLAN 4



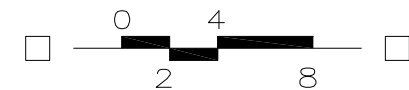
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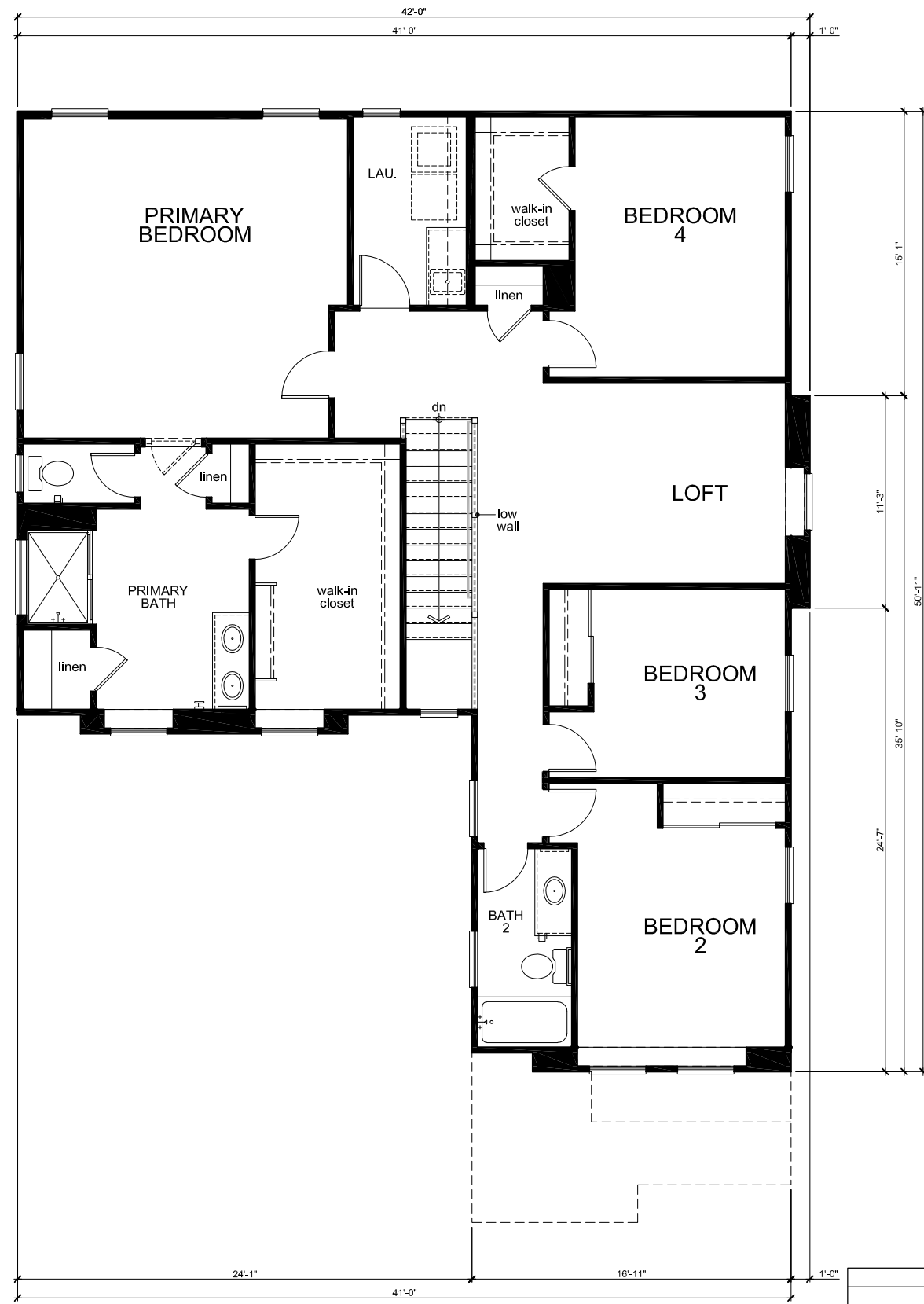
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FIRST FLOOR AREA	1291	SQ. FT.
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TOTAL AREA	2791	SQ. FT.
GARAGE AREA	450	SQ. FT.
PORCH AREA	29	SQ. FT.
ELEVATION 'A'	29	SQ. FT.
ELEVATION 'B'	35	SQ. FT.
ELEVATION 'C'	36	SQ. FT.

Elevation 'C' (American Farmhouse)  
at Lot 11



First Floor Plan 'C'

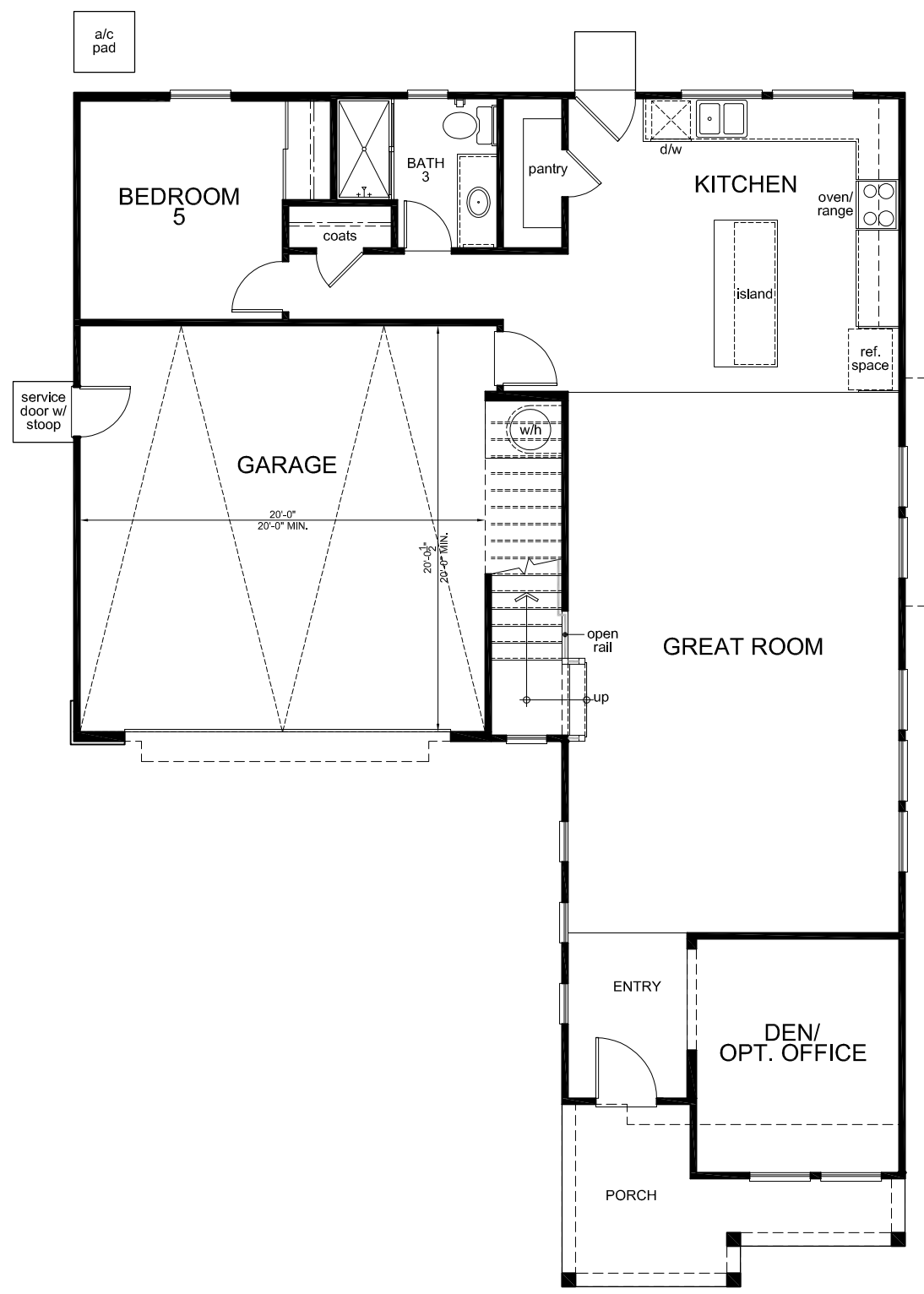




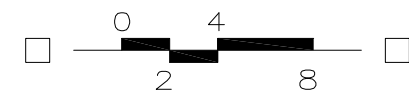
Second Floor Plan 'C'

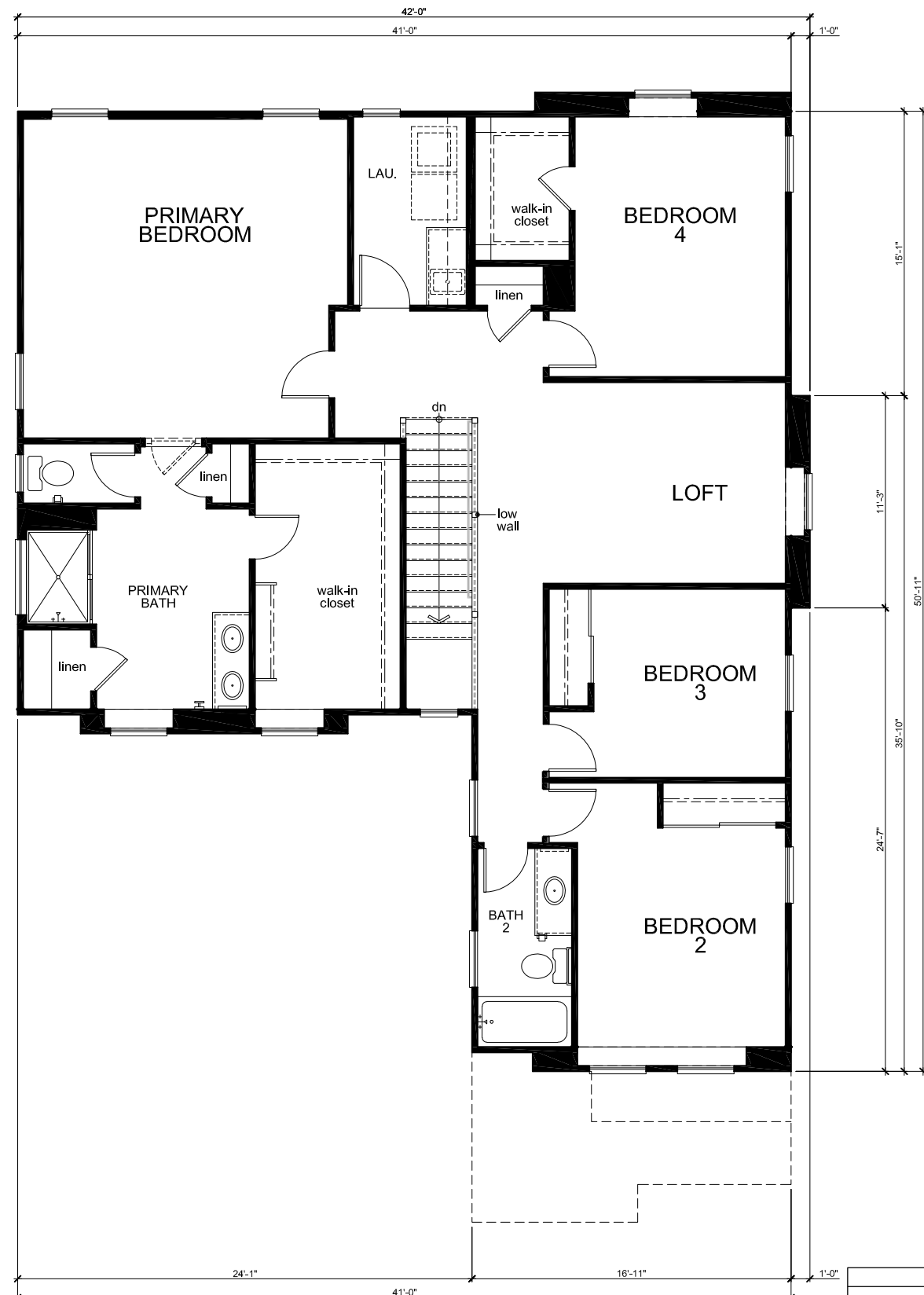
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TOTAL AREA	2791	SQ. FT.
GARAGE AREA	450	SQ. FT.
PORCH AREA		
	ELEVATION 'A'	29 SQ. FT.
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	ELEVATION 'C'	36 SQ. FT.

Elevation 'C' (American Farmhouse) at Lot 30



First Floor Plan 'C'

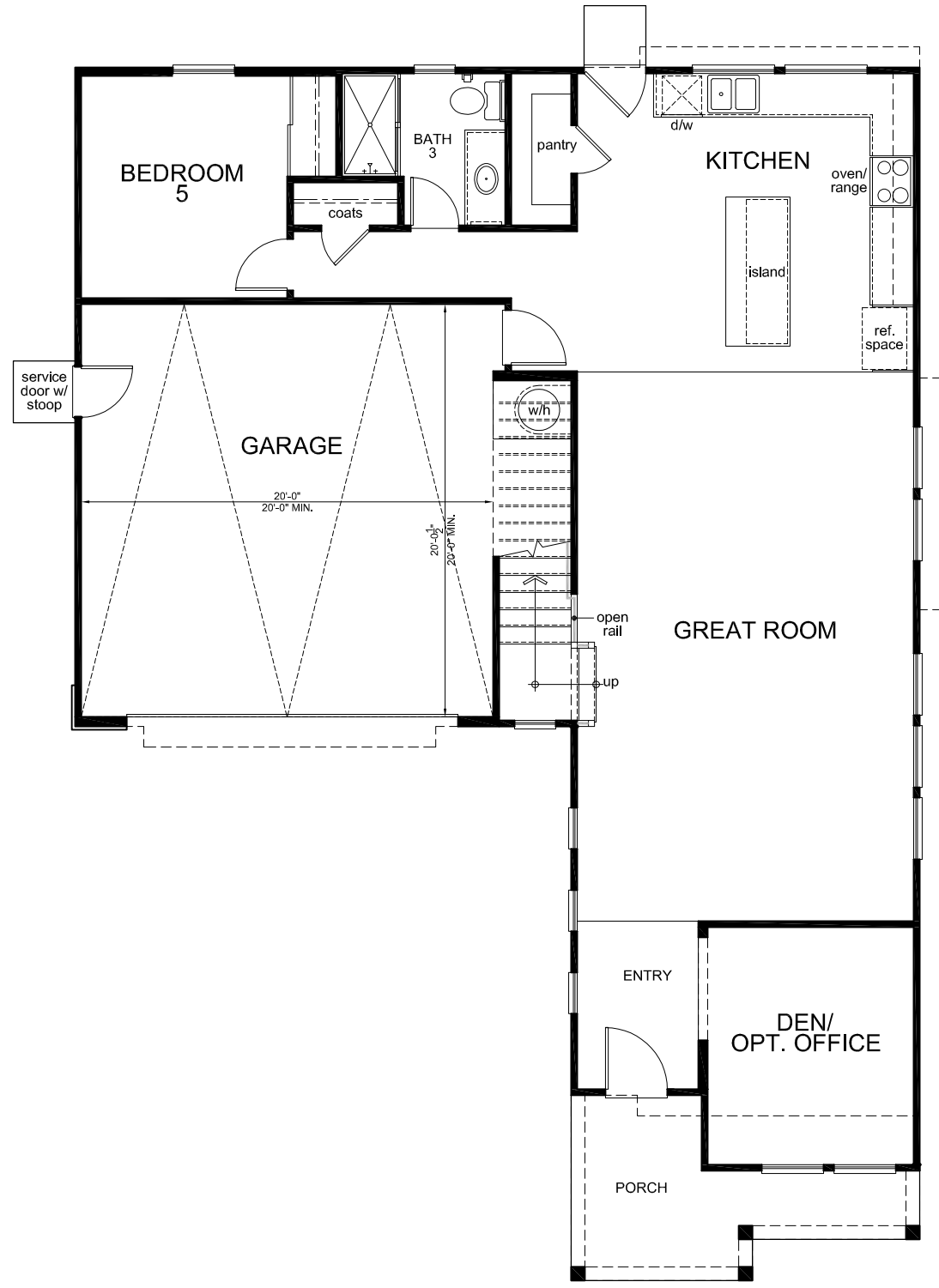




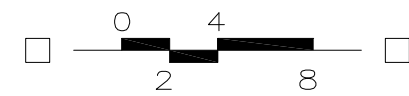
Second Floor Plan 'C'

SQUARE FOOTAGE		
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SECOND FLOOR AREA	1560	SQ. FT.
TOTAL AREA	2791	SQ. FT.
GARAGE AREA	450	SQ. FT.
PORCH AREA		
	ELEVATION 'A'	29 SQ. FT.
	ELEVATION 'B'	35 SQ. FT.
	ELEVATION 'C'	36 SQ. FT.

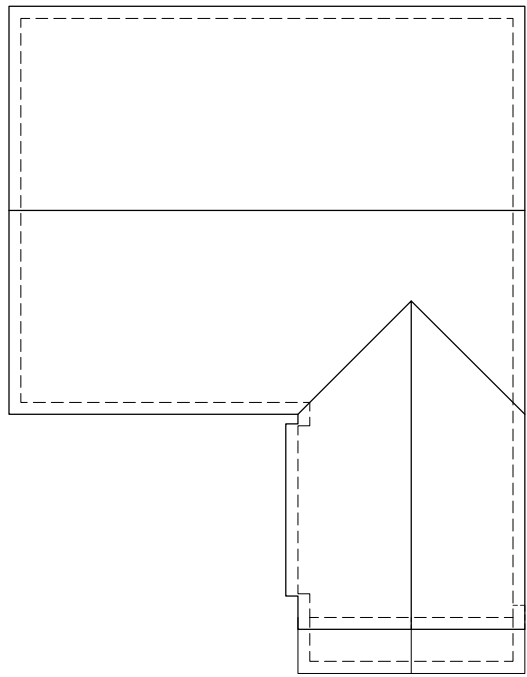
Elevation 'C' (American Farmhouse)  
at Lots 14, 23, 26 & 35



First Floor Plan 'C'

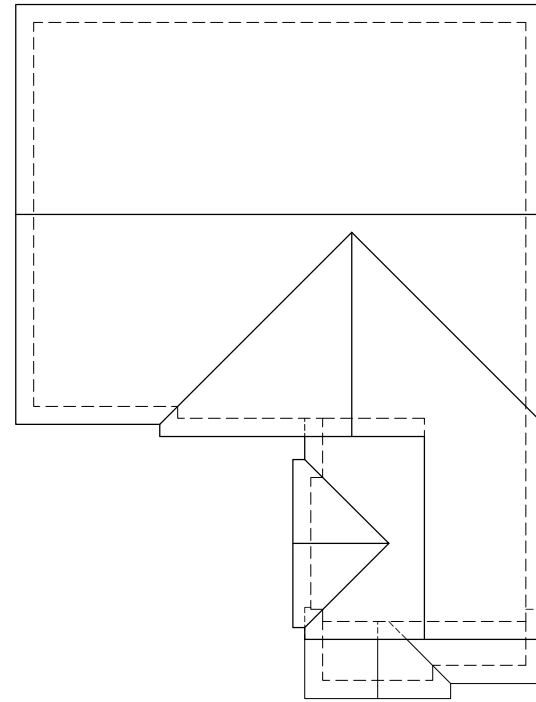


PLAN 4



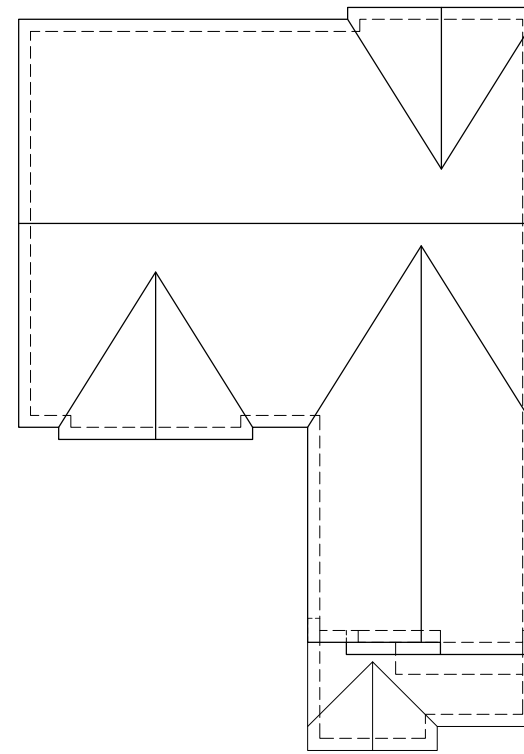
**'A' Spanish Colonial**

at Lot 6  
4:12



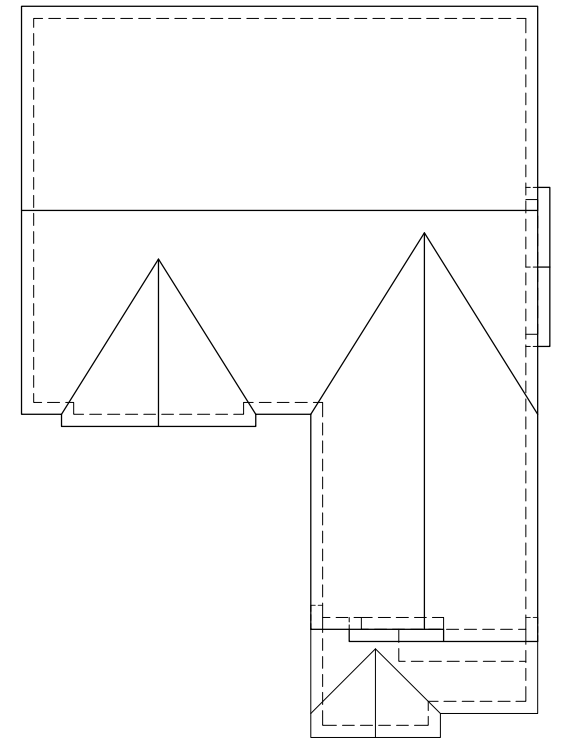
**'B' Craftsman**

at Lots 3 & 10  
4:12



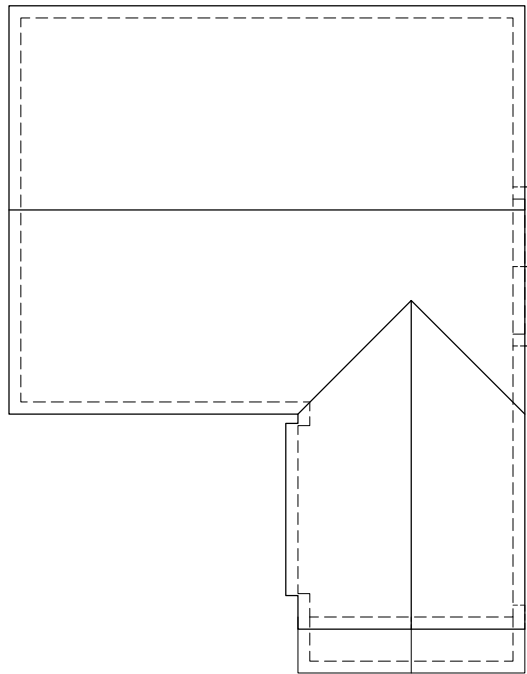
**'C' American Farmhouse**

at Lot 11  
5:12 / 8:12



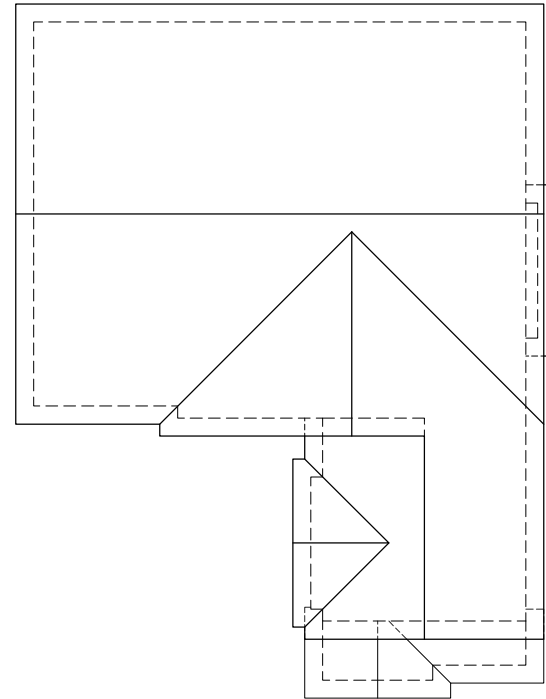
**'C' American Farmhouse**

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5:12 / 8:12



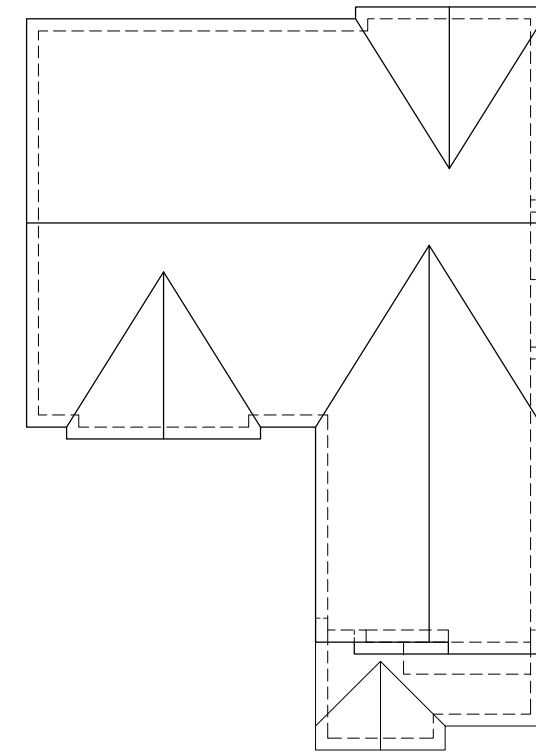
**'A' Spanish Colonial**

at Lots 18, 22 & 31  
4:12



**'B' Craftsman**

at Lots 15 & 27  
4:12



**'C' American Farmhouse**

at Lots 14, 23, 26 & 35  
5:12 / 8:12

Roof Plans





Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

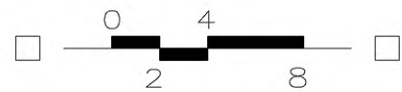


Right Elevation 'A' (Spanish Colonial)



Rear Elevation 'A' (Spanish Colonial)

Elevation 'A' (Spanish Colonial)  
at Lot 6





Left Elevation 'A' (Spanish Colonial)



Front Elevation 'A' (Spanish Colonial)

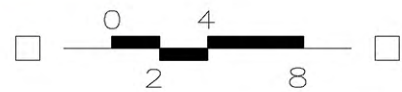


Right Elevation 'A' (Spanish Colonial)  
Visible from the Green Belt



Rear Elevation 'A' (Spanish Colonial)

Elevation 'A' (Spanish Colonial)  
at Lots 18, 22 & 31







Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

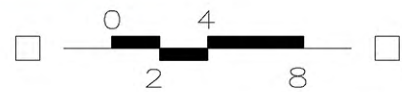


Right Elevation 'B' (Craftsman)



Rear Elevation 'B' (Craftsman)

Elevation 'B' (Craftsman)  
at Lots 3 & 10





Left Elevation 'B' (Craftsman)



Front Elevation 'B' (Craftsman)

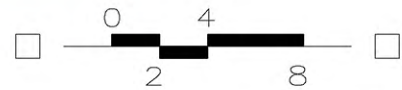


Right Elevation 'B' (Craftsman)  
Visible from the Green Belt

Elevation 'B' (Craftsman)  
at Lots 15 & 27



Rear Elevation 'B' (Craftsman)



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ONTARIO, CA

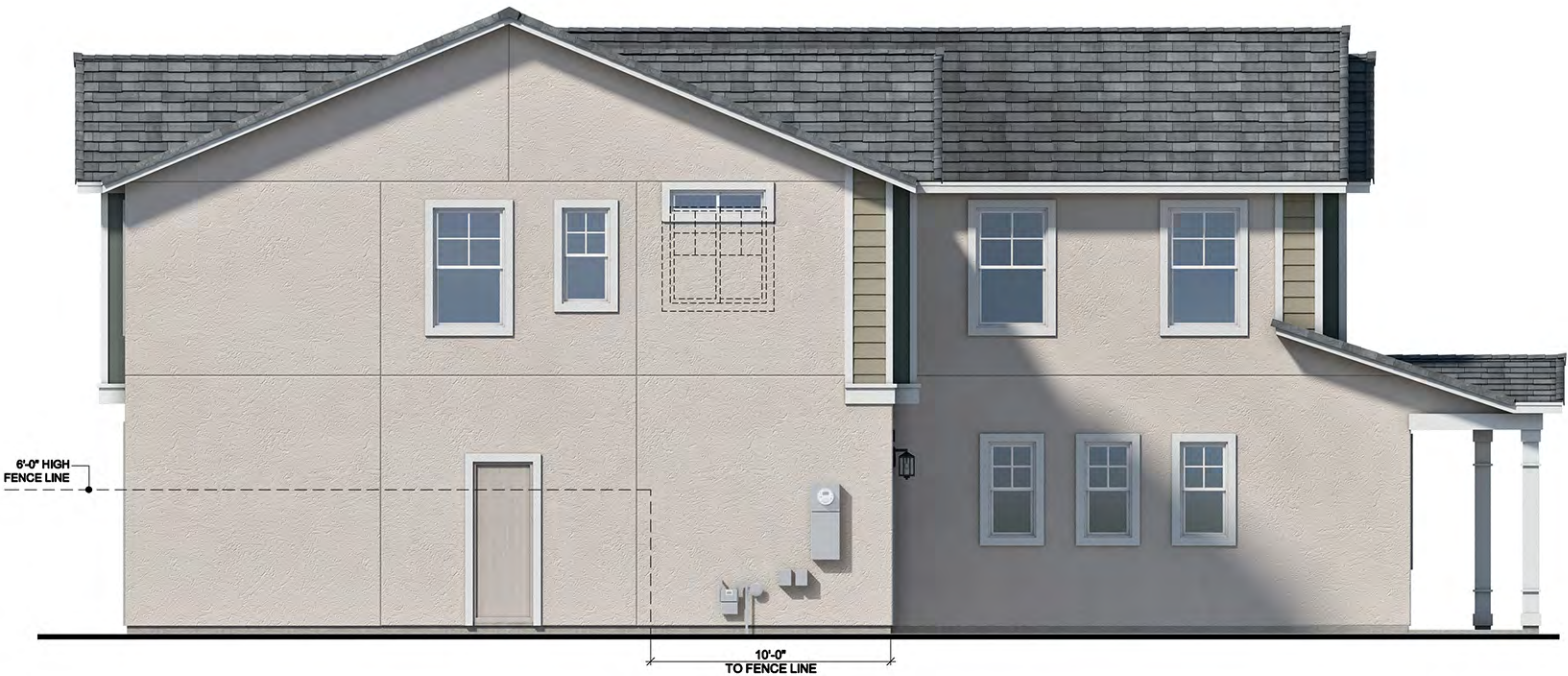
COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

PLAN 4

241.2791

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

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Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

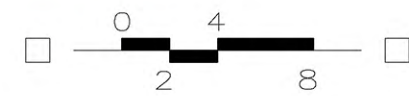


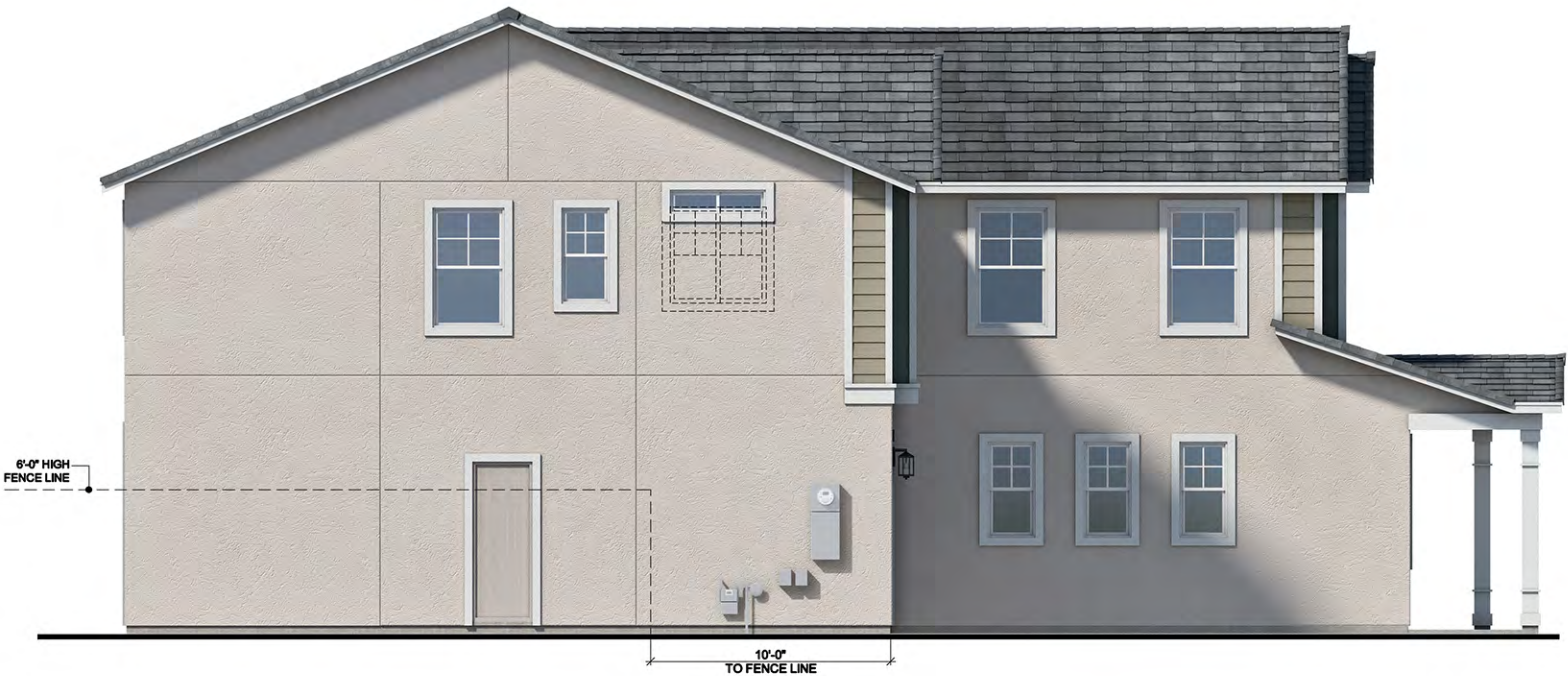
Right Elevation 'C' (American Farmhouse)



Rear Elevation 'C' (American Farmhouse)  
Visible from Archibald Avenue

Elevation 'C' (American Farmhouse)  
at Lot 11





Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

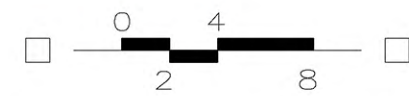


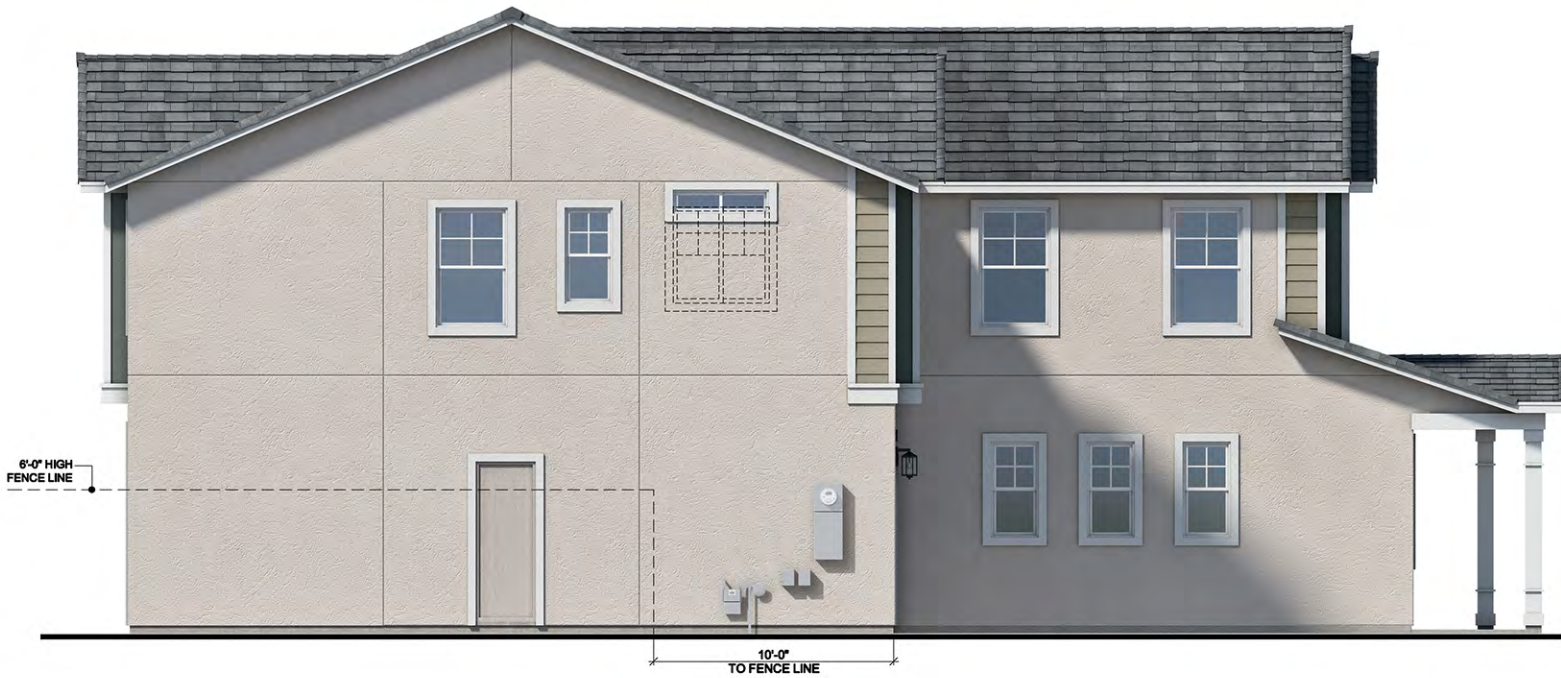
Right Elevation 'C' (American Farmhouse)  
Visible from the Green Belt



Rear Elevation 'C' (American Farmhouse)

Elevation 'C' (American Farmhouse)  
at Lot 30





Left Elevation 'C' (American Farmhouse)



Front Elevation 'C' (American Farmhouse)

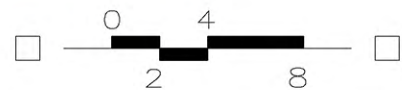


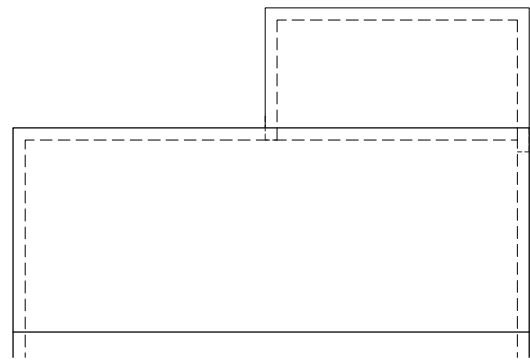
Right Elevation 'C' (American Farmhouse)  
Visible from the Green Belt

Elevation 'C' (American Farmhouse)  
at Lots 14, 23, 26 & 35

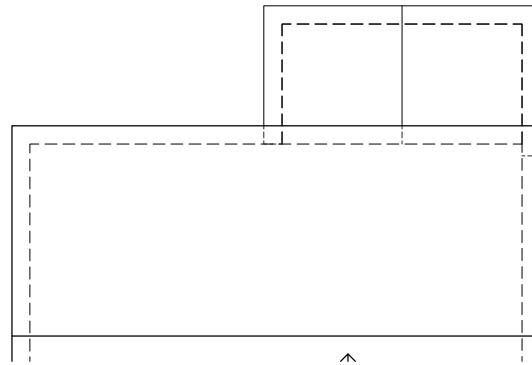


Rear Elevation 'C' (American Farmhouse)  
Visible from Kinglet Avenue & Archibald Avenue

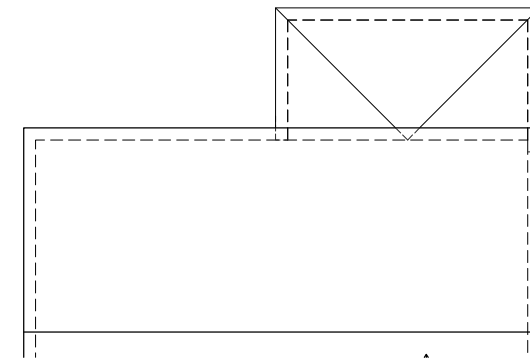




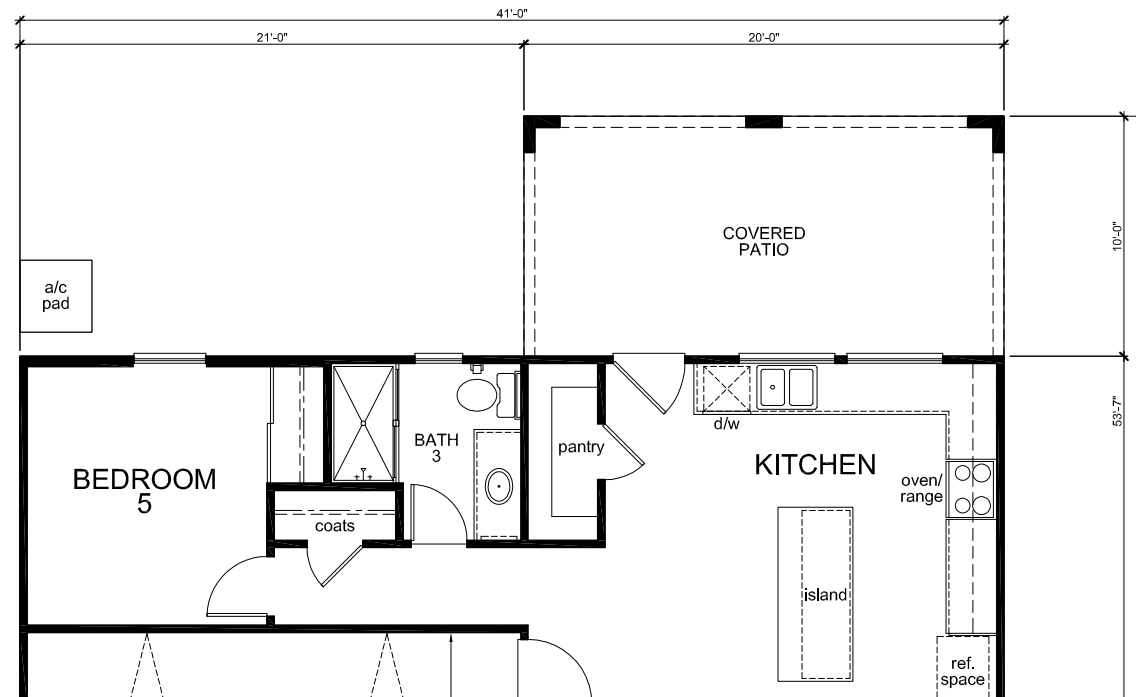
Partial Roof Plan 'A' (Spanish Colonial)  
at Covered Patio



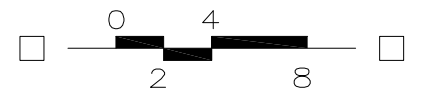
Partial Roof Plan 'B' (Craftsman)  
at Covered Patio



Partial Roof Plan 'C' (American Farmhouse)  
at Covered Patio



Partial First Floor Plan  
at Covered Patio



PLAN 4



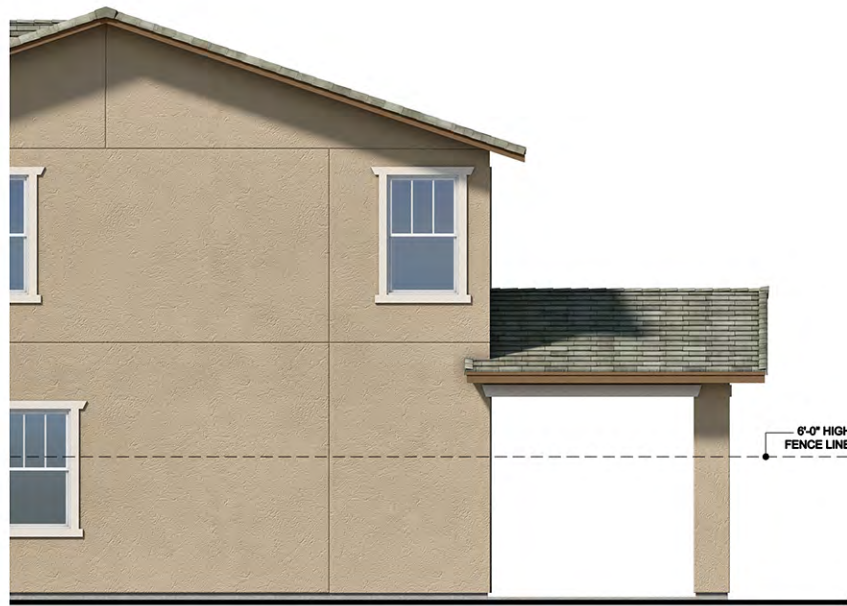
KB Home Southern California/Inland Empire  
36310 Inland Valley Drive  
Wildomar, CA 92595  
(951) 691-5300  
ONTARIO, CA

COUNTRYSIDE 36  
TENTATIVE TRACT MAP 18916

241.2791

JOB No. : 350999  
STORY: Two  
REV: December 12, 2022

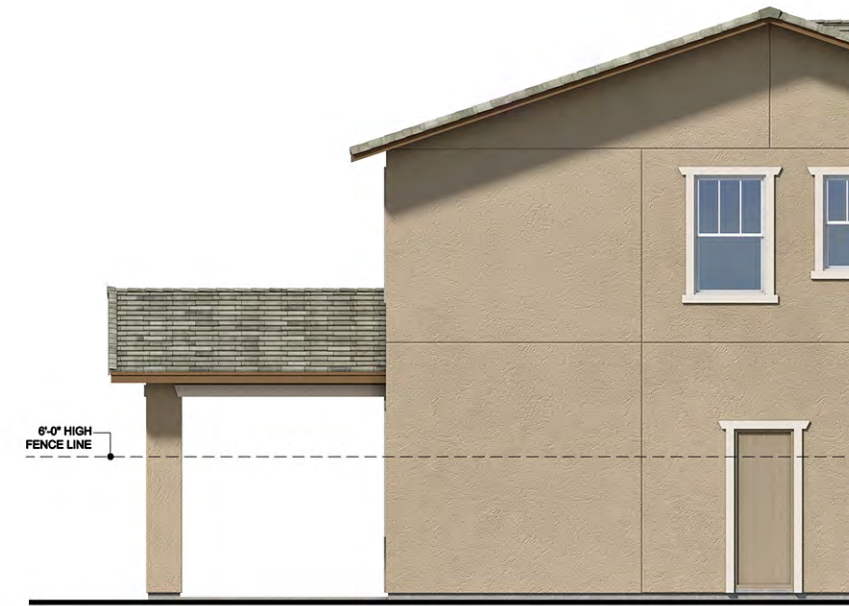
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Partial Right Elevation 'B' (Craftsman)  
at Covered Patio



Rear Elevation 'B' (Craftsman)  
at Covered Patio



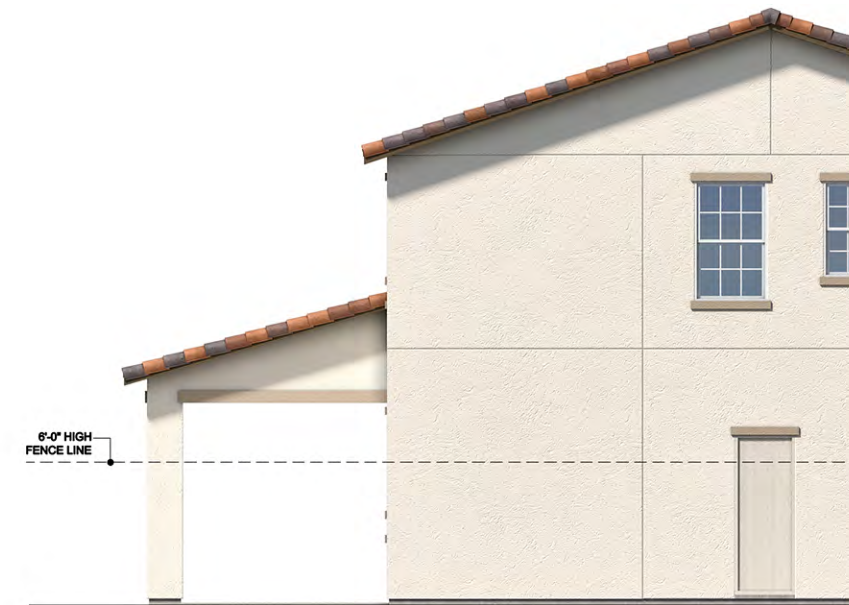
Partial Left Elevation 'B' (Craftsman)  
at Covered Patio



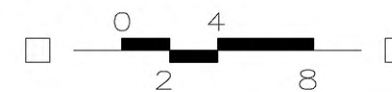
Partial Right Elevation 'A' (Spanish Colonial)  
at Covered Patio

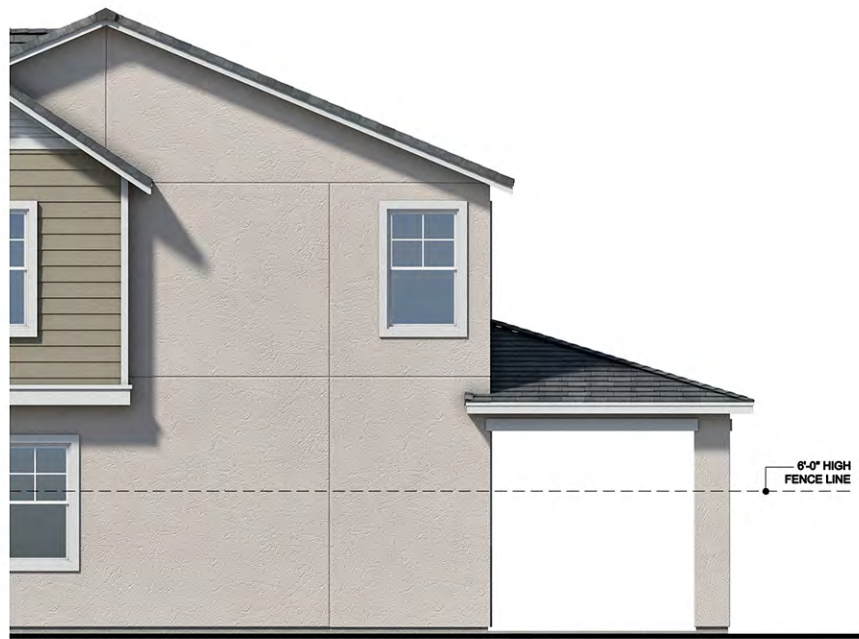


Rear Elevation 'A' (Spanish Colonial)  
at Covered Patio



Partial Left Elevation 'A' (Spanish Colonial)  
at Covered Patio

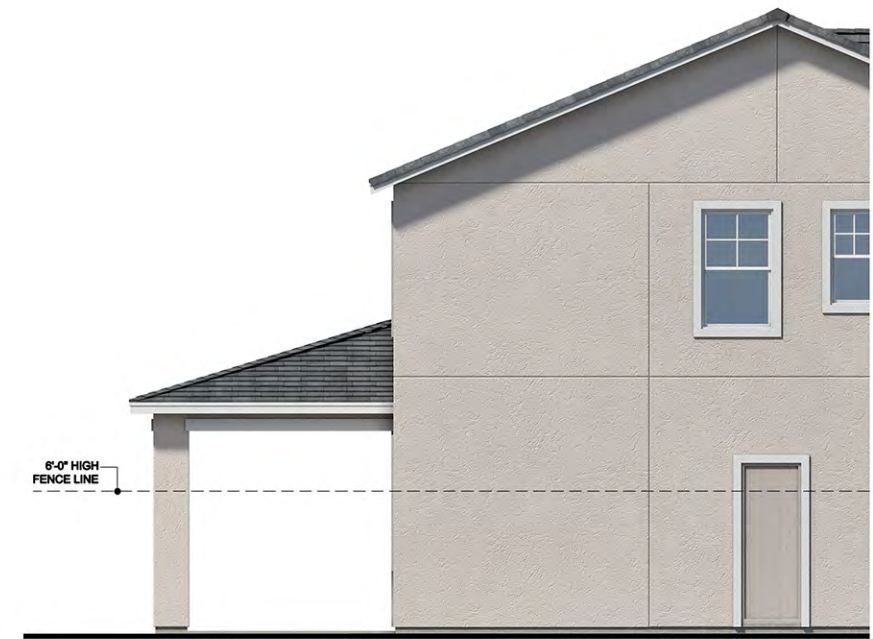




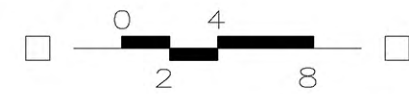
Partial Right Elevation 'C' (American Farmhouse)  
at Covered Patio



Rear Elevation 'C' (American Farmhouse)  
at Covered Patio



Partial Left Elevation 'C' (American Farmhouse)  
at Covered Patio





**Attachment B: Noise Impact Analysis**

*(Noise Impact Analysis follow this page)*



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# **Countryside 36**

## **NOISE IMPACT ANALYSIS**

### **CITY OF ONTARIO**

PREPARED BY:

Bill Maddux  
bmaddux@urbanxroads.com  
(619) 778-1971

MARCH 23, 2023

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15172-02 Noise Study



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## LIST OF ABBREVIATED TERMS

(1)	Reference
ADT	Average Daily Traffic
ANSI	American National Standards Institute
CEQA	California Environmental Quality Act
CNEL	Community Noise Equivalent Level
dBA	A-weighted decibels
EPA	Environmental Protection Agency
FHWA	Federal Highway Administration
FTA	Federal Transit Administration
Hz	Hertz
INCE	Institute of Noise Control Engineering
$L_{eq}$	Equivalent continuous (average) sound level
$L_{max}$	Maximum level measured over the time interval
$L_{min}$	Minimum level measured over the time interval
OPR	Office of Planning and Research
PPV	Peak particle velocity
Project	Countryside 36
REMEL	Reference Energy Mean Emission Level
RMS	Root-mean-square
VdB	Vibration Decibels

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## EXECUTIVE SUMMARY

Urban Crossroads, Inc. has prepared this noise study to determine the noise exposure and the necessary noise abatement measures for the proposed residential development Countryside 36 ("Project"). The Project site is generally located west of Archibald Avenue, north of Schaeffer Avenue and south of Chino Avenue, in the City of Ontario. It is our understanding that the Project consists of the development of 36 single family residential lots. This noise impact analysis was prepared to verify the City of Ontario noise level standards are complied with and identify adequate noise abatement measures, as necessary, for incorporation into the Project's design. All recommendations for exterior and interior noise abatement are identified based on the latest Project site plans.

### EXTERIOR NOISE ABATEMENT

To satisfy the City of Ontario 65 dBA CNEL exterior noise level standard for residential land uses, outdoor living areas (backyards) of all lots adjacent to Archibald Avenue will require a minimum 8-foot-high noise barrier. The noise barrier height recommendations represent the minimum wall and/or berm combination height required to satisfy the City of Ontario exterior noise level standards. With the recommended noise barriers shown on Exhibit ES-A, the future exterior noise levels will range from 64.6 to 64.8 dBA CNEL in outdoor living areas (backyards). Therefore, the recommended noise barriers will satisfy the City of Ontario 65 dBA CNEL exterior noise level standards for residential land uses and impacts will be *less than significant*.

The recommended noise control barriers shall be constructed so that the top of each wall and/or berm combination extends to the planned height above the pad elevation of the lot it is shielding. When the road is elevated above the pad elevation, the barrier shall extend to the recommended height above the highest point between the residential home and the road. The barrier shall provide a weight of at least 4 pounds per square foot of face area with no decorative cutouts or line-of-sight openings between shielded areas and the roadways. All gaps (except for weep holes) should be filled with grout or caulking. (1) The noise barrier shall be constructed using the following materials:

- Masonry block;
- Stucco veneer over wood framing (or foam core), or 1-inch-thick tongue and groove wood of sufficient weight per square foot;
- Glass (1/4-inch-thick), or other transparent material with sufficient weight per square foot capable of providing a minimum transmission loss of 20 dBA;
- Earthen berm;
- Any combination of these construction materials; or,
- Any material with a minimum transmission loss of 20 dBA.



## INTERIOR NOISE ABATEMENT

Typical building construction will provide a Noise Reduction (NR) of approximately 12 dBA with "windows open" and a minimum 25 dBA noise reduction with "windows closed." (2) (3) To satisfy the City of Ontario 45 dBA CNEL interior noise level criteria, first-floor receivers on lots adjacent to Archibald Avenue will require a Noise Reduction (NR) of up to 19.8 dBA. To satisfy the City of Ontario 45 dBA CNEL interior noise level criteria, second story receivers on lots adjacent to Archibald Avenue will require a Noise Reduction (NR) of up to 29.7 dBA. To meet the City of Ontario 45 dBA CNEL interior noise standards for residential land use the Project shall provide the following or equivalent noise abatement measures:

- Windows & Glass Doors: First story facades on lots adjacent to Archibald Avenue require windows and glass doors with well-fitted, well-weather-stripped assemblies with minimum sound transmission class (STC) ratings of 27 and a means of mechanical ventilation (e.g. air conditioning). Second Story facades on lots adjacent to Archibald Avenue require windows and glass doors with well-fitted, well-weather-stripped assemblies with minimum STC ratings of 32 and a means of mechanical ventilation (e.g. air conditioning).
- Doors (Non-Glass): All exterior doors shall be well weather-stripped. Well-sealed perimeter gaps around the doors are essential to achieve the optimal STC rating. (4)
- Walls: At any penetrations of exterior walls by pipes, ducts, or conduits, the space between the wall and pipes, ducts, or conduits shall be caulked or filled with mortar to form an airtight seal.
- Roof: Roof sheathing of wood construction shall be per manufacturer's specification or caulked plywood of at least one-half inch thick. Ceilings shall be per manufacturer's specification or well-sealed gypsum board of at least one-half inch thick. Insulation with at least a rating of R-19 shall be used in the attic space.
- Ventilation: Arrangements for any habitable room shall be such that any exterior door or window can be kept closed when the room is in use and still receive circulated air. A forced air circulation system (e.g. air conditioning) or active ventilation system (e.g. fresh air supply) shall be provided which satisfies the requirements of the Uniform Building Code.

With the interior noise abatement measures provided in this study, the proposed Project is expected to satisfy the City of Ontario 45 dBA CNEL interior noise level standards for residential development.

EXHIBIT ES-A: SUMMARY OF RECOMMENDATIONS



LEGEND:

— Planned Noise Barrier 8' Planned Noise Barrier Height (in feet)

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# **1 INTRODUCTION**

This noise analysis has been completed to determine the noise impacts associated with the development of the proposed Countryside 36 (“Project”). This noise study briefly describes the proposed Project, provides information regarding noise fundamentals, sets out the local regulatory setting, presents the study methods and procedures for noise analysis, and evaluates the future exterior noise environment. In addition, this study includes an analysis of the potential Project-related long-term stationary-source operational noise and short-term construction noise and vibration impacts.

## **1.1 SITE LOCATION**

The Project site is generally located west of Archibald Avenue, north of Schaeffer Avenue and south of Chino Avenue City of Ontario, as shown on Exhibit 1-A. The Ontario International Airport (ONT) is located approximately 3 miles north of the Project site and Chino Airport is located approximately 2.7 miles southwest of the Project site.

## **1.2 PROJECT DESCRIPTION**

The Project consists of the development of 36 single family residential lots, as shown on Exhibit 1-B.

EXHIBIT 1-A: LOCATION MAP



EXHIBIT 1-B: SITE PLAN



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## 2 FUNDAMENTALS

Noise is simply defined as "unwanted sound." Sound becomes unwanted when it interferes with normal activities, when it causes actual physical harm or when it has adverse effects on health. Noise is measured on a logarithmic scale of sound pressure level known as a decibel (dB). A-weighted decibels (dBA) approximate the subjective response of the human ear to broad frequency noise source by discriminating against very low and very high frequencies of the audible spectrum. They are adjusted to reflect only those frequencies which are audible to the human ear. Exhibit 2-A presents a summary of the typical noise levels and their subjective loudness and effects that are described in more detail below.

**EXHIBIT 2-A: TYPICAL NOISE LEVELS**

COMMON OUTDOOR ACTIVITIES	COMMON INDOOR ACTIVITIES	A - WEIGHTED SOUND LEVEL dBA	SUBJECTIVE LOUDNESS	EFFECTS OF NOISE
THRESHOLD OF PAIN		140	INTOLERABLE OR DEAFENING	HEARING LOSS
NEAR JET ENGINE		130		
		120		
JET FLY-OVER AT 300m (1000 ft)	ROCK BAND	110		
LOUD AUTO HORN		100	VERY NOISY	SPEECH INTERFERENCE
GAS LAWN MOWER AT 1m (3 ft)		90		
DIESEL TRUCK AT 15m (50 ft), at 80 km/hr (50 mph)	FOOD BLENDER AT 1m (3 ft)	80	LOUD	
NOISY URBAN AREA, DAYTIME	VACUUM CLEANER AT 3m (10 ft)	70		
HEAVY TRAFFIC AT 90m (300 ft)	NORMAL SPEECH AT 1m (3 ft)	60	MODERATE	SLEEP DISTURBANCE
QUIET URBAN DAYTIME	LARGE BUSINESS OFFICE	50		
QUIET URBAN NIGHTTIME	THEATER, LARGE CONFERENCE ROOM (BACKGROUND)	40		FAINT
QUIET SUBURBAN NIGHTTIME	LIBRARY	30		
QUIET RURAL NIGHTTIME	BEDROOM AT NIGHT, CONCERT HALL (BACKGROUND)	20		
	BROADCAST/RECORDING STUDIO	10		
LOWEST THRESHOLD OF HUMAN HEARING	LOWEST THRESHOLD OF HUMAN HEARING	0	VERY FAINT	

Source: Environmental Protection Agency Office of Noise Abatement and Control, Information on Levels of Environmental Noise Requisite to Protect Public Health and Welfare with an Adequate Margin of Safety (EPA/ONAC 550/9-74-004) March 1974.

### 2.1 RANGE OF NOISE

Since the range of intensities that the human ear can detect is so large, the scale frequently used to measure intensity is a scale based on multiples of 10, the logarithmic scale. The scale for measuring intensity is the decibel scale. Each interval of 10 decibels indicates a sound energy ten times greater than before, which is perceived by the human ear as being roughly twice as loud. (5) The most common sounds vary between 40 dBA (very quiet) to 100 dBA (very loud). Normal conversation at three feet is roughly at 60 dBA, while loud jet engine noises equate to 110 dBA



at approximately 1,000 feet, which can cause serious discomfort. (6) Another important aspect of noise is the duration of the sound and the way it is described and distributed in time.

## 2.2 NOISE DESCRIPTORS

Environmental noise descriptors are generally based on averages, rather than instantaneous, noise levels. The most used metric is the equivalent level ( $L_{eq}$ ). Equivalent sound levels are not measured directly but are calculated from sound pressure levels typically measured in A-weighted decibels (dBA). The equivalent sound level ( $L_{eq}$ ) represents a steady state sound level containing the same total energy as a time varying signal over a given sample period and is commonly used to describe the “average” noise levels within the environment.

Peak hour or average noise levels, while useful, do not completely describe a given noise environment. Noise levels lower than peak hour may be disturbing if they occur during times when quiet is most desirable, namely evening and nighttime (sleeping) hours. To account for this, the Community Noise Equivalent Level (CNEL), representing a composite 24-hour noise level is utilized. The CNEL is the weighted average of the intensity of a sound, with corrections for time of day, and averaged over 24 hours. The time-of-day corrections require the addition of 5 decibels to dBA  $L_{eq}$  sound levels in the evening from 7:00 p.m. to 10:00 p.m., and the addition of 10 decibels to dBA  $L_{eq}$  sound levels at night between 10:00 p.m. and 7:00 a.m. These additions are made to account for the noise sensitive time periods during the evening and night hours when noise can become more intrusive. CNEL does not represent the actual sound level heard at any time, but rather represents the total sound exposure. The City of Ontario relies on the 24-hour CNEL level to assess land use compatibility with transportation related noise sources.

## 2.3 SOUND PROPAGATION

When sound propagates over a distance, it changes in level and frequency content. The way noise reduces with distance depends on the following factors.

### 2.3.1 GEOMETRIC SPREADING

Sound from a localized source (i.e., a stationary point source) propagates uniformly outward in a spherical pattern. The sound level attenuates (or decreases) at a rate of 6 dB for each doubling of distance from a point source. Highways consist of several localized noise sources on a defined path and hence can be treated as a line source, which approximates the effect of several point sources. Noise from a line source propagates outward in a cylindrical pattern, often referred to as cylindrical spreading. Sound levels attenuate at a rate of 3 dB for each doubling of distance from a line source. (5)

### 2.3.2 GROUND ABSORPTION

The propagation path of noise from a highway to a receiver is usually very close to the ground. Noise attenuation from ground absorption and reflective wave canceling adds to the attenuation associated with geometric spreading. Traditionally, the excess attenuation has also been expressed in terms of attenuation per doubling of distance. This approximation is usually

sufficiently accurate for distances of less than 200 ft. For acoustically hard sites (i.e., sites with a reflective surface between the source and the receiver, such as a parking lot or body of water), no excess ground attenuation is assumed. For acoustically absorptive or soft sites (i.e., those sites with an absorptive ground surface between the source and the receiver such as soft dirt, grass, or scattered bushes and trees), an excess ground attenuation value of 1.5 dB per doubling of distance is normally assumed. When added to the cylindrical spreading, the excess ground attenuation results in an overall drop-off rate of 4.5 dB per doubling of distance from a line source. (7)

### **2.3.3 ATMOSPHERIC EFFECTS**

Receivers located downwind from a source can be exposed to increased noise levels relative to calm conditions, whereas locations upwind can have lowered noise levels. Sound levels can be increased at large distances (e.g., more than 500 feet) due to atmospheric temperature inversion (i.e., increasing temperature with elevation). Other factors such as air temperature, humidity, and turbulence can also have significant effects. (5)

### **2.3.4 SHIELDING**

A large object or barrier in the path between a noise source and a receiver can substantially attenuate noise levels at the receiver. The amount of attenuation provided by shielding depends on the size of the object and the frequency content of the noise source. Shielding by trees and other such vegetation typically only has an “out of sight, out of mind” effect. That is, the perception of noise impact tends to decrease when vegetation blocks the line-of-sight to nearby residents. However, for vegetation to provide a substantial, or even noticeable, noise reduction, the vegetation area must be at least 15 feet in height, 100 feet wide and dense enough to completely obstruct the line-of-sight between the source and the receiver. This size of vegetation may provide up to 5 dBA of noise reduction. The Federal Highway Administration (FHWA) does not consider the planting of vegetation to be a noise abatement measure. (8)

## **2.4 NOISE CONTROL**

Noise control is the process of obtaining an acceptable noise environment for an observation point or receiver by controlling the noise source, transmission path, receiver, or all three. This concept is known as the source-path-receiver concept. In general, noise control measures can be applied to these three elements.

## **2.5 NOISE BARRIER ATTENUATION**

Effective noise barriers can reduce noise levels by 10 to 15 dBA, cutting the loudness of traffic noise in half. A noise barrier is most effective when placed close to the noise source or receiver. Noise barriers, however, do have limitations. For a noise barrier to work, it must block the line-of-sight path of sound from the noise source.

## 2.6 LAND USE COMPATIBILITY WITH NOISE

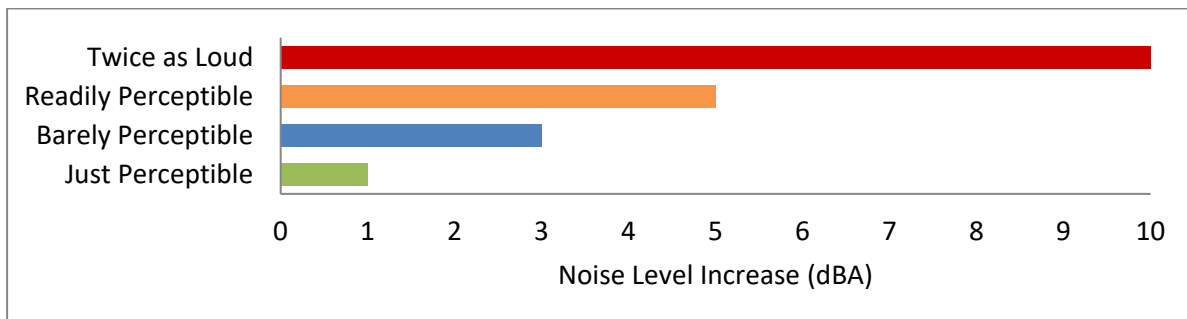
Some land uses are more tolerant of noise than others. For example, schools, hospitals, churches, and residences are more sensitive to noise intrusion than are commercial or industrial developments and related activities. As ambient noise levels affect the perceived amenity or livability of a development, so too can the mismanagement of noise impacts impair the economic health and growth potential of a community by reducing the area’s desirability as a place to live, shop and work. For this reason, land use compatibility with the noise environment is an important consideration in the planning and design process. The FHWA encourages State and Local government to regulate land development in such a way that noise-sensitive land uses are either prohibited from being located adjacent to a highway, or that the developments are planned, designed, and constructed in such a way that noise impacts are minimized. (9)

## 2.7 COMMUNITY RESPONSE TO NOISE

Approximately sixteen percent of the population has a very low tolerance for noise and will object to any noise not of their making. Consequently, even in the quietest environment, some complaints may occur. Twenty to thirty percent of the population will not complain even in very severe noise environments. (10 pp. 8-6) Thus, a variety of reactions can be expected from people exposed to any given noise environment.

Surveys have shown that community response to noise varies from no reaction to vigorous action for newly introduced noises averaging from 10 dB below existing to 25 dB above existing. (11) According to research originally published in the Noise Effects Handbook (10), the percentage of high annoyance ranges from approximately 0 percent at 45 dB or less, 10 percent are highly annoyed around 60 dB, and increases rapidly to approximately 70 percent being highly annoyed at approximately 85 dB or greater. Despite this variability in behavior on an individual level, the population can be expected to exhibit the following responses to changes in noise levels as shown on Exhibit 2-B. A change of 3 dBA is considered barely perceptible, and changes of 5 dBA are considered readily perceptible. (7)

**EXHIBIT 2-B: NOISE LEVEL INCREASE PERCEPTION**



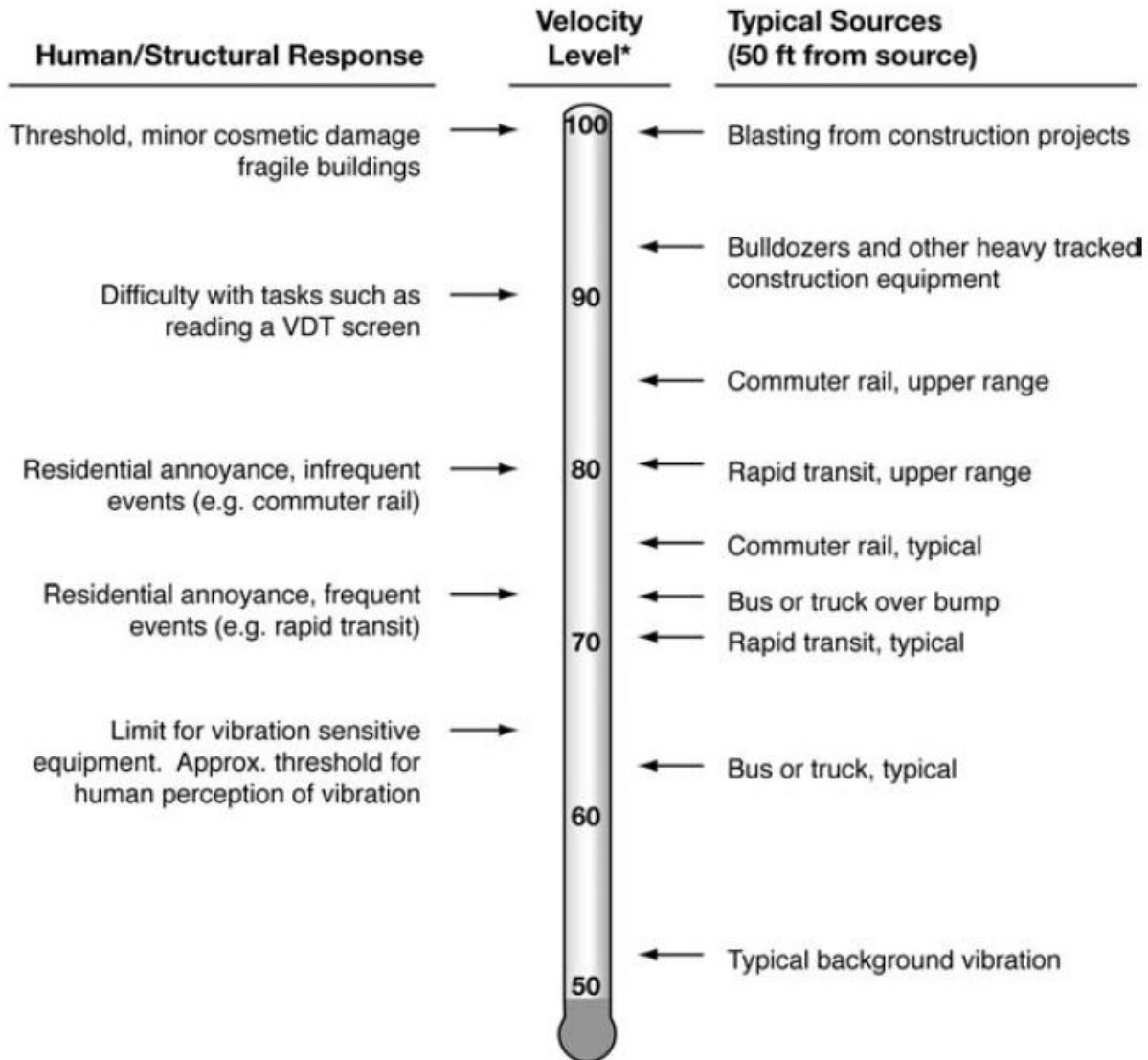
## 2.8 VIBRATION

Per the Federal Transit Administration (FTA) *Transit Noise Impact and Vibration Impact Assessment Manual* (11) , vibration is the periodic oscillation of a medium or object. The rumbling sound caused by the vibration of room surfaces is called structure-borne noise. Sources of ground-borne vibrations include natural phenomena (e.g., earthquakes, volcanic eruptions, sea waves, landslides) or human-made causes (e.g., explosions, machinery, traffic, trains, construction equipment). Vibration sources may be continuous, such as factory machinery, or transient, such as explosions. As is the case with airborne sound, ground-borne vibrations may be described by amplitude and frequency.

There are several different methods that are used to quantify vibration. The peak particle velocity (PPV) is defined as the maximum instantaneous peak of the vibration signal. The PPV is most frequently used to describe vibration impacts to buildings but is not always suitable for evaluating human response (annoyance) because it takes some time for the human body to respond to vibration signals. Instead, the human body responds to average vibration amplitude often described as the root mean square (RMS). The RMS amplitude is defined as the average of the squared amplitude of the signal and is most frequently used to describe the effect of vibration on the human body. Decibel notation (VdB) is commonly used to measure RMS. Decibel notation (VdB) serves to reduce the range of numbers used to describe human response to vibration. Typically, ground-borne vibration generated by man-made activities attenuates rapidly with distance from the source of the vibration. Sensitive receivers for vibration include structures (especially older masonry structures), people (especially residents, the elderly, and sick), and vibration-sensitive equipment and/or activities.

The background vibration-velocity level in residential areas is generally 50 VdB. Ground-borne vibration is normally perceptible to humans at approximately 65 VdB. For most people, a vibration-velocity level of 75 VdB is the approximate dividing line between barely perceptible and distinctly perceptible levels. Typical outdoor sources of perceptible ground-borne vibration are construction equipment, steel-wheeled trains, and traffic on rough roads. If a roadway is smooth, the ground-borne vibration is rarely perceptible. The range of interest is from approximately 50 VdB, which is the typical background vibration-velocity level, to 100 VdB, which is the general threshold where minor damage can occur in fragile buildings. Exhibit 2-C illustrates common vibration sources and the human and structural response to ground-borne vibration.

**EXHIBIT 2-C: TYPICAL LEVELS OF GROUND-BORNE VIBRATION**



\* RMS Vibration Velocity Level in VdB relative to 10<sup>-6</sup> inches/second

Source: Federal Transit Administration (FTA) Transit Noise and Vibration Impact Assessment Manual.

### 3 REGULATORY SETTING

The federal government, the State of California, various county governments, and most municipalities in the state have established standards and ordinances to control noise. In most areas, automobile and truck traffic is the major source of environmental noise. Traffic activity generally produces an average sound level that remains constant with time. Air and rail traffic, and commercial and industrial activities are also major sources of noise in some areas. Federal, state, and local agencies regulate different aspects of environmental noise. Federal and state agencies generally set noise standards for mobile sources such as aircraft and motor vehicles, while regulation of stationary sources is left to local agencies.

#### 3.1 STATE OF CALIFORNIA NOISE REQUIREMENTS

The State of California regulates freeway noise, sets standards for sound transmission, provides occupational noise control criteria, identifies noise standards, and provides guidance for local land use compatibility. State law requires that each county and city adopt a General Plan that includes a Noise Element which is to be prepared per guidelines adopted by the Governor's Office of Planning and Research (OPR). (12) The purpose of the Noise Element is to *limit the exposure of the community to excessive noise levels*. In addition, the California Environmental Quality Act (CEQA) requires that all known environmental effects of a project be analyzed, including environmental noise impacts.

#### 3.2 CITY OF ONTARIO GENERAL PLAN

The City of Ontario General Plan (Policy Plan) identifies several policies to minimize the impacts of excessive noise levels throughout the community. Policy Plan Section S4, Noise Hazards, establishes a goal of maintaining *an environment where noise does not adversely affect the public's health, safety, and welfare*. (13) To satisfy this goal, the Policy Plan identifies six policies related to: noise mitigation; coordination with transportation authorities; airport noise mitigation; truck traffic; roadway design; and airport noise compatibility. Noise criteria identified in The Ontario Plan 2050 (TOP 2050) Table LU-7 provides guidelines to evaluate land use compatibility within various noise environments. Table LU-7 is reproduced here as Exhibit 3-A *Noise Level Exposure and Land Use Compatibility Guidelines*. The Project residential land uses are considered *clearly acceptable* within exterior noise level environments approaching 60 dBA CNEL and *normally acceptable* within noise level environments up to 65 dBA CNEL. For noise level environments greater than 70 dBA CNEL, the Project land uses would be considered *clearly unacceptable* and no new construction should be permitted.

**EXHIBIT 3-A: NOISE LEVEL EXPOSURE AND LAND USE COMPATIBILITY GUIDELINES**

LAND USE CATEGORIES		COMMUNITY NOISE EQUIVALENT LEVEL (CNEL)					
Category	Land Use	55	60	65	70	75	80
Residential/ Lodging	Single Family / Duplex	Green	Green	Yellow	Orange	Red	Red
	Multi-Family	Green	Green	Yellow	Orange	Red	Red
	Mobile Homes	Green	Green	Yellow	Red	Red	Red
	Hotel/Motels	Green	Green	Green	Yellow	Orange	Red
Public/Institutional	Schools/Hospitals	Green	Green	Yellow	Orange	Red	Red
	Churches/ Libraries	Green	Green	Yellow	Orange	Red	Red
	Auditoriums/Concert Halls	Green	Yellow	Orange	Orange	Red	Red
Commercial	Offices	Green	Green	Green	Yellow	Yellow	Orange
	Retail	Green	Green	Green	Green	Yellow	Orange
Industrial	Manufacturing	Green	Green	Green	Green	Yellow	Orange
	Warehousing	Green	Green	Green	Green	Yellow	Yellow
Recreational/ Open Space	Parks/Playgrounds	Green	Green	Green	Yellow	Orange	Red
	Golf Courses/ Riding Stables	Green	Green	Green	Yellow	Orange	Red
	Outdoor Spectator Sports	Green	Green	Yellow	Orange	Orange	Red
	Outdoor Music Shells/ Amphitheaters	Yellow	Yellow	Orange	Red	Red	Red
	Livestock/Wildlife Preserves	Green	Green	Green	Green	Orange	Red
	Crop Agriculture	Green	Green	Green	Green	Green	Green

**LEGEND**

	<b>Clearly Acceptable:</b>	No special noise insulation required, assuming buildings of normal conventional construction.
	<b>Normally Acceptable:</b>	Acoustical reports will be required for major new residential construction. Conventional construction with closed windows and fresh air supply systems of air conditioning will normally suffice.
	<b>Normally Unacceptable:</b>	New construction should be discouraged. Noise/aviation easements required for all new construction. If new construction does proceed, a detailed analysis of noise reduction requirements must be made and necessary noise insulation features included.
	<b>Clearly Unacceptable:</b>	No new construction should be permitted.

Source: The Ontario Plan Safety Section on Noise Hazards (Table LU-7).

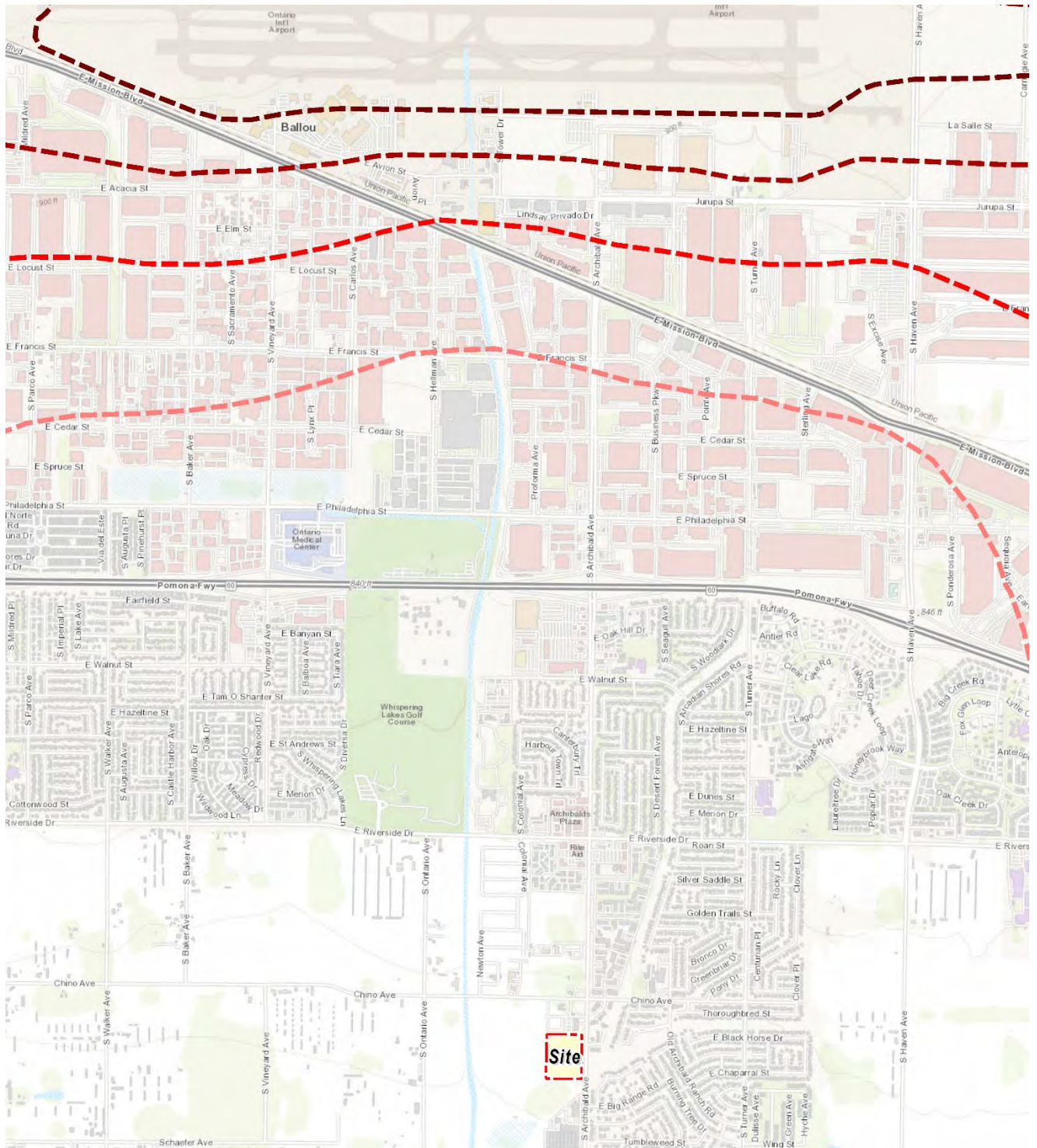
### 3.6 AIRPORT LAND USE COMPATIBILITY

The Project site is located approximately 3 miles south of the Ontario International Airport (ONT). This places the Project site within the ONT Airport Influence Area according to Policy Map 2-1 of the *Ontario International Airport Land Use Compatibility Plan (ONT ALUCP)*. The ONT ALUCP was amended July 2018 to promote compatibility between airport and the land uses that surround it (14). Since the Project site is located within the ONT Airport Influence Area, the Project is subject to the Noise Criteria established on Table 2-3 in the ONT ALUCP. As shown on Exhibit 3-B, the Project site is located within the ONT Airport Influence Area but outside the 60 dBA CNEL airport noise impact zone consistent with Policy Map 2-3. According to Table 2-3 of the ONT ALUCP, residential land uses and schools located outside the 60 dBA CNEL noise level contours of ONT, such as the Project, are considered *normally compatible land use*. For *normally compatible land use*, either the activities associated with the land use are inherently noisy or standard construction methods will sufficiently attenuate exterior noise to an acceptable indoor community noise equivalent level (CNEL).

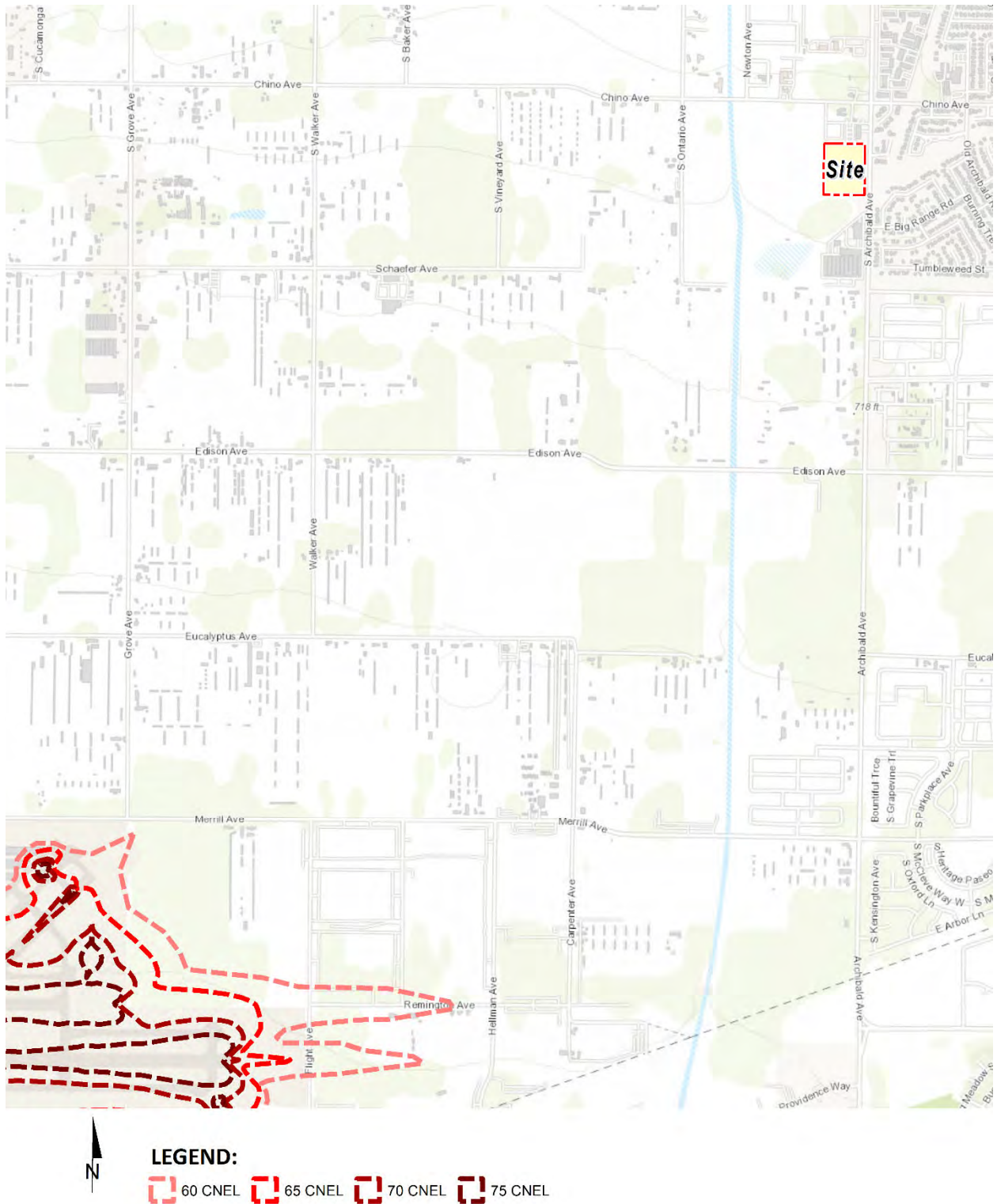
The Project site is located approximately 2.7 miles northeast of the Chino Airport (CNO). This places the Project site within the Chino Airport Influence Area according to Figure 1-4 Airport Referral Areas of the *Comprehensive Land Use Plan for the Chino Airport*. As shown on Exhibit 3-C, the Project site is outside the 60 dBA CNEL airport noise level contour and impacts would be less than significant.



EXHIBIT 3-B: ONT AIRPORT NOISE LEVEL CONTOURS



### EXHIBIT 3-C: CHINO AIRPORT NOISE LEVEL CONTOURS



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## 4 METHODS AND PROCEDURES

The following section outlines the methods and procedures used to model and analyze the future traffic noise environment.

### 4.1 FHWA TRAFFIC NOISE PREDICTION MODEL

The estimated roadway noise impacts from vehicular traffic were calculated using a computer program that replicates the Federal Highway Administration (FHWA) Traffic Noise Prediction Model- FHWA-RD-77-108. (15) The FHWA Model arrives at a predicted noise level through a series of adjustments to the Reference Energy Mean Emission Level (REMEL). In California the national REMELs are substituted with the California Vehicle Noise (Calveno) Emission Levels. (16) Adjustments are then made to the REMEL to account for: the roadway classification (e.g., collector, secondary, major or arterial), the roadway active width (i.e., the distance between the center of the outermost travel lanes on each side of the roadway), the total average daily traffic (ADT), the travel speed, the percentages of automobiles, medium trucks, and heavy trucks in the traffic volume, the roadway grade, the angle of view (e.g., whether the roadway view is blocked), the site conditions ("hard" or "soft" relates to the absorption of the ground, pavement, or landscaping), and the percentage of total ADT which flows each hour throughout a 24-hour period.

### 4.2 ON-SITE TRAFFIC NOISE PREDICTION MODEL INPUTS

The on-site roadway parameters including the average daily traffic (ADT) volumes used for this study are presented on Table 4-1. Based on the City of Ontario General Plan, Figure M-01: Roadway Classifications, Archibald Avenue is classified as a 6-lane Principal Arterial. (17) The maximum two-way traffic volumes at a level of service C, shown on Table 4-1, reflects future long-range traffic conditions needed to assess the on-site traffic noise environment and to identify the appropriate noise mitigation measures that address the worst-case future noise conditions.

**TABLE 4-1: ON-SITE ROADWAY PARAMETERS**

Roadway	Lanes	Classification <sup>1</sup>	Design Capacity <sup>2</sup>	Speeds (MPH) <sup>3</sup>	Site Conditions
Archibald Avenue	6	Principal Arterial	42,000	50	Soft

<sup>1</sup> Road classifications based upon the City of Ontario Mobility Element Figure M-01.

<sup>2</sup> LOS C design capacity traffic volumes.

<sup>3</sup> Speed Limits

Table 4-2 presents the time-of-day vehicle splits by vehicle type used to develop the 24-hour CNEL, and Table 4-3 presents the total traffic flow distributions (vehicle mixes) used for this analysis. The information in Tables 4-2 and 4-3 provides the hourly distribution percentages of automobile, medium trucks, and heavy trucks for input into the FHWA Model based on roadway types.

**TABLE 4-2: TIME OF DAY VEHICLE SPLITS**

Vehicle Type	Time of Day Splits <sup>1</sup>			Total of Time of Day Splits
	Daytime	Evening	Nighttime	
Autos	77.50%	12.90%	9.60%	100.00%
Medium Trucks	84.80%	4.90%	10.30%	100.00%
Heavy Trucks	86.50%	2.70%	10.80%	100.00%

<sup>1</sup> Typical Southern California time of day vehicle splits.

"Daytime" = 7:00 a.m. to 7:00 p.m.; "Evening" = 7:00 p.m. to 10:00 p.m.; "Nighttime" = 10:00 p.m. to 7:00 a.m.

**TABLE 4-3: DISTRIBUTION OF TRAFFIC FLOW BY VEHICLE TYPE (VEHICLE MIX)**

Roadway	Total % Traffic Flow			Total
	Autos	Medium Trucks	Heavy Trucks	
All Roadways	97.42%	1.84%	0.74%	100.00%

Source: Typical Southern California vehicle mix.

To predict the future noise environment at each lot within the Project site, coordinate information was collected to identify the noise transmission path between the noise source and receiver. The coordinate information is based on the Project site plan showing the plotting of each lot, as shown in Appendix 4.1. The site plan is used to identify the relationship between the roadway centerline elevation, the pad elevation and the centerline distance to the noise barrier, and the building façade. The first-floor exterior noise level receivers were placed five feet above the pad elevation. Second-floor receivers were placed 14 feet above the pad elevation.

## 5 ON-SITE TRAFFIC NOISE ANALYSIS

An on-site exterior noise impact analysis has been completed to determine the noise exposure levels that would result from adjacent transportation noise sources in the Project study area, and to identify potential noise mitigation measures that would achieve acceptable Project exterior and interior noise levels. The primary source of transportation noise affecting the Project site is anticipated to be from Archibald Avenue. The Project would also be exposed to nominal traffic noise from the Project's internal roads. However, due to the low traffic volume/speed, traffic noise from these internal roads will not make a substantive contribution to ambient noise conditions.

### 5.1 EXTERIOR NOISE ANALYSIS

Using the FHWA traffic noise prediction model and the parameters outlined in Tables 4-1 to 4-3, the expected future exterior noise levels for the on-site Project land uses were calculated at the receiver locations shown in Exhibit 5-1. Table 5-1 presents a summary of future on-site exterior traffic noise levels in the outdoor living areas (backyards) within the Project site. Archibald Avenue is the primary traffic noise source affecting the Project site. As indicated in Table 5-1, the outdoor living areas adjacent to Archibald Avenue will experience unshielded exterior noise levels of 75.1 dBA CNEL, which will exceed the City of Ontario exterior noise level standard of 65 dBA CNEL. The on-site traffic noise analysis calculations are provided in Appendix 5.1.

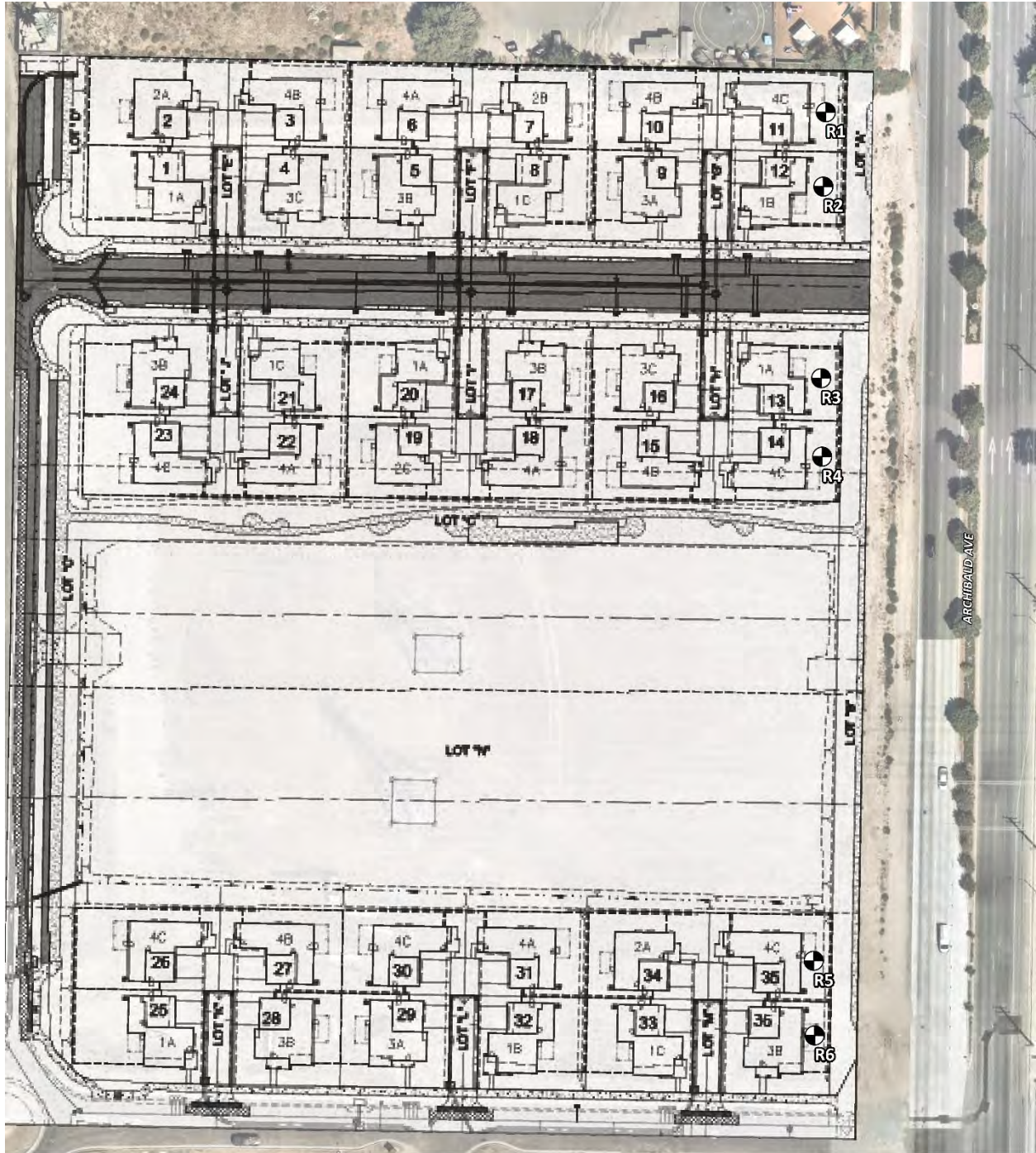
To satisfy the City of Ontario exterior noise level standards for single-family residential land uses at outdoor living areas adjacent to Archibald Avenue, the construction of a noise barrier is recommended. As indicated in Table 5-1, with a minimum 8-foot-high noise barrier shielding the outdoor living areas (backyards) of all lots adjacent to Archibald Avenue, the Project would satisfy the City of Ontario exterior noise level standard of 65 dBA CNEL for residential land uses. With the recommended noise barriers shown on Exhibit ES-A, the future exterior noise levels at the residential lots will range from 64.6 dBA to 64.8 dBA CNEL in all outdoor living areas (backyards). This noise analysis shows that the recommended noise barriers will satisfy the City of Ontario 65 dBA CNEL exterior noise level standard for residential land uses.

**TABLE 5-1: FUTURE EXTERIOR NOISE LEVELS**

Receivers	Lots	Roadway	Unmitigated Noise Level (dBA CNEL)	Mitigated Noise Level (dBA CNEL)	Barrier Height (Feet)
1 and 2	11 and 12	Archibald Avenue	75.1	64.7	8.0
3 and 4	13 and 14	Archibald Avenue	75.1	64.6	8.0
5 and 6	35 and 36	Archibald Avenue	75.1	64.8	8.0

<sup>1</sup> Exterior noise level calculations are included Appendix 5.1.

EXHIBIT 5-1: NOISE RECEIVER LOCATIONS



**LEGEND:**  
● Receiver Locations

## 5.2 ON-SITE INTERIOR NOISE ANALYSIS

As indicated in Table 5-1, lots adjacent to Archibald Avenue are predicted to experience noise levels in excess of 60 dBA CNEL. Based on the City of Ontario land use compatibility guidelines, noise levels in excess of 60 dBA CNEL should complete a noise analysis to identify any necessary insulation of structural upgrades to comply with a 45 dBA CNEL interior noise level standard. Traffic noise levels were calculated at the first and second floor building façades to verify that the interior noise levels will comply with the City of Ontario 45 dBA CNEL interior noise standards.

### 5.2.1 NOISE REDUCTION METHODOLOGY

The interior noise level is the difference between the predicted exterior noise level at the building facade and the noise reduction of the structure. Typical building construction will provide a Noise Reduction (NR) of approximately 12 dBA with "windows open" and a minimum 25 dBA noise reduction with "windows closed." (7) (3) However, sound leaks, cracks and openings within the window assembly can greatly diminish its effectiveness in reducing noise. Several methods are used to improve interior noise reduction, including: [1] weather-stripped solid core exterior doors; [2] upgraded dual glazed windows; [3] mechanical ventilation/air conditioning; and [4] exterior wall/roof assemblies free of cut outs or openings.

### 5.2.2 INTERIOR NOISE LEVEL ASSESSMENT

Table 5-2 shows that the future noise levels at the first-floor building façades are expected to range from 64.6 dBA to 64.8 dBA CNEL. Table 5-2 shows that the residential units would comply with the City of Ontario 45 dBA CNEL interior noise standard applying standard construction practices with a means of mechanical ventilation (e.g. air conditioning) and standard windows for residential lots adjacent to Archibald Avenue at first floor receivers when accounting for the recommended noise barrier discussed above.

Table 5-3 shows that the future noise levels at second floor building façades of residential units are expected to range from 74.6 dBA to 74.7 dBA CNEL. Table 5-3 shows that the residential units would not comply with the 45 dBA CNEL interior noise standard applying standard construction practices with a windows-closed condition and a means of mechanical ventilation (e.g. air conditioning) for residential lots adjacent to Archibald Avenue at second-floor receivers. Second-floor building facades adjacent to Archibald Avenue would require a windows closed condition requiring a means of mechanical ventilation (e.g. air conditioning) and windows with a minimum STC rating of 30 to satisfy City of Ontario interior noise standard of 45 dBA CNEL.

Therefore, the interior noise analysis shows that with the recommended interior noise abatement measures for second-floor facades adjacent to Archibald Avenue described in the Executive Summary, the Project will satisfy the City of Ontario 45 dBA CNEL interior noise level standards for residential development.



**TABLE 5-2: FIRST-FLOOR INTERIOR NOISE IMPACTS (CNEL)**

Receivers	Building/Lot	Noise Level at Façade <sup>1</sup>	Required Interior NR <sup>2</sup>	Estimated Interior NR <sup>3</sup>	Upgraded Windows <sup>4</sup>	Interior Noise Level <sup>5</sup>	Threshold <sup>6</sup>	Threshold Exceeded?
1 and 2	11 and 12	64.7	19.7	25.0	No	39.7	45	No
3 and 4	13 and 14	64.6	19.6	25.0	No	39.6	45	No
5 and 6	35 and 36	64.8	19.8	25.0	No	39.8	45	No

<sup>1</sup> Exterior noise level at the facade with a windows closed condition requiring a means of mechanical ventilation (e.g. air conditioning).

<sup>2</sup> Noise reduction required to satisfy the 45 dBA CNEL interior noise standards.

<sup>3</sup> Estimated minimum interior noise reduction.

<sup>4</sup> Does the required interior noise reduction trigger upgraded with a minimum STC rating of greater than 27?

<sup>5</sup> Estimated interior noise level with minimum STC rating for all windows.

"NR" = Noise reduction

**TABLE 5-3: SECOND-FLOOR INTERIOR NOISE IMPACTS (CNEL)**

Receivers	Building/Lot	Noise Level at Façade <sup>1</sup>	Required Interior NR <sup>2</sup>	Estimated Interior NR <sup>3</sup>	Upgraded Windows <sup>4</sup>	Interior Noise Level <sup>5</sup>	Threshold <sup>6</sup>	Threshold Exceeded?
1 and 2	11 and 12	74.6	29.6	25.0	No	49.6	45	<b>Yes</b>
3 and 4	13 and 14	74.6	29.6	25.0	No	49.6	45	<b>Yes</b>
5 and 6	35 and 36	74.7	29.7	25.0	No	49.7	45	<b>Yes</b>

<sup>1</sup> Exterior noise level at the facade with a windows closed condition requiring a means of mechanical ventilation (e.g. air conditioning).

<sup>2</sup> Noise reduction required to satisfy the 45 dBA CNEL interior noise standards.

<sup>3</sup> Estimated minimum interior noise reduction.

<sup>4</sup> Does the required interior noise reduction trigger upgraded with a minimum STC rating of greater than 27?

<sup>5</sup> Estimated interior noise level with minimum STC rating for all windows.

"NR" = Noise reduction

## 6 REFERENCES

1. **U.S. Department of Transportation Federal Highway Administration.** Acoustical Consideration. *Noise Barrier Design Handbook*. [Online] [Cited: November 28, 2016.] [https://www.fhwa.dot.gov/environment/noise/noise\\_barriers/design\\_construction/design/design03.cfm](https://www.fhwa.dot.gov/environment/noise/noise_barriers/design_construction/design/design03.cfm).
2. **U.S. Department of Transportation, Federal Highway Administration, Office of Environment and Planning, Noise and Air Quality Branch.** *Highway Traffic Noise Analysis and Abatement Policy and Guidance*. December 2011.
3. **California Department of Transportation.** *Traffic Noise Analysis Protocol*. May 2011.
4. **Harris, Cyril M.** *Noise Control in Buildings*. s.l. : McGraw-Hill, Inc., 1994.
5. **California Department of Transportation Environmental Program.** *Technical Noise Supplement - A Technical Supplement to the Traffic Noise Analysis Protocol*. Sacramento, CA : s.n., September 2013.
6. **Environmental Protection Agency Office of Noise Abatement and Control.** *Information on Levels of Environmental Noise Requisite to Protect Public Health and Welfare with an Adequate Margin of Safety*. March 1974. EPA/ONAC 550/9/74-004.
7. **U.S. Department of Transportation, Federal Highway Administration, Office of Environment and Planning, Noise and Air Quality Branch.** *Highway Traffic Noise Analysis and Abatement Policy and Guidance*. December 2011.
8. **U.S. Department of Transportation Federal Highway Administration.** *Highway Noise Barrier Design Handbook*. 2001.
9. **U.S. Department of Transportation, Federal Highway Administration.** *Highway Traffic Noise in the United States, Problem and Response*. April 2000. p. 3.
10. **U.S. Environmental Protection Agency Office of Noise Abatement and Control.** *Noise Effects Handbook-A Desk Reference to Health and Welfare Effects of Noise*. October 1979 (revised July 1981). EPA 550/9/82/106.
11. **U.S. Department of Transportation, Federal Transit Administration.** *Transit Noise and Vibration Impact Assessment Manual*. September 2018.
12. **Office of Planning and Research.** *State of California General Plan Guidelines*. October 2019.
13. **City of Ontario.** *The Ontario Plan Safety Section, S4, Noise Hazards*. March 2014.
14. —. *Ontario International Airport Land Use Compatibility Plan*. July 2018.
15. **U.S. Department of Transportation, Federal Highway Administration.** *FHWA Highway Traffic Noise Prediction Model*. December 1978. FHWA-RD-77-108.
16. **California Department of Transportation Environmental Program, Office of Environmental Engineering.** *Use of California Vehicle Noise Reference Energy Mean Emission Levels (Calveno REMELs) in FHWA Highway Traffic Noise Prediction*. September 1995. TAN 95-03.
17. **City of Ontario.** *The Ontario Plan 2050*. 2022.

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## 7 CERTIFICATIONS

The contents of this noise study report represent an accurate depiction of the noise environment and impacts associated with the proposed Countryside 36 Project. The information contained in this noise study report is based on the best available data at the time of preparation. If you have any questions, please contact me directly at (619) 788-1971.

William Maddux  
Senior Associate  
URBAN CROSSROADS, INC.  
(619) 788-1971  
[bmaddux@urbanxroads.com](mailto:bmaddux@urbanxroads.com)

### EDUCATION

Bachelor of Science in Urban and Regional Planning  
California Polytechnic State University, Pomona • June 2000

### PROFESSIONAL AFFILIATIONS

ASA – Acoustical Society of America  
AEP – Association of Environmental Planners  
AWMA – Air and Waste Management Association  
INCE – Institute of Noise Control Engineers

### PROFESSIONAL CERTIFICATIONS

Approved Acoustical Consultant • County of San Diego  
FHWA Traffic Noise Model of Training • November 2004  
CadnaA Basic and Advanced Training Certificate • October 2008.

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**APPENDIX 4.1:**  
**DEVELOPMENT PLANS**

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**GENERAL NOTES:**

- ALL WORK MUST BE DONE IN STRICT CONFORMANCE WITH THE CURRENT CITY OF ONTARIO STANDARD SPECIFICATIONS AND STANDARD DRAWINGS AND CURRENT STANDARD SPECIFICATIONS AND STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
- ANY CONTRACTOR PERFORMING WORK ON THIS PROJECT MUST FAMILIARIZE HIMSELF/HERSELF WITH THE SITE AND WILL BE SOLELY RESPONSIBLE FOR ANY DAMAGE TO EXISTING FACILITIES RESULTING DIRECTLY OR INDIRECTLY FROM HIS/HER OPERATIONS, WHETHER OR NOT THE FACILITY IS SHOWN ON THESE PLANS.
- ALL OBSTRUCTIONS WITHIN THE AREA TO BE IMPROVED WILL BE REMOVED AND/OR RELOCATED AT THE DIRECTION OF THE CITY ENGINEER. UTILITIES ARE TO BE RELOCATED BY THEIR RESPECTIVE OWNERS UNLESS NOTED OTHERWISE. THE CONTRACTOR IS REFERRED TO SECTION 5 OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
- UTILITY LINE LOCATIONS WERE TAKEN FROM AVAILABLE RECORD DATA AND WERE NOT LOCATED IN THE FIELD, UNLESS OTHERWISE NOTED ON THE PLAN. THE CONTRACTOR IS REFERRED TO SECTION 5 OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
- IN CASE OF ANY ACCIDENTS INVOLVING SAFETY MATTERS COVERED BY SECTION 6409(B) OF THE CALIFORNIA LABOR CODE, THE CONTRACTOR MUST IMMEDIATELY NOTIFY THE STATE DIVISION OF INDUSTRIAL SAFETY.
- STATE LAW (SB 3019) REQUIRES THE CONTRACTOR TO CONTACT UNDERGROUND SERVICE ALERT AND OBTAIN AN IDENTIFICATION NUMBER PRIOR TO THE ISSUANCE OF CITY'S ENCROACHMENT PERMIT. THE CONTRACTOR MUST NOTIFY UNDERGROUND SERVICE ALERT AT 1-800-422-4133 OR 811 A MINIMUM OF 48 HOURS IN ADVANCE OF ANY CONSTRUCTION ACTIVITIES.
- PRIOR TO THE START OF ANY WORK THE CONTRACTOR MUST OBTAIN A BUSINESS LICENSE FROM THE CITY OF ONTARIO. THE CONTRACTOR MUST ALSO OBTAIN AN ENCROACHMENT PERMIT FROM THE ENGINEERING DEPARTMENT NO LESS THAN 48 HOURS PRIOR TO START OF ANY CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY.
- THE CONTRACTOR MUST OBTAIN A TRAFFIC CONTROL PERMIT 48 HOURS PRIOR TO TIME OF CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY. THE CONTRACTOR IS REFERRED TO SECTION 7-10.3 OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION.
- THE CONTRACTOR MUST RENEW OR REPLACE ANY EXISTING TRAFFIC STRIPING AND/OR PAVEMENT MARKINGS, WHICH DURING HIS OPERATIONS HAVE BEEN EITHER REMOVED OR THE EFFECTIVENESS OF WHICH HAS BEEN REDUCED. RENEWAL OF TRAFFIC STRIPING AND MARKINGS WILL BE DONE USING REFLECTIVE THERMO-PLASTIC MARKINGS IN CONFORMANCE WITH SECTION 84 & 85 OF THE CALTRANS STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. ALL REGULATORY, WARNING, AND GUIDE SIGNS MUST HAVE 3M DIAMOND, VIP GRADE SHEETING WITH SERIES 1160 PROTECTIVE OVERLAY FILM.
- THE LAND SURVEYORS ACT, SECTION 8771 OF THE BUSINESS & PROFESSIONAL CODE, AND SECTIONS 732.5, 1492-5, 1810-5 OF THE STREETS AND HIGHWAY CODE REQUIRE THAT SURVEY MONUMENTS MUST BE PROTECTED AND PERPETUATED. IT WILL BE THE RESPONSIBILITY OF THE DEVELOPER OR OTHERS PERFORMING THE CONSTRUCTION WORK TO RETAIN A QUALIFIED REGISTERED CIVIL ENGINEER AND/OR LICENSED LAND SURVEYOR PRIOR TO THE START OF CONSTRUCTION TO LOCATE, REFERENCE AND FILE THE NECESSARY CORNER RECORDS WITH THE COUNTY SURVEYOR'S OFFICE FOR SURVEY CONTROL POINTS/ MONUMENTS THAT EXIST AS SHOWN ON RECORDED TRACT MAPS, PARCEL MAPS, RECORDS OF SURVEYS AND HIGHWAY MAPS, AND MAY BE DISTURBED OR DAMAGED BY THE PROPOSED CONSTRUCTION. AFTER THE COMPLETION OF THE PROPOSED CONSTRUCTION, SAID MONUMENTS AND/OR CONTROL SURVEY POINTS MUST BE RESET TO THE NEW SURFACE IN ACCORDANCE WITH CURRENT PROFESSIONAL LAND SURVEYING PRACTICES. CORNER RECORDS MUST BE FILED WITH THE COUNTY SURVEYOR FOR ALL THE NEW MONUMENTS SET.
- THE DEVELOPER WILL PROVIDE THE CITY WITH A COMPLETED SET OF "AS BUILT", MYLAR DRAWINGS PRIOR TO THE FINAL INSPECTION.
- A CITY ACCEPTED/APPROVED SET OF PLANS MUST BE KEPT ON THE JOB SITE AT ALL TIMES.
- A PRE-CONSTRUCTION MEETING MUST OCCUR PRIOR TO CONSTRUCTION. ATTENDEES MUST INCLUDE A CITY REPRESENTATIVE AND THE CONTRACTOR WHO WILL PERFORM THE WORK. "CUT-SHEETS" WILL BE PROVIDED TO THE CITY AT THIS MEETING FOR ITS REVIEW.
- CITY ACCEPTANCE OF PLANS DOES NOT RELIEVE THE DEVELOPER FROM RESPONSIBILITY FOR THE CORRECTION OF ERROR AND OMISSION DISCOVERED DURING CONSTRUCTION. UPON REQUEST OF THE CITY INSPECTOR, THE REQUIRED PLAN REVISIONS WILL BE PROMPTLY SUBMITTED TO THE CITY ENGINEER FOR REVIEW.
- ANY REQUIRED RIGHT-OF-WAY OR EASEMENT WILL BE DEDICATED TO AND ACCEPTED BY THE CITY PRIOR TO COMMENCEMENT OF CONSTRUCTION OF THE IMPROVEMENTS WITHIN THE REQUIRED RIGHT-OF-WAY OR EASEMENT.

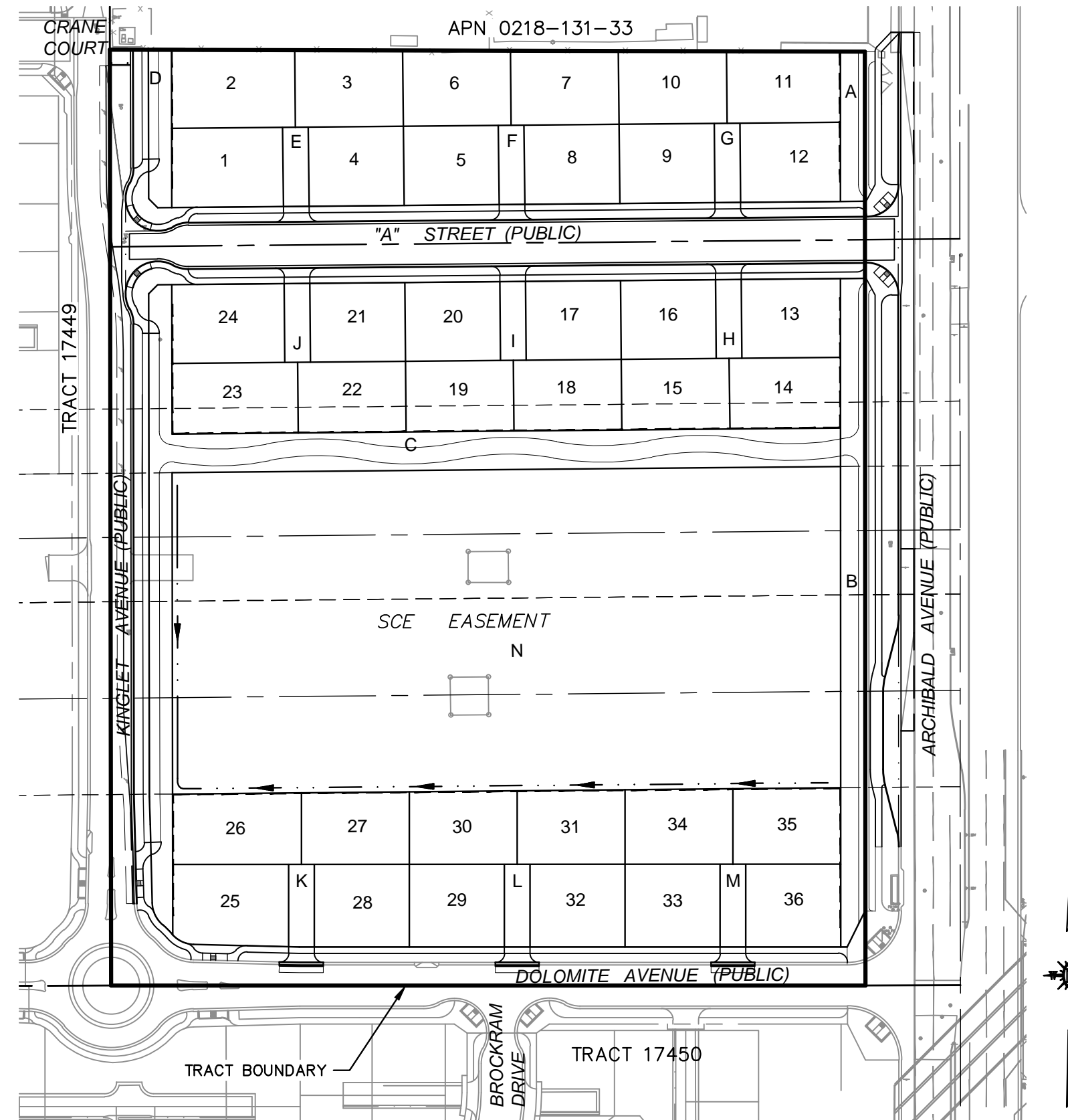
**UNDERGROUND UTILITY NOTES:**

- PRIOR TO CONSTRUCTION, THE CONTRACTOR WILL POTHOLE ALL UTILITY CROSSINGS AND CONNECTIONS AND VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES AFFECTED. THE ENGINEER WILL BE NOTIFIED OF ANY DISCREPANCIES BEFORE CONSTRUCTION BEGINS.
- LOCATIONS OF SUBSTRUCTURES AS SHOWN ON PLANS ARE SHOWN FROM THE READILY AVAILABLE INFORMATION. HOWEVER, SUCH LOCATIONS AS SHOWN OR FAILURE TO SHOW EXISTING SUBSTRUCTURES WILL NOT RELIEVE THE CONTRACTORS FROM THE RESPONSIBILITY OF VERIFYING THE LOCATION AND PROTECTING ALL SUBSTRUCTURES.
- IT MUST BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT AND MAKE NECESSARY ARRANGEMENTS WITH THE APPROPRIATE UTILITY COMPANIES PRIOR TO BEGINNING OF CONSTRUCTIONS.
- THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS ARE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES AND STRUCTURES SHOWN AND ANY OTHER LINES OR STRUCTURES NOT ON RECORD OR NOT SHOWN ON THESE PLANS. THE CONTRACTOR WILL REPORT ANY DISCREPANCIES TO THE CIVIL ENGINEER PRIOR TO THE COMMENCEMENT OF WORK.

**GENERAL GRADING NOTES:**

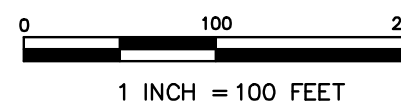
- THE CONTRACTOR MUST NOTIFY THE PROJECT GEOTECHNICAL ENGINEER AND THE CITY INSPECTOR AT LEAST THREE WORKING DAYS PRIOR TO COMMENCEMENT OF ANY CLEARING OR GRADING OPERATIONS ON-SITE.
- A REPRESENTATIVE OF THE PROJECT GEOTECHNICAL ENGINEER MUST BE ON-SITE DURING GRADING OPERATIONS AND WILL PERFORM SUCH TESTING AS DEEMED NECESSARY. THE REPRESENTATIVE WILL OBSERVE THE GRADING OPERATIONS FOR CONDITIONS WHICH SHOULD BE CORRECTED, AND IDENTIFY SUCH WITH RECOMMENDED CORRECTIVE MEASURES TO THE CONTRACTOR.
- THE CONTRACTOR MUST VERIFY THE EXISTING ELEVATIONS OF MATCH POINTS BEFORE GRADING TO ENSURE PROPER DRAINAGE AND SLOPES.
- THE CONTRACTOR MUST COORDINATE FINAL PAD SECTIONS BETWEEN THE PROJECT SOILS REPORT, THE PERMITTED STRUCTURAL PLANS, AND THE FINISHED FLOOR ELEVATIONS SHOWN ON THESE PLANS. THE CIVIL ENGINEER WILL ASSUME NO RESPONSIBILITY FOR INCONSISTENCIES BETWEEN THE STRUCTURAL PLANS AND THE SOILS REPORT.
- IN THE EVENT THAT ANY UNUSUAL CONDITIONS NOT COVERED BY THESE NOTES ARE ENCOUNTERED DURING GRADING OPERATIONS, THE PROJECT GEOTECHNICAL ENGINEER MUST BE IMMEDIATELY NOTIFIED FOR DIRECTIONS. THE CONTRACTOR MUST PROVIDE COPIES OF ALL CORRESPONDENCE WITH THE GEOTECHNICAL ENGINEER TO THE CIVIL ENGINEER.

# ROUGH GRADING PLAN FOR TRACT 18916 IN THE CITY OF ONTARIO



INDEX MAP

1" = 100'



**SHEET INDEX**

Sheet Number	Sheet Title
1	TITLE SHEET
2	DETAILS AND SECTIONS
3	GRADING PLAN
4	EROSION CONTROL PLAN
5	EROSION CONTROL NOTES

**LEGEND:**

	EXISTING	PROPOSED
TRACT BOUNDARY	---	---
PROPERTY LINE	---	---
RIGHT-OF-WAY	---	---
CENTERLINE	---	---
EASEMENT	---	---
CURB & GUTTER	---	---
FENCE	X	X
SWALE FLOWLINE	---	---
GUTTER FLOWLINE	---	---
RETAINING WALL	---	---
(PER SEPARATE PERMIT)	---	---

**ESTIMATED EARTHWORK QUANTITIES:**

ITEM	CUT (CY)	FILL (CY)
RAW QUANTITIES	3,700	9,600
NET IMPORT		5,900 CY

NOTE: EARTHWORK QUANTITIES ARE ESTIMATES FOR PERMIT PURPOSES ONLY. CONTRACTOR MUST DETERMINE HIS OR HER OWN QUANTITIES. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING THE AMOUNT AND COST OF IMPORT, EXPORT, AND OTHER GRADING QUANTITIES FOR THIS PROJECT.

**SITE AREAS:**

TOTAL TRACT AREA = 9.81 ACRES  
TOTAL DISTURBANCE AREA = 6.87 ACRES

**CONTRACTOR'S RESPONSIBILITY**

CONTRACTOR AGREES THAT HE/SHE WILL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, AND THAT THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORK HOURS, AND THAT THE CONTRACTOR WILL DEFEND, INDEMNIFY, AND HOLD THE OWNER, ENGINEER, AND CITY HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER, ENGINEER, AND/OR CITY. IN ADDITION, CONTRACTOR MUST REVIEW AND IMPLEMENT THE SWPPP.

**UTILITIES:**

DOMESTIC WATER, SEWER, STORM DRAIN, FIBER OPTIC:  
CITY OF ONTARIO (909) 395-2150

**RECYCLED WATER:**

INLAND EMPIRE UTILITIES AGENCY (909) 993-1522

**TELEPHONE:**

VERIZON (951) 469-6369

**NATURAL GAS:**

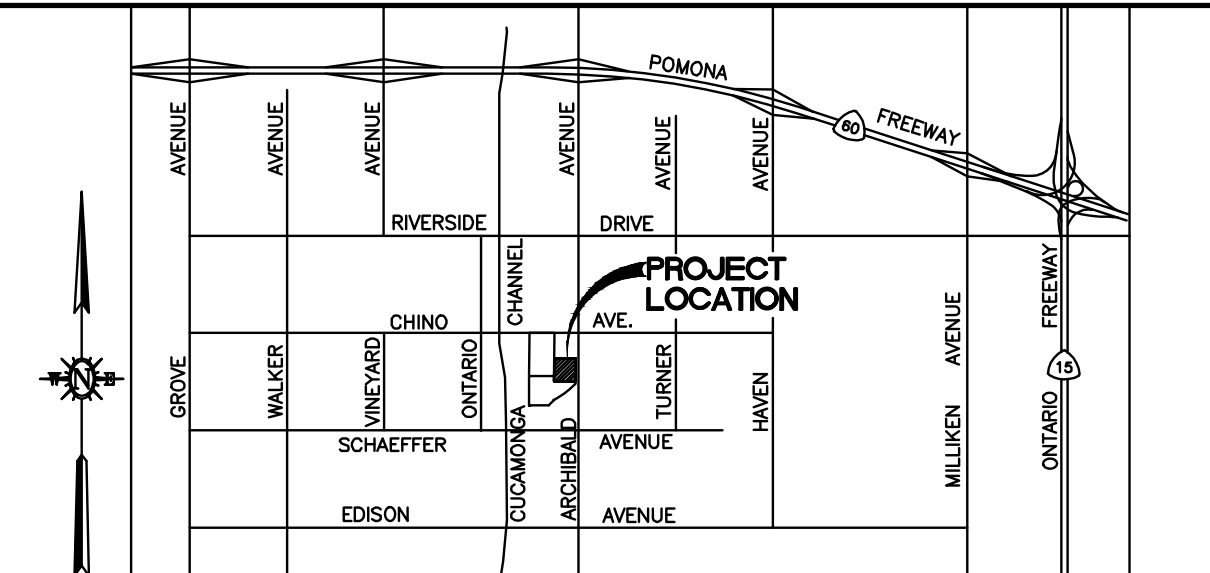
SOUTHERN CALIFORNIA GAS (909) 335-7550

**ELECTRICITY:**

SOUTHERN CALIFORNIA EDISON (951) 930-8591

**BASIS OF BEARING:**

THE BEARING NORTH 00°20'04" EAST FOR THE CENTERLINE OF ARCHIBALD AVENUE AS SHOWN ON TRACT NO. 15901-1 FILED IN BOOK 278, PAGES 28 AND 29 OF MAPS IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, WAS USED AS THE BASIS OF BEARINGS.



EAST 1/2, NE 1/4, SE 1/4, SECTION 10, TOWNSHIP 2 SOUTH, RANGE 7 WEST, S.B.M., EXCEPT THE N 604 FT. THEREOF AND EXCEPT THE STREET, IN THE COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA.

VICINITY MAP  
NOT TO SCALE

**OWNER/  
DEVELOPER:**

KB HOME, INLAND EMPIRE  
36310 INLAND VALLEY DRIVE  
WILDOMAR, CA 92595  
ATTN: HEIDI MCBROOM  
(951) 518-5965  
HMCBROOM@KBHOME.COM

**ENGINEER:**

CANNON  
16842 VON KARMAN AVENUE,  
SUITE 150  
IRVINE, CA 92606  
PHONE: (949) 753-8111

**SOILS  
ENGINEER:**

GEOTEK, INC.  
1548 NORTH MAPLE STREET  
CORONA, CA 92880  
(951) 710-1160

**LEGAL DESCRIPTION:**

BEING A SUBDIVISION OF THE EAST 1/2 OF THE NORTHEAST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 10, TOWNSHIP 2 SOUTH, RANGE 7 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF ONTARIO, COUNTY OF SAN BERNARDINO, STATE OF CALIFORNIA, ACCORDING TO THE OFFICIAL PLAT OF SAID LAND IN THE OFFICE OF THE BUREAU OF LAND MANAGEMENT, FILED SEPTEMBER 16, 1873.

**A.P.N.:**

0218-131-34

**FLOOD ZONE:**

ZONE X  
MAPS 06071C8638H & 06071C8639J

**ABBREVIATIONS:**

ASPHALT CONCRETE	AC
AGGREGATE BASE	AB
FINISHED SURFACE AT BOTTOM OF STEP	BS
BOTTOM OF WALL	BW
CENTERLINE	CL / C
CONCRETE MASONRY UNIT	CMU
DETECTABLE WARNING SURFACE	DWS
DRIVEWAY	DWY
EXISTING GRADE	EG
EXISTING	(E)
FINISHED FLOOR	FF
FINISHED GRADE	FG
FINISHED SURFACE	FS
FLOW LINE	FL / F
GARAGE FINISHED FLOOR	GFF / GF
GRADE BREAK	GB
INVERT	INV
PROPERTY LINE	PL / R
RETAINING WALL	RTW
RIGHT OF WAY	ROW / R/W
SANITARY SEWER	SS
STORM DRAIN	SD
TRACT BOUNDARY	TR
TOP OF CURB	TC
TOP OF GRATE	TG
TOP OF WALL	TW
UNLESS NOTED OTHERWISE	U.N.O.
WATERLINE	WL

REVISIONS			
MARK	DATE	BY	APPROVED / RCE NO.

DESIGNED BY: _____
DATE: _____
DRAWN BY: _____
DATE: _____
CHECKED BY: _____
DATE: _____

CITY OF ONTARIO

BENCH MARK NO. S.P.S.N. 00789 ELEV. 785.572

LOCATION: NGVD29/1990 ADJ.  
SAN BERNARDINO COUNTY BENCHMARK LOCATED AT THE INTERSECTION OF TURNER AVENUE AND RIVERSIDE DRIVE, 1/4 COR. SEC. 2/11, T2S, R7W, 2" BRASS DISK STAMPED "T2S, R7W, 1/4, S2, S11" "LS 3258" SET FLUSH IN A.C.

*West Clift*  
SIGNATURE

**Cannon**  
16842 VON KARMAN AVE, STE 150  
IRVINE, CALIFORNIA 92606  
949.777.1591

89279  
C.E. NO.

6/15/2022  
DATE

ROUGH GRADING PLAN  
TITLE SHEET



WIDW NO. XXXXXXXXX

SHEET 1 OF 5

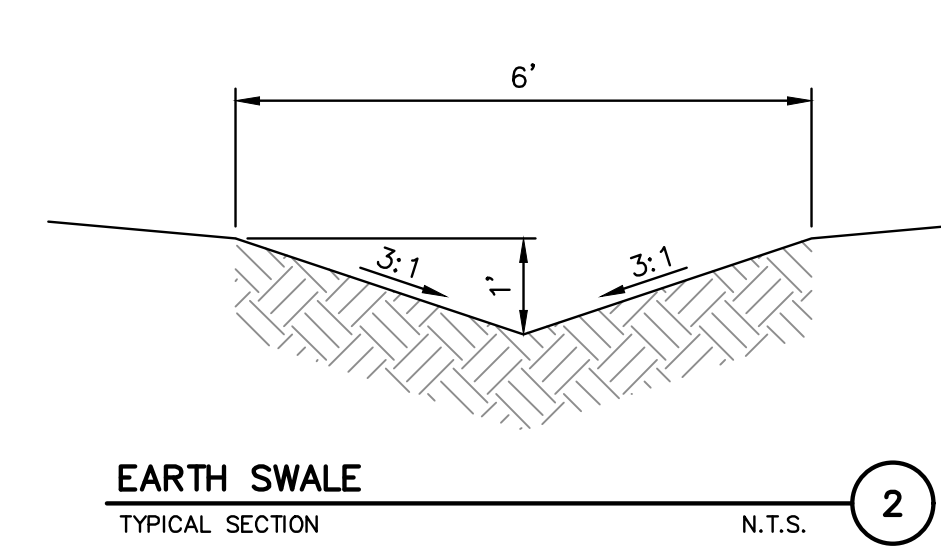
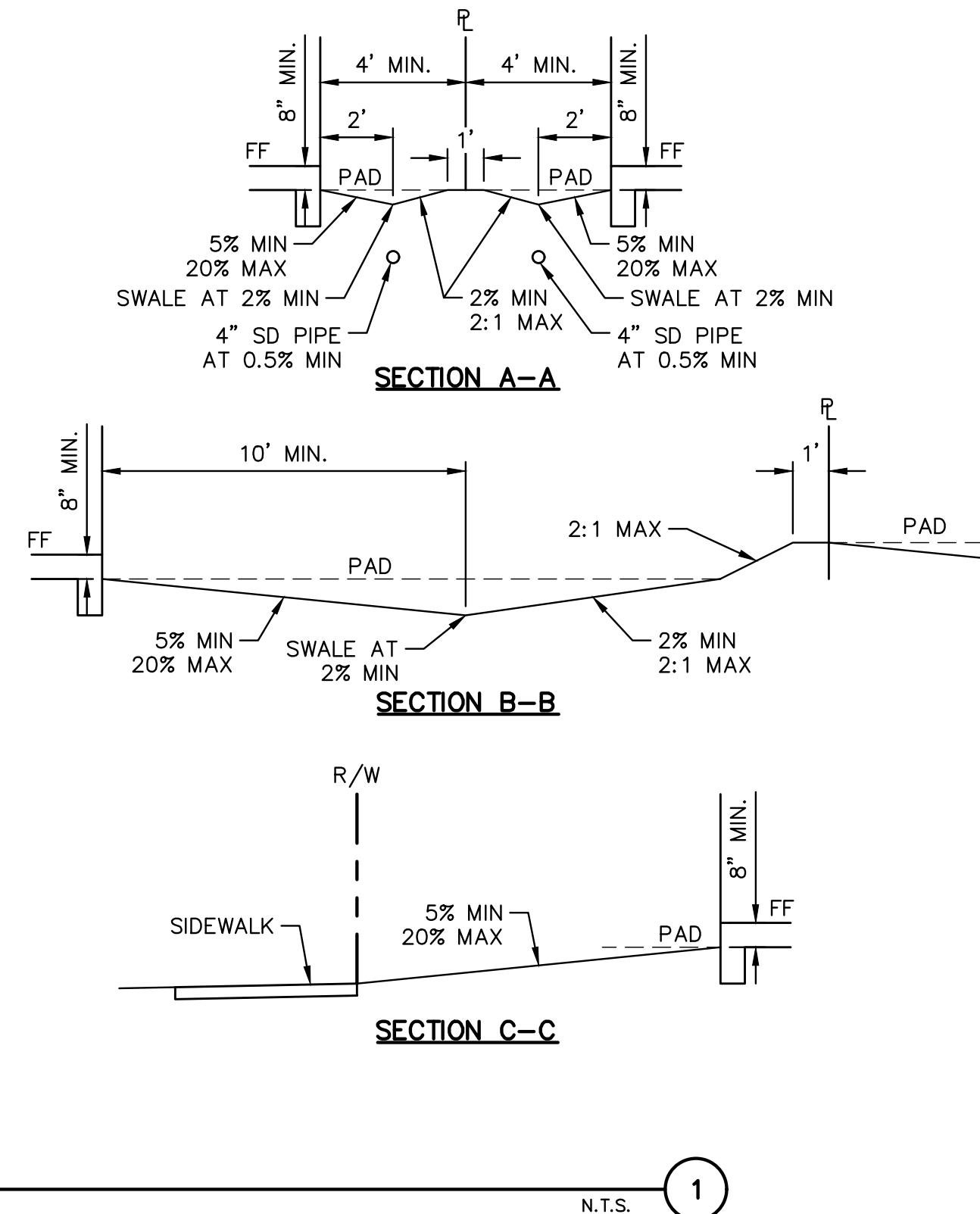
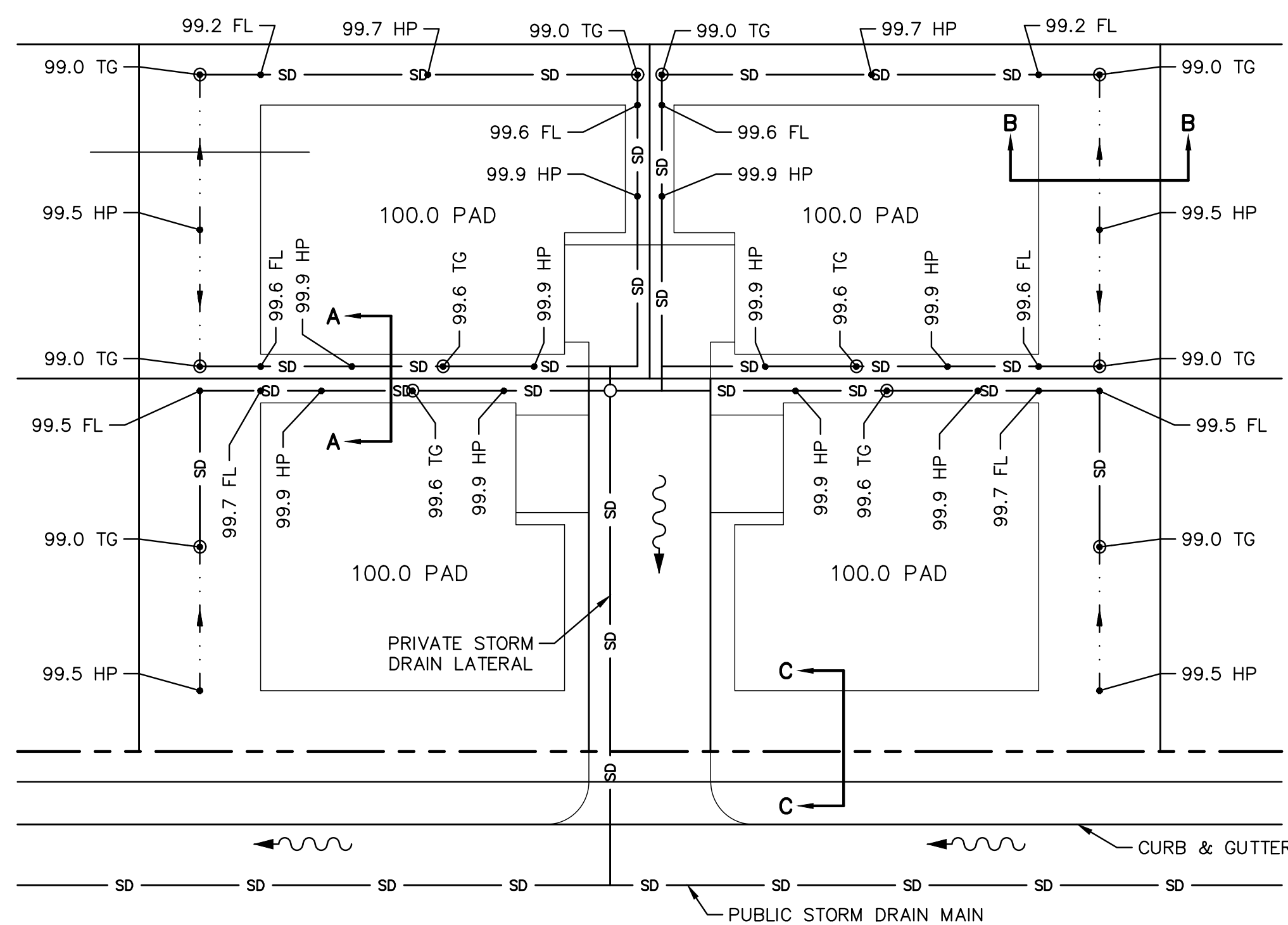
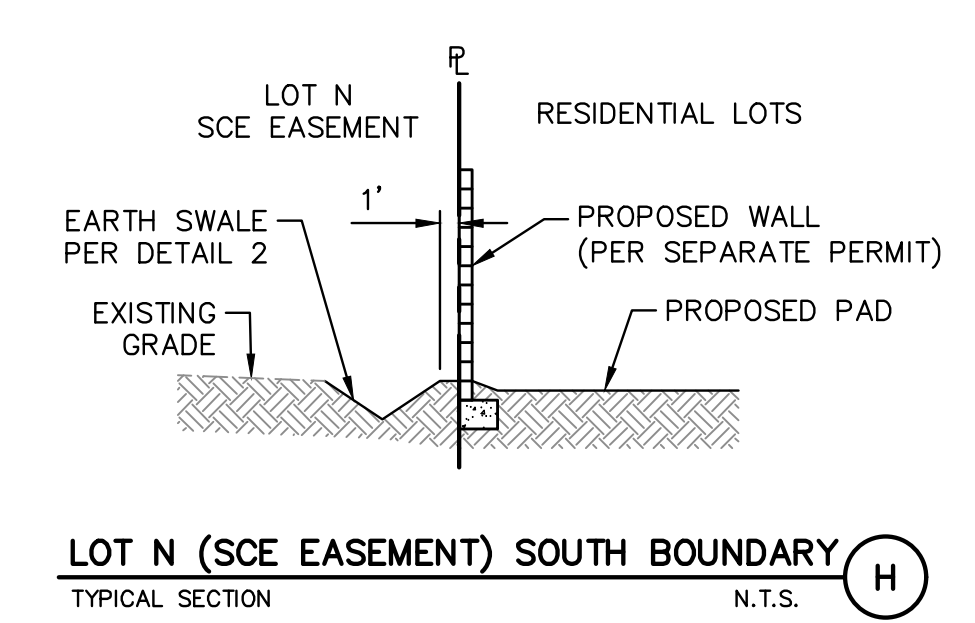
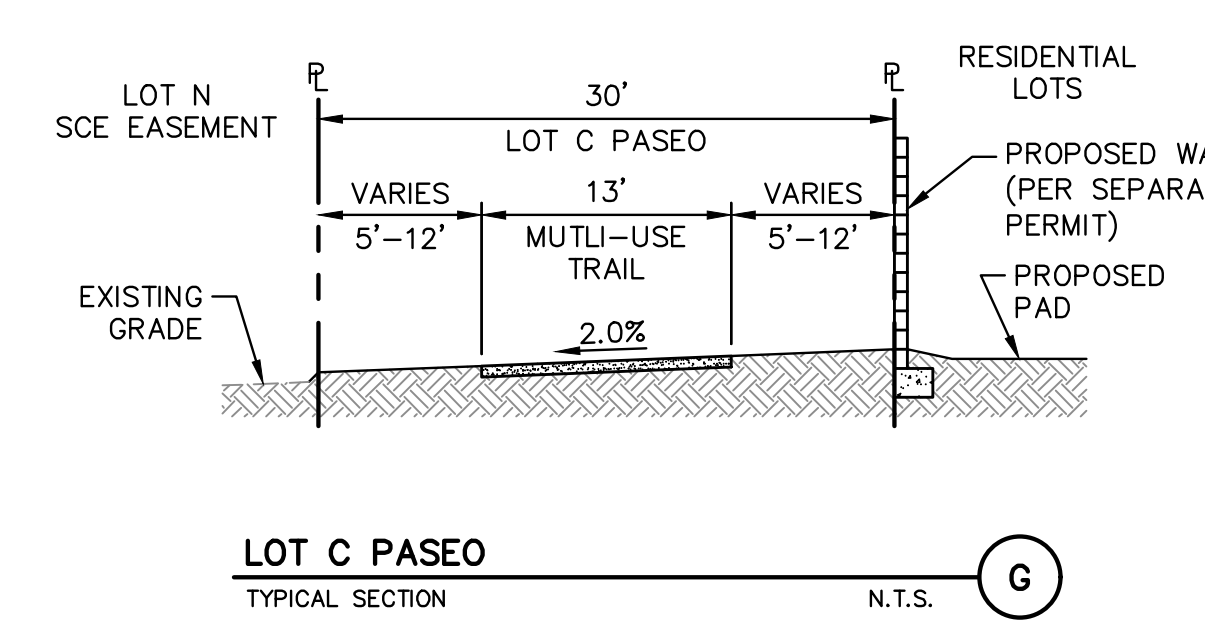
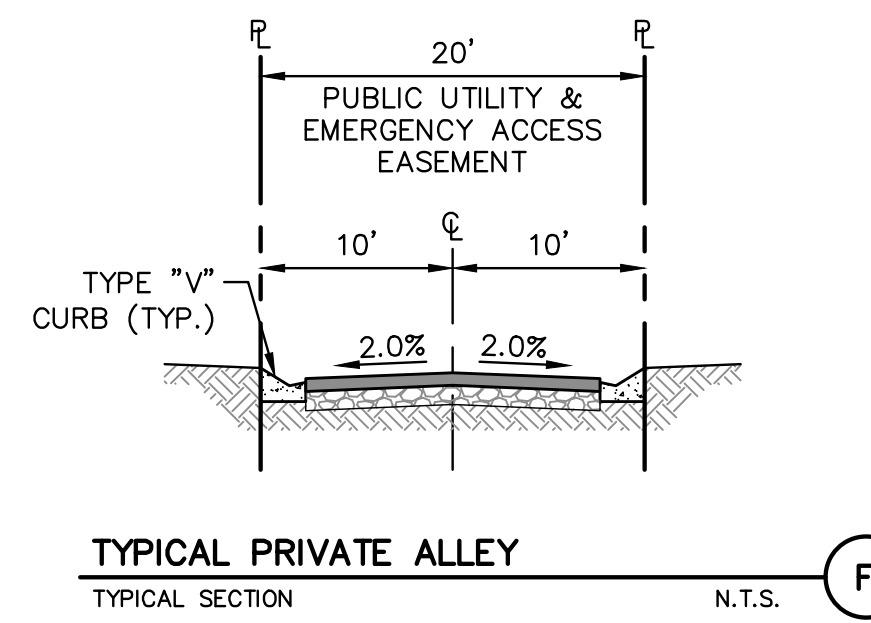
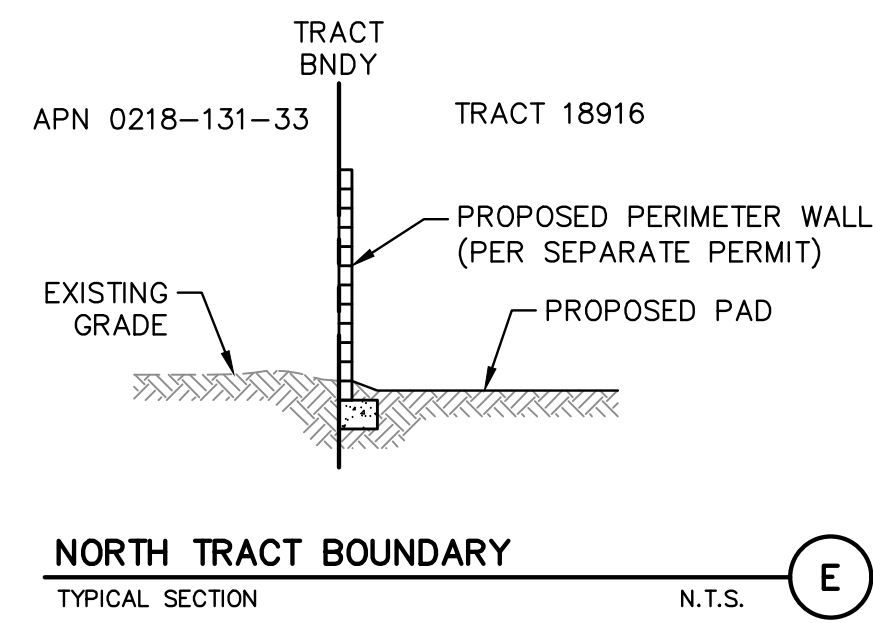
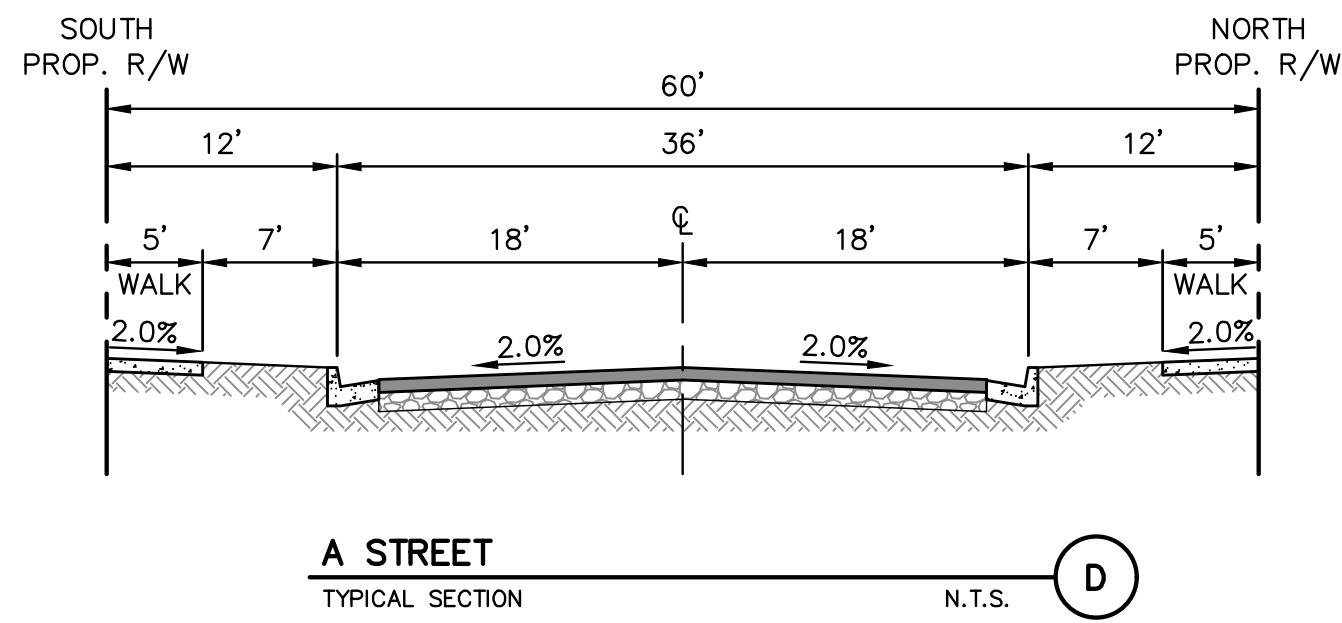
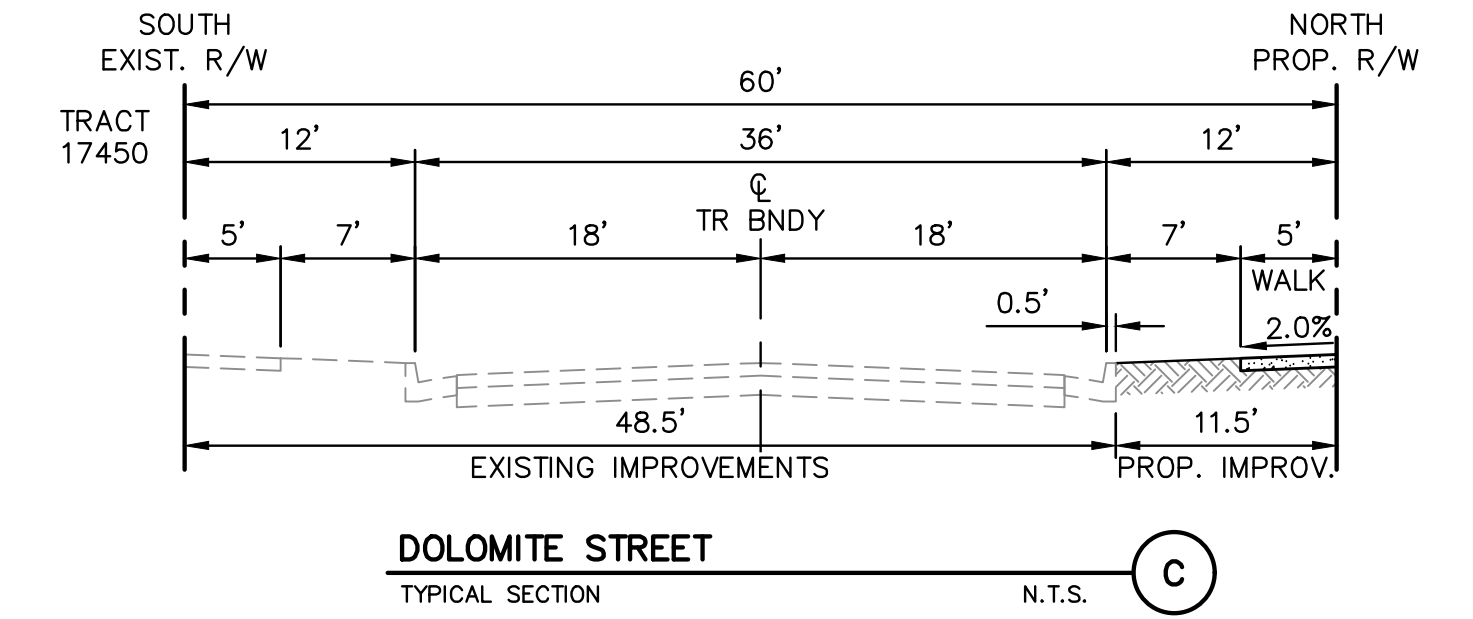
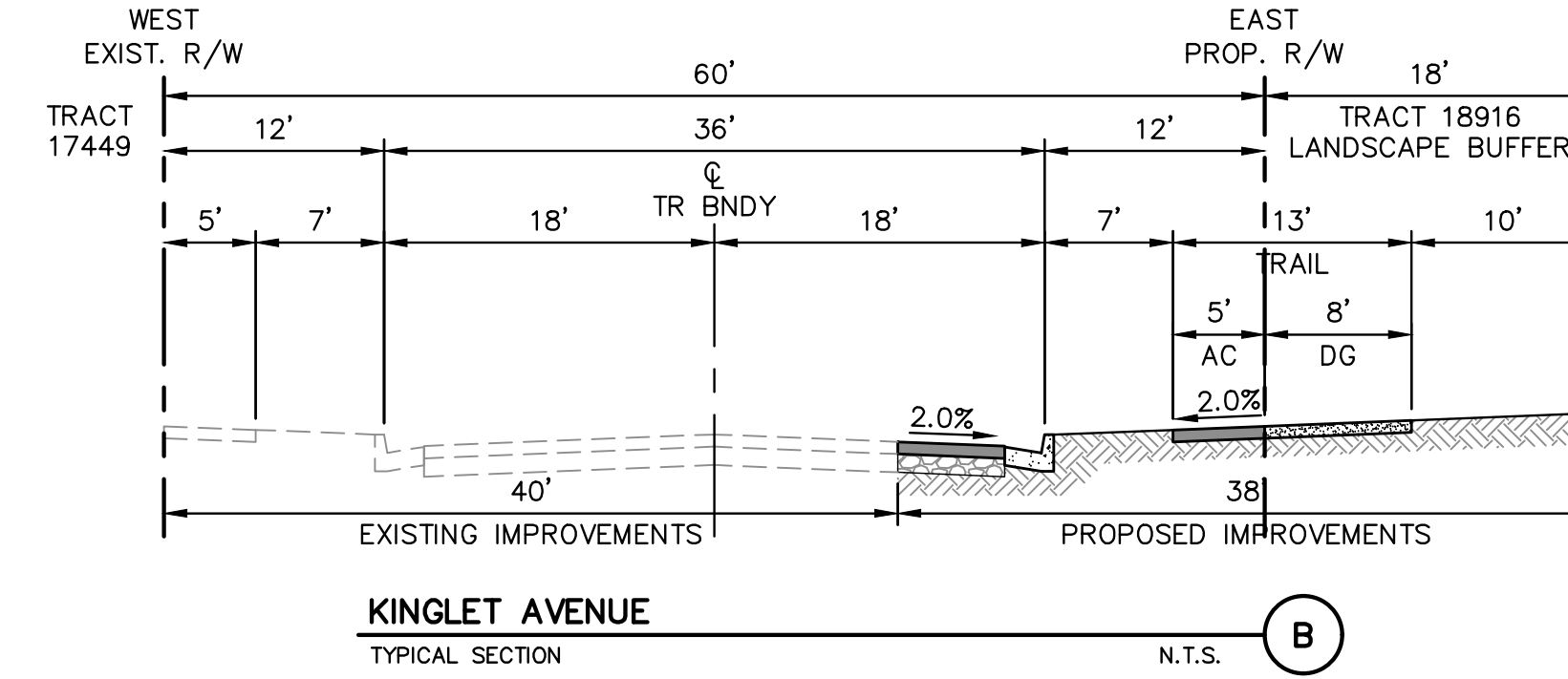
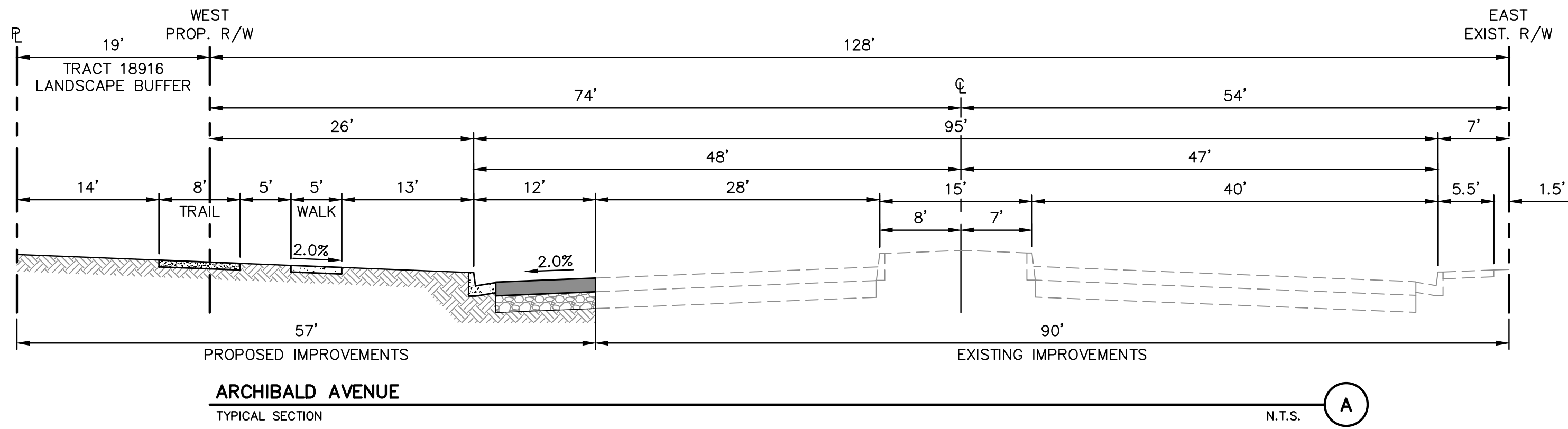
CONTRACT \_\_\_\_\_

ACCOUNT \_\_\_\_\_

DWG. NO. \_\_\_\_\_

TRACT 18916





REVISIONS			
MARK	DATE	BY	APPROVED / RCE NO.

DESIGNED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

**CITY OF ONTARIO**

BENCH MARK NO. S.P.S.N. Q0789 ELEV. 785.572  
LOCATION: NGVD29/1990 ADJ.  
SAN BERNARDINO COUNTY BENCHMARK LOCATED AT THE  
INTERSECTION OF TURNER AVENUE AND RIVERSIDE DRIVE,  
1/4 COR. SEC. 2/11, T2S, R7W, 2" BRASS DISK STAMPED  
"T2S, R7W, 1/4, S2, S11" "LS 3258" SET FLUSH IN A.C.

*West Clunif*  
SIGNATURE

**Cannon**  
16842 VON KARMAN AVE, STE 150  
IRVINE, CALIFORNIA 92606  
949.777.1591  
89279 C.E. NO. 6/15/2022 DATE

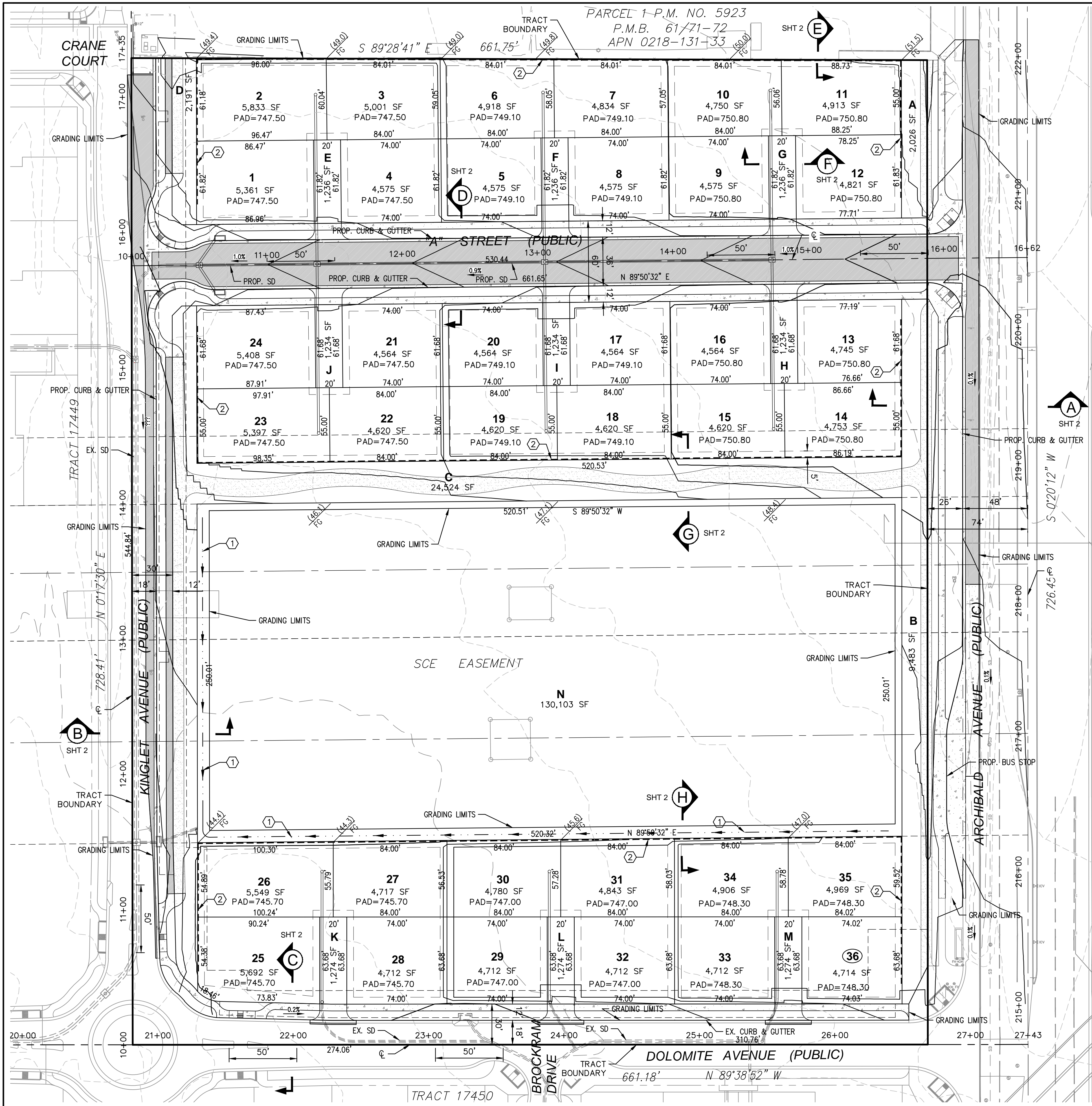
**ROUGH GRADING PLAN**  
DETAILS AND SECTIONS

**B202202263**  
**TRACT 18916**

WDD NO. XXXXXXXXX  
SHEET 2 OF 5  
CONTRACT \_\_\_\_\_  
ACCOUNT \_\_\_\_\_  
DWG. NO. \_\_\_\_\_

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PARCEL 1-P.M. NO. 5923  
P.M.B. 61/71-72  
APN 0218-131-33

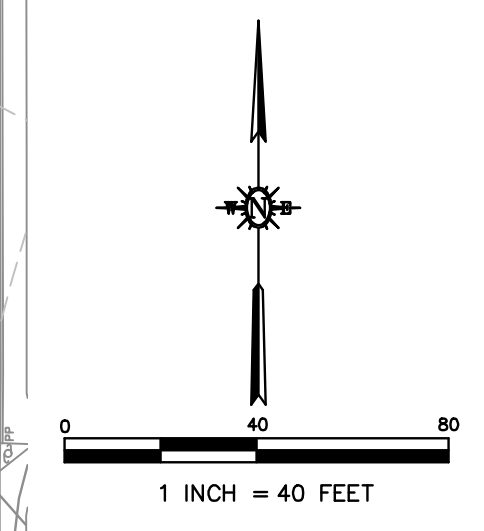


**CONSTRUCTION NOTES**

- ① CONSTRUCT EARTH SWALE PER DETAIL 2, SHEET 2.
- ② BLOCK WALL PER SEPARATE PLANS

**GENERAL NOTES**

- 1. ALL WALLS ARE PER SEPARATE PERMIT



REVISIONS			
MARK	DATE	BY	APPROVED / RCE NO.

DESIGNED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

**CITY OF ONTARIO**

BENCH MARK NO. S.P.S.N. Q0789 ELEV. 785.572

LOCATION: NGVD29/1990 ADJ.  
SAN BERNARDINO COUNTY BENCHMARK LOCATED AT THE  
INTERSECTION OF TURNER AVENUE AND RIVERSIDE DRIVE,  
1/4 COR. SEC. 2/11, T2S, R7W, 2" BRASS DISK STAMPED  
"T2S, R7W, 1/4, S2, S11" "LS 3258" SET FLUSH IN A.C.

*West Clinte*  
SIGNATURE

**Cannon**  
16842 VON KARMAN AVE, STE 150  
IRVINE, CALIFORNIA 92606  
949.777.1591  
89279 C.E. NO. 6/15/2022 DATE

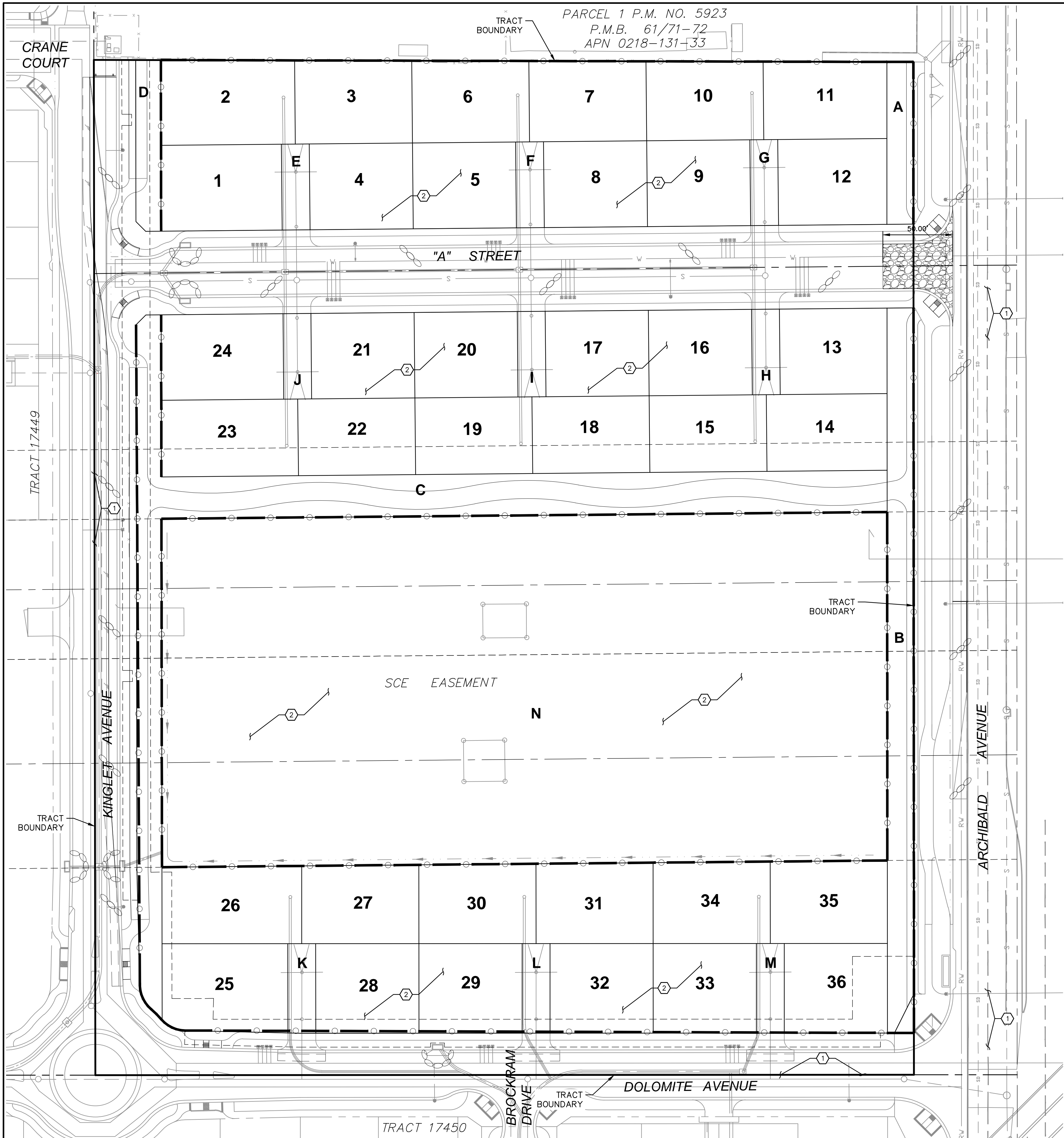
**ROUGH GRADING PLAN  
GRADING PLAN**

**B202202263**  
**TRACT 18916**

WDD NO. XXXXXXXXX  
SHEET 3 OF 5  
CONTRACT \_\_\_\_\_  
ACCOUNT \_\_\_\_\_  
DWG. NO. \_\_\_\_\_

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**LEGEND**

SYMBOL	CASQA BMP #	DESCRIPTION
	SE-1	SILT FENCE
	SE-10	STORM DRAIN INLET PROTECTION
	TC-1	STABILIZED CONSTRUCTION ENTRANCE (RELOCATE AS NECESSARY)
	SE-7	STREET SWEEPING AND VACUUMING
	WE-1	WIND EROSION CONTROL
	SE-6	GRAVEL BAG VELOCITY REDUCER

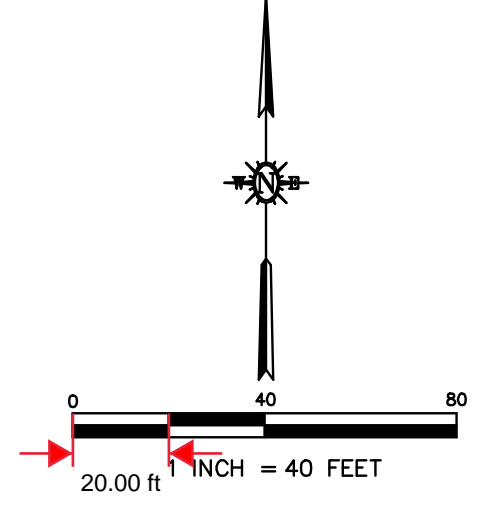
**ADDITIONAL APPLICABLE BMPS**

EC-5	SOIL BINDERS
SE-7	STREET SWEEPING AND VACUUMING
WM-1	MATERIALS STORAGE AREA
WM-5	SOLID WASTE MANAGEMENT
WM-8	CONCRETE WASHOUT AREA
WM-9	SANITARY/SEPTIC WASTE MANAGEMENT

**NOTE:** CONTRACTOR TO LOCATE BMPS WITH APPROVAL FROM COUNTY INSPECTOR.

THIS PLAN OFFERS METHODS FOR CONTROL OF SOIL EROSION DURING GRADING OPERATIONS. NO WARRANTY IS STATED OR IMPLIED THAT IMPLEMENTATION OF THESE METHODS WILL PREVENT DAMAGE AS A RESULT OF SOIL EROSION. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ANY AND ALL DAMAGE TO OWNER'S PROPERTY, ADJACENT PROPERTY, AND CONTRACTOR'S ON GOING WORK.

**NOTE:** GRAVEL BAGS SHALL BE STACKED ON SITE READY TO PLACE IN POSITION WHEN RAIN IS FORECAST, OR WHEN THE INSPECTOR DIRECT IT.



REVISIONS			
MARK	DATE	BY	APPROVED / RCE NO.

DESIGNED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_  
DRAWN BY: \_\_\_\_\_  
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CHECKED BY: \_\_\_\_\_  
DATE: \_\_\_\_\_

**CITY OF ONTARIO**

BENCH MARK NO. S.P.S.N. Q0789 ELEV. 785.572

LOCATION: NGVD29/1990 ADJ. SAN BERNARDINO COUNTY BENCHMARK LOCATED AT THE INTERSECTION OF TURNER AVENUE AND RIVERSIDE DRIVE, 1/4 COR. SEC. 2/11, T2S, R7W, 2" BRASS DISK STAMPED "T2S, R7W, 1/4, S2, S11" "LS 3258" SET FLUSH IN A.C.

*West Clunif*  
SIGNATURE

**Cannon**  
16842 VON KARMAN AVE, STE 150  
IRVINE, CALIFORNIA 92606  
949.777.1591

89279 6/15/2022  
C.E. NO. DATE

**ROUGH GRADING PLAN**  
**EROSION CONTROL PLAN**

**B20220263**

**TRACT 18916**

WDID NO. XXXXXXXXX

SHEET 4 OF 5

CONTRACT \_\_\_\_\_

ACCOUNT \_\_\_\_\_

DWG. NO. \_\_\_\_\_

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**CITY OF ONTARIO EROSION & SEDIMENT CONTROL AND CONTRACTOR ACTIVITY NOTES**

- THE CONTRACTOR SHALL MAINTAIN THE CONSTRUCTION SITE BY IMPLEMENTATION OF BEST MANAGEMENT PRACTICES (BMPs), TO THE MAXIMUM EXTENT PRACTICABLE, SO THAT POLLUTANTS (INCLUDING SOIL PARTICLES, BUILDING MATERIALS, WASTE AND SPILLED MATERIALS) ARE PREVENTED FROM DISCHARGE INTO THE MUNICIPAL STORMWATER SYSTEM.
- THE CONTRACTOR SHALL PREPARE THE CONSTRUCTION SITE PRIOR TO THE ONSET OF ANY STORM AND SHALL HAVE ALL EROSION AND SEDIMENT CONTROL MEASURES IN PLACE FOR THE RAINY SEASON, PRIOR TO OCTOBER 1.
- THE CONTRACTOR SHALL CONSIDER THE FULL RANGE OF EROSION CONTROL BEST MANAGEMENT PRACTICES FOR ALL DISTURBED SURFACES SUCH AS: BUFFER STRIPS, HYDROSEEDING, MULCHING, TRACK WALKING OR IMPRINTING, GEOTEXTILES AND MATS, SOIL STABILIZERS AND BINDERS, EARTHEN DIKES, TEMPORARY DRAINS/ROAD SWALES FOR DIVERSION OF OFF-SITE RUNOFF, TEMPORARY SLOPE DRAINS/ STORM OUTLET PROTECTION, CHECK DAMS AND SLOPE ROUGHENING AND TERRACING.
- ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE SPECIFICATIONS IN SECTION 3 OF THE CALIFORNIA STORMWATER QUALITY ASSOCIATION BMP HANDBOOK (JANUARY 2003) FOR CONSTRUCTION ACTIVITY OR EQUIVALENT.
- DUST CONTROL BMPs SHALL BE USED TO STABILIZE SOIL FROM WIND EROSION, AND REDUCE DUST GENERATED BY CONSTRUCTION ACTIVITIES AND MAY INCLUDE STABILIZATION OF UNPAVED CONSTRUCTION ROADS AND PARKING AND STAGING AREAS; WATER SPRAYING; CHEMICAL STABILIZATION; MULCHING; COVERING STOCKPILES WITH TARPS; RAPID CLEANUP OF SEDIMENTS DEPOSITED ON PAVED ROADS AND STABILIZATION OF CONSTRUCTION ENTRY/EXIT POINTS WITH GRAVEL.
- THE CONTRACTOR SHALL INSTALL A 6 FOOT HIGH SCREENING FABRIC FENCE AROUND THE PERIMETER OF THE CONSTRUCTION SITE PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL CONSTRUCT SEDIMENT CONTROLS SUCH AS FIBER ROLLS, SILT FENCES, STRAW BALE BARRIERS, GRAVEL-FILLED BAG BARRIERS, BRUSH OR ROCK FILTERS, SEDIMENT BASINS AND STORM DRAIN INLET PROTECTION.
- ALL SEDIMENT CONTROL STRUCTURES SHALL BE CONSTRUCTED PURSUANT TO THE SPECIFICATIONS IN SECTION 3 OF THE CALIFORNIA STORMWATER QUALITY ASSOCIATION BMP HANDBOOK FOR CONSTRUCTION ACTIVITY OR EQUIVALENT, UNLESS OTHERWISE REQUIRED IN THE CITY'S STANDARD CONSTRUCTION NOTES.
- THE CONTRACTOR SHALL PROTECT ALL STORM DRAIN INLETS WHICH MAY RECEIVE STORM WATER RUNOFF FROM THE CONSTRUCTION ACTIVITIES WITH A COMBINATION OF FIBER ROLLS AND GRAVEL BAGS. FIBER ROLLS MUST BE PLACED ACROSS THE OPENING OF ALL OFF-SITE AND ON-SITE CURB-INLET CATCH BASINS WITH GRAVEL BAGS PLACED OVER THE ENDS OF THE FIBER ROLL AND OVER THE TOP OF THE ROLL, AT 3 FT INTERVALS. ALL DROP INLETS MUST BE SURROUNDED BY FIBER ROLL BARRIERS, WITH GRAVEL BAGS PLACED OVER THE ENDS OF THE ROLLS AND OVER THE TOP OF ROLL, AT 3 FOOT INTERVALS. AFTER EACH STORM EVENT, ALL FIBER ROLL/GRAVEL BAG BARRIERS MUST BE REMOVED AND CLEANED OF ALL SEDIMENT BUILDUP.
- DURING THE RAINY SEASON, ALL GRADED SITES SHALL INSTALL AND MAINTAIN TEMPORARY SEDIMENT BASINS PURSUANT TO THE SPECIFICATIONS OF BMP FACTSHEET #SE-2 OF THE CASQA STORMWATER HANDBOOK FOR CONSTRUCTION ACTIVITIES.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CHECKED BEFORE AND AFTER ALL STORMS TO ENSURE MEASURES ARE FUNCTIONING PROPERLY. QUALIFIED PERSONNEL SHALL CONDUCT INSPECTIONS OF THE CONSTRUCTION SITE PRIOR TO ANTICIPATED STORM EVENTS, DURING EXTENDED STORM EVENTS AND AFTER ACTUAL STORM EVENTS TO IDENTIFY AREAS CONTRIBUTING TO A DISCHARGE OF STORM WATER ASSOCIATED WITH CONSTRUCTION ACTIVITY. PRE- STORM INSPECTIONS ARE TO ENSURE THAT THE BMPs LISTED ABOVE ARE PROPERLY INSTALLED AND MAINTAINED. POST-STORM INSPECTIONS ARE TO ASSURE THAT THE BMPs HAVE FUNCTIONED ADEQUATELY. DURING EXTENDED STORM EVENTS, INSPECTIONS SHALL BE MADE DURING EACH 24-HOUR PERIOD. EQUIPMENT, MATERIALS, AND MANPOWER MUST BE MADE AVAILABLE FOR RAPID RESPONSE TO FAILURES AND EMERGENCIES. ALL CORRECTIVE MAINTENANCE TO BMPs SHALL BE PERFORMED AS SOON AS POSSIBLE, DEPENDING UPON WORKER SAFETY.
- THE CONTRACTOR SHALL MAINTAIN A LOG AT THE CONSTRUCTION SITE OF ALL INSPECTIONS AND BMP MAINTENANCE PERFORMED. THE LOG SHALL ALSO INCLUDE ANY CORRECTIVE CHANGES TO THE BMPs OR THE EROSION AND SEDIMENT CONTROL PLAN, THE INSPECTION DATE, AND THE NAME OF THE PERSON INSPECTING THE SITE OR PERFORMING THE MAINTENANCE.
- THE CONTRACTOR SHALL INSTALL STABILIZED CONSTRUCTION ENTRANCE(S)/EXIT(S) MEASURING A MINIMUM OF 50 FEET LONG AND 30 FEET WIDE AND CONSISTING OF A 6 INCH LAYER OF 1-3 INCH STONES PRIOR TO COMMENCEMENT OF GRADING. THE LOCATION OF THE ENTRANCES MAY BE ADJUSTED BY THE CONTRACTOR TO FACILITATE GRADING OPERATIONS. ALL CONSTRUCTION TRAFFIC ENTERING OR LEAVING A CONSTRUCTION SITE, TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY, SIDEWALK, OR PARKING AREA, MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCE(S). THE STABILIZED CONSTRUCTION ENTRANCE(S) SHALL REMAIN IN PLACE UNTIL THE ROAD BASE ROCK COURSE IS COMPLETED. WHEEL WASH OVER A WASH TRACK IS RECOMMENDED FOR CLAY SOILS.
- ALL SEDIMENT DEPOSITED ON PAVED ROADWAYS SHALL BE SWEEPED AT THE END OF EACH WORKING DAY. WASHING OF ACCUMULATED SEDIMENT INTO THE STORM DRAIN IS PROHIBITED.
- IN ALL AREAS WHERE BARE SOIL IS EXPOSED TO WATER OR WIND EROSION, ACCEPTABLE SOIL STABILIZATION MATERIALS ARE REQUIRED TO BE APPLIED.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED UNTIL ALL DISTURBED AREAS ARE STABILIZED. THE ADEQUACY OF POST CONSTRUCTION SOIL STABILIZATION SHALL BE SUBJECT TO THE REVIEW AND APPROVAL OF THE CITY ENGINEERING DEPARTMENT. ADDITIONAL EROSION AND SEDIMENT CONTROL BMPs THAT ARE OVER AND ABOVE THE BMPs SHOWN ON THE APPROVED EROSION AND SEDIMENT CONTROL PLAN SHEET AND THE SWPPP, MAY BE REQUIRED TO BE IMPLEMENTED IN ORDER TO MEET FIELD CONDITIONS. IT IS THE RESPONSIBILITY OF THE CONSTRUCTION SUPERINTENDENT TO DETERMINE WHAT ADDITIONAL MEASURES ARE NECESSARY AND TO INSTALL THOSE ADDITIONAL BMPs AS NECESSARY.
- THE CONTRACTOR SHALL REPORT THE FOLLOWING INCIDENTS:
  - IF THERE IS AN ACCIDENTAL DISCHARGE OF NON-STORM WATER OR STORM WATER CONTAINING POLLUTANTS THAT HAS THE POTENTIAL TO ENTER OR ENTERS A CITY STORM DRAIN OR A FLOOD CONTROL CHANNEL, THE CONTRACTOR SHALL NOTIFY THE CITY'S STORM WATER COORDINATOR AS SOON AS POSSIBLE AT (909) 395-2389 OR AT (909) 395- 2143. IF THE DISCHARGE OCCURS AFTER BUSINESS HOURS, THE FIRE DEPARTMENT SHALL BE NOTIFIED AT 911.

- IF THERE IS A RELEASE OR THREATENED RELEASE OF A HAZARDOUS MATERIAL, THE CONTRACTOR SHALL ALSO NOTIFY THE FIRE DEPARTMENT AT 911, THE SAN BERNARDINO COUNTY FIRE DEPARTMENT HAZARDOUS MATERIALS DIVISION AT (800) 33-TOXIC OR (909) 387-3044, THE STATE OF CALIFORNIA OFFICE OF EMERGENCY SERVICES AT (800) 852-7550 AND, FOR A REPORTABLE QUANTITY, THE NATIONAL RESPONSE CENTER AT (800) 424-8802.
  - IF THERE WILL BE A DISCHARGE OF WATER FROM WATER LINE OR TANK FLUSHING OR TESTING INTO THE STORM DRAIN SYSTEM, THE CITY REQUIRES THE CONTRACTOR TO FAX A NON-STORM WATER DISCHARGE NOTIFICATION FORM (AVAILABLE FROM THE CITY OF ONTARIO'S ENGINEERING COUNTER) TO THE ENGINEERING DEPARTMENT AT (909) 395-2122, FIVE DAYS PRIOR TO A PLANNED DISCHARGE OR AS SOON AS POSSIBLE FOR UNPLANNED DISCHARGES. SAMPLING DURING THE FIRST 30 MINUTES IS REQUIRED. CHLORINATED WATER MUST BE DE-CHLORINATED TO <0.1 PPM AND SUSPENDED SOLIDS MUST BE REDUCED TO <75 PPM PRIOR TO DISCHARGE TO THE STORM DRAIN SYSTEM.
  - THE REGIONAL WATER QUALITY CONTROL BOARD SHALL BE NOTIFIED BY TELEPHONE AS SOON AS POSSIBLE, BUT NO LATER THAN 48 HOURS, AFTER IT IS DETERMINED THAT STORM WATER DISCHARGES AND/OR AUTHORIZED NON- STORM WATER DISCHARGES ARE CAUSING OR CONTRIBUTING TO AN EXCEEDANCE OF AN APPLICABLE WATER QUALITY STANDARD AT (909) 782-4130. THE NOTIFICATION SHALL BE FOLLOWED BY A REPORT WITHIN 14-CALENDER DAYS DESCRIBING: (1) THE NATURE AND CAUSE OF THE WATER QUALITY EXCEEDANCE; (2) THE BMPs CURRENTLY BEING IMPLEMENTED; (3) ANY ADDITIONAL BMPs WHICH WILL BE IMPLEMENTED TO PREVENT OR REDUCE POLLUTANTS THAT ARE CAUSING OR CONTRIBUTING TO THE WATER QUALITY EXCEEDANCE AND A SCHEDULE FOR THEIR IMPLEMENTATION; AND (4) ANY MAINTENANCE OR REPAIR OF BMPs.
18. ALL CONTRACTOR ACTIVITY BMPs SHALL BE IMPLEMENTED ACCORDING TO THE SPECIFICATIONS IN CHAPTER 4 OF THE CALIFORNIA BMP HANDBOOK FOR CONSTRUCTION ACTIVITY OR EQUIVALENT. AT A MINIMUM, THE CONTRACTOR SHALL IMPLEMENT THE FOLLOWING CONTRACTOR ACTIVITY BMPs:
- PROVIDE EMPLOYEE/SUBCONTRACTOR TRAINING IN CONSTRUCTION BMPs: OBTAIN APPROPRIATE CONSTRUCTION BEST MANAGEMENT PRACTICE FACT SHEETS FROM THE CALIFORNIA STORM WATER BMP HANDBOOK FOR CONSTRUCTION OR EQUIVALENT AND PROVIDE COPIES AND TRAINING ON THESE BMPs TO EMPLOYEES AND SUBCONTRACTORS PRIOR TO THE BEGINNING OF WORK.
  - LIQUID AND SOLID WASTE MANAGEMENT: PROVIDE A SUFFICIENT NUMBER OF DESIGNATED WASTE COLLECTION AREAS, PHYSICALLY SEPARATED FROM POTENTIAL STORM WATER RUN-OFF, WITH LIDDED CONTAINERS. ARRANGE FOR THEIR REGULAR REMOVAL AND DISPOSAL IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REQUIREMENTS. CLEAR SITE OF TRASH, INCLUDING ORGANIC DEBRIS, PACKAGING MATERIALS, SCRAP OR SURPLUS BUILDING MATERIALS AND DOMESTIC WASTE DAILY.
  - CHEMICAL AND MATERIAL DELIVERY AND STORAGE: PROVIDE A DESIGNATED CHEMICALS STORAGE AREA, LOCATED NEAR THE CONSTRUCTION ENTRANCES, AWAY FROM DRAINAGE COURSES, WHICH HAS A TEMPORARY SECONDARY SPILL CONTAINMENT SYSTEM. STORE ALL CHEMICALS IN SEALED AND LABELED CONTAINERS. PROVIDE A SPILL CLEANUP KIT AND TARPS FOR COVERING STOCKPILED MATERIALS WHEN RAIN IS PREDICTED. RELOCATE CHEMICAL STORAGE AREA INTO A TRAILER OR BUILDING SHELLED WHEN POSSIBLE. INSPECT AREA WEEKLY FOR LEAKS AND SPILLS.
  - CONCRETE WASTE: CONDUCT CONCRETE WASHOUT OFF-SITE OR PROVIDE A DESIGNATED AREA FOR A TEMPORARY PIT TO BE USED FOR CONCRETE TRUCK WASH OUT THAT IS AT LEAST 50 FEET FROM ANY STORM DRAIN OR DRAINAGE CHANNEL. REQUIRE DISPOSAL OF HARDENED CONCRETE OFFSITE. AT NO TIME SHALL A CONCRETE TRUCK DUMP ITS WASTE AND CLEAN ITS TRUCK INTO THE CITY STORM DRAINS VIA CURB AND GUTTER. INSPECT DAILY TO CONTROL RUNOFF, AND WEEKLY FOR REMOVAL OF HARDENED CONCRETE.
  - PAINTING AND DRYWALL WORK: PROVIDE INSTRUCTION TO EMPLOYEES AND SUBCONTRACTORS BEFORE THEY START WORK, REGARDING REDUCTION OF POLLUTANTS INCLUDING MATERIAL STORAGE, USE, AND CLEAN UP. DRYWALL EQUIPMENT, PAINTBRUSHES, AND ROLLERS CANNOT BE WASHED INTO THE STREET OR STORM DRAIN SYSTEM.
  - VEHICLE FUELING, MAINTENANCE AND CLEANING: IF FUELING MUST OCCUR ON-SITE, PROVIDE A DESIGNATED FUELING AREA, LOCATED AWAY FROM DRAINAGE COURSES, WITH SECONDARY SPILL CONTAINMENT. ENSURE THAT THE FUEL TANK AND NOZZLES ARE KEPT INSIDE OF THE CONTAINMENT AREA. DO NOT ALLOW MOBILE FUELING OF EQUIPMENT. INSPECT FUELING AREA WEEKLY FOR LEAKS OR SPILLS. PROVIDE DRIP PANS FOR LEAKING EQUIPMENT OR EMERGENCY EQUIPMENT MAINTENANCE. RESTRICT ONSITE MAINTENANCE OF EQUIPMENT TO A MINIMUM. DO NOT ALLOW WASHING OF VEHICLES OR EQUIPMENT INTO THE STREET OR STORM DRAIN SYSTEM.
  - ASPHALT, SAWCUTTING, CORING, AND GRINDING ACTIVITIES: INFORM EMPLOYEES AND SUBCONTRACTORS TO PROTECT CATCH BASINS WHEN APPLYING ASPHALT SEAL COAT, SLURRY SEAL OR FOG SEAL, AND TO PREVENT SAWCUTTING SLURRIES, CORING OR AC GRINDING WASTES FROM ENTERING THE STORM DRAIN SYSTEM.
  - BUILDING BLASTING AND CLEANING: PROVIDE INSTRUCTION TO EMPLOYEES AND SUBCONTRACTORS, BEFORE THEY START A JOB, TO ENSURE THAT BLAST RESIDUE FROM HIGH-PRESSURE WASHING OF BUILDINGS IS NOT ALLOWED TO ENTER THE STORM DRAIN SYSTEM. IF PAINT REMOVAL INVOLVES HAZARDOUS SUBSTANCES SUCH AS LEAD AND MERCURY, THE WASTE MUST BE PREVENTED FROM PERCOLATING INTO THE GROUND AND MUST BE HAULED OFF- SITE AS A HAZARDOUS WASTE.
  - CEMENT, GROUT AND MORTAR WORK, AND CLEAN UP: ENSURE THAT EMPLOYEES AND SUBCONTRACTORS PREVENT CEMENT, GROUT AND MORTAR SOLIDS, AND CLEAN UP WATER FROM ENTERING THE STORM DRAIN SYSTEM.
  - SANITARY AND SEPTIC WASTES: SEWAGE IS PROHIBITED IN THE STORM DRAIN SYSTEM. ENSURE THAT THE SEPTIC TANK SERVICE COMPANY IS INFORMED THAT SEWAGE AND WASTEWATER GENERATED FROM THE DISINFECTION AND WASH DOWN OF SEPTIC TANKS CANNOT BE

- DISCHARGED TO THE STORM DRAIN SYSTEM.
- WATER LINE DISINFECTION, FLUSHING, DEWATERING, AND OTHER NON-STORM WATER DISCHARGES: UNLESS EXEMPTED OR AUTHORIZED BY AN NPDES PERMIT, ALL NON-STORM WATER DISCHARGES REQUIRE PRIOR APPROVAL BY THE CITY OR THE REGIONAL WATER QUALITY CONTROL BOARD. THE DEVELOPER OR CONTRACTOR SHALL NOTIFY THE CITY'S STORM WATER COORDINATOR AT (909) 395-2389 OR (909) 395-2143, OR THE REGIONAL WATER QUALITY CONTROL BOARD AT (909)782-4379, A MINIMUM OF FIVE DAYS PRIOR TO ANY DISCHARGE TO THE STORM DRAIN SYSTEM FOR PLANNED DISCHARGES, OR AS SOON AS POSSIBLE, FOR UNPLANNED DISCHARGES. DEWATERING OF CONTAMINATED GROUNDWATER, OR DISCHARGING CONTAMINATED SOILS VIA SURFACE EROSION IS PROHIBITED. IF POSSIBLE, ATTEMPT TO CONTAIN ALL WATER LINE FLUSHING WASTES IN A TEMPORARY PIT, ON SITE.
  - HAZARDOUS WASTE MANAGEMENT: PROVIDE A DESIGNATED HAZARDOUS WASTE STORAGE AREA SUCH AS A TRAILER OR TEMPORARY SECONDARY CONTAINMENT AREA THAT IS PHYSICALLY SEPARATED FROM STORM WATER DRAINAGE CHANNELS. STORE HAZARDOUS WASTE IN SEALED AND LABELLED CONTAINERS. PROVIDE A SPILL CLEAN UP KIT AND TARPS FOR COVERING STOCKPILED MATERIALS WHEN RAIN IS PREDICTED. RELOCATE STORAGE AREA INTO TRAILER OR BUILDING SHELL WHEN POSSIBLE. INSPECT AREA WEEKLY. PREVENT THE DISCHARGE OF POLLUTANTS FROM HAZARDOUS WASTES TO THE DRAINAGE SYSTEM BY TRAINING EMPLOYEES AND SUBCONTRACTORS ON PROPER MATERIALS USE, LABELING, STORAGE, CLEAN UP, AND WASTE DISPOSAL. HAZARDOUS WASTES COMMONLY FOUND ON CONSTRUCTION SITES INCLUDE, BUT ARE NOT LIMITED TO: PVC PIPE GLUES, WOOD STAINS AND PRESERVATIVES; LIME AND CONCRETE CURING COMPOUNDS, BATTERY FLUIDS, WASTE OIL, FUEL, ASBESTOS, OIL BASED AND LEAD BASED PAINTS, SOLVENTS, ASPHALT AND OTHER PETROLEUM BASED PRODUCTS, FERTILIZERS, HERBICIDES AND PESTICIDES, AND SOME SOIL STABILIZATION PRODUCTS.

- PROHIBITED DISCHARGES: THE FOLLOWING DISCHARGES INTO THE STORM DRAIN SYSTEM ARE PROHIBITED: DISCHARGES THAT COULD HAVE AN IMPACT ON HUMAN HEALTH AND THE ENVIRONMENT, CAUSE OR THREATEN TO CAUSE POLLUTION, CONTAMINATION, OR NUISANCE; DISCHARGES THAT EXCEED ANY WATER QUALITY STANDARD CONTAINED IN THE STATEWIDE WATER QUALITY CONTROL PLAN OR LOCAL BASIN PLAN; AND DISCHARGES CONTAINING A HAZARDOUS SUBSTANCE EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY LISTED IN THE FEDERAL REGULATIONS 40 CFR PARTS 117 AND 302. MATERIALS THAT CAN CAUSE OR CONTRIBUTE TO POLLUTION OR A VIOLATION OF ANY APPLICABLE WATER QUALITY STANDARD INCLUDE, BUT ARE NOT LIMITED TO: SEDIMENTS, CONTAMINATED SOIL, SOLID OR LIQUID CHEMICAL SPILLS; WASTES FROM PAINTS, STAINS, SEALANTS, GLUES, LIMES, PESTICIDES OR HERBICIDES, WOOD PRESERVATIVES OR SOLVENTS; ASBESTOS FIBERS, PAINT FLAKES OR STUCCO FRAGMENTS; FUELS, OILS, LUBRICANTS, OR HYDRAULIC, RADIATOR AND BATTERY FLUIDS; FERTILIZERS; VEHICLE/EQUIPMENT WASHWATER OR CONCRETE WASH WATER; CONCRETE, DETERGENT OR FLOATABLE WASTES; WASTES FROM ANY ENGINE/EQUIPMENT STEAM CLEANING OR CHEMICAL DEGREASING; CONTAMINATED GROUNDWATER AND CHLORINATED POTABLE WATER LINE FLUSHING.

**GENERAL NOTES**

- THIS PLAN IDENTIFIES POTENTIAL SOURCES OF POLLUTANTS OF STORM WATER, PRESENTS POLLUTION CONTROL MEASURES, AND ASSISTS IN ENSURING IMPLEMENTATION AND MAINTENANCE OF THE BEST MANAGEMENT PRACTICES (BMPs) INDICATED HEREIN. SEDIMENT IS A POTENTIAL POLLUTANT AND THE BMPs WERE SELECTED AND PLACED FOR OPTIMAL EROSION AND SEDIMENTATION CONTROL.
- THE NEED FOR EROSION CONTROL DEVICES WILL VARY ACCORDING TO THE SEASON DURING CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PLACE BMPs WHEREVER NECESSARY TO PREVENT EROSION AND SEDIMENTATION.
- TEMPORARY EROSION CONTROL DEVICES ARE TO BE MAINTAINED BY THE CONTRACTOR AND KEPT IN OPERATION UNTIL SUBSEQUENT PROJECT WORK ELIMINATES THEIR NEED. IT MAY BE NECESSARY TO MOVE, ADJUST, REPLACE, OR CHANGE THE SHAPE OF THE EROSION CONTROL DEVICES DURING THE COURSE OF THE PROJECT. EROSION CONTROL MEASURES SHALL NOT BE REMOVED WITHOUT APPROVAL FROM THE ENGINEER OR INSPECTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLIANCE WITH THE SWPPP AND FOR MAINTENANCE OF BMPs. ALL CONTRACTORS AND THEIR PERSONNEL WHOSE WORK CAN CONTRIBUTE TO OR CAUSE POLLUTION OF STORM WATER SHOULD BE MADE FAMILIAR WITH THIS POLLUTION PREVENTION PLAN. ADEQUATE TRAINING FOR IMPLEMENTATION OF THE MEASURES PRESENTED HEREIN SHALL BE PROVIDED BY THE CONTRACTOR TO THEIR PERSONNEL.
- THE EROSION CONTROL DEVICES ON THIS PLAN ARE A GENERAL CONCEPT OF WHAT MAY BE REQUIRED. EROSION CONTROL DEVICES MAY BE RELOCATED, DELETED OR ADDITIONAL ITEMS MAY BE REQUIRED DEPENDING IN THE ACTUAL SOIL CONDITIONS ENCOUNTERED. EROSION CONTROL DEVICES MAY BE PLACED AT THE DISCRETION OF THE ENGINEER OR THE INSPECTOR.
- CHANGES IN CONSTRUCTION OR IN A CONDITION WHICH ARE NOT COVERED BY THIS PLAN SHOULD BE BROUGHT TO THE ATTENTION OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR UPDATING THE SWPPP AND THIS EROSION AND SEDIMENTATION CONTROL PLAN AS REQUIRED TO ADDRESS THE CHANGING SITE CONDITIONS.
- A STANDBY CREW FOR EMERGENCY WORK SHALL BE AVAILABLE AT ALL TIMES DURING THE RAINY SEASON. NECESSARY MATERIALS SHALL BE AVAILABLE AND STOCK PILED AT CONVENIENT LOCATIONS TO FACILITATE RAPID CONSTRUCTION OF TEMPORARY BMPs WHEN RAIN IS IMMINENT.
- ALL PROTECTIVE DEVICES DIRECTED TO BE INSTALLED SHALL BE IN PLACE AT THE END OF EACH WORK DAY WHEN THE FIVE DAY RAIN PROBABILITY EXCEEDS 40%.
- THIS SWPPP MUST BE KEPT ON-SITE DURING CONSTRUCTION ACTIVITY AND MADE AVAILABLE UPON REQUEST OF A REPRESENTATIVE OF THE REGIONAL WATER QUALITY BOARD AND/ OR THE LOCAL AGENCY.
- CONTRACTOR WILL INSPECT BMPs BEFORE, AFTER AND EVERY 24 HOURS DURING EXTENDED STORM EVENTS. THE CONTRACTOR WILL RECORD CONDITIONS OF THE BMPs AND ANY ACTIVITIES REQUIRED FOR MAINTAINING OR REPAIRING BMPs. MONITORING INCLUDES MAINTAINING A FILE DOCUMENTING ON-SITE INSPECTIONS, PROBLEMS ENCOUNTERED, CORRECTIVE ACTIONS, AND NOTES AND A REDLINE MAP OF REMEDIAL IMPLEMENTATION MEASURES. COPIES OF THESE REPORTS WILL BE KEPT ON-SITE WITH THE SWPPP AND WILL BE AVAILABLE FOR RWQCB REVIEW.
- THIS PLAN OFFERS METHODS FOR CONTROL OF SOIL EROSION AND SEDIMENT DURING GRADING AND CONSTRUCTION OPERATIONS. NO WARRANTY IS STATED OR IMPLIED THAT, DURING AN UNUSUAL STORM EVENT, IMPLEMENTATION OF THESE METHODS WILL PREVENT DAMAGE AS A RESULT OF SOIL EROSION. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PROPER INSTALLATION OF BMPs, ANY AND ALL DAMAGE TO OWNER'S PROPERTY, ADJACENT PROPERTY, AND CONTRACTOR'S ONGOING WORK.

**POLLUTION CONTROL NOTES**

- CONCRETE WASHOUT MAY BE RELOCATED AS DETERMINED BY CONTRACTOR. CONCRETE WASHOUT SHALL BE INSTALLED PER DETAIL SHOWN ON THIS SHEET.
- LOCATION OF DEBRIS RECEPCTACLES, VEHICLE STAGING AREA AND MATERIALS STORAGE AREA TO BE DETERMINED BY CONTRACTOR.
- PETROLEUM PRODUCTS, CHEMICALS, AND OTHER POTENTIAL POLLUTANT MATERIAL KEPT ON-SITE IN MINOR QUANTITIES WILL BE STORED IN WATERPROOF CONTAINERS AND CONTINUALLY MONITORED TO PREVENT LEAKS OR ACCIDENTAL RELEASES.

**EROSION & SEDIMENT CONTROL NOTES**

- PRIOR TO STORM EVENTS, SOIL BINDER, OR STRAW WATTLE, OR OTHER EROSION CONTROL DEVICES MAY BE APPLIED AND SECURED TO DISTURBED OPEN AREAS DURING GRADING OPERATIONS, IF DETERMINED NECESSARY BY CONTRACTOR.
- EXPOSED GROUND AREAS THAT ARE PLANNED TO BE REWORKED AT DATES GREATER THAN ONE MONTH AFTER INITIAL GRADING SHALL BE TREATED WITH A SOIL BINDER OR SOWN WITH FAST-GERMINATING NATIVE GRASS SEED AND WATERED UNTIL VEGETATION BECOMES ESTABLISHED.
- ALL ROADWAYS, DRIVEWAYS, SIDEWALKS, ETC. TO BE PAVED SHALL BE COMPLETED AS SOON AS POSSIBLE. IN ADDITION, STRUCTURAL FOUNDATIONS SHALL BE COMPLETED AS SOON AS POSSIBLE FOLLOWING BUILDING PAD CONSTRUCTION.
- FIBER ROLLS OR SILT FENCING TO BE INSTALLED PARALLEL TO GRADING CONTOURS.
- AFTER A RAIN STORM ALL SILT AND DEBRIS SHALL BE REMOVED FROM GRAVEL BAG CHEVRONS, DI PROTECTION DEVICES, CHECK DAMS, DESILTING BASINS, AND/OR OTHER SEDIMENT CONTROL DEVICES.
- STREETS ADJACENT TO THE PROJECT SITE SHALL BE SWEEPED AT THE END OF EACH WORKING DAY TO REMOVE DIRT, DUST, MUD AND CONSTRUCTION DEBRIS WHICH MAY HAVE ACCUMULATED FROM CONSTRUCTION ACTIVITIES.
- REASONABLE CARE SHALL BE TAKEN WHEN HAULING ANY EARTH, SAND, GRAVEL, STONE, DEBRIS OR ANY HAZARDOUS SUBSTANCE OVER ANY PUBLIC STREET, ALLEY OR OTHER PUBLIC PLACE. SHOULD ANY MATERIAL BLOW, SPILL, OR TRACK OVER AND UPON SAID PUBLIC OR ADJACENT PRIVATE PROPERTY, IMMEDIATE CLEAN UP SHALL OCCUR.
- STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO COMMENCEMENT OF GRADING. ALL CONSTRUCTION TRAFFIC ENTERING ONTO THE PAVED ROADS MUST CROSS THE STABILIZED CONSTRUCTION ENTRANCEWAY.
- DURING CONSTRUCTION, THE CONTRACTOR MUST POND ALL RUNOFF TO NON-ERODIBLE LOCATIONS THROUGH THE USE OF DIKES TO PREVENT EROSION.
- THE CONTRACTOR SHALL CONSTRUCT TEMPORARY EROSION CONTROL MEASURES AS DIRECTED BY THE ENGINEER TO CONTROL DRAINAGE WHICH HAS BEEN AFFECTED BY GRADING AND/OR TRENCHING OPERATIONS.
- THE INSPECTOR MAY REQUIRE THE DEVELOPER AT ANY TIME TO INSTALL AND/OR CONSTRUCT ADDITIONAL DRAINAGE STRUCTURES AS NECESSARY TO PREVENT OR CONTROL EROSION.

**DUST CONTROL NOTES**

- ALL MATERIAL EXCAVATED OR GRADED SHALL BE SUFFICIENTLY WATERED TO PREVENT EXCESSIVE AMOUNTS OF DUST. WATERING SHALL OCCUR AT LEAST TWICE DAILY WITH COMPLETE COVERAGE, PREFERABLY IN THE LATE MORNING AND AFTER WORK IS FINISHED FOR THE DAY.
- ALL CLEARING, GRADING, EARTH MOVING, OR EXCAVATION ACTIVITIES SHALL CEASE DURING PERIODS OF HIGH WINDS SO AS TO PREVENT EXCESSIVE AMOUNTS OF DUST OR VISIBLE SITE DISTURBANCE.
- ALL MATERIAL TRANSPORTED OFF-SITE SHALL BE EITHER SUFFICIENTLY WATERED OR SECURELY COVERED TO PREVENT EXCESSIVE AMOUNTS OF DUST.
- THE AREA DISTURBED BY CLEARING, GRADING, EARTH MOVING, OR EXCAVATION OPERATIONS SHALL BE MINIMIZED SO AS TO PREVENT EXCESSIVE AMOUNTS OF DUST.
- PERMANENT DUST CONTROL MEASURES DESCRIBED HEREIN SHALL BE IMPLEMENTED AS SOON AS POSSIBLE FOLLOWING COMPLETION OF ANY SOIL DISTURBING ACTIVITIES.
- ON-SITE VEHICLE SPEED SHALL BE LIMITED TO 15 MPH FOR ANY UNPAVED SURFACE.
- ALL UNPAVED AREAS WITH VEHICLE TRAFFIC SHALL BE WATERED AT LEAST TWICE PER DAY USING NON-POTABLE WATER.
- STREETS ADJACENT TO THE PROJECT SITE SHALL BE SWEEPED DAILY TO REMOVE SILT WHICH MAY HAVE ACCUMULATED FROM CONSTRUCTION ACTIVITIES SO AS TO PREVENT EXCESSIVE AMOUNTS OF DUST FROM LEAVING THE SITE.

REVISIONS			
MARK	DATE	BY	APPROVED / RCE NO.

DESIGNED BY: _____
DATE: _____
DRAWN BY: _____
DATE: _____
CHECKED BY: _____
DATE: _____

**CITY OF ONTARIO**

BENCH MARK NO. S.P.S.N. 00789 ELEV. 785.572  
 LOCATION: NGVD29/1990 ADJ.  
 SAN BERNARDINO COUNTY BENCHMARK LOCATED AT THE INTERSECTION OF TURNER AVENUE AND RIVERSIDE DRIVE, 1/4 COR. SEC. 2/11, T2S, R7W. 2" BRASS DISK STAMPED "T2S, R7W, 1/4, S2, S11" "LS 3258" SET FLUSH IN A.C.

WESTON G. CLUNNE  
 C 89279  
 CIVIL  
 STATE OF CALIFORNIA

**Cannon**  
 16842 VAN KARMAN AVE, STE 150  
 IRVINE, CALIFORNIA 92606  
 949.777.1591

89279 C.E. NO.      6/15/2022 DATE

*Weston Clunne*  
 SIGNATURE

**ROUGH GRADING PLAN  
 EROSION CONTROL NOTES**



WDDI NO. XXXXXXXXXX

<b>B20220263</b>	SHEET _____ OF _____
<b>TRACT 18916</b>	CONTRACT _____
	ACCOUNT _____
	DWG. NO. _____

THESE DRAWINGS ARE INSTRUMENTS OF SERVICE AND ARE THE PROPERTY OF CANNON. ALL DESIGNS AND INFORMATION ON THESE DRAWINGS ARE FOR USE OF THE SPECIFIED PROJECT AND SHALL NOT BE USED OTHERWISE OR REPRODUCED WITHOUT THE EXPRESSED WRITTEN PERMISSION OF CANNON.

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**APPENDIX 5.1:**  
**ON-SITE TRAFFIC NOISE CALCULATIONS**

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**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard No Wall  
 Road Name: Archibald Avenue  
 Lot No: 11 and 12

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 0.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 751.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 753.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 759.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 751.0 feet		Autos: 42.273				
Barrier Elevation: 750.8 feet		Medium Trucks: 42.075				
Road Grade: 0.1%		Heavy Trucks: 42.122				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.99	-1.20	-1.83	0.000	0.000
Medium Trucks:	78.79	-13.41	1.02	-1.20	-2.03	0.000	0.000
Heavy Trucks:	83.02	-17.37	1.01	-1.20	-2.56	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.3	55.8	64.2	64.5
Heavy Trucks:	65.5	64.0	55.0	56.3	64.6	64.7
Vehicle Noise:	75.6	73.8	71.4	66.0	74.6	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.3	55.8	64.2	64.5
Heavy Trucks:	65.5	64.0	55.0	56.3	64.6	64.7
Vehicle Noise:	75.6	73.8	71.4	66.0	74.6	75.1



**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard No Wall  
 Road Name: Archibald Avenue  
 Lot No: 13 and 14

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 0.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 750.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 752.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 758.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 750.0 feet		Autos: 42.399				
Barrier Elevation: 750.8 feet		Medium Trucks: 42.146				
Road Grade: 0.1%		Heavy Trucks: 42.058				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.97	-1.20	-1.75	0.000	0.000
Medium Trucks:	78.79	-13.41	1.01	-1.20	-1.94	0.000	0.000
Heavy Trucks:	83.02	-17.37	1.02	-1.20	-2.46	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.0	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.3	55.8	64.2	64.5
Heavy Trucks:	65.5	64.1	55.0	56.3	64.6	64.7
Vehicle Noise:	75.6	73.8	71.3	66.0	74.6	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.0	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.3	55.8	64.2	64.5
Heavy Trucks:	65.5	64.1	55.0	56.3	64.6	64.7
Vehicle Noise:	75.6	73.8	71.3	66.0	74.6	75.1

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard No Wall  
 Road Name: Archibald Avenue  
 Lot No: 35 and 36

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 0.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 749.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 751.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 757.01 Grade Adjustment: 0.0				
Pad Elevation: 748.3 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 749.0 feet		Autos: 42.220				
Barrier Elevation: 748.3 feet		Medium Trucks: 42.048				
Road Grade: 0.1%		Heavy Trucks: 42.163				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	1.00	-1.20	-1.88	0.000	0.000
Medium Trucks:	78.79	-13.41	1.03	-1.20	-2.08	0.000	0.000
Heavy Trucks:	83.02	-17.37	1.01	-1.20	-2.61	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.3
Medium Trucks:	65.2	63.7	57.3	55.8	64.3	64.5
Heavy Trucks:	65.5	64.0	55.0	56.2	64.6	64.7
Vehicle Noise:	75.6	73.8	71.4	66.0	74.6	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.3
Medium Trucks:	65.2	63.7	57.3	55.8	64.3	64.5
Heavy Trucks:	65.5	64.0	55.0	56.2	64.6	64.7
Vehicle Noise:	75.6	73.8	71.4	66.0	74.6	75.1

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard With Wall  
 Road Name: Archibald Avenue  
 Lot No: 11 and 12

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 751.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 753.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 759.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 751.0 feet		Autos: 41.466				
Barrier Elevation: 750.8 feet		Medium Trucks: 41.034				
Road Grade: 0.1%		Heavy Trucks: 40.602				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	1.12	-1.20	1.18	-10.660	-13.660
Medium Trucks:	78.79	-13.41	1.18	-1.20	1.04	-10.380	-13.380
Heavy Trucks:	83.02	-17.37	1.25	-1.20	0.73	-9.420	-12.420

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.9	73.0	71.2	65.1	73.8	74.4
Medium Trucks:	65.4	63.9	57.5	55.9	64.4	64.6
Heavy Trucks:	65.7	64.3	55.2	56.5	64.8	65.0
Vehicle Noise:	75.8	74.0	71.5	66.1	74.7	75.2

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.2	62.3	60.5	54.5	63.1	63.7
Medium Trucks:	55.0	53.5	47.1	45.6	54.0	54.3
Heavy Trucks:	56.3	54.9	45.8	47.1	55.4	55.6
Vehicle Noise:	65.3	63.5	60.9	55.7	64.2	64.7

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard With Wall  
 Road Name: Archibald Avenue  
 Lot No: 13 and 14

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 750.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 752.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 758.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 750.0 feet		Autos: 41.698				
Barrier Elevation: 750.8 feet		Medium Trucks: 41.205				
Road Grade: 0.1%		Heavy Trucks: 40.611				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	1.08	-1.20	1.24	-10.780	-13.780
Medium Trucks:	78.79	-13.41	1.16	-1.20	1.10	-10.500	-13.500
Heavy Trucks:	83.02	-17.37	1.25	-1.20	0.78	-9.620	-12.620

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.8	72.9	71.2	65.1	73.7	74.3
Medium Trucks:	65.3	63.8	57.5	55.9	64.4	64.6
Heavy Trucks:	65.7	64.3	55.2	56.5	64.8	65.0
Vehicle Noise:	75.7	73.9	71.4	66.1	74.7	75.2

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.0	62.1	60.4	54.3	62.9	63.6
Medium Trucks:	54.8	53.3	47.0	45.4	53.9	54.1
Heavy Trucks:	56.1	54.7	45.6	46.9	55.2	55.4
Vehicle Noise:	65.1	63.3	60.7	55.5	64.1	64.6

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Backyard With Wall  
 Road Name: Archibald Avenue  
 Lot No: 35 and 36

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 98.0 feet		Autos: 749.00				
Barrier Distance to Observer: 5.0 feet		Medium Trucks: 751.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 757.01 Grade Adjustment: 0.0				
Pad Elevation: 748.3 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 749.0 feet		Autos: 41.360				
Barrier Elevation: 748.3 feet		Medium Trucks: 40.960				
Road Grade: 0.1%		Heavy Trucks: 40.609				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	1.13	-1.20	1.15	-10.600	-13.600
Medium Trucks:	78.79	-13.41	1.20	-1.20	1.01	-10.320	-13.320
Heavy Trucks:	83.02	-17.37	1.25	-1.20	0.70	-9.300	-12.300

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.9	73.0	71.2	65.2	73.8	74.4
Medium Trucks:	65.4	63.9	57.5	56.0	64.4	64.7
Heavy Trucks:	65.7	64.3	55.2	56.5	64.8	65.0
Vehicle Noise:	75.8	74.0	71.5	66.1	74.7	75.3

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.3	62.4	60.6	54.6	63.2	63.8
Medium Trucks:	55.1	53.5	47.2	45.6	54.1	54.3
Heavy Trucks:	56.4	55.0	45.9	47.2	55.5	55.7
Vehicle Noise:	65.4	63.6	60.9	55.7	64.3	64.8

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: First Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 11 and 12

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 751.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 753.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 759.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 751.0 feet		Autos: 42.343				
Barrier Elevation: 750.8 feet		Medium Trucks: 41.912				
Road Grade: 0.1%		Heavy Trucks: 41.479				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.98	-1.20	1.06	-10.420	-13.420
Medium Trucks:	78.79	-13.41	1.05	-1.20	0.92	-10.060	-13.060
Heavy Trucks:	83.02	-17.37	1.11	-1.20	0.61	-9.030	-12.030

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.4	55.8	64.3	64.5
Heavy Trucks:	65.6	64.1	55.1	56.4	64.7	64.8
Vehicle Noise:	75.6	73.8	71.3	66.0	74.6	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.3	62.4	60.6	54.6	63.2	63.8
Medium Trucks:	55.2	53.7	47.3	45.8	54.2	54.4
Heavy Trucks:	56.5	55.1	46.1	47.3	55.7	55.8
Vehicle Noise:	65.4	63.6	61.0	55.8	64.4	64.9

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: First Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 13 and 14

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 750.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 752.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 758.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 750.0 feet		Autos: 42.575				
Barrier Elevation: 750.8 feet		Medium Trucks: 42.082				
Road Grade: 0.1%		Heavy Trucks: 41.488				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.94	-1.20	1.12	-10.540	-13.540
Medium Trucks:	78.79	-13.41	1.02	-1.20	0.98	-10.240	-13.240
Heavy Trucks:	83.02	-17.37	1.11	-1.20	0.66	-9.180	-12.180

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.0	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.3	55.8	64.2	64.5
Heavy Trucks:	65.6	64.1	55.1	56.4	64.7	64.8
Vehicle Noise:	75.6	73.8	71.3	66.0	74.5	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.1	62.2	60.5	54.4	63.1	63.7
Medium Trucks:	55.0	53.5	47.1	45.5	54.0	54.2
Heavy Trucks:	56.4	55.0	45.9	47.2	55.5	55.7
Vehicle Noise:	65.2	63.5	60.8	55.6	64.2	64.7

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: First Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 35 and 36

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 749.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 751.30				
Observer Height (Above Pad): 5.0 feet		Heavy Trucks: 757.01 Grade Adjustment: 0.0				
Pad Elevation: 748.3 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 749.0 feet		Autos: 42.237				
Barrier Elevation: 748.3 feet		Medium Trucks: 41.837				
Road Grade: 0.1%		Heavy Trucks: 41.486				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	1.00	-1.20	1.03	-10.360	-13.360
Medium Trucks:	78.79	-13.41	1.06	-1.20	0.89	-9.970	-12.970
Heavy Trucks:	83.02	-17.37	1.11	-1.20	0.58	-8.900	-11.900

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.7	72.8	71.1	65.0	73.6	74.2
Medium Trucks:	65.2	63.7	57.4	55.8	64.3	64.5
Heavy Trucks:	65.6	64.1	55.1	56.4	64.7	64.8
Vehicle Noise:	75.6	73.8	71.4	66.0	74.6	75.1

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	64.4	62.5	60.7	54.7	63.3	63.9
Medium Trucks:	55.3	53.8	47.4	45.9	54.3	54.5
Heavy Trucks:	56.7	55.2	46.2	47.5	55.8	55.9
Vehicle Noise:	65.5	63.7	61.1	55.9	64.4	64.9



**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Second Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 11 and 12

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 751.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 753.30				
Observer Height (Above Pad): 14.0 feet		Heavy Trucks: 759.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 751.0 feet		Autos: 45.513				
Barrier Elevation: 750.8 feet		Medium Trucks: 44.870				
Road Grade: 0.1%		Heavy Trucks: 43.756				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.51	-1.20	-1.43	0.000	0.000
Medium Trucks:	78.79	-13.41	0.60	-1.20	-1.62	0.000	0.000
Heavy Trucks:	83.02	-17.37	0.77	-1.20	-2.15	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.3	72.4	70.6	64.5	73.2	73.8
Medium Trucks:	64.8	63.3	56.9	55.4	63.8	64.1
Heavy Trucks:	65.2	63.8	54.8	56.0	64.4	64.5
Vehicle Noise:	75.2	73.4	70.9	65.5	74.1	74.6

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.3	72.4	70.6	64.5	73.2	73.8
Medium Trucks:	64.8	63.3	56.9	55.4	63.8	64.1
Heavy Trucks:	65.2	63.8	54.8	56.0	64.4	64.5
Vehicle Noise:	75.2	73.4	70.9	65.5	74.1	74.6

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Second Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 13 and 14

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 750.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 752.30				
Observer Height (Above Pad): 14.0 feet		Heavy Trucks: 758.01 Grade Adjustment: 0.0				
Pad Elevation: 750.8 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 750.0 feet		Autos: 45.826				
Barrier Elevation: 750.8 feet		Medium Trucks: 45.137				
Road Grade: 0.1%		Heavy Trucks: 43.899				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.46	-1.20	-1.35	0.000	0.000
Medium Trucks:	78.79	-13.41	0.56	-1.20	-1.54	0.000	0.000
Heavy Trucks:	83.02	-17.37	0.74	-1.20	-2.06	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.2	72.3	70.5	64.5	73.1	73.7
Medium Trucks:	64.7	63.2	56.9	55.3	63.8	64.0
Heavy Trucks:	65.2	63.8	54.7	56.0	64.3	64.5
Vehicle Noise:	75.1	73.3	70.8	65.5	74.1	74.6

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.2	72.3	70.5	64.5	73.1	73.7
Medium Trucks:	64.7	63.2	56.9	55.3	63.8	64.0
Heavy Trucks:	65.2	63.8	54.7	56.0	64.3	64.5
Vehicle Noise:	75.1	73.3	70.8	65.5	74.1	74.6

**FHWA-RD-77-108 HIGHWAY NOISE PREDICTION MODEL (CALVENO) - v10/31/19**

Scenario: Second Floor With Wall  
 Road Name: Archibald Avenue  
 Lot No: 35 and 36

Project Name: Countryside 36  
 Job Number: 15172  
 Analyst: B. Maddux

SITE SPECIFIC INPUT DATA		NOISE MODEL INPUTS				
<b>Highway Data</b>		<b>Site Conditions (Hard = 10, Soft = 15)</b>				
Average Daily Traffic (Adt): 42,000 vehicles		Autos: 15				
Peak Hour Percentage: 10%		Medium Trucks (2 Axles): 15				
Peak Hour Volume: 4,200 vehicles		Heavy Trucks (3+ Axles): 15				
Vehicle Speed: 50 mph		<b>Vehicle Mix</b>				
Near/Far Lane Distance: 80 feet		VehicleType	Day	Evening	Night	Daily
<b>Site Data</b>		Autos: 77.5% 12.9% 9.6% 97.42%				
Barrier Height: 8.0 feet		Medium Trucks: 84.8% 4.9% 10.3% 1.84%				
Barrier Type (0-Wall, 1-Berm): 0.0		Heavy Trucks: 86.5% 2.7% 10.8% 0.74%				
Centerline Dist. to Barrier: 93.0 feet		<b>Noise Source Elevations (in feet)</b>				
Centerline Dist. to Observer: 99.0 feet		Autos: 749.00				
Barrier Distance to Observer: 6.0 feet		Medium Trucks: 751.30				
Observer Height (Above Pad): 14.0 feet		Heavy Trucks: 757.01 Grade Adjustment: 0.0				
Pad Elevation: 748.3 feet		<b>Lane Equivalent Distance (in feet)</b>				
Road Elevation: 749.0 feet		Autos: 45.364				
Barrier Elevation: 748.3 feet		Medium Trucks: 44.744				
Road Grade: 0.1%		Heavy Trucks: 43.692				

**FHWA Noise Model Calculations**

VehicleType	REMEL	Traffic Flow	Distance	Finite Road	Fresnel	Barrier Atten	Berm Atten
Autos:	71.12	3.82	0.53	-1.20	-1.47	0.000	0.000
Medium Trucks:	78.79	-13.41	0.62	-1.20	-1.67	0.000	0.000
Heavy Trucks:	83.02	-17.37	0.78	-1.20	-2.20	0.000	0.000

**Unmitigated Noise Levels (without Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.3	72.4	70.6	64.6	73.2	73.8
Medium Trucks:	64.8	63.3	56.9	55.4	63.8	64.1
Heavy Trucks:	65.2	63.8	54.8	56.0	64.4	64.5
Vehicle Noise:	75.2	73.4	70.9	65.6	74.1	74.7

**Mitigated Noise Levels (with Topo and barrier attenuation)**

VehicleType	Leq Peak Hour	Leq Day	Leq Evening	Leq Night	Ldn	CNEL
Autos:	74.3	72.4	70.6	64.6	73.2	73.8
Medium Trucks:	64.8	63.3	56.9	55.4	63.8	64.1
Heavy Trucks:	65.2	63.8	54.8	56.0	64.4	64.5
Vehicle Noise:	75.2	73.4	70.9	65.6	74.1	74.7

**Attachment C: Conditions of Approval**

*(Conditions of Approval follow this page)*



## LAND DEVELOPMENT DIVISION CONDITIONS OF APPROVAL

303 East B Street, Ontario, California 91764 Phone: 909.395.2036 / Fax: 909.395.2420

**Date Prepared:** 4/18/2023

**File No:** PDEV22-038

**Related Files:** PMTT21-011 (Tentative Tract Map No. 18916)

**Project Description:** A public hearing to consider a Development Plan (File No. PDEV22-038) to construct 36 single-family detached 4-pack single-family dwellings with related on-site improvements for a subdivision previously approved with Tentative Tract Map No. 18916 (File No. PMTT21-011), on approximately 11.05 acres of land located on the west side of Archibald Avenue and approximately 575 feet south of Chino Avenue within Planning Area 2 (Neighborhood 7) of the Countryside Specific Plan (APN: 0218-131-34); **submitted by KB Homes Coastal Inc.**

**Prepared By:** Jocelyn Torres, Associate Planner  
Phone: 909.395.2424 (direct)  
Email: [jocelyntorres@ontarioca.gov](mailto:jocelyntorres@ontarioca.gov)

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

**1.0 Standard Conditions of Approval.** The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

**2.0 Special Conditions of Approval.** In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

**2.1 Time Limits.**

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

**2.2 General Requirements.** The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape

and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for the project, which shall be maintained on site during project construction.

### 2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

### 2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11). Final design and placement of bicycle parking facilities shall be subject to Planning Department review and approval.

## 2.6 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

## 2.7 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.8 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.9 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.10 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noise levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.11 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;  
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and  
(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

## 2.12 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.



**2.13** Environmental Requirements.

(a) The environmental impacts of this project were reviewed in conjunction with the **Countryside Specific Plan Environmental Impact Report** (State Clearinghouse No. 2004071001). All previously adopted mitigation measures are a condition of approval and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

**2.14** Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

**2.15** Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (“NOD”) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the “Clerk of the Board of Supervisors”, which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (“CEQA”). Failure to provide said fee within the time specified will result in the extension of the statute of limitations for the filing of a CEQA lawsuit from 30 days to 180 days.

(b) After the Project’s entitlement approval, and prior to issuance of final building permits, the Planning Department’s Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

**2.16** Final Occupancy. The Project Architect of record will certify that construction of each building site and the exterior elevations of each structure shall be completed in compliance with the approved plans. Any deviation to approved plans shall require a resubmittal to the Planning Department for review and approval prior to construction. The Occupancy Release Request Form/Architect Certificate of Compliance shall be provided prior to final occupancy. After the receipt of this Certification, the Planning Department will conduct a final site and exterior elevations inspection. The Owner’s Representative and Contractor shall be present.

**2.17** Additional Requirements.

**(a)** Prior to issuance of building permits, the Developer shall demonstrate that the proposed light standards will not conflict with the proposed tree location. The Developer shall provide a plan showing both lighting and landscaping on the same sheet.

**(b)** Prior to the issuance of any building permits, the Developer shall incorporate the project design features that are outlined within the Noise Impact Analysis (prepared by Urban Crossroad dated March 7, 2023) on the construction plans submitted for plan check.

**(c)** The Final Map shall be approved by the City Council prior to the issuance of any permits.

**(d)** The model sales office shall require review and approval of a Temporary Use Permit to be submitted prior to issuance of building permits for the model sales units.

**(e)** The applicant shall contact the Ontario Post Office to determine the size and location of the mailboxes for this project. The location of the mailboxes shall be submitted to the Planning Department for review and approval prior to the issuance of building permits.

**(f)** The project shall comply with all conditions of approval from all other City agencies and departments.

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV22-038  
 Address: SWC of Archibald Ave & Chino Avenue  
 APN: 0218-131-34  
 Existing Land Use: Vacant  
 Proposed Land Use: Development Plan to construct 36 single family 4 pack dwellings  
 Site Acreage: 11.05 Proposed Structure Height: 29 FT  
 ONT-IAC Project Review: N/A  
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia  
 Contact Info: 909-395-2276  
 Project Planner: Jocelyn Torres  
 Date: 1/25/2023  
 CD No.: 2022-049  
 PALU No.: N/A

### The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input type="checkbox"/> Zone 2	<input type="checkbox"/> 65 - 70 dB CNEL	<input type="radio"/> Airspace Obstruction Surfaces	<input checked="" type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="checkbox"/> Airspace Avigation Easement Area	
<input type="checkbox"/> Zone 4		Allowable Height: 200 FT+	
<input type="checkbox"/> Zone 5			

### The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1   
  Zone 2   
  Zone 3   
  Zone 4   
  Zone 5   
  Zone 6  
 Allowable Height: \_\_\_\_\_

## CONSISTENCY DETERMINATION

This proposed Project is:  Exempt from the ALUCP   
 Consistent   
 Consistent with Conditions   
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: \_\_\_\_\_

# AIRPORT LAND USE COMPATIBILITY PLANNING

## CONSISTENCY DETERMINATION REPORT

CD No.: 2022-049  
PALU No.: \_\_\_\_\_

### PROJECT CONDITIONS

1. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

#### NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.



# CITY OF ONTARIO MEMORANDUM



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**DATE:** April 24, 2023  
**TO:** Jocelyn Torres, Planning Department  
**FROM:** Blaine Ishii, Integrated Waste Department  
**SUBJECT:** DPR#1 – Integrated Waste Comments  
**PROJECT NO.:** PDEV22-038  
**ATTACHMENTS:**

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## BRIEF DESCRIPTION – PDEV22-038

### THIS SUBMITTAL IS COMPLETE.

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**CORRECTION ITEMS:** *In order to be considered for approval by the Integrated Waste Department the applicant shall address all the correction items below and resubmit the application for further review. Please note that all design shall meet the City's Design Development Guidelines, Specifications Design Criteria, and City Standards.*

***Integrated Waste Conditions:***

1. N/A



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Jocelyn Torres, Associate Planner  
Planning Department

**FROM:** Paul Ehrman, Sr. Deputy Fire Chief/Fire Marshal  
Fire Department

**DATE:** August 25, 2022

**SUBJECT:** PDEV22-038 - A Development Plan to construct 36 single-family 4-pack dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD- Alley loaded land use district of the Countryside Specific Plan (APN(s): 0218-131-34-0000). Related File(s): PMTT21-011.

- 
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
- 

### **SITE AND BUILDING FEATURES:**

- A. 2019 CBC Type of Construction: Type V-B
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): Varies (2,372 Sq. Ft. to 2,791 Sq. Ft)
- D. Number of Stories: 2
- E. Total Square Footage: Varies (2,372 Sq. Ft. to 2,791 Sq. Ft)
- F. 2019 CBC Occupancy Classification(s): R3

## **CONDITIONS OF APPROVAL:**

### **1.0 GENERAL**

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at [www.ontarioca.gov/Fire/Prevention](http://www.ontarioca.gov/Fire/Prevention).
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

### **2.0 FIRE DEPARTMENT ACCESS**

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

### **3.0 WATER SUPPLY**

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2019 California Fire Code, Appendix B, is 1500 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

### **4.0 FIRE PROTECTION SYSTEMS**

- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 13. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.





# CITY OF ONTARIO

## MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)  
Rudy Zeledon, Planning Director (Copy of memo only)  
Diane Ayala, Advanced Planning Division (Copy of memo only)  
Charity Hernandez, Economic Development  
James Caro, Building Department  
Raymond Lee, Engineering Department  
Jamie Richardson, Landscape Planning Division  
Dennis Mejia, Municipal Utility Company  
Jeremy Phillips, Police Department  
Paul Ehrman, Deputy Fire Chief/Fire Marshal  
Jay Bautista, Traffic/Transportation Manager  
Lorena Mejia, Airport Planning  
Tricia Espinoza, Engineering/NPDES  
Angela Magana, Community Improvement (Copy of memo only)  
Jimmy Chang, IT Department  
Ben Mayorga, Integrated Waste

FROM: Jocelyn Torres, Associate Planner

DATE: November 07, 2022

SUBJECT: FILE #: PDEV22-038 Revision #1 Finance Acct#:

The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

**PROJECT DESCRIPTION:** A Development Plan to construct 36 single-family dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD - Alley-Loaded land use district of the Countryside Specific Plan (APNs: 0218-131-34). Related File(s): PMTT21-011.

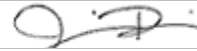
- The plan does adequately address the departmental concerns at this time.
- No comments
  - See previous report for Conditions
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Landscape Planning Division J.P. Sr. Landscape Architect 11/17/22  
Department Signature Title Date

**CITY OF ONTARIO**  
**LANDSCAPE PLANNING DIVISION**  
 303 East "B" Street, Ontario, CA 91764

**CONDITIONS OF APPROVAL**

Sign Off



11/17/2022

Jamie Richardson, Sr. Landscape Architect

Date

Reviewer's Name: **Jamie Richardson, Sr. Landscape Planner** Phone: **(909) 395-2615**

D.A.B. File No.: PDEV22-038 Case Planner: Jocelyn Torres

Project Name and Location:  
 Countryside – 36 single-family 4-pack dwellings  
 TM 18916, SW corner of Archibald Ave. and Chino Ave.

Applicant/Representative:  
 KB Home Coastal Inc  
 36310 Inland Valley Drive #300  
 Wildomar, CA 92595

- Preliminary Plans (dated 11/7/2022) meet the Standard Conditions for New Development and have been approved considering that the following conditions below are met upon the landscape construction documents submittal.**
- Preliminary Plans (dated) have not been approved. Corrections noted below are required before Preliminary Landscape Plan approval.**

**A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.**  
 Landscape construction plans with plan check number may be emailed to:  
[landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)

Civil/ Site Plans

1. Note for compaction to be less than 85% at landscape areas; all finished grades 1 ½" below finished surfaces; landscaped slopes to max 3:1.
2. Show or note transformers shall be located in planter areas and set back 3' from paving for small transformers less than 4' high and 5' setback for large transformers greater than 4' high. Coordinate with landscape plans. Locate on level grade.
3. Show or note backflow devices shall be located in planter areas and set back min 3' from paving. Locate on level grade. Coordinate with landscape plans.
4. Wall footings shall not restrict landscape; max 12" in front of footing with 12" of cover.
5. Wall openings for drainage overflow shall be max 4" wide.
6. AC units shall be located in residential side yards opposite the main backyard access path with a gate, a second gate, and a solid surface path on the opposite side added for access.
7. Stormwater infiltration devices located in landscape areas shall be reviewed and approved by the Landscape Planning Division before installation.
8. Corners; verify dimension and grade for required monumentation (see Specific Plan for detail). Show proposed locations of monumentation. Adjacent walls shall not interfere with the required monumentation.
9. DG trails and parkways at corners (Archibald Ave.) shall have the trail curve into the sidewalk rather than out to the corner ramp; OK to end parkway landscape before corner utilities.
10. Locate utilities, including light standards, fire hydrants, water, drain, and sewer lines, to not conflict with required tree locations—coordinate civil plans with landscape plans.
11. Note: The finished grade at the building wall is to be no more than 8" from the stucco/plaster face of the wall.
12. Typical lot drainage shall include a catch basin with a gravel sump below each before exiting the property if no other water quality infiltration is provided.
13. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or stormwater infiltration areas are located

shall be loosened by soil fracturing. For trees, a 12'x12'x18" deep area; for stormwater infiltration, the entire area shall be loosened. Add the following information on the plans: The backhoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The backhoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference, see Urban Tree Foundation – Planting Soil Specifications.

#### Landscape Plans

14. During plan check, coordinate with Ontario Municipal Utilities Company (OMUC) to submit irrigation plans for recycled water systems to [omucwaterquality@ontarioca.gov](mailto:omucwaterquality@ontarioca.gov). OMUC shall review and approve irrigation systems utilizing recycled water before final landscape approval. Submit an electronic approval letter or memo from OMUC with the resubmittal of the landscape package.
15. During plan check (for large HOA tracts and industrial sites), provide an overall exhibit showing water meter locations, including POC and controllers.
16. During plan check (for HOA tracts), submit an overall tree exhibit showing HOA parkways, common area trees, and typical front yard tree locations to avoid conflicts.
17. Locate light standards, fire hydrants, water, and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans
18. Show 6' diameter of mulch only at new trees—detail irrigation dripline outside of mulched root zone.
19. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
20. Provide phasing map for multi-phase projects.
21. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Landscape construction plans with building permit number for plan check may be emailed to: [landscapeplancheck@ontarioca.gov](mailto:landscapeplancheck@ontarioca.gov)



# CITY OF ONTARIO

## MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)  
Rudy Zeledon, Planning Director (Copy of memo only)  
Diane Ayala, Advanced Planning Division (Copy of memo only)  
Charity Hernandez, Economic Development  
James Caro, Building Department  
Raymond Lee, Engineering Department  
Jamie Richardson, Landscape Planning Division  
Dennis Mejia, Municipal Utility Company  
**Jeremy Phillips, Police Department**  
Paul Erhman, Deputy Fire Chief/Fire Marshal  
Jay Bautista, Traffic/Transportation Manager  
Lorena Mejia, Airport Planning  
Tricia Espinoza, Engineering/NPDES  
Angela Magana, Community Improvement (Copy of memo only)  
Jimmy Chang, IPA Department  
Ben Mayorga, Integrated Waste

FROM: , Jocelyn Torres, Associate Planner

DATE: August 19, 2022

SUBJECT: FILE #: PDEV22-038 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
  - Both DAB and Planning Commission actions are required
  - Only Planning Commission action is required
  - DAB, Planning Commission and City Council actions are required
  - Only Zoning Administrator action is required

**PROJECT DESCRIPTION:** A Development Plan to construct 36 single-family 4-pack dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD- Alley loaded land use district of the Countryside Specific Plan (APN(s): 0218-131-34-0000). Related File(s): PMTT21-011.

- The plan does adequately address the departmental concerns at this time.
  - No comments
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
  - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

ONTARIO POLICE  
Department

ANTONIO GALGAN  
Signature

POLICE OFFICER 8/31/22  
Title Date



# CITY OF ONTARIO

## MEMORANDUM

**TO:** Jocelyn Torres, Associate Planner

**FROM:** Tony Galban, Police Officer

**DATE:** August 31, 2022

**SUBJECT:** PDEV22-038- A DEVELOPMENT PLAN TO CONSTRUCT 36 SINGLE-FAMILY 4-PACK DWELLING ON 11.05 ACRES OF LAND NEAR THE SOUTHWEST CORNER OF ARCHIBALD AVENUE AND CHINO AVENUE. RELATED FILES: PMTT21-011.

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The “Standard Conditions of Approval” contained in Resolution No. 2017-027 for “Ontario ranch Projects” apply. The applicant shall read and be thoroughly familiar with these conditions, including but not limited to, the requirements listed below.

- Required lighting for all walkways, paseos, driveways, doorways, parking areas, parks, park walkways, playgrounds, recreation areas and other areas used by the public shall be provided and operate on photosensor at the prescribed foot-candle levels. Photometrics shall be provided to the Police Department. Photometrics shall include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- The Applicant shall install illuminated address numbers, powered by photocell, on each individual unit and shall not be controlled by the building occupants.
- The Applicant shall comply with all construction site security requirements as stated in the Standard Conditions. This includes the provisions for perimeter lighting, site lighting, fencing and/or uniformed security.
- Trash enclosure shall be fully secured with screens/grates to reduce crime and encampment opportunities for homeless persons.

The Applicant is invited to contact Officer Tony Galban at (909) 408-1006 with any questions or concerns regarding these conditions.



# CITY OF ONTARIO

## MEMORANDUM

TO: Scott Murphy, Community Development Director (Copy of memo only)  
 Rudy Zeledon, Planning Director (Copy of memo only)  
 Diane Ayala, Advanced Planning Division (Copy of memo only)  
 Charity Hernandez, Economic Development  
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 Angela Magana, Community Improvement (Copy of memo only)  
 Jimmy Chang, IPA Department  
 Ben Mayorga, Integrated Waste

FROM: Jocelyn Torres, Associate Planner

DATE: August 19, 2022

SUBJECT: FILE #: PDEV22-038 Finance Acct#:

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- Note:**
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  - Both DAB and Planning Commission actions are required
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  - DAB, Planning Commission and City Council actions are required
  - Only Zoning Administrator action is required

**PROJECT DESCRIPTION:** A Development Plan to construct 36 single-family 4-pack dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD- Alley loaded land use district of the Countryside Specific Plan (APN(s): 0218-131-34-0000). Related File(s): PMTT21-011.

- The plan does adequately address the departmental concerns at this time.
  - No comments
  - Report attached (1 copy and email 1 copy)
  - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
  - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Department: Community Improvement      Signature: [Handwritten Signature]      Title: Comm Imp SUPERVISOR      Date: 8-22-22



# CITY OF ONTARIO MEMORANDUM

## ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

**PROJECT ENGINEER:** Angela Truong, Assistant Engineer (909) 395-2134

**PROJECT PLANNER:** Jocelyn Torres, Associate Planner (909) 395-2424

**DAB MEETING DATE:** May 15<sup>th</sup>, 2023

**PROJECT NAME/DESCRIPTION:** PDEV22-038, a Development Plan to construct 36 single-family 4-pack dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD - Alley-Loaded land use district of the Countryside Specific Plan (APN: 0218-131-34). Related File(s): PMTT21-011 (TM-18916).

**LOCATION:** Southwest Corner of Archibald Avenue and Chino Avenue

**APPLICANT:** KB Homes Coastal Inc

**REVIEWED BY:**  5/1/23.  
Raymond Lee, P.E. Date  
Assistant City Engineer

**APPROVED BY:**  5-1-23  
Khoi Do, P.E. Date  
City Engineer

**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE LISTED BELOW. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT. SEE ATTACHED EXHIBIT 'A' FOR PLAN CHECK SUBMITTAL REQUIREMENTS.**

1. The project shall comply with the Conditions of Approval for Tract Map No. 18916, approved at the DAB meeting of November 15, 2021, attached herewith for reference, and in accordance with the Countryside Specific Plan, File No. PSP04-001, adopted April 18, 2006, as amended.
2. The applicant/developer shall comply with the Development Agreement (DA), File No. PDA21-004, by and between the City of Ontario and LHC Ontario Holdings, LLC, recorded with the San Bernardino County Recorder's Office on January 18, 2022, as Doc# 2022-0024857 and subsequent amendments to said DA.
3. The applicant/developer shall submit a copy of easement or agreement to construct the proposed v-ditch beyond the project northern limits within the north adjacent tract (APN: 0218-131-33) and within the SCE Easement (Lot N).
4. The applicant/developer shall be responsible to:
  - a. Design and construct curb return radii in accordance with City of Ontario Standard Drawing No. 1106.

Project File No. PDEV22-038 (Related to PMTT21-011, TM-18916)

Project Engineer: Angela Truong

DAB Date: 5/15/2023

- b. Install a temporary AC berm transition on east side of Kinglet Avenue between "A" Street to Crane Court due to existing telecom equipment remaining in place.
  - c. The Lot "K" driveway at Dolomite Street shall be restricted to right-in / right-out access only due to its proximity to the roundabout. See City of Ontario Traffic and Transportation Guidelines, **Figure 1: Roundabout Layout Detail** for striping requirements on roundabout approaches. Parking is restricted on approaches to roundabouts.
  - d. Install smart nodes on all new street light fixtures along project frontage on Archibald Avenue, Kinglet Avenue, Dolomite Street, and "A" Street.
5. Refer to Exhibit A for Ontario Municipal Utilities Company (OMUC) specific Conditions of Approval.





## CITY OF ONTARIO MEMORANDUM



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**DATE:** April 20, 2023  
**TO:** Angela Truong, Engineering Department  
**CC:** Jocelyn Torres, Planning Department  
**FROM:** Heather Young, Utilities Engineering Division  
Christy Stevens, Utilities Engineering Division  
**SUBJECT:** DPR#3 - Utilities Conditions of Approval (COA) (#9023)  
**PROJECT NO.:** PDEV22-038

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**BRIEF DESCRIPTION:**

*A Development Plan to construct a 36 single-family 4-pack dwellings on approximately 11.05 acres of land located near the southwest corner of Archibald Avenue and Chino Avenue, within the RD-Alley loaded land use district of the Countryside Specific Plan (APN(s): 0218-131-34). Related File(s): PMTT21-011*

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### OMUC UTILITIES ENGINEERING DIVISION CONDITIONS OF APPROVAL

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**CONDITIONS OF APPROVAL:** *The Ontario Municipal Utilities Company (OMUC) Utilities Engineering Division recommends this application for approval subject to the Conditions of Approval outlined below and compliance with the City's Design Development Guidelines, Specifications Design Criteria, and City Standards. The Applicant shall be responsible for the compliance with and the completion of all the following applicable Conditions of Approval prior to the following milestones and subject to compliance with City's Design Development Guidelines, Specifications Design Criteria, and City Standards:*

**General Conditions:**

1. **Standard Conditions of Approval:** Project shall comply with the requirements set forth in the Amendment to the Standard Conditions of Approval for New Development Projects adopted by the City Council (Resolution No. 2017-027) on April 18, 2017, or as amended or superseded by Council Resolution; as well as project-specific conditions/requirements as outlined below.

***Prior to Issuance of Any Permits (Grading, Building, Demolition and Encroachment), unless other timeline milestones are specified by individual conditions below, the Applicant Shall:***

***General Conditions (Section 2.A, Other conditions): The Applicant shall comply with the following:***

2. **Inherited Requirements and Conditions of Approval:** This project is subject to all the Requirements and Conditions of Approval of PSP04-001 Countryside Specific Plan and PMTT21-011 (TM-18916), as amended.



**ENGINEERING DEPARTMENT  
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Broadband Operations & Investment and Revenue Resources Department Conditions Incorporated)

<input type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES	<input checked="" type="checkbox"/> TRACT MAP
<b>PROJECT FILE NO. TM-18916</b>  <b>RELATED FILE NO(S). PMTT21-011</b>		
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__		

**CITY PROJECT ENGINEER & PHONE NO:** Michael Bhatanawin, P.E. (909) 395-2130

**CITY PROJECT PLANNER & PHONE NO:** Alexis Vaughn (909) 395-2416

**DAB MEETING DATE:** November 15, 2021

**PROJECT NAME / DESCRIPTION:** TM-18916, a Tentative Tract Map to subdivide 11.05 acres of land into 36 lots within the Low Density Residential land use district of the Countryside Specific Plan

**LOCATION:** Southwest corner of Archibald Avenue and Chino Avenue

**APPLICANT:** LHC Ontario Holdings, LLC

**REVIEWED BY:** Raymond Lee      11/3/21  
 Raymond Lee, P.E.      Date  
 Assistant City Engineer

**APPROVED BY:** [Signature]      11-3-21  
 Khoi Do, P.E.      Date  
 City Engineer



**THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.**

**1. PRIOR TO FINAL MAP APPROVAL, APPLICANT SHALL:** Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:** 
  - A. An additional 19 feet from the ultimate right-of-way along the project frontage on the west side of Archibald Ave for a 45 feet neighborhood edge**
  - B. "A" Street to the ultimate full street right-of-way width of 60 feet from Kinglet Ave to Archibald Ave**

**Property line corner 'cut-back' required at the intersection of:**

  - A. Kinglet Ave & "A" St**
  - B. Archibald Ave & "A" St**
  
- 1.02 Dedicate to the City of Ontario, the following easement(s):** 
  - A. 20 feet wide easement for emergency access purposes over all private alleys**
  - B. 20 feet wide easement for public utility purposes over all private alleys**
  
- 1.03 Restrict vehicular access to the site as follows:** \_\_\_\_\_
  
- 1.04 Vacate the following street(s) and/or easement(s):** 
  - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.**
  
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.**
  
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.**
  
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).**



- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Financial Services Department at (909) 395-2124 regarding this requirement.
- (1) \_\_\_\_\_
- (2) \_\_\_\_\_
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: [www.ci.ontario.ca.us](http://www.ci.ontario.ca.us)) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.
- 1.13 Ontario Ranch Developments:
- 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
- 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
- 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions:
- A. The Tract Map shall comply with the approved Countryside Specific Plan, the Development Agreement, the Conditions of Approval for Tentative Tract Map No. 18916 and the Conditions of Approval for this Tentative Tract Map.
- B. Applicant/developer shall obtain all off-site right-of-way/easements necessary to construct the required public improvements identified within Section 2 of these Conditions of Approval.

**2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:**

**A. GENERAL**  
**( Permits includes Grading, Building, Demolition and Encroachment )**

- 2.01 Record Tract Map No. 18916 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a PDF of the recorded map to the City Engineer's office.



- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per \_\_\_\_\_.
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of March 4, 1972.
- 2.05 Apply for a:
  - Certificate of Compliance with a Record of Survey;
  - Lot Line Adjustment (Record a Conforming Deed with the County of San Bernardino within six months of the recordation of the Lot Line Adjustment to conform the new LLA legal description. Submit a copy of the recorded Conforming Deed to the Engineering Department.);
  - Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan ( WQMP), as applicable to the project.
- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at [http://geotracker.waterboards.ca.gov/profile\\_report?global\\_id=T10000004658](http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658).
- 2.08 Submit a soils/geology report.
- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
  - State of California Department of Transportation (Caltrans)
  - San Bernardino County Road Department (SBCRD)
  - San Bernardino County Flood Control District (SBCFCD) – for any Improvements encroaching into their right-of-way
  - Federal Emergency Management Agency (FEMA)
  - Cucamonga Valley Water District (CVWD) for sewer/water service
  - United States Army Corps of Engineers (USACE)
  - California Department of Fish & Game
  - Inland Empire Utilities Agency (IEUA) – for connection to their 33" sewer main on Archibald Ave
  - Other: Southern California Edison (SCE) – for any improvements encroaching into their easements
- 2.10 Dedicate to the City of Ontario the right-of-way described below:
 

\_\_\_\_\_ feet on \_\_\_\_\_

Property line corner 'cut-back' required at the intersection of \_\_\_\_\_ and \_\_\_\_\_.



- 2.11 Dedicate to the City of Ontario the following easement(s): \_\_\_\_\_
  
- 2.12 Vacate the following street(s) and/or easement(s): 
  - A. All interfering on-site easements shall be quitclaimed, vacated, and/or submit non-interference letter from affected owner/utility company.
  
- 2.13 **Ontario Ranch Developments:** 
  - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
  - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
  - 3) **Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall.**
  
- 2.14 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at \_\_\_\_\_% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.
  
- 2.15 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
  
- 2.16 **Pay all Development Impact Fees (DIF) to the Building Department. Final fee shall be determined based on the approved site plan.**
  
- 2.17 Other conditions: \_\_\_\_\_



**B. PUBLIC IMPROVEMENTS**  
 (See attached Exhibit 'A' for plan check submittal requirements.)

2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Archibald Ave	Kinglet Ave (A)	"A" St (B)	Dolomite St (C)
<b>Curb and Gutter</b>	<input checked="" type="checkbox"/> New; 48 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New; 18 ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
<b>AC Pavement</b>	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> Widen 13 additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> New	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> New	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> New
<b>PCC Pavement (Truck Route Only)</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
<b>Drive Approach</b>	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Sidewalk</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>ADA Access Ramp</b>	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace
<b>Parkway</b>	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation) <input checked="" type="checkbox"/> Multi-purpose trail <input checked="" type="checkbox"/> Neighborhood edge	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation) <input checked="" type="checkbox"/> Multi-purpose trail	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)



Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input checked="" type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____





Improvement	All Alleys (Private)	Street 2	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; __ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; __ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; __ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; __ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen __ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen __ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen __ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen __ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service



Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation	<input type="checkbox"/> New <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

**Specific notes for improvements listed in item no. 2.17, above:**

- A. Limits are from the northerly tract limits to Dolomite St. If, at the time of development of TM 18916, Kinglet Ave has not been constructed by TM 17449 and TM 17450, this project will be required to construct a 14' circulation lane with a 5' paved shoulder on the west side of Kinglet Ave.**
- B. Both north and south sides from Kinglet Ave to Archibald Ave.**
- C. Limits are from Kinglet Ave to Archibald Ave. If, at the time of development of TM 18916, Dolomite St has not been constructed by TM 17450, this project will be required to construct a 14' circulation lane with a 5' paved shoulder on the south side of Dolomite St.**
- D. All master planned utilities and infrastructure shall be designed and installed to the ultimate condition**

2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): \_\_\_\_\_



- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide  water service  sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \_\_\_\_\_, for undergrounding of utilities in accordance with Section 7-7.302.e of the City's Municipal Code.
- 2.22 Other conditions: \_\_\_\_\_

**C. SEWER**

- 2.23 A 33 inch sewer main (IEUA) is available for connection by this project in Archibald Ave
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 Other conditions: 
  - A. **Sewer Infrastructure:** This development shall construct an 8" sewer main in "A" Street from Kinglet Avenue to Archibald Avenue; an 8" sewer main in Kinglet Avenue from "A" Street to Dolomite Street; and an 8" sewer main in Dolomite Street from Kinglet Avenue to Archibald Avenue.
  - B. **Sewer Laterals:** Sewer shall be private in the private alleys. Construct a cleanout at the back of the ROW of each alley for each sewer lateral, as shown on the Utility System Map dated 10/21/2021.

**D. WATER**

- 2.27 A 12 inch water main is available for connection by this project in Archibald Ave. (Ref: Water plan bar code: W10157)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately \_\_\_\_\_ feet away.
- 2.29 Other conditions: 
  - A. **Water Infrastructure:** This development shall construct an 8" potable water main in "A" Street connecting to the potable water mains in Kinglet Avenue and Archibald Avenue; a 12" potable water main in Kinglet Avenue from the northernly tract boundary to Dolomite Street; and a 12" potable water main in Dolomite Street from Kinglet Avenue connecting to the water main in Archibald Avenue.
  - B. **Water Services:** Bank two meters on each side of the alley for each 4-pack, as shown on the Utility System Map (USM) dated 10/21/2021.
  - C. **Fire Hydrants:** Construct three fire hydrants along Archibald Avenue, two fire hydrants along Dolomite Street, three fire hydrants along Kinglet Avenue, and two fire hydrants along "A" Street, as shown on the USM dated 10/21/2021. The maximum spacing between fire hydrants shall be 300'.

**E. RECYCLED WATER**

- 2.30 A 24 inch recycled water main is available for connection by this project in Archibald Ave. (Ref: Recycled Water plan bar code: P10154)



- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
  - 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
  - 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.
- Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.**
- 2.34 Other conditions: 
    - A. **Recycled Water Infrastructure:** This development shall construct an 8" recycled water main in Kinglet Avenue from the northerly tract boundary to Dolomite Street and an 8" recycled water main in Dolomite Street from Kinglet Avenue to Archibald Avenue.

**F. TRAFFIC / TRANSPORTATION**

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer: 
  - 1. On-site and off-site circulation
  - 2. Traffic level of service (LOS) at 'build-out' and future years
  - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions: 
  - A. The proposed multi-purpose trail along the eastside of Kinglet Avenue must include 5-foot sidewalk and 8-foot multipurpose trail that runs parallel to the curb. The proposed multi-purpose trail must either continue into the northerly tract or transition back to a sidewalk if the northerly tract does not include a multi-purpose trail.
  - B. Design and construct public street lights and a potential new service along its project frontages of Archibald Avenue, Kinglet Avenue, "A" Street, and Dolomite Street. Street lighting shall be LED-type and in accordance with City's Traffic and Transportation Design Guidelines.
  - C. Design and construct the necessary pavement and striping transitions from existing roadway conditions to the widened roadway portions along Archibald Avenue. Striping improvements shall include the removal existing interim signing and striping beyond the project frontage limits on Archibald Avenue and the installation of ultimate signing and striping.
  - D. Design and construct traffic signal at the following intersection:
    - i. Archibald Avenue and Dolomite Street
 The new traffic signal shall include video detection, fiber optic cable and conduit, communication equipment, emergency vehicle preemption systems and bicycle detection to the satisfaction of the City Engineer. All new signal equipment shall be installed at its ultimate location, unless precluded by right-of-way limitations.
  - E. The "A" Street entry to the tract from Archibald Avenue shall be restricted to right-in/right-out access only.
  - F. Design and construct a bus pad to serve future stops on the west side of Archibald Avenue of the approach at the signalized intersection of Archibald Avenue and Dolomite Street. The bus pad shall be designed in accordance with Omnitrans requirements and to the satisfaction of the City Engineer.
  - G. Archibald Avenue shall be signed "No Stopping Anytime".
  - H. If, at the time of development, Dolomite St has not been constructed west to TM 17450,



- the Applicant/Developer shall terminate Dolomite Street west of the roundabout as a temporary dead end condition per City of Ontario Standard Drawing No. 1310.
- I. If, at the time of development, Kinglet Avenue has not been constructed north to TM 17449 the applicant/developer shall terminate Kinglet Avenue as a temporary dead end condition north of Lot 2 per City of Ontario Standard Drawing No. 1310.
  - J. If, at the time of development, Brockram Drive has not been constructed south to TM 17450 the applicant/developer shall terminate Brockram Drive as a temporary dead end condition per City of Ontario Standard Drawing No. 1310.
  - K. All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.
  - L. The Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to start of signing and striping, traffic signal and street lighting design in preparation for the plan check stage.
  - M. Parking shall be restricted on Dolomite Street and Kinglet Avenue approaching the roundabout per the Traffic and Transportation Guidelines.

**G. DRAINAGE / HYDROLOGY**

- 2.38 A \_\_\_\_\_ inch storm drain main is available to accept flows from this project in \_\_\_\_\_.   
 (Ref: Storm Drain plan bar code: \_\_\_\_\_)
- 2.39 Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 Other conditions: 
  - A. Construct an 18" storm drain line on "A" St from Kinglet Ave to private alleys G and H.
  - B. The proposed drainage swale on the southerly edge of Lot N cannot be removed and will need to be maintained by the property owner/HOA in perpetuity. An alternative to this is to provide a permanent storm drain solution (i.e. underground pipe).
  - C. Prior to issuance of a grading permit provide one of the following:
    - I. Evidence (i.e. acknowledgment letter) from the adjacent property owner to the north (APN: 0218-131-33) showing that their property will not flood this project site.
    - II. Install a permanent storm drain solution (i.e. underground pipe) that will accept the flows upstream of the project.
  - D. If, at the time of development of TM 18916, the storm drain laterals required for TM 17449 and the westerly side of Kinglet Ave have not been constructed, this project shall be required to provide stub outs for future connections.
  - E. If, at the time of development of TM 18916, the storm drain infrastructure required for TM 17450 has not been constructed, this project shall construct all applicable storm drain improvements set forth in the COA for TM 17450.



**H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)**

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.  
 If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.  
 Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbccounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 Other conditions: \_\_\_\_\_

**J. SPECIAL DISTRICTS**

- 2.48 **File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Investment and Revenue Resources at (909) 395-2341 to initiate the CFD application process.**
- 2.49 Other conditions: \_\_\_\_\_

**K. FIBER OPTIC**

- 2.50 A \_\_\_\_\_ fiber optic line is available for connection by this project in \_\_\_\_\_ (Ref: Fiber Optic plan bar code: \_\_\_\_\_)
- 2.51 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole. Limits of work are generally located along the project frontages of Archibald Ave, Kinglet Ave, "A" St, Dolomite St and all private alleys.**
- 2.52 **Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Broadband Operations Department at (909) 395-2000, regarding this requirement.**



**L. INTEGRATED WASTE**

- 2.53 Onsite solid waste shall be designed in accordance with the City's Refuse & Recycling Planning Manual located at:   
<https://www.ontarioca.gov/OMUC/IntegratedWaste>
- 2.54 Other conditions: 
  - A. **Integrated Waste and Organics Recycling:** This site shall comply with the requirements of State Assembly Bill AB 1826 and AB 341 and the Integrated Waste Department. Organic waste shall be diverted and collected separately from recycling and other refuse wastes.
  - B. **Final Solid Waste Handling Plan (SWHP):** Submit a Final SWHP as part of the precise grading plans submittal that conforms to the Conceptual 'Parking & Trash Study' sheet dated 10/10/2021 and meets the City's SWHP requirements for Integrated Waste Department's review and approval. The Final SWHP shall be included in the CC&R's.
  - C. **Integrated Waste Collections:** The Developer shall provide all buyers an informational disclosure with map exhibit showing the designated can placement locations for collections for their units. This informational disclosure and map exhibit shall be submitted with the Precise Grading Plan for OMUC review and approval.

**3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:**

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage. 
  - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
  - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
  - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 Ontario Ranch Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).



**4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:**

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall include a completed Acceptance and Bond Release Checklist, state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**
- 4.04 Submit record drawings (PDF) for all public improvements identified within Section 2 of these Conditions of Approval.**





## **EXHIBIT 'A'**

### **ENGINEERING DEPARTMENT First Plan Check Submittal Checklist**

**Project Number: PMTT21-011, and/or Tract Map No. 18916**

**The following items are required to be included with the first plan check submittal:**

1.  **A copy of this check list**
2.  **Payment of fee for Plan Checking**
3.  **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4.  **One (1) copy of project Conditions of Approval**
5.  **Include a PDF (electronic submittal) of each required improvement plan at every submittal.**
6.  **Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).**
7.  **Three (3) sets of Public Street improvement plan with street cross-sections**
8.  **Four (4) sets of Public Water Improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9.  **Four (4) sets of Recycled Water Improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)**
10.  **Four (4) sets of Public Sewer improvement plan**
11.  **Five (5) sets of Public Storm Drain improvement plan**
12.  **Three (3) sets of Public Street Light improvement plan**
13.  **Three (3) sets of Signing and Striping Improvement plan**
14.  **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15.  **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16.  **Three (3) sets of Traffic Signal Improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.**
17.  **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18.  **One (1) copy of Hydrology/Drainage study**
19.  **One (1) copy of Soils/Geology report**
20.  **Payment for Final Map processing fee**



- 21.  **Three (3) copies of Final Map**
- 22.  **One (1) copy of approved Tentative Map**
- 23.  **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24.  **One (1) copy of Traverse Closure Calculations**
- 25.  **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 26.  **Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use**
- 27.  **Other:** \_\_\_\_\_