

CITY OF ONTARIO PLANNING COMMISSION/ HISTORIC PRESERVATION MEETING AGENDA

April 28, 2020

**Ontario City Hall
303 East "B" Street, Ontario, California 91764**

6:30 PM

SPECIAL AND URGENT NOTICE ELIMINATING IN-PERSON PUBLIC PARTICIPATION AT CITY OF ONTARIO PLANNING COMMISSION MEETINGS

In accordance with the Governor's Declarations of Emergency for the State of California (Executive Orders N-25-20 and N-29-20) and the Governor's Stay at Home Order (Executive Order N-33-20), the Ontario Planning Commission Meetings are being conducted via teleconference to limit in-person attendance at the upcoming meeting of the City of Ontario Planning / Historic Preservation Commission.

Members of the public may utilize alternative measures established by the City of Ontario to view the Planning Commission meetings and/or to address the Commissioners.

The meeting will be internet live streamed:
www.ontarioca.gov/Agendas/PlanningCommission

We appreciate your understanding during this unprecedented time of social distancing under the Stay at Home Order. These procedures may be modified in the future as social and public gathering protocols change.

WELCOME to a meeting of the Ontario Planning / Historic Preservation Commission.

- All documents for public review are on file with the Records Management/City Clerk's Department located at 303 East B Street, Ontario, CA 91764.
- Anyone wishing to provide public comment or to address the Commission have been provided alternative measures including U.S. mail, email, a website comment form, and the ability to dial in and record a 5 minute voicemail. All public comments received by the established deadline for this meeting will be included as part of the official meeting record.
- In accordance with State Law, remarks during public comment are to be limited to subjects within the Planning Commission's jurisdiction. Remarks on other agenda items will be limited to those items.

ROLL CALL

DeDiemar __ Downs __ Gage __ Gregorek __ Reyes __ Ricci __ Willoughby __

PLEDGE OF ALLEGIANCE TO THE FLAG

ANNOUNCEMENTS

- 1) Agenda Items
- 2) Commissioner Items

PUBLIC COMMENTS

Citizens wishing to address the Planning/Historic Preservation Commission on any matter that is not on the agenda may do so at this time. Please state your name and address clearly for the record and limit your remarks to five minutes.

Please note that while the Planning/Historic Preservation Commission values your comments, the Commission cannot respond nor take action until such time as the matter may appear on the forthcoming agenda.

CONSENT CALENDAR ITEMS

All matters listed under CONSENT CALENDAR will be enacted by one summary motion in the order listed below. There will be no separate discussion on these items prior to the time the Commission votes on them, unless a member of the Commission or public requests a specific item be removed from the Consent Calendar for a separate vote. In that case, the balance of the items on the Consent Calendar will be voted on in summary motion and then those items removed for separate vote will be heard.

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of March 26, 2020, approved as written.

A-02. ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-040:

A Development Plan to construct one industrial building totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1050-201-02) **submitted by Alere Property Group LLC.**

PUBLIC HEARING ITEMS

For each of the items listed under PUBLIC HEARING ITEMS, the public will be provided an opportunity to speak. After a staff report is provided, the chairperson will open the public hearing. At that time the applicant will be allowed five (5) minutes to make a presentation on the case. Members of the public will then be allowed five (5) minutes each to speak. The Planning/Historic Preservation Commission may ask the speakers questions relative to the case and the testimony provided. The question period will not count against your time limit. After all persons have spoken, the applicant will be allowed three minutes to summarize or rebut any public testimony. The chairperson will then close the public hearing portion of the hearing and deliberate the matter.

HISTORIC PRESERVATION COMMISSION ITEMS

- B. TWENTIETH ANNUAL MODEL COLONY AWARDS FILE NO. PHP20-004:** A request for the Historic Preservation Commission to accept the nominations for the Twentieth Annual Model Colony Awards; **submitted by City of Ontario. City Council presentation of Awards.**

PLANNING COMMISSION ITEMS

- C. ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP, AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-013 (PM 20157) AND PDEV19-050:** A Tentative Parcel Map (File No. PMTT19-013) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district. The environmental impacts of this project were previously analyzed with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-232-21) **submitted by Herdman Architecture and Design.**

1. CEQA Determination

No action necessary – use of previous EIR

2. File No. PMTT19-013 (PM 20157) (Parcel Map)

Motion to Approve/Deny

3. File No. PDEV19-050 (Development Plan)

Motion to Approve/Deny

- D. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA19-009**: An Amendment (File No. PSPA19-009) to the Piemonte Overlay of the Ontario Center Specific Plan, to modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking up to a maximum of 50% of the required parking. The Ontario Center Specific Plan-Piemonte Overlay encompasses 84.43 acres of land, generally located north of Concours Street, south of Fourth Street, west of Via Alba, and east of Haven Avenue. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16) **submitted by LCD Residential at Ontario, LLC. City Council action is required.**

1. CEQA Determination

No action necessary – use of previous Mitigated Negative Declaration

2. File No. PSPA19-009 (Specific Plan Amendment)

Motion to recommend Approval/Denial

- E. ENVIRONMENTAL ASSESSMENT, TENTATIVE TRACT MAP AND DEVELOPMENT PLAN REVIEW FOR FILE NOS. PMTT19-016 (TT 20308) AND PDEV19-054**: A Tentative Tract Map (File No. PMTT19-016) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes in conjunction with a Development Plan (File No. PDEV19-054) to construct 72 multi-family residential units (Townhomes). The project is located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Ontario Center Specific Plan-Piemonte Overlay. The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 0210-204-40) **submitted by The New Home Company Southern California LLC.**

1. CEQA Determination

No action necessary – use of previous Mitigated Negative Declaration

2. File No. PMTT19-016 (TT 20308) (Tract Map)

Motion to Approve/Deny

3. File No. PDEV19-054

Motion to Approve/Deny

F. ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA19-004:

An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140), certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 0238-121-75) **submitted by MIG. This item was continued from the March 26, 2020 Planning Commission meeting. City Council action is required.**

1. CEQA Determination

Motion to recommend Approval/Denial of an Addendum to a previous EIR

2. File No. PSPA19-004 (Specific Plan Amendment)

Motion to recommend Approval/Denial

G. ENVIRONMENTAL ASSESSMENT AND REVIEW FOR FILE NO. PSPA19-007:

An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure the residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 19; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). The environmental impacts of this project were analyzed in an Addendum to the Parkside Specific Plan (File No. PSP03-002) Environmental Impact Report (SCH# 2004011008) certified by the City Council on September 5, 2006. This application is consistent with the previously adopted EIR and introduces no new significant environmental impacts. All previously adopted mitigation measures shall be a

condition of project approval and are incorporated herein by reference. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP). (APN(s): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC. City Council action is required.**

This Item is being requested to be continued to the May 26, 2020 meeting.

1. File No. PSPA19-007 (Specific Plan Amendment)

Motion to continue to the May 26, 2020 Planning Commission meeting

MATTERS FROM THE PLANNING/HISTORIC PRESERVATION COMMISSION

- 1) Old Business
 - Reports From Subcommittees
 - Historic Preservation (Standing): Did not meet this month
- 2) New Business
- 3) Nominations for Special Recognition

DIRECTOR'S REPORT

- 1) Monthly Activity Report

If you wish to appeal any decision of the Planning/Historic Preservation Commission, you must do so within ten (10) days of the Commission action. Please contact the Planning Department for information regarding the appeal process.


If you challenge any action of the Planning/Historic Preservation Commission in court, you may be limited to raising only those issues you or someone else raised at the public hearing described in this notice, or in written correspondence delivered to the Planning/Historic Preservation Commission at, or prior to, the public hearing.



I, Gwen Berendsen, Administrative Assistant, of the City of Ontario, or my designee, hereby certify that a true, accurate copy of the foregoing agenda was posted on **April 24, 2020**, at least 72 hours prior to the meeting per Government Code Section 54954.2 at 303 East "B" Street, Ontario.



Gwen Berendsen, Secretary Pro Tempore



Cathy Wahlstrom, Planning Director
Planning/Historic Preservation
Commission Secretary

**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION SPECIAL MEETING**

MINUTES

March 26, 2020

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**CITY OF ONTARIO PLANNING COMMISSION/
HISTORIC PRESERVATION SPECIAL MEETING**

MINUTES

March 26, 2020

SPECIAL MEETING: City Hall, 303 East B Street
Called to order by Chairman Willoughby at 6:30 PM

Mr. Murphy stated that no emails or comments were received by the 5:00 PM deadline.

COMMISSIONERS

Present: Chairman Willoughby, Vice-Chairman DeDiemar, Downs, Gage, Gregorek, Reyes and Ricci

Absent: None

OTHERS PRESENT: Executive Director Development Agency Murphy, Planning Director Wahlstrom, Assistant Planning Director Zeledon, City Attorney Graham, and Planning Secretary Berendsen

PLEDGE OF ALLEGIANCE TO THE FLAG

The Pledge of Allegiance was led by Executive Director Development Agency Murphy.

ANNOUNCEMENTS

Ms. Wahlstrom stated that Item D is being requested to be continued to the next meeting.

Mr. Willoughby stated that the nominations for Planning Commission officers will be postponed to a future meeting.

PUBLIC COMMENTS

No one responded from the public.

CONSENT CALENDAR ITEMS

A-01. MINUTES APPROVAL

Planning/Historic Preservation Commission Minutes of February 25, 2020, approved as written.

A-02. ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP EXTENSION REVIEW FOR FILE NO. PMTT16-013 (TM 20050): A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT

20050), subdividing 3.47 acres of land, located at 1910 South Euclid Avenue, within the MDR-18 (Medium Density Residential - 11.1 to 18.0 DUs/acre) and EA (Euclid Avenue) Overlay zoning districts. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15304 (Class 4, Minor Alterations to Land) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT), and was evaluated and found to be consistent with the policies and criteria of the ONT Airport Land Use Compatibility Plan; (APNs: 1050-381-04, 1050-381-05, 1050-381-06, 1050-381-07, 1050-381-08 and 1050-381-09) **submitted by 1902 Euclid Property LLC.**

- A-03. ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP EXTENSION REVIEW FOR FILE NO. PMTT17-006 (TT 19832):** A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT 19832), subdividing a 2.7 acre site located at the northeast corner of Euclid Avenue and Riverside Drive, within the CN (Neighborhood Commercial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1051-614-08) **submitted by Creative Design Associates.**

PLANNING COMMISSION ACTION

It was moved by Reyes, seconded by Gregorek, to approve the Consent Calendar including the Planning Commission Minutes of February 25, 2020, as written, File No. PMTT16-013, Time Extension, and File No. PMTT17-006, Time Extension, subject to conditions of approval. The motion was carried 7 to 0, with Downs abstaining from Item A-01, as he was not at this meeting.

PUBLIC HEARING ITEMS

Ricci recused himself from Item B, as he works for AT & T.

- B. ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV19-020 AND PCUP19-018:** A Development Plan (File No. PDEV19-020) and Conditional Use Permit (File No. PCUP19-018) to construct a 65-foot tall stealth wireless telecommunication facility (monopine) on 1.9 acres of land generally located on the south side of Riverside Drive, approximately 180 feet west of Sultana Avenue, at 7247 East Riverside Drive, within the SP(AG) (Specific Plan/Agriculture Overlay) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the State CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1052-071-05) **submitted by AT&T.**

Assistant Director Zeledon presented the staff report. He described the project location, surrounding area, landscaping, access and parking, and elevations. He stated that staff is recommending the Planning Commission approve File Nos. PCUP19-018 and PDEV19-020, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Reyes wanted to know if the renderings realistically reflect the density on the finished product.

Mr. Zeledon stated that the renderings are pretty close as we have many within the city and the conditions of approval for the project, include a branch count.

Mr. Gregorek wanted to know if there would be any tapering of the branches.

Mr. Zeledon stated yes, the applicant will work with landscape planner Richardson to make sure it looks realistic, but that only the top portion would be visible.

Mr. Gregorek wanted clarification regarding the future zoning of residential and the setbacks.

Mr. Zeledon stated the property has a medium density zoning and there is a conditional use permit for it, but future development would be required to work with the applicant to relocate the monopine and a written agreement must be signed and executed before development can begin.

Mr. Gregorek wanted clarification that the developer would have to conform to the written agreement.

Mr. Zeledon stated that was correct.

Mr. Gregorek wanted to know what the setback is from residential development.

Mr. Zeledon stated that within 300 feet a conditional use permit is required, but there is no minimum setback.

Mr. Gregorek clarified there was no minimum setback.

Mr. Zeledon stated that is correct, but typically we require a 25 – 35-foot setback, but this is agricultural overlay area so there are no setback requirements and it will be addressed at the time of development.

Mr. Gregorek wanted to clarify that we don't have anything in writing.

Mr. Zeledon stated the conditional use permit has a 5-year term on it where we will reevaluate it.

Mr. Gage wanted to know if the surrounding properties were noticed.

Mr. Zeledon stated yes, this item was properly advertised, and notices were sent out to properties within a 300-foot radius and no comments were received.

Mr. Downs wanted to know if there were any plans for lighting on the road on the west of the property.

Mr. Zeledon stated no, the site would be accessed by a 24-foot wide drive isle and access road is on the adjacent parcel and not part of the project.

Ms. DeDiemar wanted to know if the property owner resides on the property.

Mr. Zeledon stated yes.

Ms. DeDiemar wanted to know if they had any objections to it being modified.

Mr. Zeledon stated they had signed the application for the project.

PUBLIC TESTIMONY

Mr. Murphy stated no questions or responses were received.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Reyes stated he thought Mr. Gregorek's question was very appropriate and he has comfort that we will be looking at it again in 5 years, but suggested that in the future as we look at zoning of specific residential areas we look at shorter times in the future.

Mr. Willoughby wanted clarification that the CUP has a condition that notifies the developer that this site antenna may have to be relocated based upon the future development.

Mr. Zeledon stated it allows us to review the application every 5 years to determine that we want to extend it and prior to development the applicant must enter into an agreement with us and agree with the conditions and terms. He also stated that this is on the west side and most likely won't be developed for 10 – 15 years.

Mr. Willoughby wanted clarification that if development comes in a year from now, we don't have to wait for the 5-year period.

Mr. Zeledon stated that is correct.

PLANNING COMMISSION ACTION

It was moved by Gage, seconded by Downs, to adopt a resolution to approve the Conditional Use Permit, File No., PCUP19-018, and the Development Plan, File No. PDEV19-020, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, and Willoughby; NOES, none; RECUSE, Ricci; ABSENT, none. The motion was carried 6 to 0.

- C. **ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN REVIEW AND VARIANCE FOR FILE NOS. PDEV19-056 AND PVAR19-007:** A Development Plan (File No. PDEV19-056) to construct one industrial building totaling 71,800 square feet on 3 acres of land in conjunction with a Variance (File No. PVAR19-007) to reduce the

south property line building setback from 25 feet to 5 feet, located on the west side of Milliken Avenue, approximately 300 feet north of Greystone Drive, within the Light Industrial land use district of the Bridgestone/Firestone Industrial Park Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15305 (Class 5, Minor Alterations in Land Use) and Section 15332 (Class 32, Infill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1083-351-09) **submitted by David L. Ball.**

Assistant Director Zeledon presented the staff report. He described the location and surrounding area, and site plan including parking, landscaping, architecture design and access. He described the reason for the variance. He stated that staff is recommending the Planning Commission approve File Nos. PVAR19-007 and PDEV19-056, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval.

Mr. Downs wanted clarification that the property to the south wouldn't be developed.

Mr. Zeledon stated the property to the south is Delhi Fly preserve and can't be developed.

Mr. Gage wanted clarification regarding street parking on Greystone Drive.

Mr. Zeledon stated all the parking will be contained on the site.

Mr. Reyes wanted to know if the south elevation of the building landscaping would be a 6-foot high tubular steel fence with tall vertical shrubs, and if staff could work with the applicant to make sure to use the appropriate shrub for the height needed.

Mr. Zeledon stated yes, they can work with our landscape planner and we keep it to a 6-foot landscape which would allow them to maintain the building.

Mr. Willoughby wanted to know if the trees can extend to the end of the building.

Mr. Zeledon stated they could work with the applicant to get the right tree shrubs and spacing.

Mr. Willoughby wanted to clarify that the buildings to the north have on parking in the front of the building and this will have landscaping, which will give it a better look.

Mr. Zeledon stated that is correct.

PUBLIC TESTIMONY

Mr. Murphy stated no questions or responses were received.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

Mr. Gage stated he appreciates the extra parking spaces.

Mr. Reyes stated he was satisfied with landscaping along Milliken Ave. and the trees in the median. He would like staff to continue to work with the applicant on the south side for lower screening.

PLANNING COMMISSION ACTION

It was moved by Reyes, seconded by Gregorek, to adopt a resolution to approve the Variance, File No., PVAR19-007 and the Development Code, File No., PDEV19-056, subject to conditions of approval. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.

- D. **ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO. PSPA19-004:** An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This Application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0238-121-75) **submitted by MIG. City Council action is required.**

This Item is being requested to be continued to the April 28, 2020 meeting.

PUBLIC TESTIMONY

Mr. Murphy stated no questions or responses were received.

As there was no one else wishing to speak, Chairman Willoughby closed the public testimony

PLANNING COMMISSION ACTION

It was moved by Downs, seconded by Gage, to continue the Specific Plan Amendment, File No., PPSPA19-004 to the April 28, 2020 Planning Commission meeting. Roll call vote: AYES, DeDiemar, Downs, Gage, Gregorek, Reyes, Ricci and Willoughby; NOES, none; RECUSE, none; ABSENT, none. The motion was carried 7 to 0.

MATTERS FROM THE PLANNING COMMISSION

Old Business Reports From Subcommittees

Historic Preservation (Standing): This subcommittee met on March 12, 2020

Mr. Gregorek stated they reviewed a house for the Ontario Register of Historic Resources and had discussions regarding the Emmons Building and Granada Theater, Bank of Italy adaptive reuse project, Armsley Square lamp post restoration and Jay Littleton Ballpark.

Development Code Review (Ad-hoc): This subcommittee met on March 12, 2020.

Mr. Reyes stated that staff did a quick presentation/update on the Downtown District areas.

Zoning General Plan Consistency (Ad-hoc): This subcommittee did not meet.

New Business

Mr. Willoughby stated that nominations for Planning Commission officers will be postponed until a future meeting due to the COVID-19 circumstances.

NOMINATIONS FOR SPECIAL RECOGNITION

None at this time.

DIRECTOR'S REPORT

Ms. Wahlstrom stated the Monthly Reports are in their packets.

ADJOURNMENT

Mr. Gregorek motioned to adjourn. The meeting was adjourned at 7:18 PM.

Secretary Pro Tempore

Chairman, Planning Commission



PLANNING COMMISSION STAFF REPORT

April 28, 2020

FILE NO: PDEV19-040

SUBJECT: A Development Plan to construct one industrial building totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district; (APN: 1050-201-02) **submitted by Alere Property Group LLC.**

PROPERTY OWNER: Cucamonga APG, LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve File No. PDEV19-040, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district, and is depicted in Figure 1: Project Location, below. The project site is presently developed with nine buildings totaling approximately 105,000 square feet. Land uses immediately surrounding the project site are all zoned IG (General Industrial) and are developed with manufacturing, warehouse and storage uses. The surrounding existing land uses, Policy Plan (General Plan) and zoning information are tabulated in the Technical Appendix Section of this report.

PROJECT ANALYSIS:

[1] Background — The project site was initially developed in the 1960s and a Conditional Use Permit (“CUP”), File No. PCUP08-030 was approved on October 27, 2008 by the Zoning Administrator to establish a metal manufacturing business that is currently operated by Simplex. The CUP approval allowed metal



Figure 1: Project Location

Case Planner:	Lorena Mejia
Planning Director Approval:	
Submittal Date:	7/11/2019

Hearing Body	Date	Decision	Action
DAB	4/20/2020	Approval	Recommend
PC	4/28/2020		Final
CC			

manufacturing, with approved processes that include powder coating, punching, bending, shearing, welding, and sawing of raw steel materials into finished goods for the construction markets. Simplex specializes in the manufacturing of engineered dowel structural units typically utilized in highway paving projects. Simplex is planning to continue operations until July 2020, after which the Applicant plans to move forward with the proposed project, demolishing all existing buildings/structures/walls to accommodate a new industrial building.

SCS Engineers completed a Phase I Environmental Site Assessment (Dated: April 24, 2019) and Phase II Soil Vapor Investigation (Dated: April 24, 2019) for the project site. The Phase 1 Assessment concluded that no further investigations were warranted or recommended based upon the scope and limitations of Federal EPA All Appropriate Inquiry Standards (40 CFR 312 and ASTM E1527-13) for evaluating environmental conditions of a property. However, due to past uses and operations on the project site a Phase II Assessment was completed, including 12 soil vapor samples collected throughout the project site, concluding that no further investigation was warranted or recommended. Any PCE (perchloromethane) concentrations found on-site were below the DTSC (California Department of Toxic Substances Control) recommended SLs (Screening Levels) for existing or future buildings under a residential/commercial/industrial land use scenario.

On April 20, 2020, the Development Advisory Board conducted a hearing to consider the subject Applications and concluded the hearing, voting to recommend that the Planning Commission approve the Application subject to conditions of approval, which have been included with the Planning Commission resolution.

[2] Site Design/Building Layout — Proposed, is the construction of a 211,358-square foot industrial building, having a floor area ratio (FAR) of 0.52. The rectangular building is centered on the project site. The front of the building and two office entries, located at the northeast and southeast corners of the building, are oriented to the east, facing Cucamonga Avenue. The building is setback approximately 46 feet from the north property line, approximately 46 feet from the south property line, approximately 125 feet from the west property line, and 78 feet from the east property line (Cucamonga Avenue). The project will provide the majority of its parking along the eastern property line, adjacent to the front of the building, in addition to a smaller parking area located at the southwest corner of the site (see Exhibit B—Site Plan, attached). A yard area, designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is centrally located on the west side (rear) of the proposed building, out of view from Cucamonga Avenue.

[3] Site Access/Circulation — The Project has two points of vehicular access from Cucamonga Avenue, including a 35-foot wide driveway located at the northeast corner of the site and a 35-foot wide driveway located at the southeast corner of the site that will be shared by both standard vehicles and tractor-trailers accessing the yard area and parking lot. A 26-foot to 35-foot wide drive-aisle is proposed that will surround the entire

building, connecting the two points of access. This will allow vehicles to enter the site at either entry point and continue northbound or southbound onto Cucamonga Avenue.

[4] Parking — The Project has provided off-street parking pursuant to the “Warehouse and Distribution” parking standards specified in the Development Code. The industrial building requires a total of 137 off-street parking spaces, and 141 spaces have been provided. In addition, a minimum of one tractor-trailer parking space for each 4 dock-high loading spaces is required to be provided. There are 23 dock-high loading doors proposed, requiring 6 tractor-trailer parking spaces and 25 spaces have been provided.

[5] Architecture — The proposed industrial warehouse building will be of concrete tilt-up construction. Architecturally, the building incorporates smooth-painted concrete, horizontal and vertical reveals, color blocking, wood paneling with walnut stained finish, clerestory windows with clear anodized aluminum mullions and solar blue glazing with a custom silk screen, steel canopies over the main office entries, and first and second story windows (see Exhibit C—Elevations, attached). The mechanical equipment for the building will be roof-mounted and obscured from public view by the parapet walls and, if necessary, equipment screens, which will incorporate design features consistent with the building architecture. Staff believes that the proposed Project illustrates the type of high-quality architecture promoted by the Development Code. This is exemplified through the use of the following:

- Articulation in the building footprint, incorporating a combination of recessed and popped-out wall areas
- Articulation in the building parapet/roof line, which serves to accentuate the building’s entries and breaks up large expanses of building wall
- A mix of exterior materials, finishes and fixtures
- Incorporation of base and top treatments defined by changes in color, materials, and recessed wall areas
- Design features that ensure that the building’s massing and proportion, along with its colors and architectural detailing, are consistent throughout all four building elevations

[6] Landscaping — The project provides landscaping along Cucamonga Avenue and throughout the perimeter of the project site. The Development Code requires that the project provide a minimum 10 percent landscape coverage, which has been provided. The project includes right-of-way improvements (curb, gutter, sidewalk and parkway) and street trees. The proposed on-site and off-site landscape improvements will assist towards creating a walkable safe area for pedestrians to access the project site. The landscape plan incorporates 24-inch box Arizona Sycamore trees within the parkway along Cucamonga Avenue. A combination of 24-inch and 48-inch box accent and shade trees will be provided throughout the project site that include Western Redbud, Willow Peppermint, Golden Raintree, Sweet Bay, Fruitless Olive, Camphor and Live Oak. The landscape plan also includes a variety of shrubs, agave, grasses and groundcovers that are low water usage and drought tolerant, to be planted throughout the project site.

Moreover, two employee break areas have been provided along the building street frontage, with benches and tables that will be shaded by the canopy of two Golden Raintrees that grow approximately 20 to 30 feet high and 35 feet wide (see Exhibit D—Landscape Plan, attached).

[7] Utilities (drainage, sewer) — Public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan (“PWQMP”), which establishes the Project’s compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (“LID”) best management practices (“BMPs”), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes that on-site run-off will be collected by a catch basin and conveyed to an underground infiltration system located on the western portion of the site, within the tractor-truck trailer yard area. Any overflow drainage will be conveyed to an existing 60-inch storm drainpipe located within Cucamonga Avenue.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City’s Economy
- Maintain the Current High Level of Public Safety
- Invest in the City’s Infrastructure (Water, Streets, Sewers, Parks, Storm Drains and Public Facilities)
- Operate in a Businesslike Manner

[2] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

➤ G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[3] Policy Plan (General Plan)

Land Use Element:

- Goal LU2: Compatibility between a wide range of uses.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.
 - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
 - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
 - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
 - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.
 - S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

- Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.
 - CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
 - A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
 - Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.
- CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.
- CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.
- CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.
- CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.
- CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.
- CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.
- CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) that was certified by the City Council on January 27, 2010. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Manufacturing	Industrial	General Industrial (IG)	N/A
<i>North</i>	Outdoor Storage	Industrial	General Industrial (IG)	N/A
<i>South</i>	Manufacturing	Industrial	General Industrial (IG)	N/A
<i>East</i>	Industrial Business Park	Industrial	General Industrial (IG)	N/A
<i>West</i>	Vehicle/Equipment Auction & Vacant	Industrial	General Industrial (IG)	N/A

General Site & Building Statistics

<i>Item</i>	<i>Proposed</i>	<i>Min./Max. Standard</i>	<i>Meets Y/N</i>
<i>Lot/Parcel Size:</i>	9.3 acres	10,000 SF (Min.)	Y
<i>Building Area:</i>	211,358 SF	N/A	Y
<i>Floor Area Ratio:</i>	0.52	0.55 (Max.)	Y
<i>Building Height:</i>	49 FT	55 FT (Max.)	Y

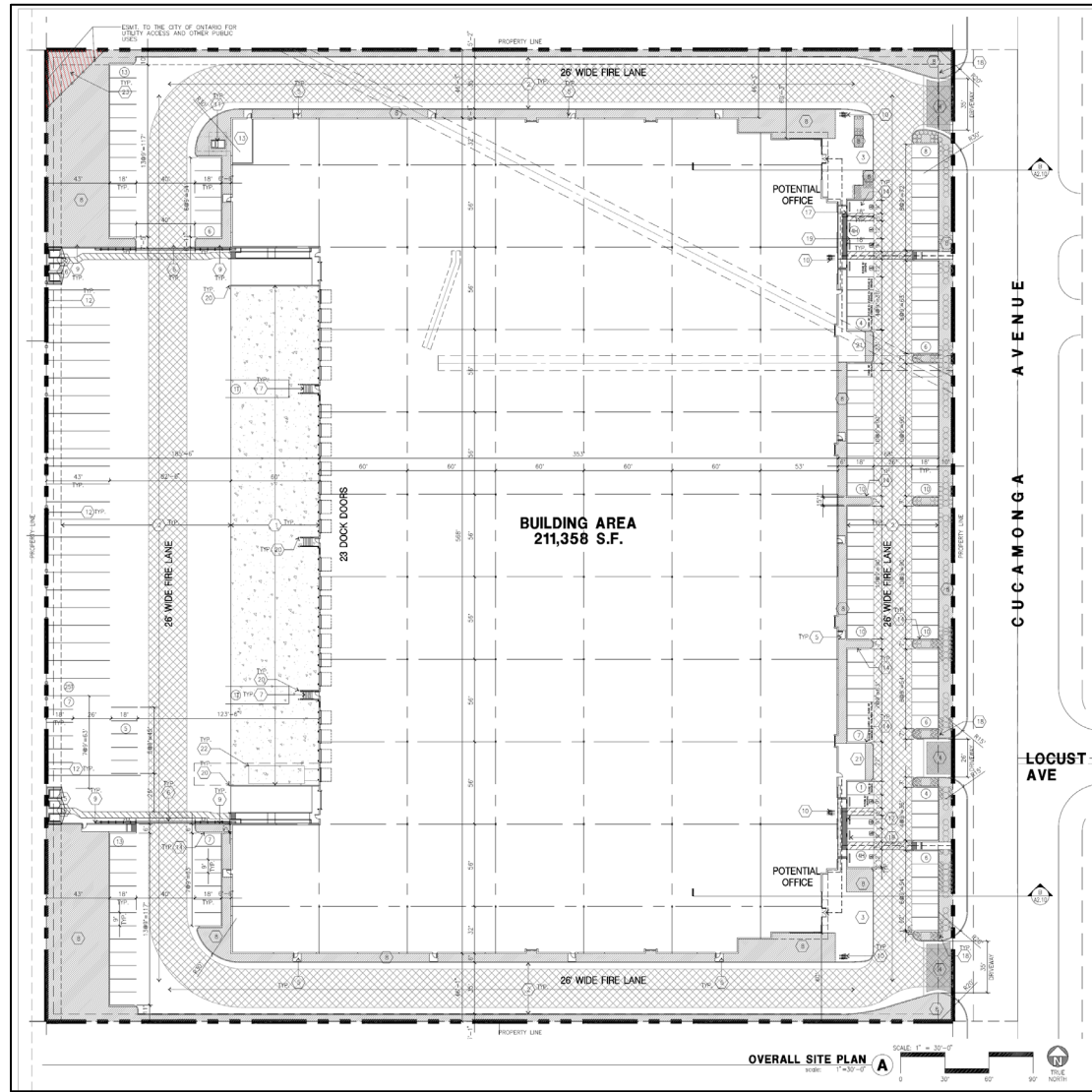
Off-Street Parking:

<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
<i>Warehouse/Distribution:</i> <ul style="list-style-type: none"> • <i>Automobile Parking</i> • <i>Tractor-Trailer Parking</i> 	211,358 SF	One space per 1,000 SF (0.001/SF) for portion of GFA <20,000 SF, plus 0.5 space per 1,000 SF (0.0005/SF) for GFA > 20,000 SF One tractor-trailer parking space per 4 dock-high loading doors (23 dock-high loading doors are proposed)	137 6	141 25

Exhibit A—PROJECT LOCATION MAP



Exhibit B—SITE PLAN



**Exhibit C—EXTERIOR ELEVATIONS
(Materials Board)**

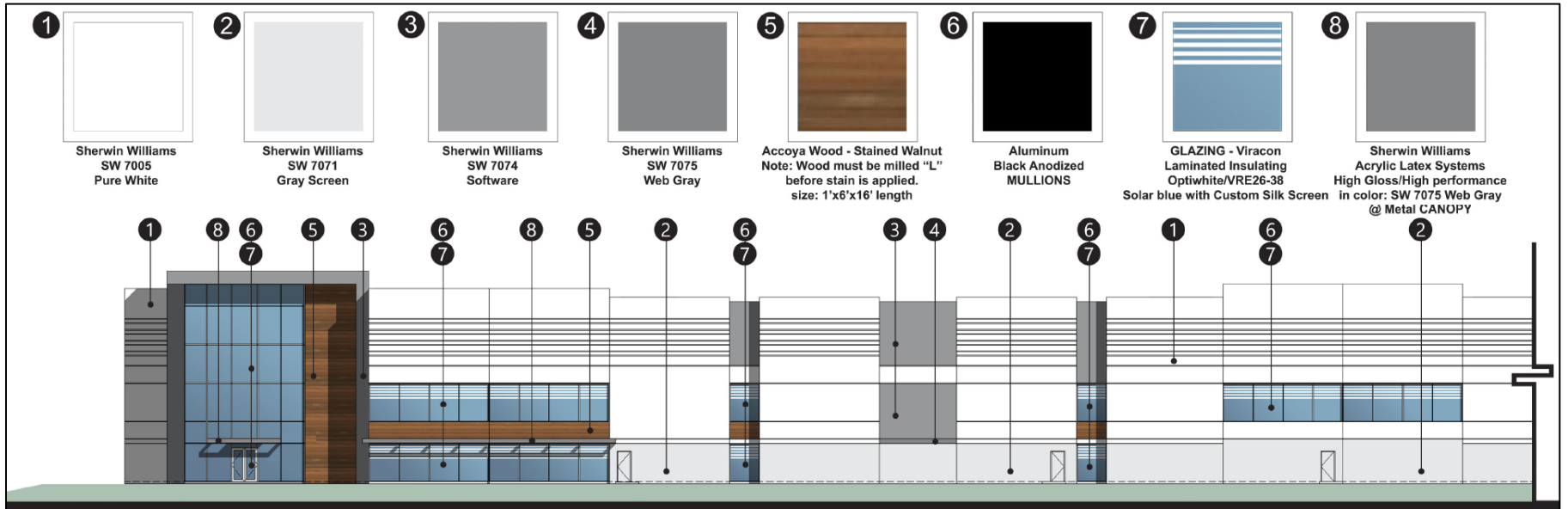


Exhibit C—EXTERIOR ELEVATIONS CONTINUED

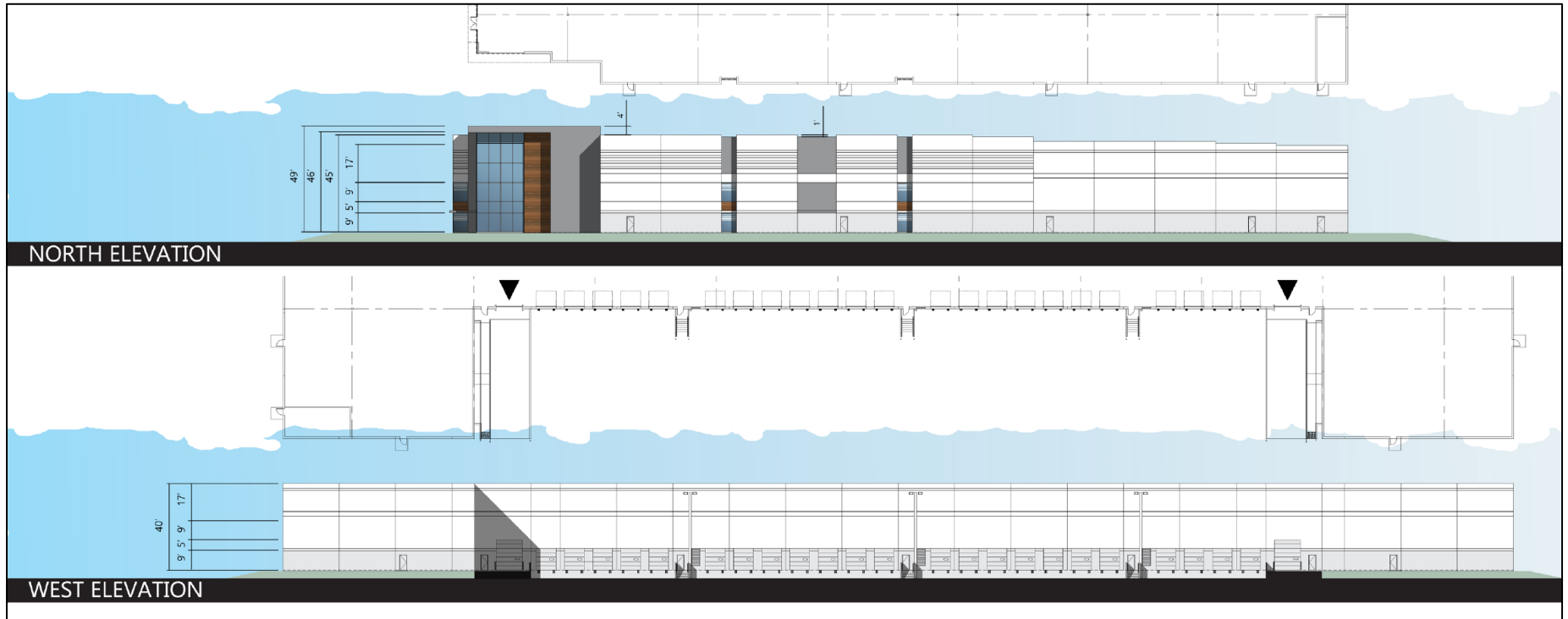


Exhibit C—EXTERIOR ELEVATIONS CONTINUED

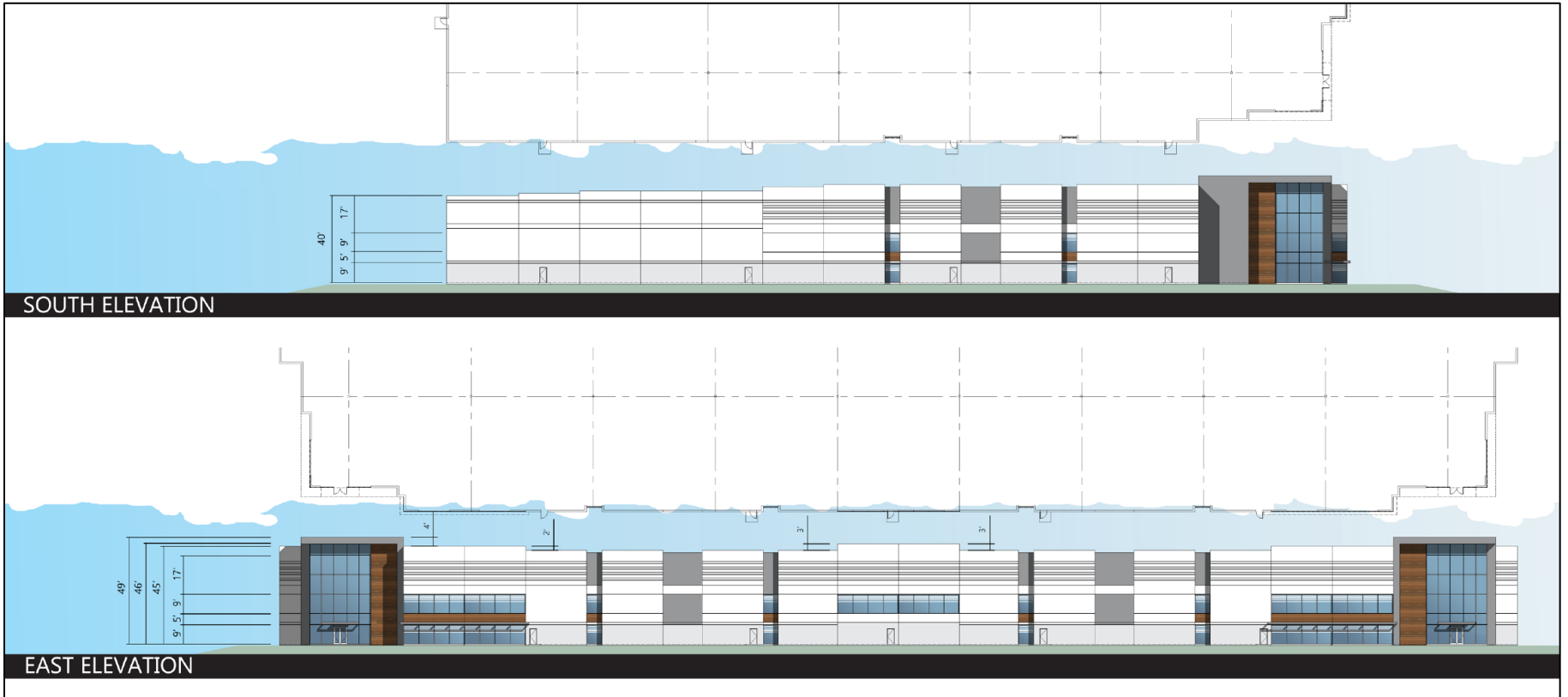
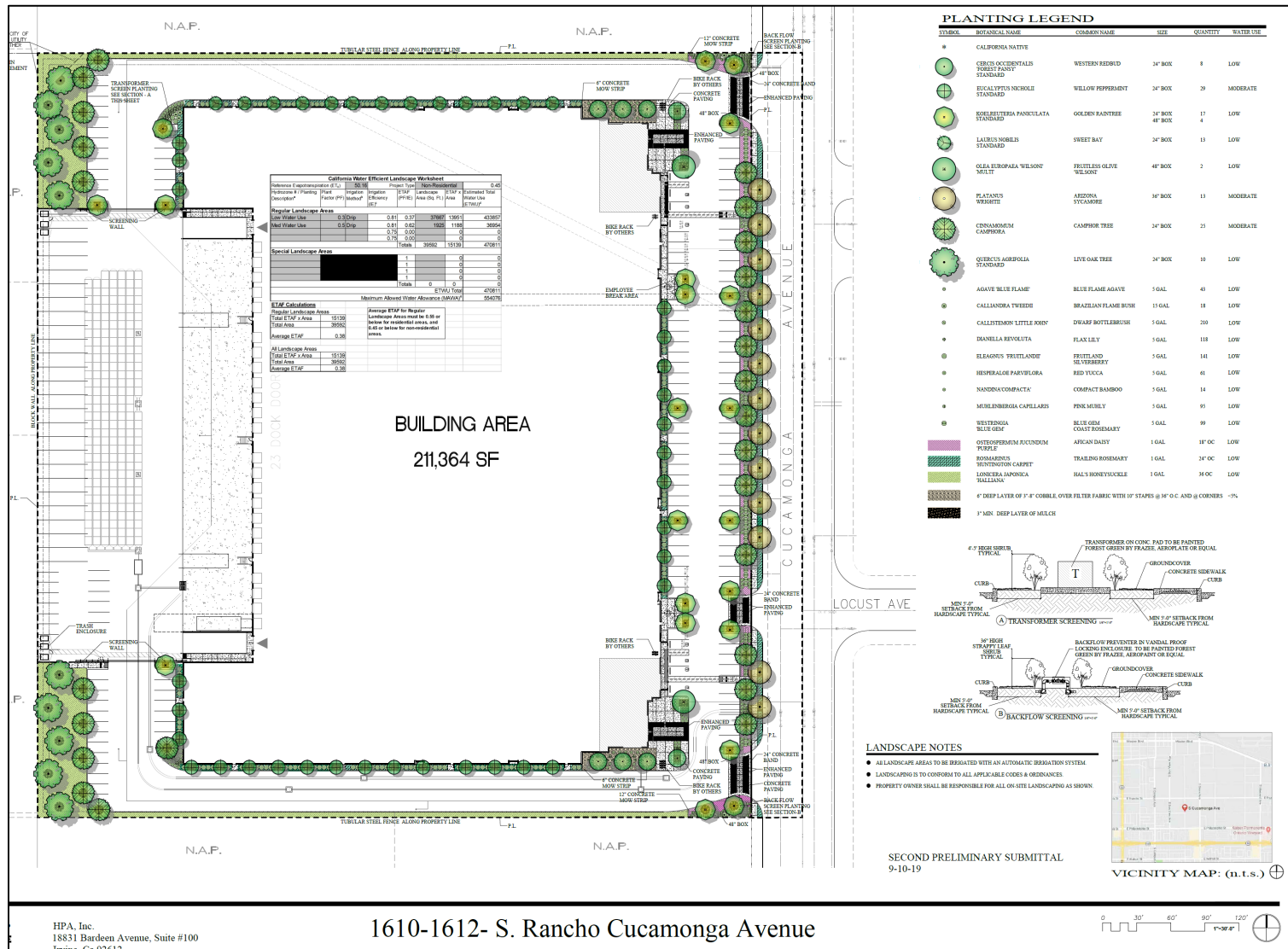


Exhibit D—LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-040, A DEVELOPMENT PLAN TO CONSTRUCT ONE INDUSTRIAL BUILDING TOTALING 211,358 SQUARE FEET ON 9.34 ACRES OF LAND LOCATED AT 1610 AND 1612 SOUTH CUCAMONGA AVENUE, WITHIN THE IG (GENERAL INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 1050-201-02.

WHEREAS, Alere Property Group, LLC ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-040, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 9.3 acres of land located at 1610 and 1612 South Cucamonga Avenue within the IG (General Industrial) zoning district, and is presently improved with nine industrial buildings totaling approximately 105,000 square feet; and

WHEREAS, the property to the north of the Project site is within the IG (General Industrial) zoning district and is developed with an outdoor storage use. The property to the east is within the IG (General Industrial) zoning district and is developed with an industrial business park. The property to the south is within the IG (General Industrial) zoning district and is developed with an industrial manufacturing use. The property to the west is within the IG (General Industrial) zoning district, is unimproved, and is utilized for the auction of vehicles and building equipment; and

WHEREAS, the proposed Development Plan is for the construction of a 211,358-square foot industrial building, with a floor area ratio (FAR) of 0.52; and

WHEREAS, the rectangular building is centered on the project site, with the front of the building and two office entries located at the northeast and southeast corners of the building, and oriented to the east, facing Cucamonga Avenue. The project will provide parking along the eastern property line, adjacent to the building, and at the southwest corner of the site; and

WHEREAS, a yard area, designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is centrally located on the west side (rear) of the proposed building, out of view from Cucamonga Avenue; and

WHEREAS, the Project has two points of vehicular access from Cucamonga Avenue, including a 35-foot wide driveway located at the northeast corner of the site and a 35-foot wide driveway located at the southeast corner of the site, that will be shared by both standard vehicles and tractor-trailers accessing the yard area and parking lot. A 26-

foot to 35-foot wide drive-aisle is proposed that will surround the entire building, connecting the two points of access; and

WHEREAS, the Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The industrial building requires a total of 137 off-street parking spaces, and 141 spaces have been provided. In addition, a minimum of one tractor-trailer parking space for each 4 dock-high loading spaces is required to be provided. There are 23 dock-high loading doors proposed, requiring 6 tractor-trailer parking spaces and 25 spaces have been provided; and

WHEREAS, the proposed building will be of concrete tilt-up construction. Architecturally, the building incorporates smooth-painted concrete, horizontal and vertical reveals, color blocking, wood paneling with walnut stained finish, clerestory windows with clear anodized aluminum mullions and solar blue glazing with a custom silk screen, steel canopies over the main office entries, and first and second story windows; and

WHEREAS, the project provides landscaping along Cucamonga Avenue and throughout the perimeter of the project site. The Development Code requires that the project provide a minimum 10 percent landscape coverage, which has been provided; and

WHEREAS, public utilities (water and sewer) are available to serve the Project. Furthermore, the Applicant has submitted a Preliminary Water Quality Management Plan ("PWQMP"), which establishes the Project's compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"), such as retention and infiltration, biotreatment, and evapotranspiration. The PWQMP proposes that on-site runoff will be collected by a catch basin and conveyed to an underground infiltration system located on the western portion of the site, within the tractor-truck trailer yard area. Any overflow drainage will be conveyed to an existing 60-inch storm drainpipe located within Cucamonga Avenue; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) that was certified by the City Council on January 27, 2010, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 20, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-020, recommending the Planning Commission approve the Application; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) that was certified by the City Council on January 27, 2010.

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial land use district of the Policy Plan Land Use Map, and the General Industrial (IG) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the General Industrial (IG) zoning district, including standards relative to the particular land use proposed (Industrial Warehouse), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions.

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Development Advisory Board has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the project will not endanger the public health, safety or general welfare; [iii] the project will not result in any significant environmental impacts; [iv] the project will be in harmony with the area in which it is located; and [v] the project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Development Code.

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site

landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (Industrial Warehouse). As a result of this review, the Development Advisory Board has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 6: *Planning Commission Action.* Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 7: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV19-040
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
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Planning Department Land Development Division Conditions of Approval

Meeting Date: April 20, 2020

File No: PDEV19-040

Related Files: N/A

Project Description: Development Plan to construct one industrial building totaling 211,358 square feet on 9.34 acres of land located at 1610 and 1612 South Cucamonga Avenue, within the IG (General Industrial) zoning district. (APN: 1050-201-02); **submitted by Alere Property Group LLC.**

Prepared By: Lorena Mejia, Senior Planner
Phone: 909.395.2276 (direct)
Email: lmejia@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

<i>Screen Wall Height</i>	<i>Minimum Gate Height</i>
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with The Ontario Plan (File No. PGPA06-001) Environmental Impact Report (SCH# 2008101140) that was certified by the City Council on January 27, 2010. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. The previously adopted mitigation measures shall be a condition of project approval, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.15 Additional Requirements.

(a) Provide a 6-foot-high steel tubular fence shall be constructed along the south, north and west property lines.

(b) The Ontario Climate Action Plan (CAP) requires new development to be 25% more efficient. The applicant has elected to utilize the Screening Tables provided in the CAP instead of preparing separate emissions calculations. By electing to utilize the Screening Tables the applicant shall be required to garner a minimum of 100 points to be consistent with the reduction quantities outlined in the CAP. The applicant shall identify on the construction drawings the items identified in the Screening Tables.



**ENGINEERING DEPARTMENT
CONDITIONS OF APPROVAL**

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input type="checkbox"/> PARCEL MAP <input type="checkbox"/> TRACT MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES
PROJECT FILE NO. PDEV19-040 RELATED FILE NO(S). _____	
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__	

CITY PROJECT ENGINEER & PHONE NO:

Dean A. Williams, Associate Engineer DAW
(909) 395-2135

CITY PROJECT PLANNER & PHONE NO:

Lorena Mejia, Senior Planner
(909) 395-2413

DAB MEETING DATE:

April 20, 2020

PROJECT NAME / DESCRIPTION:

212,753 sf Industrial Building on 9.34 acres of land

LOCATION:

1610 S. Cucamonga Avenue

APPLICANT:

Alere Property Group, LLC
Clark Neuhoff (949) 509-5002

REVIEWED BY:

Bryan Lirley, P.E.
Principal Engineer

Date

APPROVED BY:

Raymond Lee, P.E.
Assistant City Engineer

4/14/20
Date



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
_____ feet on _____
Property line corner 'cut-back' required at the intersection of _____
and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s): _____
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.
- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
(1) _____
(2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost



estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.

- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
 - 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions: _____

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

A. GENERAL (Permits includes Grading, Building, Demolition and Encroachment)

- 2.01 Record Parcel Map/Tract Map No. _____ pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario per Lot 11, Block G Blackburn's Addition, in the City of Ontario, County of San Bernardino, State of California, as per map recorded in Book 12, page 55 of Maps in the Office of the County Recorder.**
- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____ .
- 2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 - Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.
- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure



Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 Submit a soils/geology report.
- 2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA)
 - Other: _____
- 2.10 Dedicate to the City of Ontario the right-of-way described below:

_____ feet on _____

Property line corner 'cut-back' required at the intersection of _____ and _____.
- 2.11 **Dedicate to the City of Ontario the following easement(s): an 800 s.f. corner cut-off (triangle) easement at the northwest corner of the site (40'x40') for public utility, driveway access and other public purposes for the Ontario Municipal Utilities Company. The easement document and language shall be provided by the City. Developer/applicant shall provide legal description and exhibit. Project perimeter fencing/wall shall not encroach within the easement area, including footings.**
- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100 % of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.14 **The developer/applicant shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. This shall include areas related to construction of the proposed storm**



drain (see item no. 2.43). These documents are to be reviewed and approved by the City Survey Office.

- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately \$213,000.00, shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.
- 2.16 Other conditions: _____

B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Cucamonga Avenue	Street 2	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace
Sidewalk	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)



Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input checked="" type="checkbox"/> abandon	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	Abandoned sewer on-site	_____	_____	_____



Other Improvements				
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Specific notes for improvements listed in item no. 2.17, above: _____

- 2.18 **Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): Cucamonga Avenue**
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project in Cucamonga Avenue. (Ref: Sewer plan bar code: S10608)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions: 1) Developer/applicant shall cut and cap the existing 18-inch abandoned sewer crossing the site diagonally at the north property line and the east property line in accordance with the approved site plan. The sewer pipe shall be removed from the site and properly disposed of.**
 - 2) Developer/applicant shall abandon all existing sewer laterals not used to provide service to the site back to the main in accordance with current City standards and practices.
 - 3) Developer/applicant shall construct a monitoring manhole from each lateral point of connection from the on-site sewer system connecting to the public sewer system.
 - 4) The *occupant*/applicant shall apply for a wastewater Discharge Permit for their establishment and shall comply with all requirements of the Wastewater Discharge Permit (<http://www.ontario.gov/municipal-utilities-company/utilities/industrial-wastewater-discharge-permit>). Requirements of the Wastewater Discharge Permit may include, but not be limited to, installation of wastewater pre-treatment equipment such as clarifiers. Permit application questions, please contact Mr. Michael Birmelin, Environmental Programs Manager at (909) 395-2661 (omucenvironmental@ontario.gov).

D. WATER



- 2.27 An 8-inch water main is available for connection by this project in Cucamonga Avenue. (Ref: Water plan bar code: W11605)
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions: 1) Developer/applicant shall design and construct all improvements along Cucamonga Avenue to allow (i.e. preserve a corridor) for the future construction of a 42-inch CML&C potable water main in Cucamonga Avenue between Francis Street and Belmont Street. The horizontal alignment would be within 10-feet west of the centerline of the street and the west curb face and the vertical alignment of the main 4-feet below the finished surface. All Department of Drinking Water (DDW) water main separations per the California Code of Regulations will need to be met within this 20-foot wide corridor. This includes all other non-potable fluid utility mains crossing at a minimum of 1-foot below the bottom of the outside wall of the 42-inch main.**
2) Developer/applicant shall abandon all existing water services not being used to provide service to the site back to the main in accordance with current City standards and practices.

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____. (Ref: Recycled Water plan bar code: _____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 **Other conditions: 1) Developer/applicant shall design and construct street improvements along property frontage of Cucamonga Avenue in accordance with the Ontario Municipal Code and conditions described herein. These improvements shall include all missing or substandard improvements, but not be limited to, concrete curb and gutter, sidewalk, LED street lights, signing and striping, and parkway landscaping.**
2) Cucamonga Avenue shall be signed "No Parking Anytime" along the project frontage.
3) All drive approaches shall be designed and constructed in accordance with City of Ontario



Standard Drawing No. 1204.

4) All landscaping, block walls and other obstructions shall be compatible with the stopping site distance requirements per City of Ontario Standard Drawing No.1309.

5) Developer/applicant shall design and construct in-fill public street lights on Cucamonga Avenue. Street lighting shall be LED-type and in accordance with the City's Traffic and Transportation Design Guidelines.

G. DRAINAGE / HYDROLOGY

- 2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
(Ref: Storm Drain plan bar code: _____)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions: Developer/applicant shall design and construct a segment of Master Planned Storm Drain Line FRNS-V-2 (Reference 'Master Plan of Storm Drainage City of Ontario Final Report March 2012'. The storm drain line may vary in size from 60-inch to 72-inch RCP. Construction shall be from connection to the existing stub-out from the Francis Street Storm Drain to the southerly end of the project site. Design and construction shall include catch basin inlets on both sides of Cucamonga Avenue and a half width grind and overlay (i.e. gutter to centerline of street and beyond, if deemed necessary after construction). All drainage from the site shall drain into the newly constructed storm drain.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP) based on the approved PWQMP for the site. This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater**



Program template, available at: <http://www.sbcountry.gov/dpw/land/npdes.asp>.

- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 **Other conditions: Activities resulting in land disturbance of one acre or more requires the developer to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Storm Water Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at: <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>**

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.**
- 2.51 **Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.**

L. Solid Waste

- 2.52 **Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at: <http://www.ontarioca.gov/municipal-utilities-company/solid-waste>**
- 2.53 **Other conditions: 1) Developer/applicant shall submit a Final Solid Waste Handling Plan Sheet accompanying the Precise Grading Plan to the City/OMUC for review and approval prior to issuance of a Building Permit. See Solid Waste Handling Plan (SWHP) Requirements document for details.**

a) **Organics Separation and Collection: This site shall comply with the Requirements for State Assembly Bill AB 1826, which requires organic waste to be diverted and collected separately from recycling and other refuse wastes**

b) **At a minimum, this site requires a trash enclosure sized to store three 4-cubic yard bins (one for refuse, one for recycling and one for organics) for each office area of the building.**

2) Developer/applicant shall submit an Integrated waste management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (refuse, recycling, organics, etc.). The IWMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV 19-040

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. Three (3) sets of Public Street improvement plan with street cross-sections
7. Three (3) sets of Private Street improvement plan with street cross-sections
8. Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. Four (4) sets of Public Sewer improvement plan
11. **Five (5) sets of Public Storm Drain improvement plan**
12. **Three (3) sets of Public Street Light improvement plan**
13. **Three (3) sets of Signing and Striping improvement plan**
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. **Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)**
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. **Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).**
18. **One (1) copy of Hydrology/Drainage study**
19. **One (1) copy of Soils/Geology report**
20. Payment for Final Map/Parcel Map processing fee
21. Three (3) copies of Final Map/Parcel Map
22. One (1) copy of approved Tentative Map



- 23. One (1) copy of Preliminary Title Report (current within 30 days)
- 24. One (1) copy of Traverse Closure Calculations
- 25. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



CITY OF ONTARIO

MEMORANDUM

TO: Lorena Mejia, Senior Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: July 17, 2019

SUBJECT: PDEV19-040 - A Development Plan to construct one (1) industrial building totaling 212,753 square feet on 9.34 acres of land located at 1610 S Cucamonga Avenue, within the General Industrial (IG) zoning district - (APN(s): 1050-201-02).

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Not Listed (Assumed II-B)
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 209,753 Sq. Ft.
- D. Number of Stories: 1 plus Mezzanine (3,000 Sq. Ft.)
- E. Total Square Footage: 212,753 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- ☒ 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 4000 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- ☒ 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- ☒ 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- ☒ 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- ☒ 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and locations(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- ☒ 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard Choose an item. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- ☒ 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- ☒ 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO

MEMORANDUM

TO: Lorena Mejia, Senior Planner

FROM: Officer Emily Hernandez, Police Department

DATE: July 22, 2019

SUBJECT: PDEV19-040 – A DEVELOPMENT PLAN TO CONSTRUCT A 212,753 SQUARE FOOT INDUSTRIAL BUILDING LOCATED AT 1610 SOUTH CUCAMONGA AVENUE.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Lorena Mejia
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: July 16, 2019
SUBJECT: PDEV19-040

-
- The plan **does** adequately address the departmental concerns at this time.
 No comments
 Report below.

Conditions of Approval

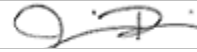
1. The Site address for this project will be 1656 S. Cucamonga Ave
2. Standard conditions of approval apply.

KS:lr

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off



3/9/2020

Jamie Richardson, Sr. Landscape Planner

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV18-040

Case Planner:

Lorena Mejia

Project Name and Location:

Industrial Bldg
 1610 S Cucamonga Ave

Applicant/Representative:

HPA, Inc.(949) 863-1770 hpa@hparchs.com
 18831 Bardeen Ave., Suite 100
 Irvine, CA 92612



A Preliminary Landscape Plan (dated 2/20/2020) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.



A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
3. Show transformers set back 5' from paving all sides. Coordinate with landscape plans.
4. Show backflow devices set back 4' from paving all sides. Locate on level grade
5. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
6. Provide a utility clear space 8' wide in parkways 30' apart for street trees. Move water meters, drain

lines, light standards to the utility minimum spacing and show utility lines at the edges of the parkway, toward the driveway apron, to allow space for street trees.

7. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 1/2" below finished surfaces. Slopes to be maximum 3:1.
8. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened. Add the following information on the plans: The back hoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

9. Provide an arborist report and tree inventory as noted in #1.
10. Address the property lines; show block walls, fences, wrought iron at property lines. Contact the project planner for requirements.
11. Do not encircle utilities, show as masses and duplicate masses in other locations on regular intervals.
12. Revise site plan to show 10% (40,685 Sq. Ft.) of the site with landscaping not including right of way or paving areas. Match with site plan. Do not include the non-buildable easement in the site/landscape requirement; update calculations.
13. Landscape construction plans shall meet the requirements of the Landscape Development Guidelines. See <http://www.ontarioca.gov/landscape-planning/standards>
14. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—5 or more acres.....	\$2,326.00
Inspection—Construction (up to 3 inspections per phase).....	<u>\$278.00</u>
Total.....	\$2,604.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-040
 Address: 1610 South Cucamonga Ave
 APN: 1050-201-02
 Existing Land Use: Industrial Manufacturing
 Proposed Land Use: 212,753 SF Industrial Building
 Site Acreage: 9.34 Proposed Structure Height: 43 ft
 ONT-IAC Project Review: N/A
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Lorena Mejia
 Date: 11/6/19
 CD No.: 2019-051
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="radio"/> High Terrain Zone	<input type="radio"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input checked="" type="checkbox"/> Recorded Overflight Notification
<input type="radio"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="radio"/> Real Estate Transaction Disclosure
<input type="radio"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input type="radio"/> Airspace Avigation Easement Area	
<input type="radio"/> Zone 4		Allowable Height: 200 FT +	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Airport Planner Signature: _____

PLANNING / HISTORIC PRESERVATION COMMISSION STAFF REPORT



DATE: April 28, 2020
FILE NO: PHP20-004
SUBJECT: 2020 "Model Colony" Awards
LOCATION: Citywide
APPLICANT: City Initiated
PROPERTY OWNER: N/A

RECOMMENDATION:

That the Planning/Historic Preservation Commission consider and approve the 2020 "Model Colony" Award nominations.

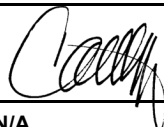
BACKGROUND:

In 2000, the City Council adopted the Model Colony Awards to recognize outstanding efforts to restore, rehabilitate, and preserve Ontario's historic places. This is the twentieth consecutive year that the City has conducted the awards program. The award categories include: Restoration, Rehabilitation, John S. Armstrong Landscape, Founder's Heritage Award, George Chaffey Memorial, and Merit. Past Model Colony Award recipients included Ontario's schools, churches, single-family residences, multi-family properties, and joint public/private preservation projects.

There are 2 nominations this year, both for projects at historic Chaffey High School. The first is for the successful infill construction of the new Math and Science Building on the south edge of the campus. The second is for the restoration of the Gardiner W. Spring Auditorium. The 2020 Model Colony Awards will be presented to award recipients by the City Council during a special ceremony and reception which has been tentatively scheduled for June 2, 2020.

HISTORIC CONTEXT:

Ontario was founded in September of 1882 by George and William B. Chaffey. The Chaffey's established three principles for the "Colony" that had social and economic implications including a mutual water company concept, a grand thoroughfare, and an agricultural college

<p>Case Planner: Elly Antuna, Associate Planner</p> <hr/> <p>Planning Director Approval: </p> <hr/> <p>Submittal Date: N/A</p> <hr/> <p>Hearing Deadline: N/A</p> <hr/>	<table border="0"> <thead> <tr> <th style="text-align: left;">Hearing Body</th> <th style="text-align: left;">Date</th> <th style="text-align: left;">Decision</th> <th style="text-align: left;">Action</th> </tr> </thead> <tbody> <tr> <td>HPSC:</td> <td></td> <td></td> <td></td> </tr> <tr> <td>PC / HPC:</td> <td>04/28/2020</td> <td></td> <td>Final</td> </tr> <tr> <td>CC:</td> <td>06/02/2020</td> <td></td> <td>Presentation</td> </tr> </tbody> </table>	Hearing Body	Date	Decision	Action	HPSC:				PC / HPC:	04/28/2020		Final	CC:	06/02/2020		Presentation
Hearing Body	Date	Decision	Action														
HPSC:																	
PC / HPC:	04/28/2020		Final														
CC:	06/02/2020		Presentation														

for general education. In 1882, the first year of the Model Colony, George Chaffey set aside a 20-acre site at the corner of Euclid Avenue and Fourth Street for an agricultural college.

The site of Chaffey College was established in 1885 as an affiliate of USC. Both high school and college courses were offered here from 1901 to 1960. Between 1911 and 1933 several new buildings and additions were constructed on the campus to accommodate the growing needs of the surrounding communities. Then, in 1933 the Long Beach earthquake shook Ontario, damaging most of the buildings on the high school and junior college campus, resulting in the condemnation of all the high school buildings. This disaster occurred during the Great Depression when the school district was in dire financial condition.

By taking full advantage of recovery act funding sources such as State Emergency Relief Act (SERA), Public Works Administration (PWA), and Work Projects Administration (WPA), school superintendent Gardiner W. Spring obtained funding to rebuild all the high school buildings and add new buildings to the junior college. The availability of these funding sources leveled the playing field for communities throughout the region that would not typically be able to secure well-known architects, let alone rebuild an entire school campus.

Gardiner W. Spring Auditorium, designed by famed architects Allison and Allison of Los Angeles, was one of the buildings constructed in 1939 by the WPA and the PWA. The reinforced concrete auditorium replaced the 1912 auditorium and library building that was condemned after the earthquake. Allison and Allison belonged to a preeminent school of architects that have been characterized as “true romantics” leading the revival period of architecture in Southern California. The buildings they designed were adapted and heavily influenced by Spanish, Mexican and Italian architecture because of the similar climate to Southern California. Just a few of the buildings that bare their name include schools and collegiate projects, churches, libraries, post offices, factories and warehouses. Allison and Allison helped to shape Southern California architecture, leaving their mark at Chaffey High School with Gardiner W. Spring Auditorium and Tower Hall.

Since then, the high school has continued to evolve to accommodate a growing and changing community. The Chaffey High School campus had been determined to meet the National Register of Historic Places holding historic significance on a local, state and national level. On January 19, 1999, the Ontario City Council designated the campus as Local Landmark No. 58, and on June 4, 2013 it was designated as a Contributor to the Euclid Avenue Historic District.

2020 AWARD NOMINEES:

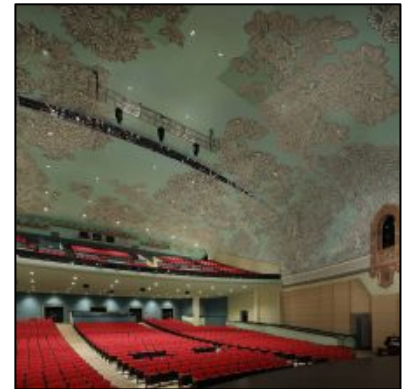
For their outstanding efforts in the field of historic preservation, the nominees are:

Restoration Award:	Gardiner W. Spring Auditorium, Chaffey High School
Award Recipient:	Chaffey Joint Union High School District

Since its construction and its very first show in 1939, the Gardiner W. Spring Auditorium has been revered by the community, which is evident by the continued efforts of not only the school district, but local organizations, to preserve, restore and protect this iconic historic resource.



In 2017, with funds from Measure P, efforts began to completely restore and update the Spanish Colonial style 2,400 seat auditorium. One of many significant interior features is a massive 20,000 square foot Art Deco mural on the acoustic plaster ceiling. The mural was originally painted on the auditorium ceiling by artist E. Peterson. Over time, the mural had turned from blue to a bluish green, mostly as a result of a period where smoking was allowed in the auditorium. Architectural arts restoration experts were brought on to conduct a finishes investigation and conservation testing of the auditorium. It was determined that the acoustic plaster substrate contained asbestos and required abatement along with the complete replacement of the ceiling and mural. The architectural art restoration team carefully measured and documented the entire ceiling by digital photography. Pounce patterns were created, and a grid system was used to layout and recreate the mural on the new acoustic plaster surface. Additionally, all the original decorative painting was reinstated throughout the auditorium and public spaces.



The modernization to the auditorium also presented a unique set of challenges, one being that there were no as-built drawings of the historic structure to guide the project. The complexity of the auditorium building required a sophisticated approach to accurately capture the building dimensions. To achieve this, 3D laser scanning was used to scan the entire structure and provide complete as-built drawings. The modernization of the historic theater provides not only improvements of the existing structure, but structural repairs and a new cooling and ventilation system. Renovations to the structure include theatrical and acoustical upgrades, a new catwalk and steel stairway, two new elevators, accessibility upgrades and a lobby restroom expansion. The sound system is especially impressive as it was completely upgraded with professional, state-of-the-art equipment, making it one of the best equipped venues in the region.

Throughout the project, every effort was made to preserve and protect original features, and where necessary, replace with materials that were custom made to replicate the original. The original exterior doors featured an ornate design and when new doors needed to be made, the original design was replicated on each door. The auditorium seats were also special ordered to closely replicate the intricate designs on the aisle seats. The restoration project not only preserved this iconic auditorium but brought it into the 21st century with state-of-the-art technology, making it a resource that the community

will cherish and enjoy for generations to come.

Rehabilitation Award:
Award Recipient:

Chaffey High School Math & Science Building
Chaffey Joint Union High School District

One of the newest additions to the Chaffey High School campus is a state of the art, three-story Math and Science building located at the southern edge of the campus along East Fourth Street. The school district was committed to constructing a new building that did not compromise the historic integrity of the campus and knew that the campus was not only important to the district, but the community. With input from community members, students, teachers, parents, and other stakeholders and through a facilities bond (Measure P), the new 55,000 square foot, three-story, \$20 Million Math and Science Building was completed and welcomed students for the 2017-2018 academic year. The structural steel frame building with an exterior plaster finish contains 18 state-of-the-art classrooms and 12 science laboratories. The design of the new building is both respectful and complementary of the historic campus and incorporates the latest technology for the classrooms. Rather than compete with the unique Mission Revival architectural styled, WPA era buildings and their ornate churrigueresque entryways, the new building exhibits a quiet elegance that blends in effortlessly with the existing historic buildings. The new building is rectangular in plan with a flat roof and features a smooth plaster finish with powder coated steel windows, matching the finishes on the existing historic buildings. The Art Deco style entryway is in a central tower element covered with a hipped tile roof and wide overhanging boxed eaves. Secondary entrances are framed by deep recessed arched entryways.



The successful infill project is an excellent example of appropriate infill on a historic property and in a Historic District. The sensitive infill construction of this state-of-the-art building ensures that Chaffey High School will remain one of the City's premium historic properties.

COMPLIANCE WITH THE ONTARIO PLAN:

The Model Colony Awards Program is consistent with the principles, goals and policies contained in the following components of The Ontario Plan (TOP), including: (1) Vision, (2) Governance, and (3) Policy Plan (General Plan):

[1] City Council Priorities

Goals:

- Invest in the Growth and Evolution of the City's Economy
- Focus Resources in Ontario's Commercial and Residential Neighborhoods
- Encourage, Provide or Support Enhanced Recreational, Educational, Cultural and Healthy City Programs, Policies and Activities.

[2] Vision

Distinctive Development

- Development Quality: A community that is so well maintained and litter-free that its properties uniformly convey a sense of prosperity that is readily apparent and a symbol of community pride.

Dynamic Balance

- An appreciation for the “personality and charm” of this community, preserving important characteristics and values even as growth and change occur, all the while retaining a distinctive local feel where people love to be.

[3] Governance

Governance – Decision Making

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices
 - G1-1 Consistency with Policies. We require that staff recommendations to the City Council be consistent with adopted City Council Priorities (Goals and Objectives) and the Policy Plan.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision.

[4] Policy Plan (General Plan)

Community Design – Image & Identity

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses
 - CD1-3: Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

Community Design – Historic Preservation

- Goal CD4: Historic buildings, streets, landscapes and neighborhoods, as well as the story of Ontario's people, businesses, and social and community organizations, that have been preserved and serve as a focal point for civic pride and identity.
 - CD4-6: Promotion of Public Involvement in Preservation. We engage in programs to publicize and promote the City's and the public's involvement in preservation efforts.
 - CD4-7: Public Outreach. We provide opportunities for our residents to research and learn about the history of Ontario through the Planning Department, Museum of History and Art, Ontario and the Robert E. Ellingwood Model Colony History Room.

Community Design – Protection of Investment

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.
 - CD5-4: Neighborhood Involvement. We encourage active community involvement to implement programs aimed at the beautification and improvement of neighborhoods.



PLANNING COMMISSION STAFF REPORT

April 28, 2020

FILE NOS.: PMTT19-013 (PM 20157) and PDEV19-050

SUBJECT: A Tentative Parcel Map (File No. PMTT19-013/PM 20157) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district; (APN: 1049-232-21) **submitted by Herdman Architecture and Design.**

PROPERTY OWNER: Kevin Rice – Patriot Development Partners, LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve File Nos. PMTT19-013 (PM 20157) and PDEV19-050, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site is comprised of approximately 5 acres of land located at 617 East Sunkist Street, within the IL (Light Industrial) zoning district, and is depicted in Figure 1: Project Location, below. An approximate 30,000-square-foot refrigerated warehouse building exists on the Project site, along with a wireless telecommunications facility, which are proposed to be razed. The properties to the north and south of the Project site are zoned IL (Light Industrial) and are developed with warehouse uses. The property to the west of the Project site is zoned IL (Light Industrial) and is developed with a Southern Pacific Rail Line. The property to the east of the Project site is zoned IG (General Industrial) and is developed with a warehouse. The surrounding existing land uses, Policy Plan (General Plan) and zoning information are tabulated in the Technical Appendix Section of this report.

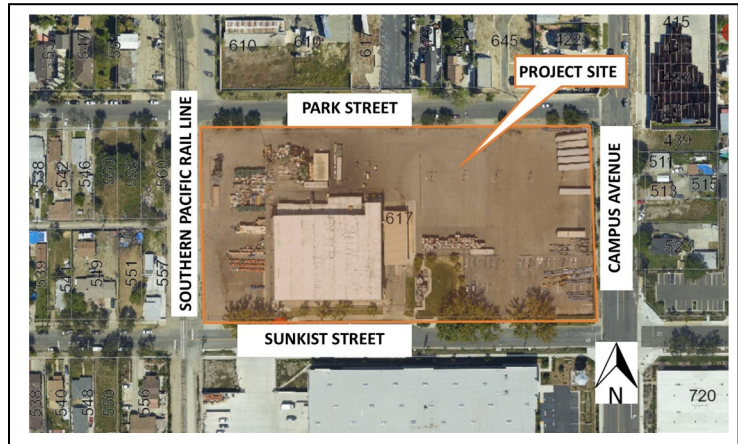


Figure 1: Project Location

PROJECT ANALYSIS:

[1] Background — On January 27, 2010, the Ontario City Council

Case Planner:	Alexis Vaughn
Planning Director Approval:	
Submittal Date:	09/04/2019

Hearing Body	Date	Decision	Action
DAB	04/20/2020	Approval	Recommend
PC	04/28/2020		Final
CC			

certified The Ontario Plan Environmental Impact Report in conjunction with File No. PGPA06-001 (City Council Resolution No. 2010-006). The Ontario Plan and associated Environmental Impact Report analyzed the Project site and established guidelines for development, including but not limited to, general land use (industrial), maximum FAR (0.55), and assumed building area (approximately 119,800 square feet).

On September 4, 2019, the Applicant submitted a Tentative Parcel Map (File No. PMTT19-013/ PM 20157) to subdivide 5 acres of land into a single parcel on the above-described Project site, to facilitate its development. In conjunction with the Tentative Parcel Map, the Applicant has submitted a Development Plan (File No. PDEV19-050) to construct a 104,993 square foot industrial building on the Project site, having a floor area ratio (FAR) of 0.49.

[2] Tentative Parcel Map (File No. PMTT19-013/PM 20157) — The proposed subdivision (see Exhibit B—Tentative Parcel Map No. 20157) will clean up previous lot lines and incorporate a portion of street right-of-way on the south side of the Project site, which previously functioned as a bus/carpool turn-out, thereby consolidating the Project area into a single parcel. The proposed subdivision exceeds the minimum requirements of the IL (Light Industrial) zoning district, including minimum lot area (10,000 square feet (0.23-acre) required and 5 acres is proposed) and dimensions.

[3] Development Plan (File No. PDEV19-050) —

[a] Site Design/Building Layout. The Project site will maintain street access to the north, east, and south, and will be bound on the west by a Southern Pacific railroad right-of-way. The proposed warehouse/distribution building is sited against the Project's Park Street (north) frontage, setback 10 feet behind the street property line, thereby allowing for parking and drive aisle access along the west, south, and east areas of the site (see Exhibit C—Proposed Site Plan, attached). Each truck court entrance will be screened by a decorative iron gate, and the dock door area will be screened from public view by a combination of landscaping and a 14-foot high decorative concrete tilt-up wall, treated to match the building's architecture. Fourteen dock doors are proposed along the southern elevation of the building, and a 3,000 square foot office with 3,000 square foot mezzanine has been proposed for both the southwest and southeast corners of the building. The remainder of the building will allow for light industrial uses, such as general warehousing and storage uses.

[b] Site Access/Circulation. Vehicular access to the site is provided at three locations, including the southwest corner of the property, from Sunkist Street, at the northwest corner of the property, from Park Street, and along Campus Avenue, midway between Park and Sunkist Streets. Truck traffic will largely utilize the Sunkist Street and Campus Avenue drive approaches, while passenger vehicles will utilize all three drive approaches to access passenger vehicle parking areas and the proposed offices.

[c] Parking. The Project has provided off-street parking pursuant to the “Warehouse and Distribution” parking standards specified in the Development Code. The off-street parking calculations are as follows:

<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
Warehousing and Distribution	94,993 SF	One space per 1,000 SF for portion of GFA greater than or equal to 20,000 SF, plus 0.5 space per 1,000 SF of GFA greater than 20,000 SF; plus required parking for “general business offices”; plus 1 trailer parking space per 4 dock doors	58 passenger 4 trailer	58 passenger 4 trailer
Office	10,000 SF	One space per 250 SF of GFA in excess of 10% of the warehousing and distribution use (appx 500SF)	2	11
TOTAL	104,993 SF		61	69

The Project is required to provide a minimum of 61 off-street passenger vehicle parking spaces and 4 trailer parking spaces pursuant to the parking standards specified in the Development Code and has provided a total of 69 passenger parking spaces and 4 trailer parking spaces. Parking spaces provided in excess of 10 percent of the required number of spaces require Planning Commission review and approval. The applicant has provided additional ADA, Electric Vehicle, and Clean Air designated parking spaces, which brings the total number of spaces slightly above 10 percent of the required number of stalls.

[d] Architecture. The applicant has proposed a contemporary industrial architectural style that will complement the existing industrial buildings in the neighborhood of the Project site. The applicant is proposing the following architectural treatments (see Exhibit D—Exterior Elevations, attached):

- Concrete tilt-up building with recessed panels
- Horizontal, vertical, and angled reveal lines to break down the massing
- Varying paint styles/sections to provide visual interest
- Blue glazing at the main entry points and spaced out along the full elevations
- Decorative metal window brows
- Corrugated metal siding to emphasize the entry points
- Blue LED lighting strips throughout the elevations

Staff has required revisions to the elevations (see PDEV19-050 Resolution Attachment A—Conditions of Approval; Exhibit A—Exterior Elevations Required Revisions) as Project conditions of approval, to be incorporated during the plan check process. The revisions are intended to further break down the massing and provide added visual interest along prominent public-facing elevations. Decorative metal panels with an 18-inch projection depth have been required along the north elevation, facing Park Street. In key office entrance areas where the decorative metal panels have already been proposed by the

applicant, additional variation in projection depths has been required, in 18-inch and 9-inch depths. Additionally, staff has conditioned that the decorative metal paneling be provided with a return to the building, to screen the concrete wall panels and rear of the decorative panels from view.

[e] Landscaping. The Project proposes landscaping along the entire perimeter of the site, and adjacent to exterior building walls. A substantial landscape area has been provided along the Sunkist Street frontage, which will help to soften the appearance of the architectural screen wall for the dock doors along Sunkist Street. Minimum 15 percent landscape coverage is required per the Ontario Development Code and 15.4 percent landscape coverage has been provided. A variety of plantings are proposed, including Western Redbud, Camphor Trees, Italian Cypress, Chinese Flame Trees, California Sycamore, Coast Live Oak, Brisbane Box, Chinese Elm, and an assortment of water-tolerant shrubs and groundcovers.

[f] Health Risk Assessment. The Project site is within 1,000 feet of existing residential land uses located within Industrial zoning and TOP Policy Plan land use designations. As such, the Applicant was required to have a Health Risk Assessment (“HRA”) prepared to determine whether the proposed Project would pose a health risk to the existing housing units. The HRA analyzed the cancer burden estimates as well as the Project operational Toxic Air Contaminants (“TACs”) impact from Diesel Particulate Matter (“DPM”) emissions. Both analyses concluded that these factors would be less than significant; therefore, no mitigation is required for the Project beyond that which was previously analyzed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), as certified by the Ontario City Council on January 27, 2010.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City’s Economy
- Operate in a Businesslike Manner
- Maintain the Current High Level of Public Safety

[2] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[3] Policy Plan (General Plan)

Land Use Element:

- LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.

Community Economics Element:

- CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
- CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
- CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
- CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

- Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-11 Entry Statements. We encourage the inclusion of amenities, signage and landscaping at the entry to neighborhoods, commercial centers, mixed use areas, industrial developments, and public places that reinforce them as uniquely identifiable places.

➤ CD2-12 Site and Building Signage. We encourage the use of sign programs that utilize complementary materials, colors, and themes. Project signage should be designed to effectively communicate and direct users to various aspects of the development and complement the character of the structures.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: Consistent with the requirements of the California State Aeronautics Act (Public Utilities Code Section 21670 et seq.), the Ontario City Council approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”) on April 19, 2011, establishing the Airport Influence Area for Ontario International Airport and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project was evaluated and found to be consistent with the policies and criteria of the ALUCP. The Project site is impacted by height restrictions and Safety Zones

2 and 4. The allowable building heights range from 65 feet on the northeast corner of the site and 75 feet on the southwest corner of the site. The majority of the Project site and building are located within Safety Zone 2 which requires sitewide average and single-acre occupancy limits. Zone 2 allows for a maximum sitewide average of 60 people per acre and allows a single acre intensity of 120 people. The Project does not exceed Safety Zone 2 occupancy limits and will generate a sitewide average of 24 people per acre and a single-acre intensity of 57 people. Zone 4 allows for a maximum sitewide average of 160 people per acre and allows a single acre intensity of 400 people. The Project does not exceed Safety Zone 4 occupancy limits and will generate a sitewide average of less than 1 person per acre and a single-acre intensity of 1 person. The Project has been conditioned to submit plans to FAA for review and received a "Determination of No Hazard to Air Navigation" for any construction equipment (such as cranes) exceeding 65 feet in height prior to operating any such equipment. Additional special conditions of approval have been placed on the Project to conform with OIAA, FAA and City standards, and are attached to this report.

ENVIRONMENTAL REVIEW: The environmental impacts of this Project were previously reviewed in conjunction with File No. PGPA06-001, a General Plan Amendment for which The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the Ontario City Council on January 27, 2010. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of Project approval and are incorporated herein by this reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Refrigerated Warehouse	Industrial (0.55 FAR)	IL (Light Industrial)	N/A
<i>North</i>	Warehouse	Industrial (0.55 FAR)	IL (Light Industrial)	N/A
<i>South</i>	Warehouse	Business Park (0.6 FAR)	IL (Light Industrial)	N/A
<i>East</i>	Warehouse	Industrial (0.55 FAR)	IG (General Industrial)	N/A
<i>West</i>	Southern Pacific Rail Line	Industrial (0.55 FAR)	IL (Light Industrial)	N/A

General Site & Building Statistics

<i>Item</i>	<i>Proposed</i>	<i>Min./Max. Standard</i>	<i>Meets Y/N</i>
<i>Project Area:</i>	5 acres	N/A	N/A
<i>Lot/Parcel Size:</i>	5 acres	10,000 SF (Min.)	Y
<i>Building Area:</i>	104,993 SF	N/A	N/A
<i>Floor Area Ratio:</i>	0.49	0.55 (Max.)	Y
<i>Building Height:</i>	40 FT	55 FT (Max.)	Y

Off-Street Parking:

<i>Type of Use</i>	<i>Building Area</i>	<i>Parking Ratio</i>	<i>Spaces Required</i>	<i>Spaces Provided</i>
Warehousing and Distribution	94,993 SF	One space per 1,000 SF for portion of GFA greater than or equal to 20,000 SF, plus 0.5 space per 1,000 SF of GFA greater than 20,000 SF; plus required parking for "general business offices"; plus 1 trailer parking space per 4 dock doors	58 passenger 4 trailer	58 passenger 4 trailer
Office	10,000 SF	One space per 250 SF of GFA in excess of 10% of the warehousing and distribution use (appx 500SF)	2	11
TOTAL	104,993 SF		61	69

Exhibit C—PROPOSED SITE PLAN

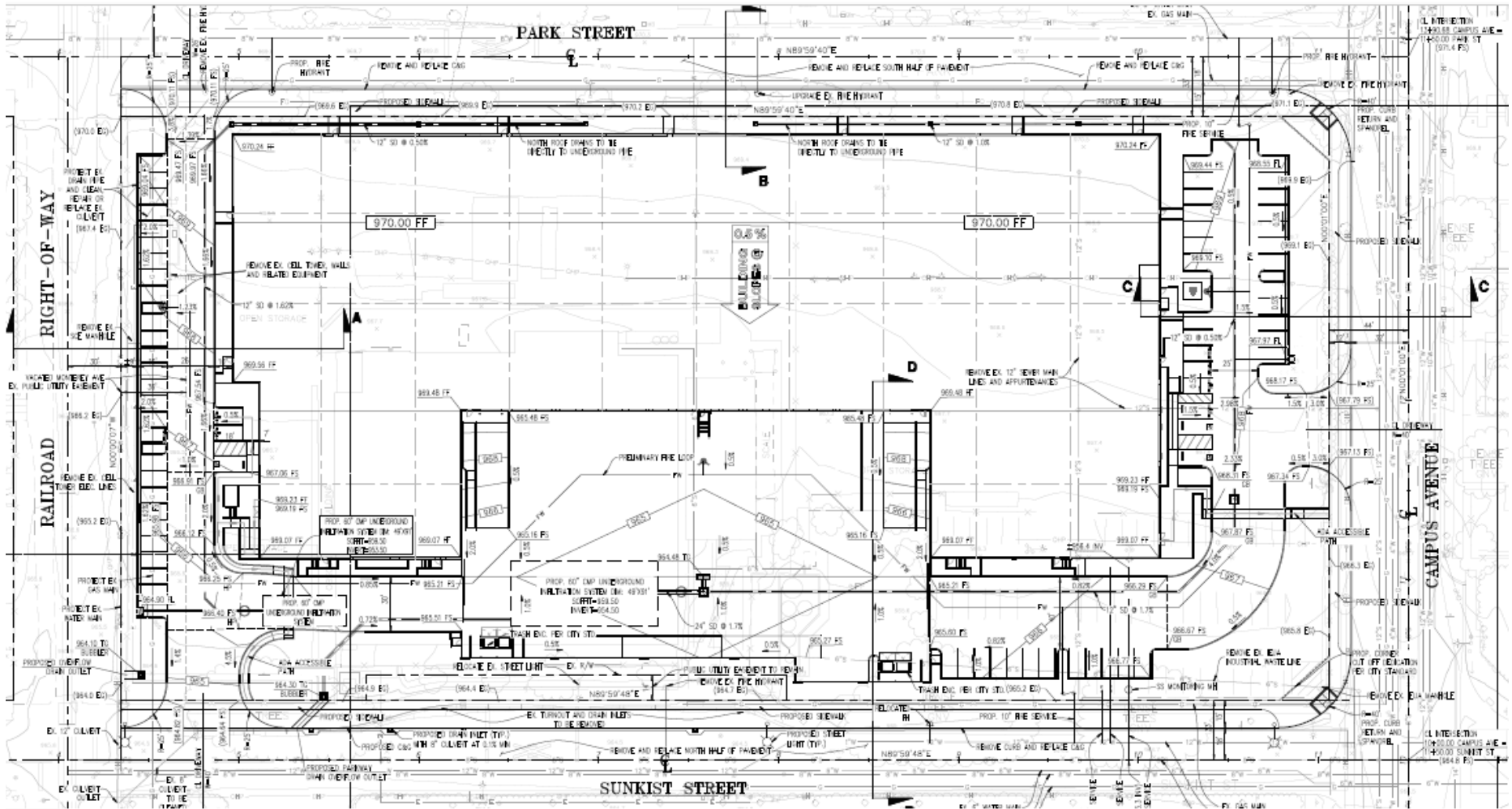


Exhibit D—EXTERIOR ELEVATIONS



Main office entry at Campus Avenue



NORTH ELEVATION @ PARK STREET



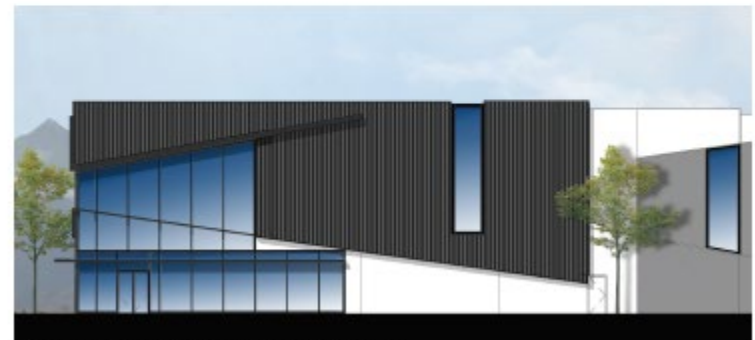
SOUTH ELEVATION @ SUNKIST STREET



WEST ELEVATION

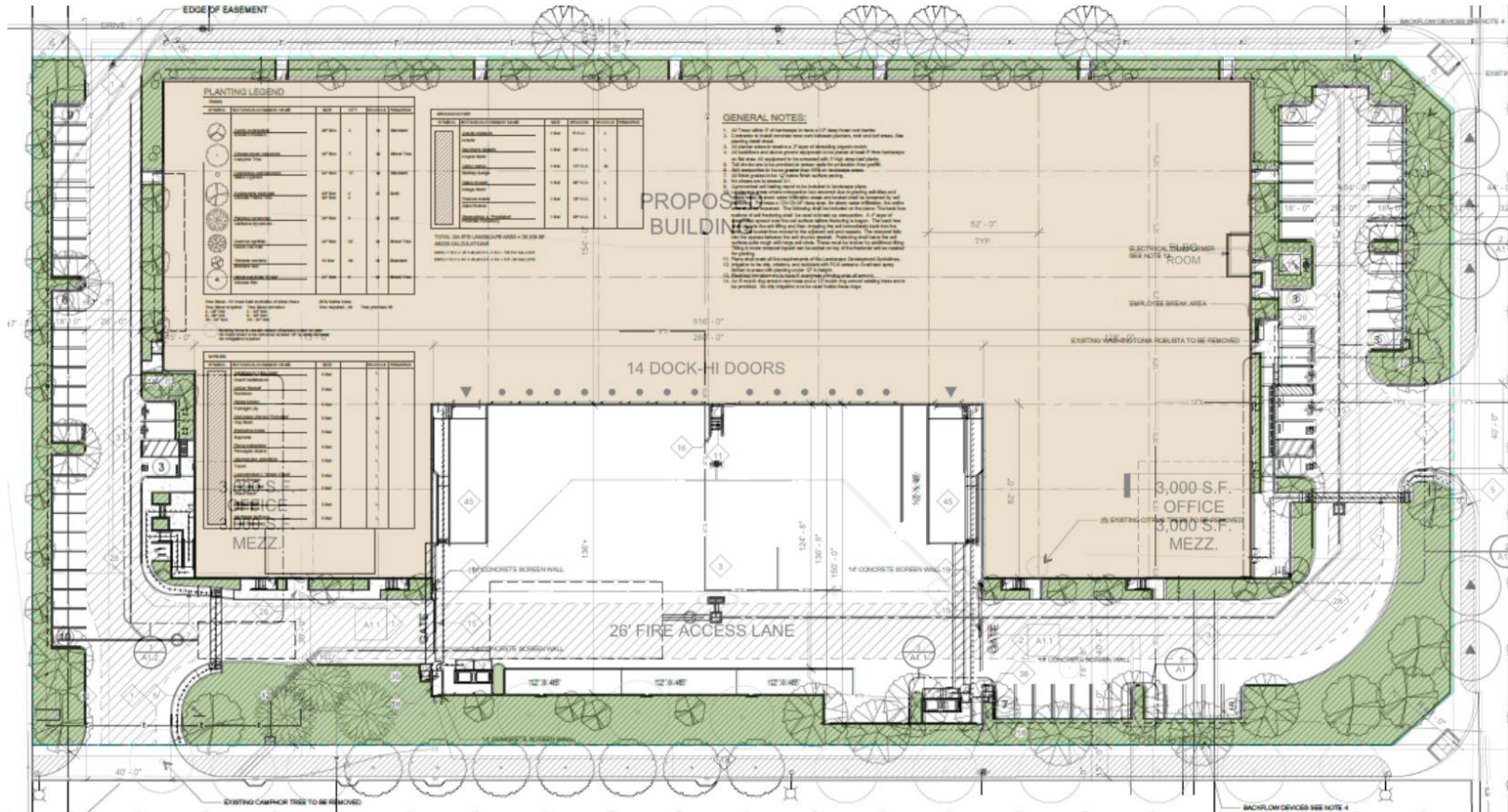


EAST ELEVATION @ CAMPUS AVENUE



ENLARGED VIEW @ OFFICE ENTRY

Exhibit E—LANDSCAPE PLAN



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT19-013, A TENTATIVE PARCEL MAP (PM 20157) TO SUBDIVIDE 5 ACRES OF LAND INTO A SINGLE PARCEL, IN CONJUNCTION WITH A DEVELOPMENT PLAN (FILE NO. PDEV19-050) TO CONSTRUCT A 104,993-SQUARE FOOT INDUSTRIAL BUILDING, LOCATED AT THE NORTHWEST CORNER OF SUNKIST STREET AND CAMPUS AVENUE, AT 617 EAST SUNKIST STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF— APN: 1049-232-21.

WHEREAS, Herdman Architecture and Design ("Applicant") has filed an Application for the approval of a Tentative Parcel Map, File No. PMTT19-013 (PM 20157), as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to approximately 5 acres of land generally located at the northwest corner of Campus Avenue and Sunkist Street, at 617 East Sunkist Street, within the IL (Light Industrial) zoning designation, and is presently improved with an approximate 30,000 square foot refrigerated warehouse and a wireless telecommunications facility, which are proposed to be razed to facilitate construction of the proposed project under File No. PDEV19-050; and

WHEREAS, the properties to the north and south of the project site are zoned IL (Light Industrial) and are developed with warehouse uses. The property to the west of the project site is zoned IL (Light Industrial) and is developed with the Southern Pacific Rail Line. The property to the east of the project site is zoned IG (General Industrial), and is developed with a warehouse; and

WHEREAS, the Project proposes to subdivide the Project site to facilitate the construction of a 104,993 square foot industrial warehouse building. The subdivision will clean up previous lot lines and consolidate the project area into a single parcel, as well as incorporate right-of-way on the south side of the project site, which previously functioned as a bus/carpool turn-out; and

WHEREAS, the Project site will maintain street access to the north, east, and south, and will be bound on the west by Southern Pacific railroad right-of-way. The proposed building will be sited along the northern edge of the property, allowing for parking and drive aisle access along the west, south, and eastern areas of the site; and

WHEREAS, the Project has three points of vehicular access, from Sunkist Street, Park Street, and Campus Avenue. Truck traffic will largely utilize the Sunkist Street and

Campus Avenue drive approaches, while passenger vehicles may utilize all three drive approaches to access passenger vehicle parking areas and the proposed offices; and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. File No. PGPA06-001, a General Plan Amendment for which The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the Ontario City Council on January 27, 2010, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 20, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-016, recommending the Planning Commission approve the Application; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. PGPA06-001, a General Plan Amendment for which The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the Ontario City Council on January 27, 2010.

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder, and the City of Ontario Local CEQA Guidelines; and

(4) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant

environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* Consistent with the requirements of the California State Aeronautics Act (Public Utilities Code Section 21670 et seq.), the Ontario City Council approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”) on April 19, 2011, establishing the Airport Influence Area for Ontario International Airport and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project was evaluated and found to be consistent with the policies

and criteria of the ALUCP. The Project site is impacted by height restrictions and Safety Zones 2 and 4. The allowable building heights range from 65 feet on the northeast corner of the site and 75 feet on the southwest corner of the site. The majority of the Project site and building are located within Safety Zone 2 which requires sitewide average and single-acre occupancy limits. Zone 2 allows for a maximum sitewide average of 60 people per acre and allows a single acre intensity of 120 people. The Project does not exceed Safety Zone 2 occupancy limits and will generate a sitewide average of 24 people per acre and a single-acre intensity of 57 people. Zone 4 allows for a maximum sitewide average of 160 people per acre and allows a single acre intensity of 400 people. The Project does not exceed Safety Zone 4 occupancy limits and will generate a sitewide average of less than 1 person per acre and a single-acre intensity of 1 person. The Project has been conditioned to submit plans to FAA for review and received a "Determination of No Hazard to Air Navigation" for any construction equipment (such as cranes) exceeding 65 feet in height prior to operating any such equipment. Additional special conditions of approval have been placed on the Project to conform with OIAA, FAA and City standards, and are included in Attachment A of this resolution.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract/Parcel Map is located within the Industrial (0.55 FAR) land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The proposed subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to the establishment of "[a] dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses" (Goal CD1). Furthermore, the project will promote the City's policy to "take actions that are consistent with the City being a leading urban center in Southern California, while recognizing the diverse character of our existing viable neighborhoods" (Policy CD1-1 *City Identity*). The proposed Tentative Parcel Map (File No. PMTT19-013) and related Development Plan (File No. PDEV19-050) have been designed to be consistent with both The Ontario Plan and the zoning designation and will harmonize with the surrounding industrial uses.

(2) ***The design or improvement of the proposed Tentative Tract/Parcel Map is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan,***

and applicable specific plans and planned unit developments. The proposed Tentative Tract/Parcel Map is located within the Industrial (0.55 FAR) land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will provide “[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct (Goal CD2). Furthermore, the project will promote the City’s policy to “collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques” (Policy CD2-7 *Sustainability*). The proposed Tentative Parcel Map (File No. PMTT19-013) and related Development Plan (File No. PDEV19-050) have been designed to be consistent with both The Ontario Plan and the zoning designation and will harmonize with the surrounding industrial uses.

(3) ***The site is physically suitable for the type of development proposed.*** The project site meets the minimum lot area and dimensions of the IL (Light Industrial) zoning district and is physically suitable for the type of industrial development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions. The site is currently developed with an industrial refrigerated warehouse building and is proposed to be replaced with a general warehouse and storage building. The proposed design will be complementary to the industrial buildings in the neighborhood and will utilize a decorative screen wall to shield the dock doors from public view and extensive landscaping to soften the appearance of the screen wall and the building. The proposed building will utilize a variety of building materials and techniques to break up the massing and provide visual interest from the public view.

(4) ***The site is physically suitable for the intensity of development proposed.*** The project site is proposed for industrial development at a floor area ratio of approximately 0.48. The project site meets the minimum lot area and dimensions of the IL (Light Industrial) zoning district and is physically suitable for this proposed intensity of development. The IL zoning district and Industrial General Plan Designation allow for development up to 0.55 FAR. The project site is also surrounded by similarly sized industrial developments.

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does

the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat. Furthermore, the site is currently developed with an existing industrial refrigerated warehouse facility and associated paved parking lot and drive aisle access.

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the industrial improvements existing or proposed on the project site, are not likely to cause serious public health problems, as the project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, including the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants to the project site. Moreover, the project is located within the IL (Light Industrial) zoning district, which prohibits the storage, handling, or manufacturing of high-hazard land uses. While the Project site is within 1,000 feet of existing residential land uses located within Industrial zoning and TOP Policy Plan land use designations. As such, the Applicant was required to have a Health Risk Assessment (“HRA”) prepared to determine whether the proposed Project would pose a health risk to the existing housing units. The HRA analyzed the cancer burden estimates as well as the Project operational Toxic Air Contaminants (“TACs”) impact from Diesel Particulate Matter (“DPM”) emissions. Both analyses concluded that these factors would be less than significant.

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 7: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April, 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PMTT19-013
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: April 28, 2020

File No: PMTT19-013

Related File: PDEV19-050

Project Description: A Tentative Parcel Map (File No. PMTT19-013) to subdivide 5 acres of land into a single parcel, in conjunction with a Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district (APN: 1049-232-21); **submitted by Herdman Architecture and Design.**

Prepared By: Alexis Vaughn, Assistant Planner
Phone: 909.395.2416 (direct)
Email: avaughn@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Parcel Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 Subdivision Map.

(a) The Final Parcel Map shall be in conformance with the approved Tentative Parcel Map on file with the City. Variations from the approved Tentative Parcel Map may be reviewed and approved by the Planning Department. A substantial variation from the approved Tentative Parcel Map may require review and approval by the Planning Commission, as determined by the Planning Director.

(b) Tentative Parcel Map approval shall be subject to all conditions, requirements and recommendations from all other departments/agencies provided on the attached reports/memorandums.

(c) Pursuant to California Government Section 66474.9, the subdivider agrees that it will defend, indemnify, and hold harmless the City of Ontario or its agents, officers and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer of this subdivision, which action is brought within the time period provided for in Government Code Section 66499.37. The City of Ontario shall promptly notify the subdivider of any such claim, action or proceeding and the City of Ontario shall cooperate fully in the defense.

2.3 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

2.6 Environmental Review.

(a) The environmental impacts of this project were previously analyzed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.7 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.8 Additional Requirements.

(a) Tentative Parcel Map approval shall not be final and complete until such time that File No. PDEV19-050 has been approved by the Planning Commission.

(b) The Tentative Parcel Map (File No. PMTT19-013) and the related Development Plan (File No. PDEV19-050) shall be coordinated for consistency.



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input checked="" type="checkbox"/> PARCEL MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES	<input type="checkbox"/> TRACT MAP
PROJECT FILE NO. <u>PMTT19-013/PM20157 and PDEV19-050</u> RELATED FILE NO(S). _____		
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__		

CITY PROJECT ENGINEER & PHONE NO: **Nailim Khoury, Associate Engineer** *NK*
(909) 395-2152

CITY PROJECT PLANNER & PHONE NO: **Alexis Vaughn, Assistant Planner**
(909) 395-2431

DAB MEETING DATE: **March 16, 2020**

PROJECT NAME / DESCRIPTION: **A tentative parcel map and development project to consolidate 30 lots into one lot and add approximately 42,000 S.F. building on 4.9 acres of land within the IL (light industrial) Zone**

LOCATION: **617 E. Sunkist Street**

APPLICANT: **Patriot Development Partners**

REVIEWED BY: *[Signature]* 2/28/20
Bryan Lirley, P.E. Date
Principal Engineer

APPROVED BY: *[Signature]* 3/2/20
Raymond Lee, P.E. Date
Assistant City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL AND DEVELOPMENT PROJECT, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO PARCEL MAP/FINAL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:

Property line corner 'cut-back' required at the following intersections:

 - a) **Northwest corner of Campus Avenue and Sunkist Street to accommodate a new curb return of 40-feet.**
 - b) **Southwest corner Campus Avenue and Park Street to accommodate a new curb return of 40-feet.**

- 1.02 Dedicate to the City of Ontario, the following easement(s):

- 1.03 Restrict vehicular access to the site as follows:

- 1.04 **Vacate the following street(s) and/or easement(s):**
 - a) **Vacate the existing City of Ontario easement (behind the half street right-of-way width of 33 feet) along the north side of the existing turnout area on Sunkist Street, west of Campus Avenue and reserve easements for any other existing utilities as required. This turn out shall be removed and the curb line shall be aligned with the existing curbs to the east and west. See item 2.17-a for additional details.**
 - b) **Vacate existing on-site public utility easements (PUE's) on the tentative parcel map except the existing 36 feet Public utility easement within the previously vacated Monterey Avenue (Ref. Resolution No. 6950). Submit copies of non-interference letters and/or other form of approval from the all affected utility companies. The existing City of Ontario public sewer system within the PUE's shall be abandoned or become private.**

- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.

- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.

- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure



requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
- (1) _____
- (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
- 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
- 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
- 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions:

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map No. 20157 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per _____



- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____

- 2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 Make a Dedication of Easement.

- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 2.08 **Submit a soils/geology report.**

- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit, non-interference letter and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA) for any connection, abandonment and/or modification to the existing IEUA facility located on site and at the northwest corner of Campus Avenue and Sunkist Street.**
 - Utility Companies (Edison, Gas, etc.) for any easements within the limits of the subject project (vacated Monterey Avenue and alleys, truck turnout area along Campus Avenue, etc.)**

- 2.10 Dedicate to the City of Ontario the right-of-way described below:
 _____ feet on _____

- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in



accordance with the San Bernardino County Health Department guidelines.

2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).

- 2.13 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.

- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately 111,950.00 shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.

- 2.16 Other conditions: _____



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Campus Avenue	Sunkist Street	Park Street	PUE in Vacated Monterrey Avenue
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> replace due to construction and installation of utilities	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove existing and construct new Curb and Gutter along entire site frontage	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove existing and construct new Curb and Gutter along entire site frontage	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> grind and overlay due to construction and installation of utilities	<input checked="" type="checkbox"/> Remove existing and construct new pavement along project frontage from centerline to new gutter line <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Remove existing and construct new pavement along project frontage from centerline to new gutter line <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Close all existing approaches	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace damaged and uplifted areas	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove existing and construct new sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove existing and construct new sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



ADA Access Ramp	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New curb return, cross gutter & spandrel and ramp at Campus <input checked="" type="checkbox"/> Remove Existing at midblock	<input checked="" type="checkbox"/> New curb return, cross gutter & spandrel and ramp at Campus <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral/M.H.	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Protect existing water main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Modify or replace existing curb inlets and lateral pipes as required	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Modify or replace existing curb inlets and lateral pipes as required	<input checked="" type="checkbox"/> Future 24-inch Main is planned to be constructed in the 30-foot easement. Adequate horizontal clearance shall be provided <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input checked="" type="checkbox"/> Underground or Pay in-lieu fee <input checked="" type="checkbox"/> Relocate power poles due to proposed improvements as required	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

- a) Remove existing turnout along the property frontage of Sunkist Street and realign the curb location to be at 18 feet from street center line to join existing curb lines to the east and west. Removal of this truck bay area shall include but not limited to removal and reconstruction of the following improvements: curb and gutter, sidewalk and landscaped parkway, irrigation, street light, fire hydrant, drains and pull boxes. As a result of these required improvements, the City will vacate the excess public street easement; however the applicant/developer shall be responsible to relocate any exiting dry utilities (e.g. electrical, cable, telephone, etc.) to be placed in the public right-of-way or reserve private utility easements for the existing dry utilities on private property. See item 1.04 for additional details.
- b) Construct new full pavement structural section along the entire project frontages of Sunkist Street and Park Street from street centerline to gutter line plus a minimum of 5 feet grind and overlay beyond the street centerline to the satisfaction of the City Engineer.



c) **The applicant/developer shall be responsible resolve any existing drainage (ponding) issues as part of the new pavement and curb and gutter design on Sunkist Street and Park Street to the satisfaction of the City Engineer.**

- 2.18 **Construct a 2" AC grind and overlay on Campus Avenue from street centerline to gutter line due to improvements and utility trenching as required.**
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \$136,850.00 (391 L.F. x \$350/L.F.) for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.**
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **12-inch and 8-inch sewer mains are available for connection by this project in Campus Avenue and Sunkist street. (Ref: Sewer Atlas sheet Nos. K14).**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 - a) **Final Utilities Systems Map:** The applicant shall submit a complete Final Utilities Systems Map accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. The systems map shall comply with the Ontario Municipal Utilities Company requirements which include showing and labeling all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, Monitoring Manholes, etc.), points of connection, and sizes. This shall include private onsite systems to point of connection with public systems. See USM Requirements document for details.
 - b) **Existing Public Sewer Main:** the existing Public Sewer Main that is located within the Project site shall be abandoned including the segment located in Campus Avenue and the corresponding Public Utility Easement (PUE) shall be vacated.
 - c) **New Public Sewer Manhole:** Install a new manhole on the western end of the Sewer Main stub in Sunkist Street prior to installing and connecting sewer laterals to this main.
 - d) **Wastewater Discharge Permit:** The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to following: installing monitoring manhole, clarifier, or other sewer pretreatment equipment.



D. WATER

- 2.27 12-inch and 8-inch water mains are available for connection by this project in Campus Avenue, Sunkist Street and Park street (Ref: Water Atlas Sheet No. K14).
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 Other conditions:
 - e) **Final Utilities Systems Map:** The applicant shall submit a complete Final Utilities Systems Map accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. The systems map shall comply with the Ontario Municipal Utilities Company requirements which include showing and labeling all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, Monitoring Manholes, etc.), points of connection, and sizes. This shall include private onsite systems to point of connection with public systems. See USM Requirements document for details.
 - f) **Protection of Existing Water Mains:** the existing Public Water Main along the western Project boundary (Vacated Monterey Ave) shall be protected in place and a minimum 20 feet wide Public Utilities Easement (PUE) shall be granted to the City around the main. No permanent structures, infiltration/bioswales, or vegetation with thick roots (such as trees) shall be Place above or within 10 feet of the public water main

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____.
(Ref: Recycled Water plan bar code:_____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

 Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions:
 - a) Applicant/Developer shall be responsible to design and reconstruct the existing curb returns at the southwest corner of Park Street/Campus Avenue and northwest corner of



Sunkist Street/Campus Avenue in accordance with City of Ontario Standard Drawing No. 1106. The new radius shall be 40-feet.

- b) **All proposed drive approaches shall be in accordance with City of Ontario Standard Drawing No. 1204 for commercial driveway approaches. Driveway curb return radii dimensions shall be provided on the site plan.**
- c) **Applicant/Developer shall be responsible to relocate/replace the existing street lights on the north side of Sunkist Street to satisfy minimum spacing requirements due to the installation of new lights on the south side.**
- d) **Applicant/Developer shall be responsible to relocate/replace any existing street lights, conduits, conductors and handholes along project frontages of Campus Avenue, Sunkist Street and Park Street impacted by new curb return and new driveway construction.**
- e) **Applicant/Developer shall replace existing streetlight fixtures with City-approved LED equivalent fixtures, along project frontages of Campus Avenue, Sunkist Street and Park Street. Please refer to the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans.**
- f) **Property frontages along Campus Avenue, Sunkist Street and Park Street shall be signed "No Parking Anytime".**
- g) **All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.**
- h) **Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to starting street lighting design to discuss items such as tie-ins to existing or future street light circuits.**

G. DRAINAGE / HYDROLOGY

- 2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
 (Ref: Storm Drain plan bar code: _____)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions:**
 - a) **The applicant/developer shall be responsible resolve any existing drainage (ponding) issues as part of the new pavement and curb and gutter design on Sunkist Street and Park Street to the satisfaction of the City Engineer.**



- b) **The applicant/developer shall verify the conditions of four (4) existing drain grated inlets along the north side of Sunkist Street and storm drain pipes that traverse diagonally across Sunkist Street just east of the R.R. Tracks and correct and modify any drainage issues associated with the construction of new pavement and curb and gutter. The correction and modification will include but not limited to the following: clean, repair, relocate or replace the storm drain pipes to the satisfaction of the City Engineer. This item must be field verified by City Inspector and/or maintenance personnel prior to submittal of improvement plans.**
- c) **The applicant/developer shall verify the conditions of two (2) existing curb drain inlets along the south side of Park Street including storm drain pipes that traverse diagonally across the PUE in Monterey Avenue just east of the R.R. Tracks and correct any drainage issues associated with this storm drain system. The correction will include but not limited to the following: clean, repair, replace or reroute the storm drain pipes to the satisfaction of the City Engineer. This item must be field verified by City inspector and/or maintenance personnel prior to submittal of improvement plans.**
- d) **A future 24-inch (priority A-13) storm drain main is planned to be located within the existing 36 feet public easement (vacated Monterey Avenue) between Park Street and Sunkist Avenue per the Master Plan of Drainage. The applicant/developer shall verify and show all the existing and proposed utilities including conceptual storm drain main location and demonstrate adequate horizontal spacing is provided for the future storm drain main. Also, no permeant structures with footings, trees, or any other plant materials with intrusive roots systems shall be planted within Place above or within 10 feet of the future public storm main.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP) base on the approved PWQMP. This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 **Other conditions:**
 - a) **Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>**



- b) This project shall comply with the new Trash Amendment by installing 5 mm Connector Pipe Screen, stainless steel screen filters on all new storm drain inlets associated with this project.

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:
<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions:
 - a) Final Solid Waste Handling Plan (SWHP): Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted in to a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
 - b) Integrated Waste Management Report (IWMMR): The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 **Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 **Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 **Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 **Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 **Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 **The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV 19-050 and Parcel Map No. 20157

The following items are required to be included with the first plan check submittal:

1. A copy of this check list
2. Payment of fee for Plan Checking
3. One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.
4. One (1) copy of project Conditions of Approval
5. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. Three (3) sets of Public Street improvement plan with street cross-sections
7. Three (3) sets of Private Street improvement plan with street cross-sections
8. Water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. Four (4) sets of Public Sewer improvement plan
11. Five (5) sets of Public Storm Drain improvement plan
12. Three (3) sets of Public Street Light improvement plan
13. Three (3) sets of Signing and Striping improvement plan
14. Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15. Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).
18. One (1) copy of Hydrology/Drainage study
19. One (1) copy of Soils/Geology report
20. Payment for Final Map/Parcel Map processing fee
21. Three (3) copies of Final Map/Parcel Map



- 22. **One (1) copy of approved Tentative Map**
- 23. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24. **One (1) copy of Traverse Closure Calculations**
- 25. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



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- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Assistant Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: September 17, 2019

SUBJECT: PDEV19-050 – A Development Plan to construct one industrial building totaling 104,993 square feet on 4.91 acres of land located at the northwest corner of Sunkist Street and Campus Avenue at 617 E. Sunkist Street, within the Light Industrial zoning district (APN 1049-232-21). Related File: PMTT19-013.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Type II B
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 98,993 Sq. Ft.
- D. Number of Stories: 1 w/ mezzanine
- E. Total Square Footage: 104,993 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 3375 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO MEMORANDUM

TO: Scott Murphy, Development Director
 Cathy Wahlstrom, Planning Director (Copy of memo only)
 Diane Ayala, Advanced Planning Division (Copy of memo only)
 Charity Hernandez, Economic Development
 Kevin Shear, Building Official
 Khoi Do, City Engineer
 Jamie Richardson, Landscape Planning Division
 Ahmed Aly, Municipal Utility Company
 Emily Hernandez, Police Department
 Paul Ehrman, Deputy Fire Chief/Fire Marshal
 Jay Bautista, T. E., Traffic/Transportation Manager
 Lorena Mejia, Airport Planning
 Eric Woosley, Engineering/NPDES
 Joe De Sousa, Code Enforcement (Copy of memo only)
 Jimmy Chang, IT Department

FROM: Alexis Vaughn, Assistant Planner

DATE: September 06, 2019

SUBJECT: FILE #: PMTT19-013 Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
 - Both DAB and Planning Commission actions are required
 - Only Planning Commission action is required
 - DAB, Planning Commission and City Council actions are required
 - Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Parcel Map to subdivide 4.91 acres of land into one parcel to clean up the site's previous lot lines located at the northwest corner of Sunkist Street and Campus Avenue at 617 E Sunkist Street, within the Light Industrial zoning district (APN: 1049-232-21). Related File(s): PDEV19-050)

- The plan does adequately address the departmental concerns at this time.
 - No comments
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
 - The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

POLICE
Department

EMILY HERNANDEZ
Signature

POLICE OFFICER
Title

9/17/19
Date



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Assistant Planner

FROM: Officer Emily Hernandez, Police Department

DATE: September 17, 2019

SUBJECT: PDEV19-050 – A DEVELOPMENT PLAN TO CONSTRUCT A
CONCRETE TILT-UP WAREHOUSE/DISTRIBUTION FACILITY
LOACTED AT 617 E. SUNKIST STREET.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- First floor common stairwells shall be constructed so as to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Alexis Vaughn
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: September 09, 2019
SUBJECT: PMTT19-013

-
- The plan **does** adequately address the departmental concerns at this time.
 No comments
 Report below.

Conditions of Approval

1. The Site address for this project will be 550 S Campus Ave
2. Standard Conditions of Approval apply.

KS:lr

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Alexis Vaughn
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: September 9, 2019
SUBJECT: PDEV19-050

1. The plan **does** adequately address the departmental concerns at this time.
No comments.

KS:lr

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-050 & PMTT19-013

Address: 617 East Sunkist St

APN: 1049-232-21

Existing Land Use: Industrial Building

Proposed Land Use: Development Plan to construct a 104,993 SF Industrial building and Parcel Map to subdivide the lot into one parcel

Site Acreage: 4.91 Proposed Structure Height: 43

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Lorena Mejia

Date: 4/3/2020

CD No.: 2019-065 REV 1

PALU No.: N/A

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input checked="" type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input checked="" type="checkbox"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input checked="" type="checkbox"/> Zone 4		Allowable Height: <u>65 FT</u>	
<input type="radio"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT provided the attached conditions are met.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2019-065 Rev 1
PALU No.: _____

PROJECT CONDITIONS

1. Project is located within Safety Zone 2 and 4, above ground storage of hazardous materials greater than 6,000 gallons is not allowed (ALUCP Policy S4b (Hazardous Material Storage)).
2. Attached are the land use intensity calculations for the proposed building. Future land uses that deviate from what is currently being approved must meet the policies and criteria of the Ontario ALUCP.
3. The maximum height limit for the project site is 65 feet and as such, any construction equipment such as cranes or any other equipment exceeding 65 feet in height will need a determination of "No Hazard" from the FAA. An FAA Form 7460-1 for any temporary objects will need be filed and approved by the FAA prior to operating such equipment on the project site during construction.
4. New development located within any of the Ontario International Airport Safety Zones are required to have a "Property Located within Ontario International Airport Safety Zone Notification appearing on the Property Deed and Title incorporating the following language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.) The property is presently located in a Safety Zone which limits land uses and the number of people on site. Land uses are required to meet the policies and criteria of the Ontario International Airport Land Use Compatibility Plan.

5. This project is located within Safety Zone 2 and 4 and is required to file and record an Avigation Easement with the OIAA prior to obtaining a Certificate of Occupancy.

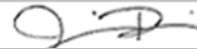
Intensity Calculations for PDEV19-050 PMTT19-013

Intensity Calculations								
				Load Factors	Sitewide Average Calculations (Zone 2 = 60 P/AC max)	Sitewide Average Calculations (Zone 4 = 160 P/AC max)	Zone 2 Single Acre Land Use SF (Zone 2 = 120 P/AC max)	Single Acre Intensity Calculations (Zone 4 = 400 P/AC max)
Building No.	Proposed Land Use	Zone 2 Land Use SF	Zone 4 Land Use SF	ALUCP Load Factor	ALUCP Load Factor	ALUCP Load Factor		ALUCP Load Factor
617 East Sunkist Street	Warehouse	81,962	-	1,000	82	0	43.51	0
	Office	3,000	119	215	14	1	14	1
Totals					24	0.1	57	1
Site Information								
Safety Zone	Acreage	Square Footage						
Zone 2	3.96542	172,734						
Zone 4	0.23615	10,287						
Totals	4.20157	183,020						
Sitewide Average Calculation				Single Acre Intensity Calculation				
Safety Zone 2 = 24				Safety Zone 2 = 57				
Safety Zone 4 = 0.1				Safety Zone 4 = 1				

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

PRELIMINARY PLAN CORRECTIONS

Sign Off



01/08/20

Jamie Richardson, Sr. Landscape Planner

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV19-050

Case Planner:

Alexis Vaughn

Project Name and Location:

Subdivide 4.91 acres

617 E Sunkist Street

Applicant/Representative:

Herdman Architecture and Design

16201 Scientific Way

Irvine, CA 92618



A Preliminary Landscape Plan (dated 2/19/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.



A Preliminary Landscape Plan () has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
3. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans. Identify if the Fiber Optic lines running through the parkways are existing or proposed; if proposed relocate out of parkways and away from the Protected Root Zone (PRZ) of existing trees. Relocate gas and electrical lines out of island planters (west side of the building).

4. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
5. Locate employee break area to location of bike racks for south/west summer shade and locate bicycle racks to northern side of building entrance.
6. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened. Add the following information on the plans: The back hoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

7. Provide a planter space at the westerly entry adjacent to low wall at steps.
8. Coordinate with utility consultant (civil plans show transformer off Sunkist St) to relocate the fire hydrant shown in front of the transformer; dimension 5' on all sides of transformer to avoid bollards.
9. Coordinate with civil on all utility locations (see #3 above).
10. Provide an arborist report and tree inventory as noted in #1.
11. Overhead spray systems shall be designed for plant material less than the height of the spray head.
12. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—less than 5 acres	\$1,301.00
Inspection—Construction (up to 3 inspections per phase)	<u>\$278.00</u>
Total.....	\$1,579.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to:

landscapeplancheck@ontarioca.gov



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Development Director
Cathy Wahlstrom, Planning Director (Copy of memo only)
Diane Ayala, Advanced Planning Division (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, City Engineer
Jamie Richardson, Landscape Planning Division
Ahmed Aly, Municipal Utility Company
Emily Hernandez, Police Department
Paul Ehrman, Deputy Fire Chief/Fire Marshal
Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Joe De Sousa, Code Enforcement (Copy of memo only)
Jimmy Chang, IT Department

FROM: Alexis Vaughn, Assistant Planner

DATE: November 25, 2019

SUBJECT: FILE #: PMTT19-013

Finance Acct#:


The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Monday, December 9, 2019**.

PROJECT DESCRIPTION: A Parcel Map to subdivide 4.91 acres of land into a single parcel located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district (APN: 1049-232-21). Related File: PDEV19-050.

- The plan does adequately address the departmental concerns at this time.
- No comments
 - See previous report for Conditions
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Landscape Planning Division [Signature] Landscape Planner 11/15/19
Department Signature Title Date

**CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764**

DAB CONDITIONS OF APPROVAL	
Sign Off	
	12/19/19
Jamie Richardson, Sr. Landscape Planner	Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner	Phone: (909) 395-2615
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D.A.B. File No.: PMTT19-013	Related Files: PDEV19-050	Case Planner: Alexis Vaughn
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Project Name and Location:
Subdivide 4.91 acres
617 E Sunkist Street

Applicant/Representative:
Herdman Architecture and Design
16201 Scientific Way
Irvine, CA 92618

<input checked="" type="checkbox"/>	A Tentative Tract Map (dated 09/06/2019) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Tentative Tract Map (dated) has not been approved. Corrections noted below are required prior to DAB approval.

CORRECTIONS REQUIRED

1. Relocate utilities to minimum clearances to allow parkway trees. Parkway trees are to be 30' apart and where residential driveways occur, a maximum 45' apart. Show and note a 10' parkway tree space, 5' clearance each side of tree from any utility or hardscape including water, sewer, drain lines and driveways; and min. 10' clear from street lights.

On Grading or Utility Construction Plans:

2. Note on grading plans for compaction to not be greater than 85% at landscape areas; all finished grades 1 1/2" below finished surfaces; landscaped slopes to be max 3:1.
3. Wall footings shall not restrict landscape; max 12" in front of footing with of 12" of cover.
4. Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans.
5. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—less than 5 acres.....	\$1,301.00
Inspection—Construction (up to 3 inspections per phase)	<u>\$278.00</u>
Total.....	\$1,579.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to:
landscapeplancheck@ontarioca.gov

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-050, A DEVELOPMENT PLAN TO CONSTRUCT A 104,993-SQUARE-FOOT INDUSTRIAL BUILDING ON FIVE ACRES OF LAND, LOCATED AT THE NORTHWEST CORNER OF SUNKIST STREET AND CAMPUS AVENUE, AT 617 EAST SUNKIST STREET, WITHIN THE IL (LIGHT INDUSTRIAL) ZONING DISTRICT, AND MAKING FINDINGS IN SUPPORT THEREOF— APN: 1049-232-21.

WHEREAS, Herdman Architecture and Design ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-050, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to approximately five acres of land generally located at the northwest corner of Campus Avenue and Sunkist Street, at 617 East Sunkist Street within the IL (Light Industrial) zoning designation, and is presently improved with an approximate 30,000 square foot refrigerated warehouse and a wireless telecommunications facility which are proposed to be razed to facilitate construction of the proposed Project; and

WHEREAS, the properties to the north and south of the Project site are zoned IL (Light Industrial) and are developed with warehouse uses. The property to the west of the Project site is zoned IL (Light Industrial) and is developed with the Southern Pacific Rail Line. The property to the east of the Project site is zoned IG (General Industrial), and is developed with a warehouse; and

WHEREAS, the proposed Development Plan is for the construction of a 104,993-square-foot industrial building, with a floor area ratio (FAR) of 0.49; and

WHEREAS, the building is sited along the northern edge of the Project site, oriented south, facing Sunkist Street. Two office entries are located at the southwest and southeast corners of the building, facing Sunkist Street and Campus Avenue. The Project will provide parking along the western, southern, and eastern property lines and adjacent to the building; and

WHEREAS, a yard area designed for tractor-trailer parking, truck maneuvering, loading activities, and outdoor staging, is located on the south side of the proposed building and will be screened from public view by decorative fencing and landscaping; and

WHEREAS, the Project has three points of vehicular access, from Sunkist Street, Park Street, and Campus Avenue. Truck traffic will largely utilize the Sunkist Street and

Campus Avenue drive approaches, while passenger vehicles may utilize all three drive approaches to access passenger vehicle parking areas and the proposed offices; and

WHEREAS, the Project has provided off-street parking pursuant to the "Warehouse and Distribution" parking standards specified in the Development Code. The industrial building requires a total of 61 off-street parking spaces and 4 tractor-trailer spaces, and has provided a total of 69 off-street parking spaces and 4 tractor-trailer spaces; and

WHEREAS, the proposed building will be of concrete tilt-up construction, incorporating recessed and smooth painted concrete panels; horizontal, vertical, and angled reveal lines; color blocking; metal window brows and corrugated siding; blue glazing; and blue LED lighting strips; and

WHEREAS, the Project provides landscaping along all property lines and adjacent to the building. The Development Code requires that the Project provide a minimum of 15 percent landscape coverage, and 15.4 percent coverage has been provided; and

WHEREAS, the Project site is within 1,000 feet of existing residential land uses located with Industrial zoning and TOP Policy Plan land use designations, and a Health Risk Assessment ("HRA") was prepared to determine whether the proposed Project would pose a health risk to the existing housing units. The HRA analyzed the cancer burden estimates as well as the Project operational Toxic Air Contaminants ("TACs") impact from Diesel Particulate Matter ("DPM") emissions. Both analyses concluded that these factors would be less than significant; therefore, no mitigation is required for the Project beyond that which was previously analyzed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), as certified by the Ontario City Council on January 27, 2010; and

WHEREAS, the environmental impacts of this Project were previously reviewed in conjunction with File No. File No. PGPA06-001, a General Plan Amendment for which The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the Ontario City Council on January 27, 2010, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (“ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 20, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-017, recommending the Planning Commission approve the Application; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous Certified EIR and supporting documentation. Based upon the facts and information contained in the previous Certified EIR and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this Project were previously reviewed in conjunction with File No. PGPA06-001, a General Plan Amendment for which The Ontario

Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified by the Ontario City Council on January 27, 2010.

(2) The previous Certified EIR contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous Certified EIR was completed in compliance with CEQA and the Guidelines promulgated thereunder, and the City of Ontario Local CEQA Guidelines; and

(4) The previous Certified EIR reflects the independent judgment of the Planning Commission; and

(5) The proposed Project will introduce no new significant environmental impacts beyond those previously analyzed in the previous Certified EIR, and all mitigation measures previously adopted with the Certified EIR, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Certified EIR is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* Consistent with the requirements of the California State Aeronautics Act (Public Utilities Code Section 21670 et seq.), the Ontario City Council approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”) on April 19, 2011, establishing the Airport Influence Area for Ontario International Airport and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project was evaluated and found to be consistent with the policies and criteria of the ALUCP. The Project site is impacted by height restrictions and Safety Zones 2 and 4. The allowable building heights range from 65 feet on the northeast corner of the site and 75 feet on the southwest corner of the site. The majority of the Project site and building are located within Safety Zone 2 which requires sitewide average and single-acre occupancy limits. Zone 2 allows for a maximum sitewide average of 60 people per acre and allows a single acre intensity of 120 people. The Project does not exceed Safety Zone 2 occupancy limits and will generate a sitewide average of 24 people per acre and a single-acre intensity of 57 people. Zone 4 allows for a maximum sitewide average of 160 people per acre and allows a single acre intensity of 400 people. The Project does not exceed Safety Zone 4 occupancy limits and will generate a sitewide average of less than 1 person per acre and a single-acre intensity of 1 person. The Project has been conditioned to submit plans to FAA for review and received a “Determination of No Hazard to Air Navigation” for any construction equipment (such as cranes) exceeding 65 feet in height prior to operating any such equipment. Additional special conditions of approval have been placed on the Project to conform with OIAA, FAA and City standards, and are included in Attachment A of this resolution.

SECTION 5: Concluding Facts and Reasons. Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Industrial (0.55 FAR) land use district of the Policy Plan Land Use Map, and the IL (Light Industrial) zoning district. The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. The proposed construction of an industrial building will contribute to the surrounding industrial neighborhood, and the overall landscaping and site improvements will contribute to the streetscapes along Sunkist Street, Park Street, and Campus Avenue, which meets CD2-9 (Landscape Design) and CD3-6 (Landscaping). Further, the proposed building has been designed to be compatible with the surrounding industrial neighborhood in terms of scale, design, massing, and use. The Project proposes to remove the existing refrigerated warehouse facility, whose aging building and lack of landscaping or proper screen walls provides a visual nuisance to the neighborhood; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the IL (Light Industrial) zoning district, including standards relative to the particular land use proposed (industrial/warehouse), as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. The proposed Project was designed to be compatible with the neighboring industrial buildings in terms of massing, architectural design and features, and use, and will not cause any privacy, view, or physical constraint issues for any of the neighboring properties; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Development Code are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in

which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Development Code. The proposed Project is complementary to the surroundings in terms of use, massing, and architecture, and will install an extensive landscape buffer along the southern and eastern property lines to soften the appearance of the decorative screen wall and parking areas. Conditions have also been imposed upon the Project to provide adequate site lighting for safety; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Development Code that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (industrial/warehouse). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the development standards and guidelines described in the Development Code.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April, 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV19-050
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: April 28, 2020

File No: PDEV19-050

Related File: PMTT19-013

Project Description: A Development Plan (File No. PDEV19-050) to construct a 104,993 square-foot industrial building on five acres of land, located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district (APN: 1049-232-21); **submitted by Herdman Architecture and Design.**

Prepared By: Alexis Vaughn, Assistant Planner
Phone: 909.395.2416 (direct)
Email: avaughn@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.6 Outdoor Loading and Storage Areas.

(a) Loading facilities shall be designed and constructed pursuant to Development Code Division 6.03 (Off-Street Parking and Loading).

(b) Areas designated for off-street parking, loading, and vehicular circulation and maneuvering, shall not be used for the outdoor storage of materials or equipment.

(c) Outdoor loading and storage areas, and loading doors, shall be screened from public view pursuant to the requirements of Development Code Paragraph 6.02.025.A.2 (Screening of Outdoor Loading and Storage Areas, and Loading Doors) Et Seq.

(d) Outdoor loading and storage areas shall be provided with gates that are view-obstructing by one of the following methods:

(i) Construct gates with a perforated metal sheet affixed to the inside of the gate surface (50 percent screen); or

(ii) Construct gates with minimum one-inch square tube steel pickets spaced at maximum 2-inches apart.

(e) The minimum gate height for screen wall openings shall be established based upon the corresponding wall height, as follows:

Screen Wall Height	Minimum Gate Height
14 feet:	10 feet
12 feet:	9 feet
10 feet:	8 feet
8 feet:	8 feet
6 feet:	6 feet

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Environmental Review.

(a) The environmental impacts of this project were previously analyzed in The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This application introduces no new significant environmental impacts. The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts. All previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference. All previously adopted mitigation measures shall be a condition of project approval, as they are applicable, and are incorporated herein by this reference.

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.13 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.14 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.15 Additional Requirements.

(a) Development Plan approval shall not be final and complete until such time that File No. PMTT19-030 (PM 20157) has been approved by the Planning Commission.

(b) The Development Plan (File No. PDEV19-050) and the related Tentative Parcel Map (File No. PMTT19-013) shall be coordinated for consistency.

(c) The exterior building elevations shall be revised pursuant to Exhibit A (Exterior Elevations Required Revisions) of these Planning Department, Land Development Division, conditions of approval.

(d) The projecting metal siding shall be provided with a return to the building wall, so as to screen the open area between the concrete wall panels and the back of the metal siding panels.

EXHIBIT A—Exterior Elevations Required Revisions



NORTH ELEVATION @ PARK STREET



SOUTH ELEVATION @ SUNKIST STREET



WEST ELEVATION



EAST ELEVATION @ CAMPUS AVENUE



ENLARGED VIEW @ OFFICE ENTRY



ENGINEERING DEPARTMENT CONDITIONS OF APPROVAL

(Engineering Services Division [Land Development Section and Environmental Section], Traffic & Transportation Division, Ontario Municipal Utilities Company and Information Technology & Management Services Department Conditions incorporated)

<input checked="" type="checkbox"/> DEVELOPMENT PLAN <input type="checkbox"/> OTHER	<input checked="" type="checkbox"/> PARCEL MAP <input type="checkbox"/> FOR CONDOMINIUM PURPOSES	<input type="checkbox"/> TRACT MAP
PROJECT FILE NO. <u>PMTT19-013/PM20157 and PDEV19-050</u> RELATED FILE NO(S). _____		
<input checked="" type="checkbox"/> ORIGINAL <input type="checkbox"/> REVISED: __/__/__		

CITY PROJECT ENGINEER & PHONE NO: **Nailim Khoury, Associate Engineer** *NK*
(909) 395-2152

CITY PROJECT PLANNER & PHONE NO: **Alexis Vaughn, Assistant Planner**
(909) 395-2431

DAB MEETING DATE: **March 16, 2020**

PROJECT NAME / DESCRIPTION: **A tentative parcel map and development project to consolidate 30 lots into one lot and add approximately 42,000 S.F. building on 4.9 acres of land within the IL (light industrial) Zone**

LOCATION: **617 E. Sunkist Street**

APPLICANT: **Patriot Development Partners**

REVIEWED BY: *[Signature]* 2/28/20
Bryan Lirley, P.E. Date
Principal Engineer

APPROVED BY: *[Signature]* 3/2/20
Raymond Lee, P.E. Date
Assistant City Engineer



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO PARCEL MAP APPROVAL AND DEVELOPMENT PROJECT, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO PARCEL MAP/FINAL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:

Property line corner 'cut-back' required at the following intersections:

 - a) **Northwest corner of Campus Avenue and Sunkist Street to accommodate a new curb return of 40-feet.**
 - b) **Southwest corner Campus Avenue and Park Street to accommodate a new curb return of 40-feet.**

- 1.02 Dedicate to the City of Ontario, the following easement(s): _____

- 1.03 Restrict vehicular access to the site as follows: _____

- 1.04 **Vacate the following street(s) and/or easement(s):**
 - a) **Vacate the existing City of Ontario easement (behind the half street right-of-way width of 33 feet) along the north side of the existing turnout area on Sunkist Street, west of Campus Avenue and reserve easements for any other existing utilities as required. This turn out shall be removed and the curb line shall be aligned with the existing curbs to the east and west. See item 2.17-a for additional details.**
 - b) **Vacate existing on-site public utility easements (PUE's) on the tentative parcel map except the existing 36 feet Public utility easement within the previously vacated Monterey Avenue (Ref. Resolution No. 6950). Submit copies of non-interference letters and/or other form of approval from the all affected utility companies. The existing City of Ontario public sewer system within the PUE's shall be abandoned or become private.**

- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.

- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open space/easements. In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.

- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure



requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
- (1) _____
- (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required, or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
- 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
- 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm Water Treatment Equivalents).
- 3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).
- 1.14 Other conditions:

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

- 2.01 Record Parcel Map No. 20157 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.
- 2.02 Submit a duplicate photo mylar of the recorded map to the City Engineer's office.
- 2.03 Note that the subject parcel is a recognized parcel in the City of Ontario per _____



- 2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____.
- 2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 Make a Dedication of Easement.
- 2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.
- 2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 2.08 **Submit a soils/geology report.**
- 2.09 **Other Agency Permit/Approval: Submit a copy of the approved permit, non-interference letter and/or other form of approval of the project from the following agency or agencies:**
 - State of California Department of Transportation (Caltrans)
 - San Bernardino County Road Department (SBCRD)
 - San Bernardino County Flood Control District (SBCFCD)
 - Federal Emergency Management Agency (FEMA)
 - Cucamonga Valley Water District (CVWD) for sewer/water service
 - United States Army Corps of Engineers (USACE)
 - California Department of Fish & Game
 - Inland Empire Utilities Agency (IEUA) for any connection, abandonment and/or modification to the existing IEUA facility located on site and at the northwest corner of Campus Avenue and Sunkist Street.**
 - Utility Companies (Edison, Gas, etc.) for any easements within the limits of the subject project (vacated Monterey Avenue and alleys, truck turnout area along Campus Avenue, etc.)**
- 2.10 Dedicate to the City of Ontario the right-of-way described below:
 _____ feet on _____
- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in



accordance with the San Bernardino County Health Department guidelines.

2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.

3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).

- 2.13 Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at _____% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.

- 2.14 The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.

- 2.15 Pay all Development Impact Fees (DIF) to the Building Department. Storm Drain Development Impact Fee, approximately 111,950.00 shall be paid to the Building Department. Final fee shall be determined based on the approved site plan.

- 2.16 Other conditions: _____



B. PUBLIC IMPROVEMENTS

(See attached Exhibit 'A' for plan check submittal requirements.)

- 2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Campus Avenue	Sunkist Street	Park Street	PUE in Vacated Monterrey Avenue
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input checked="" type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> replace due to construction and installation of utilities	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove existing and construct new Curb and Gutter along entire site frontage	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input checked="" type="checkbox"/> Remove existing and construct new Curb and Gutter along entire site frontage	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input checked="" type="checkbox"/> grind and overlay due to construction and installation of utilities	<input checked="" type="checkbox"/> Remove existing and construct new pavement along project frontage from centerline to new gutter line <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input checked="" type="checkbox"/> Remove existing and construct new pavement along project frontage from centerline to new gutter line <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input checked="" type="checkbox"/> Close all existing approaches	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove and replace damaged and uplifted areas	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove existing and construct new sidewalk	<input type="checkbox"/> New <input checked="" type="checkbox"/> Remove existing and construct new sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace



ADA Access Ramp	<input checked="" type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input checked="" type="checkbox"/> New curb return, cross gutter & spandrel and ramp at Campus <input checked="" type="checkbox"/> Remove Existing at midblock	<input checked="" type="checkbox"/> New curb return, cross gutter & spandrel and ramp at Campus <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input checked="" type="checkbox"/> Trees <input checked="" type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New/ Upgrade as required <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Lateral/M.H.	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Water (see Sec. 2.D)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input checked="" type="checkbox"/> Protect existing water main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input checked="" type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input checked="" type="checkbox"/> New / Upgrade to LED <input checked="" type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation



Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Modify or replace existing curb inlets and lateral pipes as required	<input type="checkbox"/> Main <input checked="" type="checkbox"/> Modify or replace existing curb inlets and lateral pipes as required	<input checked="" type="checkbox"/> Future 24-inch Main is planned to be constructed in the 30-foot easement. Adequate horizontal clearance shall be provided <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input checked="" type="checkbox"/> Underground or Pay in-lieu fee <input checked="" type="checkbox"/> Relocate power poles due to proposed improvements as required	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above:

- a) Remove existing turnout along the property frontage of Sunkist Street and realign the curb location to be at 18 feet from street center line to join existing curb lines to the east and west. Removal of this truck bay area shall include but not limited to removal and reconstruction of the following improvements: curb and gutter, sidewalk and landscaped parkway, irrigation, street light, fire hydrant, drains and pull boxes. As a result of these required improvements, the City will vacate the excess public street easement; however the applicant/developer shall be responsible to relocate any exiting dry utilities (e.g. electrical, cable, telephone, etc.) to be placed in the public right-of-way or reserve private utility easements for the existing dry utilities on private property. See item 1.04 for additional details.
- b) Construct new full pavement structural section along the entire project frontages of Sunkist Street and Park Street from street centerline to gutter line plus a minimum of 5 feet grind and overlay beyond the street centerline to the satisfaction of the City Engineer.



c) **The applicant/developer shall be responsible resolve any existing drainage (ponding) issues as part of the new pavement and curb and gutter design on Sunkist Street and Park Street to the satisfaction of the City Engineer.**

- 2.18 **Construct a 2" AC grind and overlay on Campus Avenue from street centerline to gutter line due to improvements and utility trenching as required.**
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.
- 2.21 **Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately \$136,850.00 (391 L.F. x \$350/L.F.) for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.**
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **12-inch and 8-inch sewer mains are available for connection by this project in Campus Avenue and Sunkist street. (Ref: Sewer Atlas sheet Nos. K14).**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions:**
 - a) **Final Utilities Systems Map:** The applicant shall submit a complete Final Utilities Systems Map accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. The systems map shall comply with the Ontario Municipal Utilities Company requirements which include showing and labeling all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, Monitoring Manholes, etc.), points of connection, and sizes. This shall include private onsite systems to point of connection with public systems. See USM Requirements document for details.
 - b) **Existing Public Sewer Main:** the existing Public Sewer Main that is located within the Project site shall be abandoned including the segment located in Campus Avenue and the corresponding Public Utility Easement (PUE) shall be vacated.
 - c) **New Public Sewer Manhole:** Install a new manhole on the western end of the Sewer Main stub in Sunkist Street prior to installing and connecting sewer laterals to this main.
 - d) **Wastewater Discharge Permit:** The Occupant of the building shall apply for a Wastewater Discharge Permit for their Establishment and shall comply with all the requirements of their Wastewater Discharge Permit. Requirements of Wastewater Discharge Permit may include, but not limited to following: installing monitoring manhole, clarifier, or other sewer pretreatment equipment.



D. WATER

- 2.27 12-inch and 8-inch water mains are available for connection by this project in Campus Avenue, Sunkist Street and Park street (Ref: Water Atlas Sheet No. K14).
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 Other conditions:
 - e) **Final Utilities Systems Map:** The applicant shall submit a complete Final Utilities Systems Map accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. The systems map shall comply with the Ontario Municipal Utilities Company requirements which include showing and labeling all existing and proposed utilities (including all appurtenances such as backflow devices, DCDAs, Monitoring Manholes, etc.), points of connection, and sizes. This shall include private onsite systems to point of connection with public systems. See USM Requirements document for details.
 - f) **Protection of Existing Water Mains:** the existing Public Water Main along the western Project boundary (Vacated Monterey Ave) shall be protected in place and a minimum 20 feet wide Public Utilities Easement (PUE) shall be granted to the City around the main. No permanent structures, infiltration/bioswales, or vegetation with thick roots (such as trees) shall be Place above or within 10 feet of the public water main

E. RECYCLED WATER

- 2.30 A _____ inch recycled water main is available for connection by this project in _____.
(Ref: Recycled Water plan bar code:_____)
- 2.31 Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project, but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.
- 2.33 Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.

 Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.
- 2.34 Other conditions: _____

F. TRAFFIC / TRANSPORTATION

- 2.35 Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:
 - 1. On-site and off-site circulation
 - 2. Traffic level of service (LOS) at 'build-out' and future years
 - 3. Impact at specific intersections as selected by the City Engineer
- 2.36 New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.
- 2.37 Other conditions:
 - a) Applicant/Developer shall be responsible to design and reconstruct the existing curb returns at the southwest corner of Park Street/Campus Avenue and northwest corner of



Sunkist Street/Campus Avenue in accordance with City of Ontario Standard Drawing No. 1106. The new radius shall be 40-feet.

- b) **All proposed drive approaches shall be in accordance with City of Ontario Standard Drawing No. 1204 for commercial driveway approaches. Driveway curb return radii dimensions shall be provided on the site plan.**
- c) **Applicant/Developer shall be responsible to relocate/replace the existing street lights on the north side of Sunkist Street to satisfy minimum spacing requirements due to the installation of new lights on the south side.**
- d) **Applicant/Developer shall be responsible to relocate/replace any existing street lights, conduits, conductors and handholes along project frontages of Campus Avenue, Sunkist Street and Park Street impacted by new curb return and new driveway construction.**
- e) **Applicant/Developer shall replace existing streetlight fixtures with City-approved LED equivalent fixtures, along project frontages of Campus Avenue, Sunkist Street and Park Street. Please refer to the Traffic and Transportation Design Guidelines Section 1.4 Street Light Plans.**
- f) **Property frontages along Campus Avenue, Sunkist Street and Park Street shall be signed "No Parking Anytime".**
- g) **All landscaping, block walls, and other obstructions shall be compatible with the stopping sight distance requirements per City of Ontario Standard Drawing No. 1309.**
- h) **Applicant/Developer's engineer-of-record shall meet with City Engineering staff prior to starting street lighting design to discuss items such as tie-ins to existing or future street light circuits.**

G. DRAINAGE / HYDROLOGY

- 2.38 A _____ inch storm drain main is available to accept flows from this project in _____.
 (Ref: Storm Drain plan bar code: _____)
- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study.**
- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**
- 2.41 Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.
- 2.42 Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.
- 2.43 **Other conditions:**
 - a) **The applicant/developer shall be responsible resolve any existing drainage (ponding) issues as part of the new pavement and curb and gutter design on Sunkist Street and Park Street to the satisfaction of the City Engineer.**



- b) **The applicant/developer shall verify the conditions of four (4) existing drain grated inlets along the north side of Sunkist Street and storm drain pipes that traverse diagonally across Sunkist Street just east of the R.R. Tracks and correct and modify any drainage issues associated with the construction of new pavement and curb and gutter. The correction and modification will include but not limited to the following: clean, repair, relocate or replace the storm drain pipes to the satisfaction of the City Engineer. This item must be field verified by City Inspector and/or maintenance personnel prior to submittal of improvement plans.**
- c) **The applicant/developer shall verify the conditions of two (2) existing curb drain inlets along the south side of Park Street including storm drain pipes that traverse diagonally across the PUE in Monterey Avenue just east of the R.R. Tracks and correct any drainage issues associated with this storm drain system. The correction will include but not limited to the following: clean, repair, replace or reroute the storm drain pipes to the satisfaction of the City Engineer. This item must be field verified by City inspector and/or maintenance personnel prior to submittal of improvement plans.**
- d) **A future 24-inch (priority A-13) storm drain main is planned to be located within the existing 36 feet public easement (vacated Monterey Avenue) between Park Street and Sunkist Avenue per the Master Plan of Drainage. The applicant/developer shall verify and show all the existing and proposed utilities including conceptual storm drain main location and demonstrate adequate horizontal spacing is provided for the future storm drain main. Also, no permeant structures with footings, trees, or any other plant materials with intrusive roots systems shall be planted within Place above or within 10 feet of the future public storm main.**

H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 **Submit a Water Quality Management Plan (WQMP) base on the approved PWQMP. This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at: <http://www.sbcounty.gov/dpw/land/npdes.asp>.**
- 2.46 Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications, and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.
- 2.47 **Other conditions:**
 - a) **Activities resulting in land disturbance of one acre or more is required to obtain coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at <https://smarts.waterboards.ca.gov/smarts/faces/SwSmartsLogin.xhtml>**



- b) This project shall comply with the new Trash Amendment by installing 5 mm Connector Pipe Screen, stainless steel screen filters on all new storm drain inlets associated with this project.

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.
- 2.51 Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.

L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual location at:
<http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions:
 - a) Final Solid Waste Handling Plan (SWHP): Prior to approval of the any building permits, the Conceptual Solid Waste Handling Plan needs to be updated and converted in to a Final Solid Waste Handling Plan Sheet and shall be submitted accompanying the Precise Grading Plan Submittal to the City/OMUC for review and approval. See "Solid Waste Handling Plan (SWHP) Requirements" document for details.
 - b) Integrated Waste Management Report (IWMMR): The applicant shall submit an Integrated Waste Management Report for review and approval with the Precise Grading Plan. This report shall address the management of all integrated waste (Refuse, Recycling, Organics, etc.). The IWMMR shall demonstrate compliance with the "Integrated Waste Management Report Requirements" document.



3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.**
- 3.02 Complete all requirements for recycled water usage.**
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.**
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.**
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studie and reports (i.e. hydrology, traffic, WQMP, etc.).**

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.**
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.**
- 4.03 The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.**



EXHIBIT 'A'

ENGINEERING DEPARTMENT First Plan Check Submittal Checklist

Project Number: PDEV 19-050 and Parcel Map No. 20157

The following items are required to be included with the first plan check submittal:

1. A copy of this check list
2. Payment of fee for Plan Checking
3. One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.
4. One (1) copy of project Conditions of Approval
5. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. Three (3) sets of Public Street improvement plan with street cross-sections
7. Three (3) sets of Private Street improvement plan with street cross-sections
8. Water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. Four (4) sets of Public Sewer improvement plan
11. Five (5) sets of Public Storm Drain improvement plan
12. Three (3) sets of Public Street Light improvement plan
13. Three (3) sets of Signing and Striping improvement plan
14. Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)
15. Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).
18. One (1) copy of Hydrology/Drainage study
19. One (1) copy of Soils/Geology report
20. Payment for Final Map/Parcel Map processing fee
21. Three (3) copies of Final Map/Parcel Map



- 22. **One (1) copy of approved Tentative Map**
- 23. **One (1) copy of Preliminary Title Report (current within 30 days)**
- 24. **One (1) copy of Traverse Closure Calculations**
- 25. **One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.**
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



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 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 **The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.**
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- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Assistant Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: September 17, 2019

SUBJECT: PDEV19-050 – A Development Plan to construct one industrial building totaling 104,993 square feet on 4.91 acres of land located at the northwest corner of Sunkist Street and Campus Avenue at 617 E. Sunkist Street, within the Light Industrial zoning district (APN 1049-232-21). Related File: PMTT19-013.

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.
-

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Type II B
- B. Type of Roof Materials: Panelized
- C. Ground Floor Area(s): 98,993 Sq. Ft.
- D. Number of Stories: 1 w/ mezzanine
- E. Total Square Footage: 104,993 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): S

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department (“Fire Department”) requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards (“Standards.”) It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on “Fire Department” and then on “Standards and Forms.”
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25’) inside and forty-five feet (45’) outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150’) in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 3375 gallons per minute (g.p.m.) for 4 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.3 Buildings that exceed 100,000 square feet in floor area shall provide an onsite looped fire protection water line around the building(s.) The loops shall be required to have two or more points of connection from a public circulating water main.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.
- ☒ 5.7 Placards shall be installed in acceptable locations on buildings that store, use or handle hazardous materials in excess of the quantities specified in the CFC. Placards shall meet the requirements of National Fire Protection Association (NFPA) Standard 704.

6.0 OTHER SPECIAL USES

- ☒ 6.1 The storage, use, dispensing, or handling of any hazardous materials shall be approved by the Fire Department, and adequate fire protection features shall be required. If hazardous materials are proposed, a Fire Department Hazardous Materials Information Packet, including Disclosure Form and Information Worksheet, shall be completed and submitted with Material Safety Data Sheets to the Fire Department along with building construction plans.
- ☒ 6.2 Any High Piled Storage, or storage of combustible materials greater than twelve (12') feet in height for ordinary (Class I-IV) commodities or storage greater than six feet (6') in height of high hazard (Group A plastics, rubber tires, flammable liquids, etc.) shall be approved by the Fire Department, and adequate fire protection features shall be required. If High Piled Storage is proposed, a Fire Department High Piled Storage Worksheet shall be completed and detailed racking plans or floor plans submitted prior to occupancy of the building.
- ☒ 6.3 Underground fuel tanks, their associated piping and dispensers shall be reviewed, approved, and permitted by Ontario Building Department, Ontario Fire Department, and San Bernardino County Fire Department Hazardous Materials Division. In fueling facilities, an exterior emergency pump shut-off switch shall be provided.



CITY OF ONTARIO MEMORANDUM

TO: Scott Murphy, Development Director
 Cathy Wahlstrom, Planning Director (Copy of memo only)
 Diane Ayala, Advanced Planning Division (Copy of memo only)
 Charity Hernandez, Economic Development
 Kevin Shear, Building Official
 Khoi Do, City Engineer
 Jamie Richardson, Landscape Planning Division
 Ahmed Aly, Municipal Utility Company
 Emily Hernandez, Police Department
 Paul Ehrman, Deputy Fire Chief/Fire Marshal
 Jay Bautista, T. E., Traffic/Transportation Manager
 Lorena Mejia, Airport Planning
 Eric Woosley, Engineering/NPDES
 Joe De Sousa, Code Enforcement (Copy of memo only)
 Jimmy Chang, IT Department

FROM: Alexis Vaughn, Assistant Planner

DATE: September 06, 2019

SUBJECT: FILE #: PMTT19-013

Finance Acct#:

The following project has been submitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by .

- Note:
- Only DAB action is required
 - Both DAB and Planning Commission actions are required
 - Only Planning Commission action is required
 - DAB, Planning Commission and City Council actions are required
 - Only Zoning Administrator action is required

PROJECT DESCRIPTION: A Parcel Map to subdivide 4.91 acres of land into one parcel to clean up the site's previous lot lines located at the northwest corner of Sunkist Street and Campus Avenue at 617 E Sunkist Street, within the Light Industrial zoning district (APN: 1049-232-21). Related File(s): PDEV19-050)

The plan does adequately address the departmental concerns at this time.

- No comments
- Report attached (1 copy and email 1 copy)
- Standard Conditions of Approval apply

The plan does not adequately address the departmental concerns.

- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

POLICE
Department

EMILY HERNANDEZ
Signature

POLICE OFFICER
Title

9/17/19
Date



CITY OF ONTARIO

MEMORANDUM

TO: Alexis Vaughn, Assistant Planner

FROM: Officer Emily Hernandez, Police Department

DATE: September 17, 2019

SUBJECT: PDEV19-050 – A DEVELOPMENT PLAN TO CONSTRUCT A
CONCRETE TILT-UP WAREHOUSE/DISTRIBUTION FACILITY
LOACTED AT 617 E. SUNKIST STREET.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for all walkways, driveways, doorways, parking lots, hallways and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 6 feet tall and 2 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- First floor common stairwells shall be constructed so as to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.

The Applicant is invited to contact Officer Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Alexis Vaughn
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: September 09, 2019
SUBJECT: PMTT19-013

-
- The plan **does** adequately address the departmental concerns at this time.
 No comments
 Report below.

Conditions of Approval

1. The Site address for this project will be 550 S Campus Ave
2. Standard Conditions of Approval apply.

KS:lr

CITY OF ONTARIO

MEMORANDUM

TO: PLANNING DEPARTMENT, Alexis Vaughn
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: September 9, 2019
SUBJECT: PDEV19-050

1. The plan **does** adequately address the departmental concerns at this time.
No comments.

KS:lr

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PDEV19-050 & PMTT19-013

Address: 617 East Sunkist St

APN: 1049-232-21

Existing Land Use: Industrial Building

Proposed Land Use: Development Plan to construct a 104,993 SF Industrial building and Parcel Map to subdivide the lot into one parcel

Site Acreage: 4.91 Proposed Structure Height: 43

ONT-IAC Project Review: N/A

Airport Influence Area: ONT

Reviewed By: Lorena Mejia

Contact Info: 909-395-2276

Project Planner: Lorena Mejia

Date: 4/3/2020

CD No.: 2019-065 REV 1

PALU No.: N/A

The project is impacted by the following ONT ALUCP Compatibility Zones:

Safety	Noise Impact	Airspace Protection	Overflight Notification
<input type="radio"/> Zone 1	<input type="radio"/> 75+ dB CNEL	<input type="checkbox"/> High Terrain Zone	<input checked="" type="checkbox"/> Avigation Easement Dedication
<input type="radio"/> Zone 1A	<input type="radio"/> 70 - 75 dB CNEL	<input checked="" type="checkbox"/> FAA Notification Surfaces	<input type="checkbox"/> Recorded Overflight Notification
<input checked="" type="checkbox"/> Zone 2	<input checked="" type="checkbox"/> 65 - 70 dB CNEL	<input checked="" type="checkbox"/> Airspace Obstruction Surfaces	<input type="checkbox"/> Real Estate Transaction Disclosure
<input type="checkbox"/> Zone 3	<input type="checkbox"/> 60 - 65 dB CNEL	<input checked="" type="checkbox"/> Airspace Avigation Easement Area	
<input checked="" type="checkbox"/> Zone 4		Allowable Height: <u>65 FT</u>	
<input type="checkbox"/> Zone 5			

The project is impacted by the following Chino ALUCP Safety Zones:

Zone 1
 Zone 2
 Zone 3
 Zone 4
 Zone 5
 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP
 Consistent
 Consistent with Conditions
 Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT provided the attached conditions are met.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2019-065 Rev 1
PALU No.: _____

PROJECT CONDITIONS

1. Project is located within Safety Zone 2 and 4, above ground storage of hazardous materials greater than 6,000 gallons is not allowed (ALUCP Policy S4b (Hazardous Material Storage)).
2. Attached are the land use intensity calculations for the proposed building. Future land uses that deviate from what is currently being approved must meet the policies and criteria of the Ontario ALUCP.
3. The maximum height limit for the project site is 65 feet and as such, any construction equipment such as cranes or any other equipment exceeding 65 feet in height will need a determination of "No Hazard" from the FAA. An FAA Form 7460-1 for any temporary objects will need be filed and approved by the FAA prior to operating such equipment on the project site during construction.
4. New development located within any of the Ontario International Airport Safety Zones are required to have a "Property Located within Ontario International Airport Safety Zone Notification appearing on the Property Deed and Title incorporating the following language:

NOTICE OF AIRPORT IN VICINITY: This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.) The property is presently located in a Safety Zone which limits land uses and the number of people on site. Land uses are required to meet the policies and criteria of the Ontario International Airport Land Use Compatibility Plan.

5. This project is located within Safety Zone 2 and 4 and is required to file and record an Avigation Easement with the OIAA prior to obtaining a Certificate of Occupancy.

Intensity Calculations for PDEV19-050 PMTT19-013

Intensity Calculations								
				Load Factors	Sitewide Average Calculations (Zone 2 = 60 P/AC max)	Sitewide Average Calculations (Zone 4 = 160 P/AC max)	Zone 2 Single Acre Land Use SF (Zone 2 = 120 P/AC max)	Single Acre Intensity Calculations (Zone 4 = 400 P/AC max)
Building No.	Proposed Land Use	Zone 2 Land Use SF	Zone 4 Land Use SF	ALUCP Load Factor	ALUCP Load Factor	ALUCP Load Factor		ALUCP Load Factor
617 East Sunkist Street	Warehouse	81,962	-	1,000	82	0	43.51	0
	Office	3,000	119	215	14	1	14	1
Totals					24	0.1	57	1
Site Information								
Safety Zone	Acreage	Square Footage						
Zone 2	3.96542	172,734						
Zone 4	0.23615	10,287						
Totals	4.20157	183,020						
Sitewide Average Calculation				Single Acre Intensity Calculation				
Safety Zone 2 = 24				Safety Zone 2 = 57				
Safety Zone 4 = 0.1				Safety Zone 4 = 1				

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

PRELIMINARY PLAN CORRECTIONS

Sign Off



01/08/20

Jamie Richardson, Sr. Landscape Planner

Date

Reviewer's Name:

Jamie Richardson, Sr. Landscape Planner

Phone:

(909) 395-2615

D.A.B. File No.:

PDEV19-050

Case Planner:

Alexis Vaughn

Project Name and Location:

Subdivide 4.91 acres
 617 E Sunkist Street

Applicant/Representative:

Herdman Architecture and Design
 16201 Scientific Way
 Irvine, CA 92618



A Preliminary Landscape Plan (dated 2/19/20) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.



A Preliminary Landscape Plan () has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

1. Provide an arborist report and tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans to protect trees to remain. Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020.
2. Show on demo plans and landscape construction plans trees to be preserved, removed or mitigation measures for trees removed, such as:
 - a. New 15 gallon trees min 1" diameter trunk, in addition to trees required.
 - b. New 24" box trees min 1.5" diameter trunk, in addition to trees required.
 - c. Upsizing trees on the plan one size larger such as 15 gallon to 24" box, or 24" to 36" box size.
 - d. Monetary value of the trees removed as identified in the "Guide for Plant Appraisal", approved certified arborist plant appraiser, or may be equal to the value of the installation cost of planting, fertilizing, staking and irrigating 15 gallon trees, (100\$ each) to the City of Ontario Historic Preservation Fund for city tree planting or city approved combination of the above items.
3. Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans. Identify if the Fiber Optic lines running through the parkways are existing or proposed; if proposed relocate out of parkways and away from the Protected Root Zone (PRZ) of existing trees. Relocate gas and electrical lines out of island planters (west side of the building).

4. Note for compaction to be no greater than 85% at landscape areas. All finished grades at 1 ½" below finished surfaces. Slopes to be maximum 3:1.
5. Locate employee break area to location of bike racks for south/west summer shade and locate bicycle racks to northern side of building entrance.
6. Add Note to Grading and Landscape Plans: Landscape areas where compaction has occurred due to grading activities and where trees or storm water infiltration areas are located shall be loosened by soil fracturing. For trees a 12'x12'x18" deep area; for storm water infiltration the entire area shall be loosened. Add the following information on the plans: The back hoe method of soil fracturing shall be used to break up compaction. A 4" layer of Compost is spread over the soil surface before fracturing is begun. The back hoe shall dig into the soil lifting and then drop the soil immediately back into the hole. The bucket then moves to the adjacent soil and repeats. The Compost falls into the spaces between the soil chunks created. Fracturing shall leave the soil surface quite rough with large soil clods. These must be broken by additional tilling. Tilling in more Compost to the surface after fracturing per the soil report will help create an A horizon soil. Imported or reused Topsoil can be added on top of the fractured soil as needed for grading. The Landscape Architect shall be present during this process and provide certification of the soil fracturing. For additional reference see Urban Tree Foundation – Planting Soil Specifications.

Landscape Plans

7. Provide a planter space at the westerly entry adjacent to low wall at steps.
8. Coordinate with utility consultant (civil plans show transformer off Sunkist St) to relocate the fire hydrant shown in front of the transformer; dimension 5' on all sides of transformer to avoid bollards.
9. Coordinate with civil on all utility locations (see #3 above).
10. Provide an arborist report and tree inventory as noted in #1.
11. Overhead spray systems shall be designed for plant material less than the height of the spray head.
12. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—less than 5 acres	\$1,301.00
Inspection—Construction (up to 3 inspections per phase)	<u>\$278.00</u>
Total.....	\$1,579.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to:

landscapeplancheck@ontarioca.gov



CITY OF ONTARIO

MEMORANDUM

TO: Scott Murphy, Development Director
Cathy Wahlstrom, Planning Director (Copy of memo only)
Diane Ayala, Advanced Planning Division (Copy of memo only)
Charity Hernandez, Economic Development
Kevin Shear, Building Official
Khoi Do, City Engineer
Jamie Richardson, Landscape Planning Division
Ahmed Aly, Municipal Utility Company
Emily Hernandez, Police Department
Paul Ehrman, Deputy Fire Chief/Fire Marshal
Jay Bautista, T. E., Traffic/Transportation Manager
Lorena Mejia, Airport Planning
Eric Woosley, Engineering/NPDES
Joe De Sousa, Code Enforcement (Copy of memo only)
Jimmy Chang, IT Department

FROM: Alexis Vaughn, Assistant Planner

DATE: November 25, 2019

SUBJECT: FILE #: PMTT19-013

Finance Acct#:


The following project has been resubmitted for review. Please send one (1) copy and email one (1) copy of your DAB report to the Planning Department by **Monday, December 9, 2019**.

PROJECT DESCRIPTION: A Parcel Map to subdivide 4.91 acres of land into a single parcel located at the northwest corner of Sunkist Street and Campus Avenue, at 617 East Sunkist Street, within the IL (Light Industrial) zoning district (APN: 1049-232-21). Related File: PDEV19-050.

- The plan does adequately address the departmental concerns at this time.
- No comments
 - See previous report for Conditions
 - Report attached (1 copy and email 1 copy)
 - Standard Conditions of Approval apply
- The plan does not adequately address the departmental concerns.
- The conditions contained in the attached report must be met prior to scheduling for Development Advisory Board.

Landscape Planning Division [Signature] Landscape Planner 11/15/19
Department Signature Title Date

**CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
303 East "B" Street, Ontario, CA 91764**

DAB CONDITIONS OF APPROVAL	
Sign Off	
	12/19/19
Jamie Richardson, Sr. Landscape Planner	Date

Reviewer's Name: Jamie Richardson, Sr. Landscape Planner	Phone: (909) 395-2615
--	---------------------------------

D.A.B. File No.: PMTT19-013	Related Files: PDEV19-050	Case Planner: Alexis Vaughn
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Project Name and Location:
Subdivide 4.91 acres
617 E Sunkist Street

Applicant/Representative:
Herdman Architecture and Design
16201 Scientific Way
Irvine, CA 92618

<input checked="" type="checkbox"/>	A Tentative Tract Map (dated 09/06/2019) has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.
<input type="checkbox"/>	A Tentative Tract Map (dated) has not been approved. Corrections noted below are required prior to DAB approval.

CORRECTIONS REQUIRED

- Relocate utilities to minimum clearances to allow parkway trees. Parkway trees are to be 30' apart and where residential driveways occur, a maximum 45' apart. Show and note a 10' parkway tree space, 5' clearance each side of tree from any utility or hardscape including water, sewer, drain lines and driveways; and min. 10' clear from street lights.
- On Grading or Utility Construction Plans:
- Note on grading plans for compaction to not be greater than 85% at landscape areas; all finished grades 1 1/2" below finished surfaces; landscaped slopes to be max 3:1.
- Wall footings shall not restrict landscape; max 12" in front of footing with of 12" of cover.
- Provide a tree inventory for existing trees include genus, species, trunk diameter, canopy width and condition. Show and note existing trees in good condition to remain and note trees proposed to be removed. Include existing trees within 15' of adjacent property that would be affected by new walls, footings or on-site tree planting. Add tree protection notes on construction and demo plans.
- After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—less than 5 acres.....	\$1,301.00
Inspection—Construction (up to 3 inspections per phase)	<u>\$278.00</u>
Total.....	\$1,579.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to: landscapeplancheck@ontarioca.gov



PLANNING COMMISSION STAFF REPORT

April 28, 2020

FILE NO.: PSPA19-009

SUBJECT: An Amendment to the Piemonte Overlay of The Ontario Center Specific Plan (File No. PSPA19-009), modifying the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement. The Piemonte Overlay area encompasses 84.43 acres of land generally located north of Concours Street, south of Fourth Street, west of Milliken Avenue, and east of Haven Avenue; (APNs: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16) **submitted by LCD Residential at Ontario, LLC. City Council action is required.**

PROPERTY OWNER: LCD Residential at Ontario, LLC.

RECOMMENDED ACTION: That the Planning Commission recommend that the City Council consider and approve File No. PSPA19-009, pursuant to the facts and reasons contained in the staff report and attached resolution, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The project site is comprised of 84.43 acres generally located north of Concours Street, east of Haven Avenue, South of Fourth Street and west of Milliken Avenue, and is depicted in Figure 1: Project Location, below. The north and west sides of the project site are generally developed with multiple-family residential and commercial developments. To the south of the project site, the area is developed with the Toyota Arena, several Arena parking lots, and commercial office buildings. To the east of the project site, the area is developed with commercial land uses. To the west of the project site, the area is developed with retail and multiple-family residential developments.

PROJECT ANALYSIS:

[1] Background — The Piemonte Overlay of The Ontario Center Specific Plan (“TOCSP”) approved in 2006 and

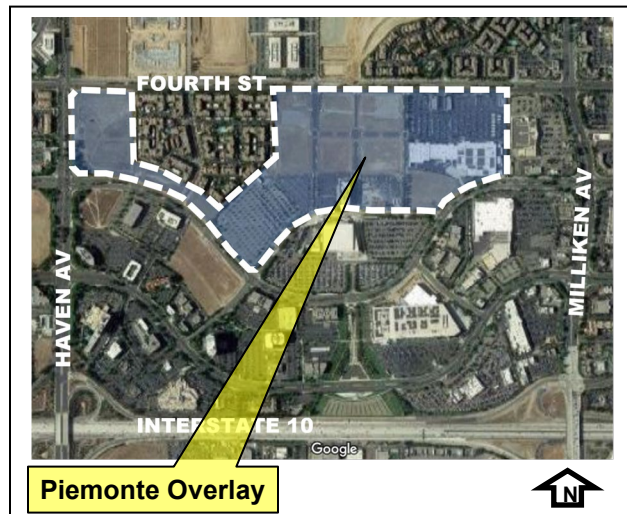


Figure 1: Project Location

Case Planner:	Luis E. Batres
Planning Director Approval:	
Submittal Date:	10-16-19

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	
PC	4-28-20		Recommend
CC	6-2-20		Final

later substantially amended in 2017, established the standards, regulations and design guidelines for the development of the project area. The objectives of the Specific Plan are to:

- Provide special design and development standards for the subject area;
- Implement a mixture of related and supporting land uses;
- Provide for the orderly and master planned development of land uses within the Specific Plan, ensuring the development of economically viable developments;
- Ensure that development of the area is consistent with The Ontario Plan (“TOP”) policies, objectives, and implementation programs; and
- Provide special standards for the evaluation of individual developments within the Piemonte Overlay.

The land use and site development concept of the Piemonte Overlay is to recognize the TOCSP’s potential for special uses, entertainment land uses, office, hotels, commercial, and high-density residential land uses; and to take advantage of the excellent freeway access and proximity to the Toyota Arena and the Ontario International Airport. The Interstate 10 Freeway access at Haven and Milliken Avenue provides convenient access for residents, employees and customers. In order to allow for development flexibility, the Piemonte Overlay was divided into five different planning areas, each having a specific listing of allowed uses and development guidelines. The five land use areas are (see Figure 2: Piemonte Overlay Land Use Plan, right):

- Commercial
- Entertainment/Retail
- Office
- Special Use
- Residential

[2] Specific Plan Amendment — The Applicant is requesting approval of an amendment to the Piemonte Overlay of TOCSP to modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking (one parking space located in front of another) to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement, excluding guest/visitor parking. The Specific Plan currently defaults to the

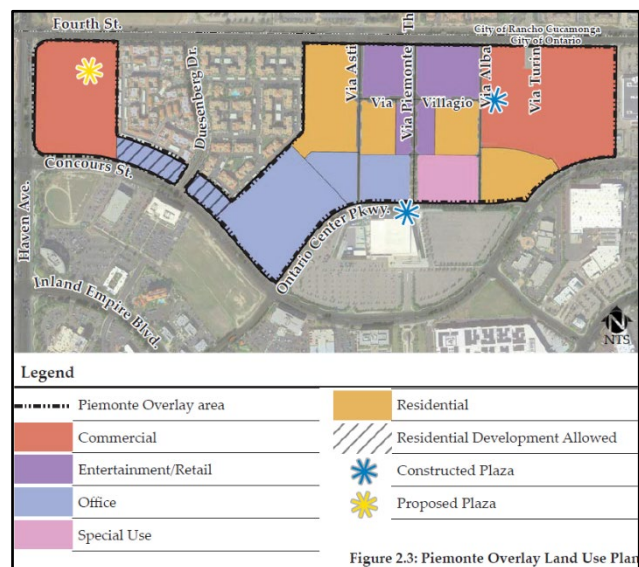


Figure 2: Piemonte Overlay Land Use Plan

Development Code (Section 6.03.025) for the number of tandem parking spaces allowed to be counted towards required parking, which is a maximum of 12 percent. The Development Code provides standards and requirements for orderly development in areas throughout the City. The Piemonte Overlay of TOCSP area is distinct from other areas of the City in that it is envisioned as more intense and more urban. The proposed increase in the number of allowed tandem parking spaces will allow for a more urbanized pattern of residential development as envisioned by TOCSP, the Piemonte Overlay and The Ontario Plan (TOP).

This amendment has been proposed to facilitate the future development of two multiple-family residential development projects totaling 182 townhouse units, which are currently undergoing City review. The Planning Commission will have the opportunity to review and act on these projects, as well as all other future residential development projects affected by this proposed amendment.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner
- Focus Resources in Ontario's Commercial and Residential Neighborhoods

[2] Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.

- G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.

- LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario.

- Goal LU2: Compatibility between a wide range of uses.

- LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.

- H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

- Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

- H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Community Economics Element:

- Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be

consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Specific Plan Amendment is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with future development projects and/or land uses in close proximity to the airport will be included in conditions of approval specifically imposed on those future development projects and/or land uses at the time of their approval.

ENVIRONMENTAL REVIEW: The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Vacant	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay	Residential
<i>North</i>	Multi-Family Residential and Commercial	City of Rancho	City of Rancho	n/a
<i>South</i>	Toyota Arena, Arena Parking & Office	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay	Urban Commercial & Garden Commercial
<i>East</i>	Commercial	MU (Mixed Use)	Ontario Center Specific Plan	Urban Commercial
<i>West</i>	Commercial & Multi-Family Residential	MU (Mixed Use)	Wagner Specific Plan & Ontario Center Specific Plan	Multi-Family Residential & Garden Commercial

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING CITY COUNCIL APPROVAL OF FILE NO. PSPA19-009, AN AMENDMENT TO THE PIEMONTE OVERLAY OF THE ONTARIO CENTER SPECIFIC PLAN TO MODIFY THE MINIMUM PARKING REQUIREMENTS (SECTION 3.3.5.1) TO ALLOW TANDEM PARKING TO BE COUNTED TOWARD A MAXIMUM OF 50 PERCENT OF THE DWELLING UNIT PARKING SPACE REQUIREMENT. THE PIEMONTE OVERLAY ENCOMPASSES 84.43 ACRES OF LAND GENERALLY LOCATED NORTH OF CONCOURS STREET, SOUTH OF FOURTH STREET, WEST OF MILLIKEN AVENUE, AND EAST OF HAVEN AVENUE, AND MAKING FINDINGS IN SUPPORT THEREOF— APN: APNS: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16.

WHEREAS, LCD Residential at Ontario, LLC. ("Applicant") has filed an Application for the approval of a Specific Plan Amendment, File No. PSPA19-009, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to the Piemonte Overlay of The Ontario Center Specific Plan, consisting of 84.43 acres of land generally located north of Concours Street, east of Haven Avenue, South of Fourth Street and west of Milliken Avenue within the Residential zoning designation, and is presently vacant; and

WHEREAS, the north and west sides of the project site are generally developed with multiple-family residential and commercial developments. To the south of the project site, the area is developed with the Toyota Arena, several Arena parking lots and office buildings. To the west of the project site, the area is developed with retail and multiple-family residential developments. To the east of the project site, the area is developed with commercial land uses; and

WHEREAS, the proposed Specific Plan Amendment is related to two Development Plan applications: (1) File No. PDEV19-054 proposing the development of 72 townhouse units on 3.02 acres of land located at the southwest corner of Via Alba and Via Villagio, and (2) File No. PDEV19-061 proposing the development of 110 townhouse units on 4.63 acres of land located at the northeast corner of Ontario Center Parkway and Via Alba, Both projects are within the Residential land use district of the Piemonte Overlay; and

WHEREAS, the proposed Specific Plan Amendment proposes to modify the Minimum Parking Requirements (Section 3.3.5.1) of the Piemonte Overlay of The Ontario Center Specific Plan to allow tandem parking to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement as shown on Attachment "A"; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, a Specific Plan Amendment, for which a Mitigated Negative Declaration ("MND") was adopted by the City Council on May 16, 2017, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan ("ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, the Planning Commission has reviewed and

considered the information contained in the previous MND and supporting documentation. Based upon the facts and information contained in the previous MND and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously analyzed in conjunction with File No. PSPA16-003, a Specific Plan Amendment for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017.

(2) The previous MND contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous MND was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous MND reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous MND, and all mitigation measures previously adopted with the MND are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental MND is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the MND that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the MND was prepared, that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the MND was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the MND; or

(b) Significant effects previously examined will be substantially more severe than shown in the MND; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the MND would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project is consistent with the policies and criteria set forth within the ALUCP.

SECTION 4: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan, or amendment thereto, is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed amendment to the Piemonte Overlay of The Ontario Center Specific Plan will modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement. The proposed amendment is consistent with TOP’s Vision, which states “[i]n order to take advantage

opportunities or remove impediments to achieving our Vision, we need the ability to quickly respond to changing market needs,” and TOP’s Policy Plan (General Plan) goals and policies, which states “*LU3-1 Development Standards*. We maintain clear development standards which allow flexibility to achieve our Vision;” and

(2) ***The proposed Specific Plan, or amendment thereto, would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** With the proposed amendment to the Piemonte Overlay of The Ontario Center Specific Plan, the proposed revision to the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement will be in conformance with The Ontario Plan (TOP) Policy Plan Land Use Plan and will comply with the Policy Plan goals and policies applicable to the Specific Plan. The proposed amendment will not be detrimental to the public interest, health, safety, convenience, or general welfare of the City because it will allow for more urbanized residential development consistent with the vision for residential development within the Piemonte Overlay; and

(3) ***In the case of an application affecting specific property(ies), the proposed Specific Plan, or amendment thereto, will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The project site is located in an area that will be developed with entertainment, hotels, commercial, and multiple-family residential land uses. The proposed Specific Plan Amendment will facilitate a more urbanized pattern of residential development that is consistent with the overall vision of The Ontario Center Specific Plan and the Piemonte Overlay, and will not, therefore, adversely affect the harmonious relationship with adjacent properties and land uses; and

(4) ***In the case of an application affecting specific property(ies), the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The proposed amendment to the Piemonte Overlay of The Ontario Center Specific Plan will amend the minimum parking requirements for residential land uses, allowing more flexibility in the allocation and use of tandem parking. The physical suitability of increased tandem parking, up to a maximum of 50 percent of the dwelling unit parking space requirement, will be analyzed by the Planning Commission on a case-by-case basis as residential development projects are submitted.

SECTION 5: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 4, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment B,” and incorporated herein by this reference.

SECTION 6: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 7: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 8: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHEMENT A:

**File No. PSPA19-009
Piemonte Overlay Amended
Section 3.3.5.1**

(Departmental conditions of approval to follow this page)

All development is highly encouraged to leverage transit, multi-modal, and shared parking opportunities to reduce required parking demand. Parking reductions may be achieved through shared parking, or other strategies that reduce the amount of area devoted to parking and to increase the use of alternative forms of mobility, as validated by a Parking Analysis Study as provided in Section 6.03.020 Reduction in the Required Number of Parking Spaces of the Ontario Development Code.

3.3.5.1 Minimum Parking Requirements

Off-site parking and loading facilities for each of the Piemonte Overlay Land Use Subareas shall be provided pursuant to the requirements of Ontario Development Code Division 6.03 (Off-Street Parking and Loading), except that parking facilities for residential uses shall be provided as follows:

- Studio Unit – 1.0 space per unit.
- One bedroom unit – 1.0 space per unit.
- Two bedroom unit – 1.75 spaces per unit.
- Three or more bedroom unit – 2.0 spaces per unit.
- Residential guest parking – 0.2 space per unit. Guest parking shall be accommodated within the host lot/building, in common parking areas along abutting private drives, or on adjacent private drives.

Tandem parking spaces may be counted toward a maximum of 50 percent of resident parking space requirements as established in this section. The Tandem parking regulations and design shall be subject to the provisions of the Ontario Development Code, Division 6.03 – Off Street Parking and Loading, Section 6.03.025 C: Tandem Parking for Multi-Family projects.

3.3.6 Structured Wiring

Residential (single-family and multi-family), commercial and industrial developments shall adhere to the City's Structured Wiring ordinance. An integrated structured wiring system (low-voltage wiring) provides infrastructure for today's technology applications and the framework for the future technology advances. Requirements and benefits of a structured wiring system include:

ATTACHEMENT B:

**File No. PSPA19-009
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: April 28, 2020

File No: PSPA19-009

Related Files: n/a

Project Description: An Amendment to the Piemonte Overlay of the Ontario Center Specific Plan (File No. PSPA19-009), to modify the Minimum Parking Requirements (Section 3.3.5.1) to allow tandem parking up to a maximum of 50% of the required parking. The Ontario Center Specific Plan-Piemonte Overlay encompasses 84.43 acres of land, and is generally located north of Concours Street, south of Fourth Street, west of Milliken Avenue, and east of Haven Avenue; (APNs: 0210-204-26, 0210-204-37, 0210-204-40, 0210-531-15, 0210-531-16) **submitted by LCD Residential at Ontario, LLC.**

Prepared By: Luis E. Batres, Senior Planner
Phone: 909.395.2431 (direct)
Email: Lbatres@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Specific Plan/Specific Plan Amendment. The following shall be submitted to the Planning Department within 30 days following City Council approval of the Specific Plan Amendment:

- (a) Fifteen copies of the final revised Specific Plan document;
- (b) One complete, unbound copy of the final Specific Plan document;
- (c) One CD containing a complete Microsoft Word copy of the final Specific Plan document, including all required revisions;
- (d) Five CDs, each containing a complete PDF copy of the final Specific Plan document, including all required revisions; and
- (e) One CD containing a complete electronic website version of the final Specific Plan document, including all required revisions.

2.2 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval.

(b) Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.



PLANNING COMMISSION STAFF REPORT

April 28, 2020

FILE NOS.: PMTT19-016 & PDEV19-054

SUBJECT: A Tentative Tract Map (File No. PMTT19-016) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes in conjunction with a Development Plan (File No. PDEV19-054) to construct 72 multiple-family residential units (townhomes), located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay of The Ontario Center Specific Plan; (APN: 0210-204-40) **submitted by The New Home Company Southern California LLC.**

PROPERTY OWNER: LCD Residential at Ontario, LLC

RECOMMENDED ACTION: That the Planning Commission consider and approve File Nos. PMTT19-016 (TT 20308) and PDEV19-054, pursuant to the facts and reasons contained in the staff report and attached resolutions, and subject to the conditions of approval contained in the attached departmental reports.

PROJECT SETTING: The Project site is comprised of 3.02 acres of vacant land located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay district of the Ontario Center Specific Plan, and is depicted in Figure 1: Project Location, right. The Project site is irregular in shape, with a lot depth of approximately 399-feet and a lot width of 352-feet. The site is relatively flat, with a gentle north to south slope of just over one percent. The property to the north of the Project site is within the Entertainment/Retail land use district and is currently vacant. The property to the east is within the Commercial land use district and is developed with a Commercial Shopping Center. The property to the south is within the Special Use land use district and a new

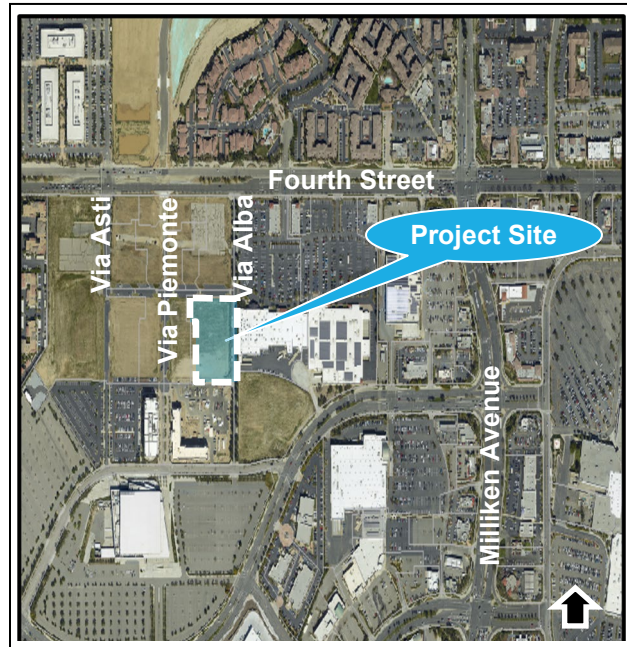


Figure 1: Project Location

Case Planner:	Luis E. Batres
Planning Director Approval:	
Submittal Date:	10/1/19

Hearing Body	Date	Decision	Action
DAB	4/20/20	Approval	Recommend
PC	4/28/20		Final
CC			

hotel (Element) is currently under construction. The property to the west is within the Entertainment/Retail land use district and is currently vacant.

PROJECT ANALYSIS:

[1] Background — The Piemonte Overlay of The Ontario Center Specific Plan (“TOCSP”) was established in 2006, and later substantially amended in 2017, to allow for the development of a mix of urban commercial, retail, residential, and entertainment land uses within a portion of the Specific Plan area. Additionally, special land use and development standards and guidelines were established that are unique to the Overlay area.

On October 1, 2019, the Applicant submitted a Development Plan (File No. PDEV19-054) application to construct 72 multiple-family residential units (townhomes) on the above-described Project site. In conjunction with the proposed Development Plan, the Applicant has filed a Tentative Tract Map for condominium purposes (File No. PMTT19-016/TT 20308), which will subdivide the Project site into one numbered lot and two lettered lots to facilitate the individual sale of the proposed townhomes.

On April 20, 2020, the Development Advisory Board (DAB) of the City of Ontario conducted a hearing to consider the Development Plan and Tentative Tract Map, and concluded the hearing voting to recommend that the Planning Commission approve the Applications subject to conditions of approval, which have been included with the Planning Commission resolution for each application.

[2] Tentative Tract Map (File No. PMTT19-016/TT 20308) — The proposed subdivision complies with the development standards and guidelines of the Piemonte Overlay district of TOCSP, as the Residential land use district does not have a minimum lot size requirement. Lots A and B will provide for street dedications along Via Alba and a private drive along the southern Project boundary (Drive “C”). The table below lists the proposed parcels and their sizes:

Parcel No.	Acres
Lot 1	2.81
Lot A	0.12
Lot B	0.09
Total	3.02

Covenants, Conditions and Restrictions (CC&Rs) are required for the proposed subdivision as a condition of Project approval. The CC&Rs must be submitted, reviewed, and approved by the City, and will be recorded with the final map to ensure ongoing maintenance of private roads, common landscape areas, amenities, and common drainage/easement areas.

[3] Development Plan (File No. PDEV19-054) —

[a] Site Design/Building Layout. The Development Plan is composed of ten buildings comprised of a total of 72 townhomes. Four buildings (Buildings A through D), located along the west portion of the site will have six units per building (6-Plex). The remaining six buildings (Buildings E through J), located along the north and east portions of the site, are comprised of eight units per building (8-Plex). All buildings are proposed at three stories, with an overall height of 38 feet. The Project is proposed at a density of 24.1 dwellings per acre, consistent with the Piemonte Overlay district.

The Project has been designed as a gated community, with one point of vehicular access from a private drive (Drive “C”) that is common with the Element Hotel property to the south. The ten row town buildings are situated along a 24-foot-wide, T-shaped centralized drive aisle (Drive “A” and Drive “B”). Each building has been designed to front onto Via Villagio street (Buildings E and F), main drive aisle (Buildings A, D, G, and J), or pedestrian paseo (Buildings B, C, H, and I). Garage access is taken from the rear of each unit, from a 24-foot wide alley.

The Project provides four different floor plans for both the 6-Plex and the 8-Plex row town products, which range in size from 1,360 to 2,205 square feet, as demonstrated within the tables below:

6-Plex Floor Plan (Buildings A, B, C & D)

Plan No.	Area (in SF)	No. Bedrooms	No. Baths
Plan 1	1,360	2 bedrooms	2.5 bath
Plan 2	1,494	3 bedrooms	2.5 bath
Plan 3	1,655	3 bedrooms	3.5 bath
Plan 4	2,203	4 bedrooms	3 bath-Option of Loft or ADU

8-Plex Floor Plan (Buildings E, F, G, H, I & J)

Plan No.	Area (In SF)	No. Bedrooms	No. Baths
Plan 1	1,375	2 bedrooms	2.5 bath
Plan 2	1,494	3 bedrooms	2.5 bath
Plan 3	1,655	3 bedrooms	3.5 bath
Plan 4	2,205	4 bedrooms	3 bath-Option of 5 bedrooms/4 bath, Loft or ADU

[b] Site Access/Circulation — As previously discussed, the Project has been designed as a gated community, with one point of vehicular ingress and egress from a private drive aisle (Drive “C”) located along the south side of the site. The 26-foot wide Drive “C”, which runs east to west, is common with the Element Hotel parking lot to the

south and is accessed from Via Alba to the east. Vehicular access through the Project site is by way of a T-shaped private drive (Drive “A” and Drive “B”) that is designed with 5-foot wide sidewalks along each side. In addition, emergency vehicle access is provided at the east leg of the T (northeast corner of the site), with access from Via Alba. Both the gated entry and emergency access point have been designed with a Knox-box, for emergency vehicle access.

Project pedestrian ingress and egress is gated (modern tubular steel design). Accesses are provided at the northwest corner of the Community Park, at the east and west entries of the pedestrian paseo, and at the east and west sides of Drive “B”.

[c] Parking —Off-street parking in the form of enclosed garage spaces and uncovered surface parking is distributed throughout the Project site. A total of 156 off-street parking spaces are required for the Project, which includes 14 guest parking spaces. The Project provides 158 parking spaces, including the 14 guest parking spaces, and therefore, exceeds the minimum number of parking spaces required. Of the uncovered parking spaces, 9 guest spaces will be located along the west side of Via Alba.

In conjunction with the proposed Development Plan, the applicant has submitted an Amendment to TOCSP (File No. PSPA19-009), modifying the Minimum Parking Requirements of the Piemonte Overlay district (Section 3.3.5.1) to specify that tandem parking may be provided for up to 50 percent of the dwellings within a proposed residential or mixed use development project. Consequently, 36 of the proposed 72 residential units have been designed with tandem enclosed garage parking spaces. The tandem garage units will measure approximately 11 feet wide by 40 feet deep and will be able to accommodate two vehicles (see Exhibit L: First Floor Plan, attached). Project approval is contingent on City Council approval of the Specific Plan Amendment.

[d] Architecture —The Project proposes a contemporary modern architectural design, exemplifying the type of high-quality architecture promoted by the Piemonte Overlay of TOCSP and The Ontario Plan vision (see Figure 2: Front Main Entry Perspective, below, and the exterior elevations contained in Exhibits E through K, attached). The mass and scale of the buildings are designed to be proportionate to the site, open space, and scale of the neighborhood. Special attention was given to colors, materials, massing, building form, and architectural details. This is exemplified through the use of:

- Articulation in the building’s roof lines;
- Incorporation of flat and gable roof lines;
- Cantilevered architectural elements along the second and third floors;
- Composite roof shingles;
- An exterior sand stucco finish;
- Horizontal and vertical cement lap siding;
- Decorative sconce lighting fixtures;
- Incorporation of several exterior building colors;

- Decorative metal railings along balconies; and
- Metal window awnings along key locations of buildings.



Figure 2: Front Main Entry Perspective

[e] Landscaping —The proposed Project exceeds the minimum landscape requirements established by the Piemonte Overlay of TOCSP. The Project will provide a 20-foot wide landscape setback along the north property line (Via Villagio), a 15-foot average landscape setback along the east property line (Via Alba), a 6-foot wide landscape setback along the south property line, and a 9-foot average landscape setback along the west property line.

The Project will also provide an average of 104 square feet of private open space per unit (50 sq. ft. minimum required), in the form of a front entry porch and a second story balcony. In addition, the Project will provide approximately 294 square feet of common open space for each unit, exceeding the 215 per unit square feet minimum requirement. Common open space (totaling 21,218 square feet) is provided by a Community Park that is 40 feet wide by 45 feet in depth. The Community Park is located at the north end of the Project, at the terminus of the Projects central drive aisle. In addition to the park, the Project provides a pedestrian paseo (23 feet wide by 300 feet in length), that runs east-west through the site, between buildings B and C, and buildings H and I (see Exhibit R—Landscape Plan and Exhibit V—Projects Total Landscaped Area, attached). The peso will provide access to the Toyota Arena area, shopping/entertainment areas, and to the Ontario Center paseo system, that runs east-west from Milliken to Haven Avenue. The park and Project amenities proposed include:

- A decorative metal shade structure;
- An outdoor dining area;

- A fire table seating area;
- Decorative paving areas;
- Built-in BBQ and counter areas;
- Accent lounge chairs;
- Walking green belt within the pedestrian Paseo;
- Dog stations;
- Outdoor benches within the pedestrian Paseo area; and
- A decorative 30-foot long by 11-foot wide metal trellis over the BBQ area.

The plant pallet will consist of a mixture of shade trees, ground cover, and shrubs. At key areas of the Project, accent planting is featured, including Australian Willow, London Plane, Chinese Pistache, Natchez Crape Myrtle, and Laurel Cherry.

[f] Utilities (drainage, sewer) —Public utilities (water and sewer) are available to serve the Project. Additionally, the applicant has submitted a Preliminary Water Quality Management Plan (PWQMP), which establishes the Project’s compliance with storm water discharge/water quality requirements. The PWQMP includes site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development (LID) best management practices (BMPs), such as retention and infiltration, bio treatment, and evapotranspiration. The subject property was part of a larger subdivision map (Parcel Map 17550) for the area, and at the time the larger map was subdivided, the Project site was approved to incorporate a hydro-dynamic separator to service the Best Management Practices (BMPs) for the site.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City’s Economy
- Maintain the Current High Level of Public Safety
- Operate in a Businesslike Manner
- Focus Resources in Ontario’s Commercial and Residential Neighborhoods

[2] Vision.

Distinctive Development:

- Commercial and Residential Development
 - Development quality that is broadly recognized as distinctive and not exclusively tied to the general suburban character typical of much of Southern California.

[3] Governance.

Decision Making:

- Goal G1: Sustained decision-making that consistently moves Ontario towards its Vision by using The Ontario Plan as a framework for assessing choices.
 - G1-2 Long-term Benefit. We require decisions to demonstrate and document how they add value to the community and support the Ontario Vision

[4] Policy Plan (General Plan)

Land Use Element:

- Goal LU1: A community that has a spectrum of housing types and price ranges that match the jobs in the City and that make it possible for people to live and work in Ontario and maintain a quality of life.
 - LU1-1 Strategic Growth. We concentrate growth in strategic locations that help create place and identity, maximize available and planned infrastructure, and foster the development of transit.
 - LU1-6 Complete Community: We incorporate a variety of land uses and building types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario. (Refer to Complete Community Section of Community Economics Element).
- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Housing Element:

- Goal H2: Diversity of types of quality housing that are affordable to a range of household income levels, accommodate changing demographics, and support and reinforce the economic sustainability of Ontario.
 - H2-5 Housing Design. We require architectural excellence through adherence to City design guidelines, thoughtful site planning, environmentally sustainable practices and other best practices.

Goal H5: A full range of housing types and community services that meet the special housing needs for all individuals and families in Ontario, regardless of income level, age or other status.

➤ H5-2 Family Housing. We support the development of larger rental apartments that are appropriate for families with children, including, as feasible, the provision of services, recreation and other amenities.

Community Economics Element:

▪ Goal CE1: A complete community that provides for all incomes and stages of life.

➤ CE1-6 Diversity of Housing. We collaborate with residents, housing providers and the development community to provide housing opportunities for every stage of life; we plan for a variety of housing types and price points to support our workforce, attract business and foster a balanced community.

▪ Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.

➤ CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.

➤ CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.

➤ CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.

➤ CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Safety Element:

▪ Goal S1: Minimized risk of injury, loss of life, property damage and economic and social disruption caused by earthquake-induced and other geologic hazards.

➤ S1-1 Implementation of Regulations and Standards. We require that all new habitable structures be designed in accordance with the most recent California Building Code adopted by the City, including provisions regarding lateral forces and grading.

Community Design Element:

▪ Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-2 Growth Areas. We require development in growth areas to be distinctive and unique places within which there are cohesive design themes.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-1 Quality Architecture. We encourage all development projects to convey visual interest and character through:

- Building volume, massing, and height to provide appropriate scale and proportion;
- A true architectural style which is carried out in plan, section and elevation through all aspects of the building and site design and appropriate for its setting; and
- Exterior building materials that are visually interesting, high quality, durable, and appropriate for the architectural style.

➤ CD2-2 Neighborhood Design. We create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety;
- Variable setbacks and parcel sizes to accommodate a diversity of housing types;
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows;
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and
- Landscaped parkways, with sidewalks separated from the curb.

➤ CD2-7 Sustainability. We collaborate with the development community to design and build neighborhoods, streetscapes, sites, outdoor spaces, landscaping and

buildings to reduce energy demand through solar orientation, maximum use of natural daylight, passive solar and natural ventilation, building form, mechanical and structural systems, building materials and construction techniques.

➤ CD2-8 Safe Design. We incorporate defensible space design into new and existing developments to ensure the maximum safe travel and visibility on pathways, corridors, and open space and at building entrances and parking areas by avoiding physically and visually isolated spaces, maintenance of visibility and accessibility, and use of lighting.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-1 Design. We require that pedestrian, vehicular, bicycle and equestrian circulation on both public and private property be coordinated and designed to maximize safety, comfort and aesthetics.

➤ CD3-2 Connectivity Between Streets, Sidewalks, Walkways and Plazas. We require landscaping and paving be used to optimize visual connectivity between streets, sidewalks, walkways and plazas for pedestrians.

➤ CD3-3 Building Entrances. We require all building entrances to be accessible and visible from adjacent streets, sidewalks or public open spaces.

➤ CD3-5 Paving. We require sidewalks and road surfaces to be of a type and quality that contributes to the appearance and utility of streets and public spaces.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

- Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

- CD5-1 Maintenance of Buildings and Property. We require all public and privately-owned buildings and property (including trails and easements) to be properly and consistently maintained.

- CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making body for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the proposed Project will not be consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The Project site is listed in the Available Land Inventory of the Housing Element Technical Report (Parcel ID No. 125) and the proposed Project is not consistent with the number of dwelling units (minimum 233 units required) and/or density specified (minimum 46 DUs/acre) in the Available Land Inventory; however, the removal will not impact the City's Regional Housing Needs Allocation obligations, as an amendment to the Available Land Inventory was previously made, which will offset the loss in units (161 unit difference) required on the Project site. Recently added to the inventory is 22.39 acres of land located at the southeast corner of Vineyard Avenue and Inland Empire Boulevard (APNs: 0110-311- 52, 0110-311-53, 0110-311-54, and 0110-311-55), which was approved by the Planning Commission on November 26, 2019 (File No. PDEV19-025/Resolution PC19-095) for the construction of 925 multiple-family dwellings, at a density of 47 DUs/acre. In addition, on March 3, 2020, The City Council Approved a General Plan Amendment (File No. PGPA20-001) to update the Future Buildout Table (Exhibit LU-03) to include the 925 multi-family units (Resolution No. 2020-016). Therefore, the subject property deficit of 161 residential units from the inventory will not impact the City's Regional Housing Needs Allocation obligations, as there are an adequate number of sites identified in the Available Land Inventory to meet the RHNA obligation.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan ("ALUCP"), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety,

airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

ENVIRONMENTAL REVIEW: The environmental impacts of this Project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference.

CONDITIONS OF APPROVAL: See attached department reports.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Vacant	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay	Residential
<i>North</i>	Vacant	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay Area	Entertainment/Retail
<i>South</i>	Element Hotel	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay Area	Special Use
<i>East</i>	Commercial Shopping Center	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay Area	Commercial
<i>West</i>	Vacant	MU (Mixed Use)	Ontario Center Specific Plan-Piemonte Overlay Area	Entertainment/Retail

General Site & Building Statistics

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Project area (in acres):</i>	None	3.02 Acres	Y
<i>Maximum project density (dwelling units/ac):</i>	24.1	24.1	Y
<i>Maximum coverage (in %):</i>	75%	41.9%	Y
<i>Via Alba setback (in FT):</i>	14-feet	15-feet	Y
<i>Via Villagio setback (in FT):</i>	20-feet	20-feet	Y
<i>Interior setback (in FT):</i>	5-feet	6-feet avg.	Y
<i>Drive aisle setback (in FT):</i>	5-feet	5-feet	Y
<i>Maximum height (in FT):</i>	Comply with ALUCP	38-feet	Y
<i>Parking – resident: Studio – 1 space. One bedroom– 1 space. Two bedroom– 1.75 spaces. Three bedrooms – 2.0 spaces. Guest parking – 0.2 space per unit.</i>	156	158	Y
<i>Parking – guest:</i>	14	14	Y
<i>Open space – private:</i>	50 sq. ft./Unit	69 to 107 sq. ft./Unit	Y

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Open space – common:</i>	215 sq. ft./Unit	294 sq. ft./Unit	Y

Dwelling Unit Count:

<i>Item</i>	<i>Required Min./Max.</i>	<i>Provided (Ranges)</i>	<i>Meets Y/N</i>
<i>Total no. of units</i>	72	72	Y
<i>Total no. of buildings</i>	N/A	10	Y
<i>No. units per building</i>	N/A	4 Buildings-6 Units each 6 Buildings-8 Units each	Y

Dwelling Unit Statistics:

<i>Unit Type</i>	<i>Size (in SF)</i>	<i>No. Bedrooms</i>	<i>No. Bathrooms</i>	<i>No. Stories</i>	<i>Private Open Space (in FT)</i>
Plan 1	1,360	2	2.5	3	107
Plan 2	1,494	3	2.5	3	69
Plan 3	1,655	3	3.5	3	151
Plan 4	2,205	4	4	3	90

Exhibit A—AERIAL PHOTOGRAPH

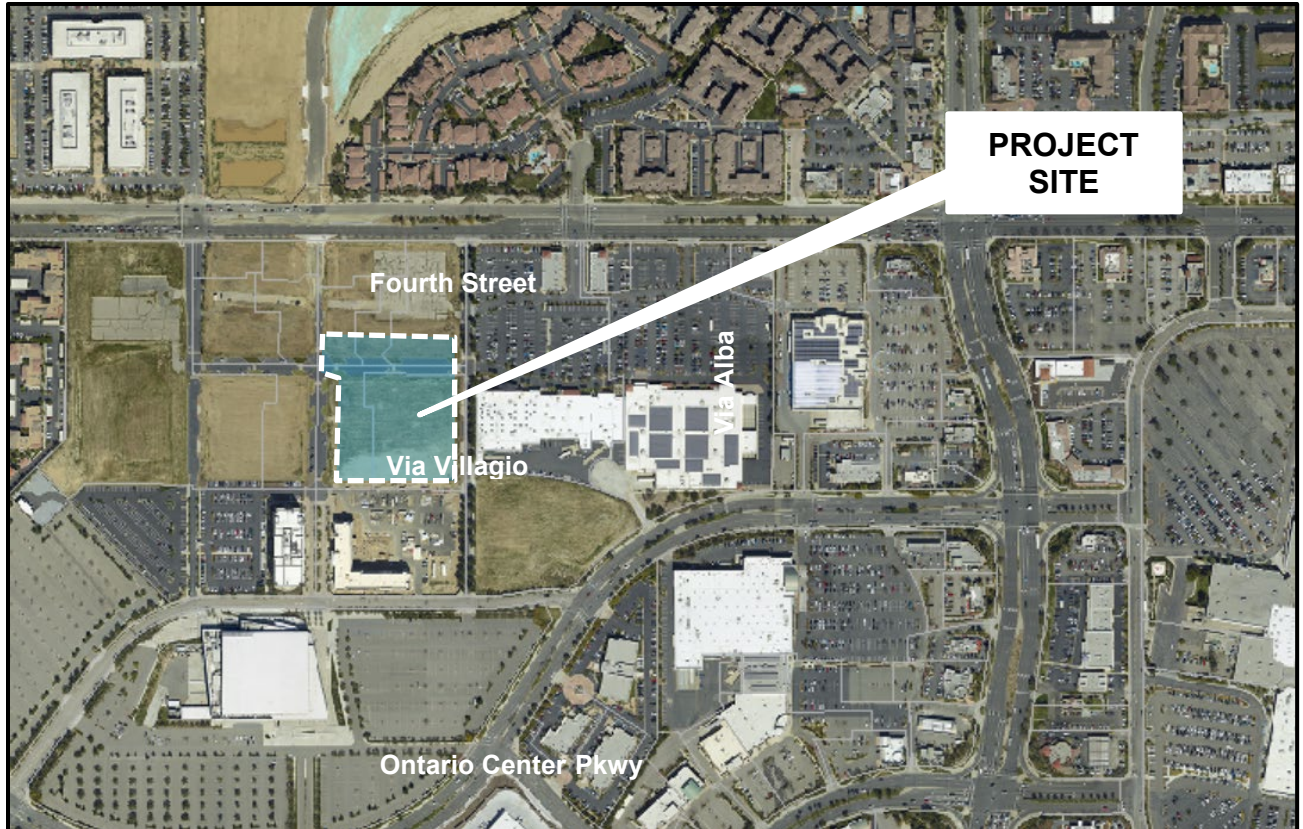


Exhibit B—PIEMONTE OVERLAY-ONTARIO CENTER SPECIFIC PLAN

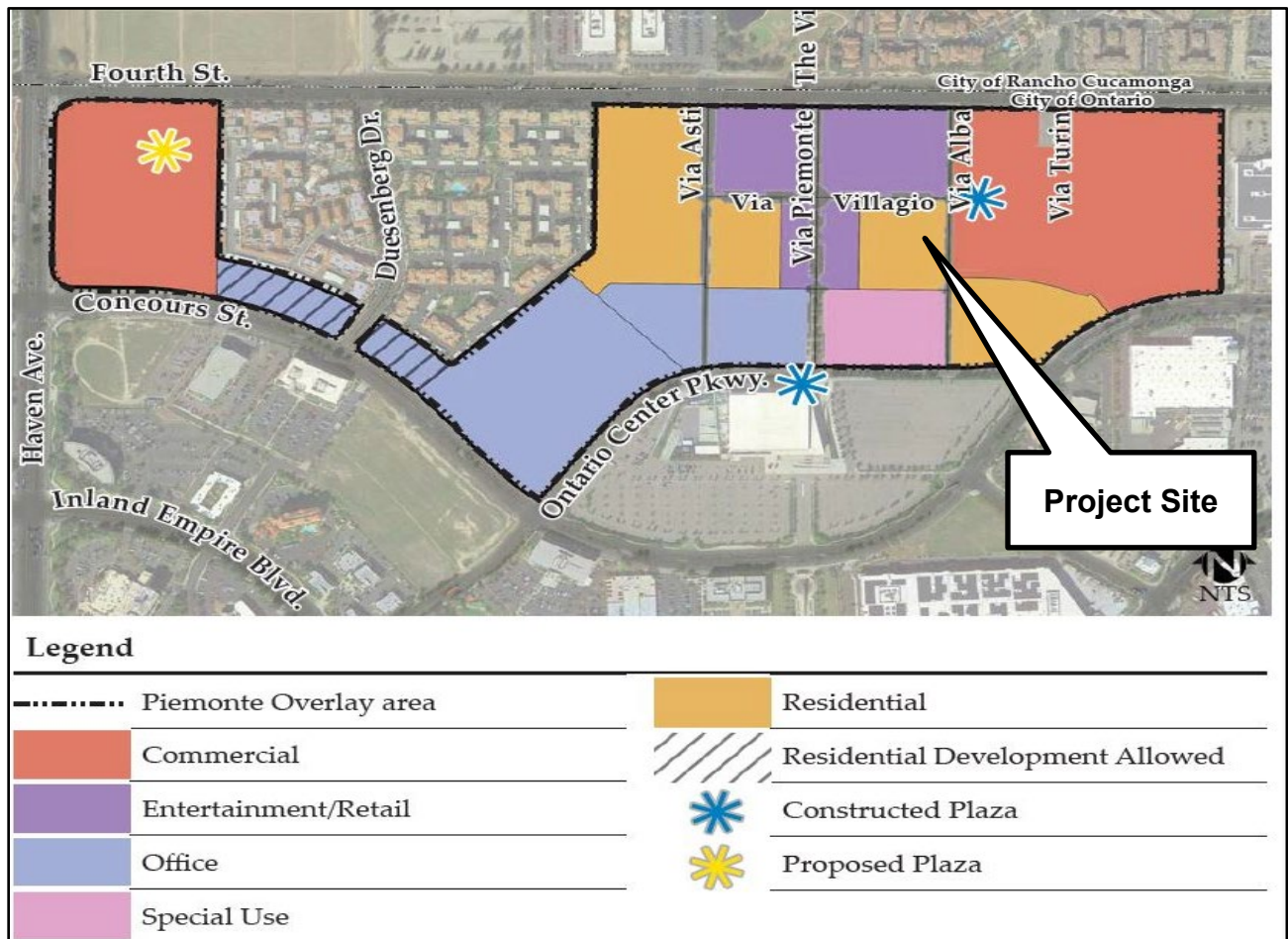


Exhibit C—SITE PLAN

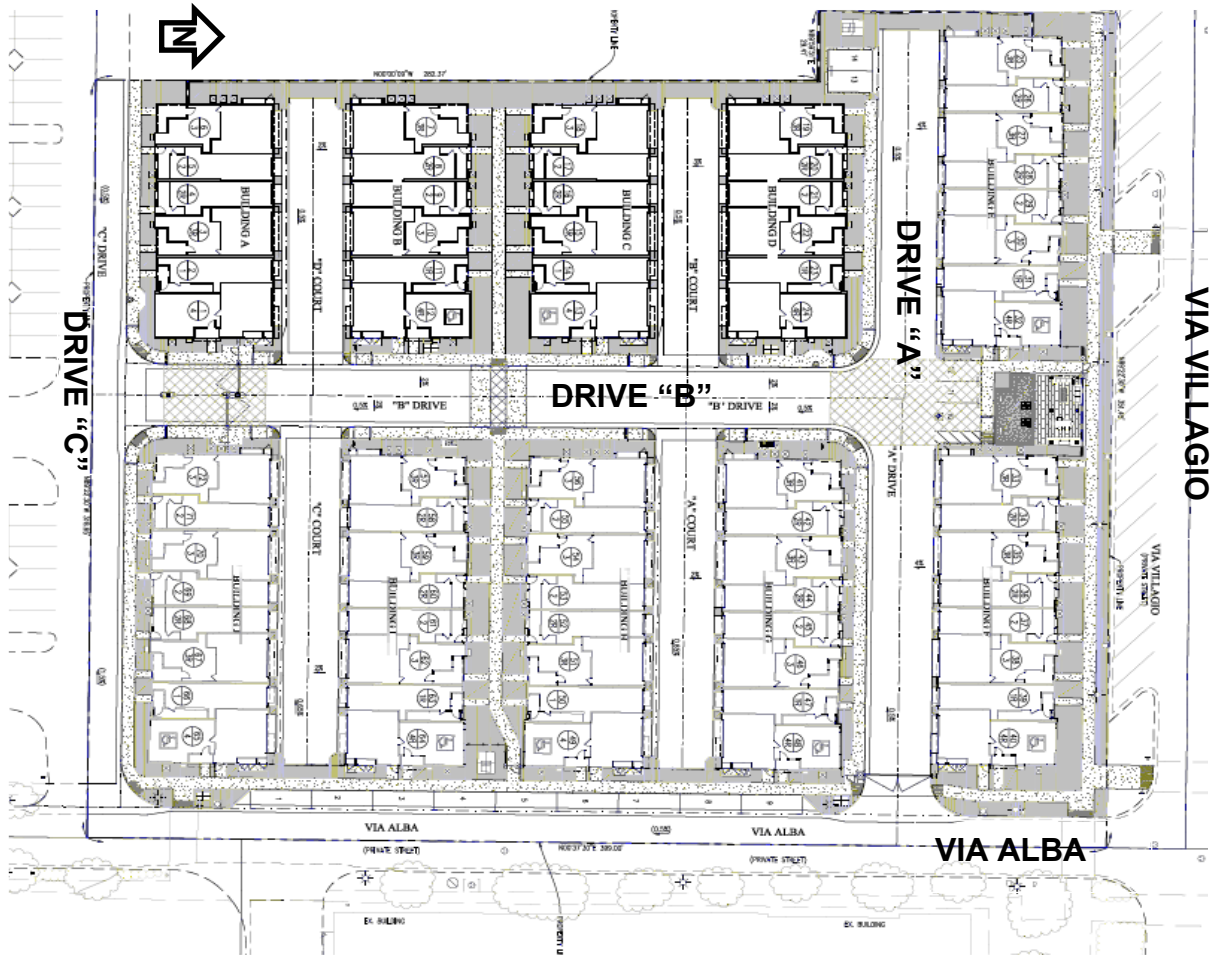


Exhibit D—FRONT PERSPECTIVE (MAIN ENTRY AREA)



Exhibit E—CORNER PERSPECTIVE (Northeast corner of Via Alba & Via Villaggio)



Exhibit F—FRONT EXTERIOR ELEVATION (6-PLEX TYPICAL ELEVATION)



Exhibit G—SIDES EXTERIOR ELEVATIONS (6-PLEX TYPICAL)



Exhibit H—REAR EXTERIOR ELEVATION (6-PLEX TYPICAL)



Exhibit I—FRONT EXTERIOR ELEVATION (8-PLEX TYPICAL)



Exhibit J—SIDES OF EXTERIOR ELEVATIONS (8-PLEX TYPICAL)



Exhibit K—REAR EXTERIOR ELEVATIONS (8-PLEX TYPICAL)



Exhibit L—FIRST FLOOR PLAN (6-PLEX TYPICAL)

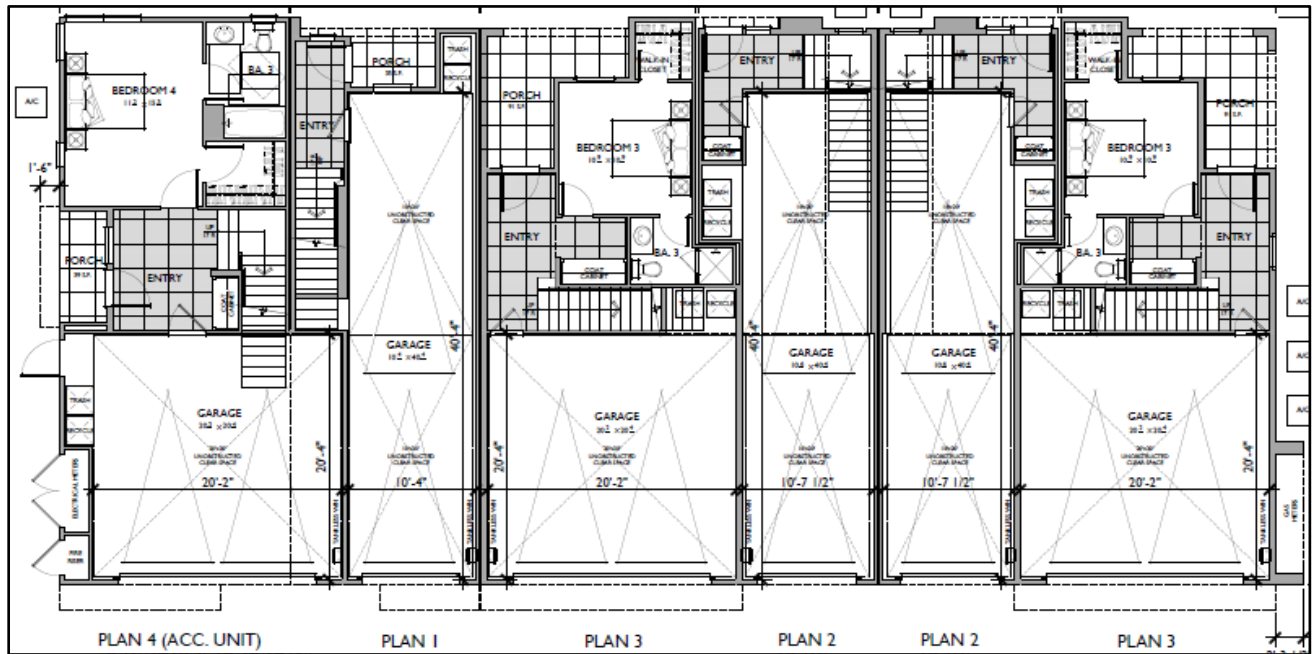


Exhibit M—SECOND FLOOR PLAN (6-PLEX TYPICAL)

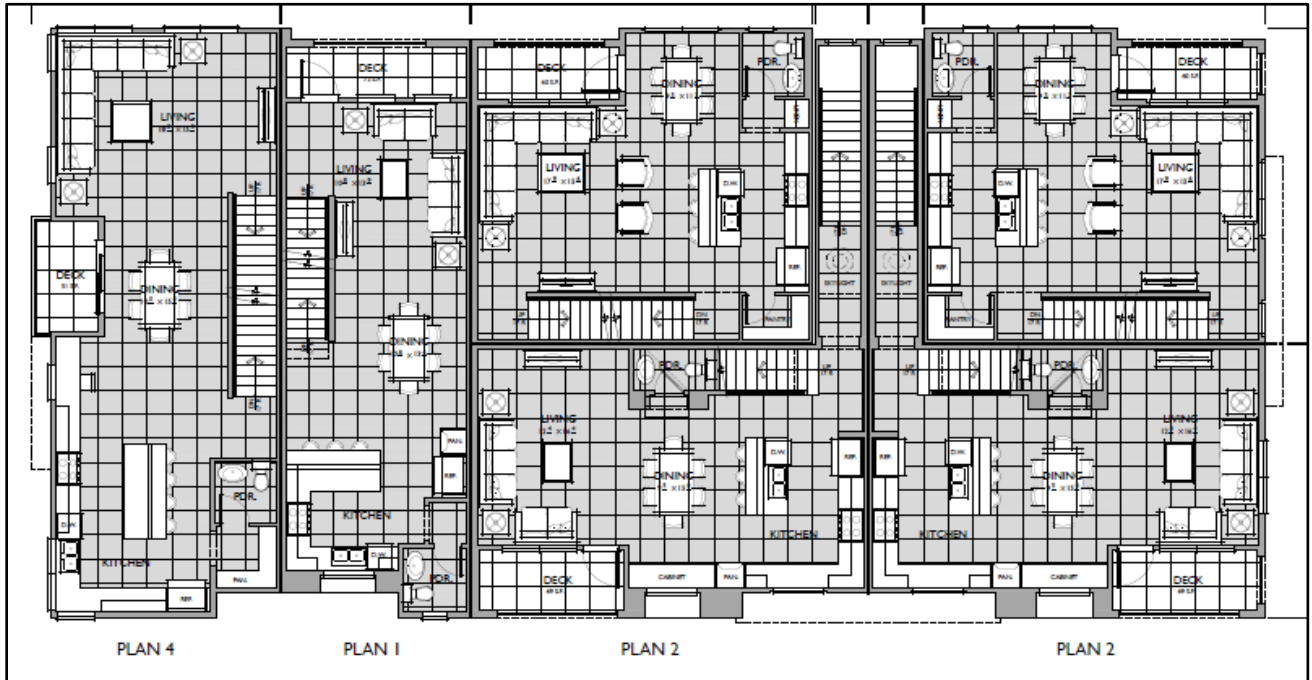


Exhibit N—THIRD FLOOR PLAN (6-PLEX TYPICAL)

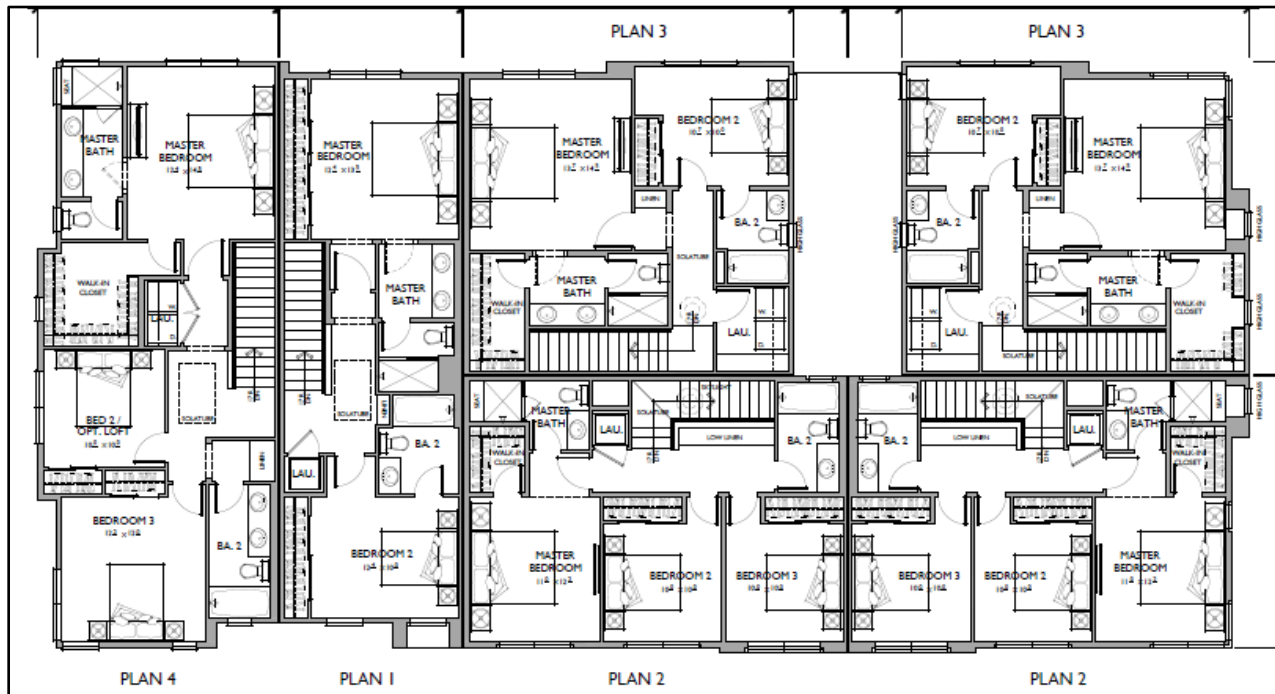


Exhibit O—FIRST FLOOR PLAN (8 PLEX TYPICAL)

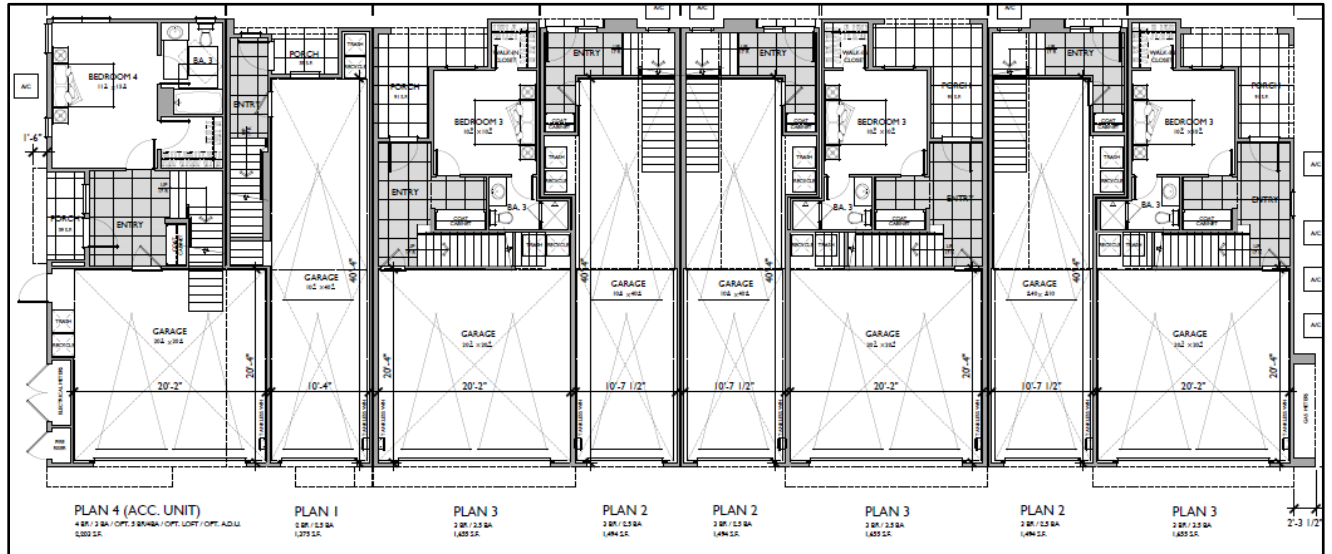


Exhibit P—SECOND FLOOR PLAN (8-PLEX TYPICAL)

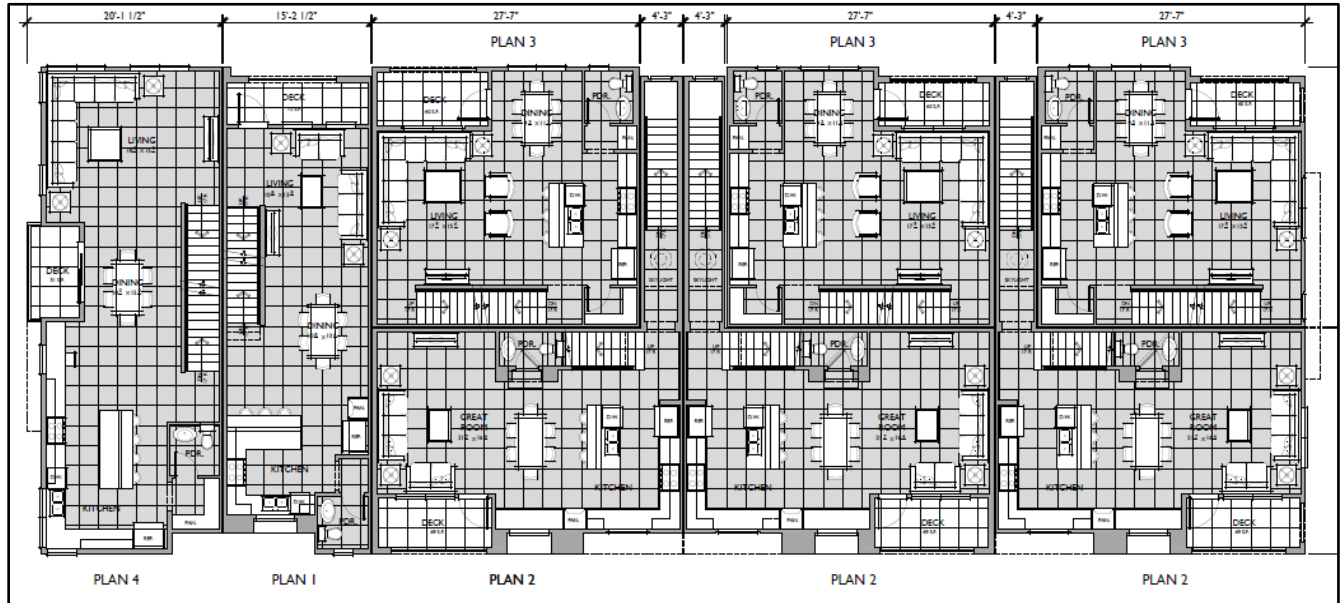


Exhibit Q—THIRD FLOOR PLAN (8-PLEX TYPICAL)

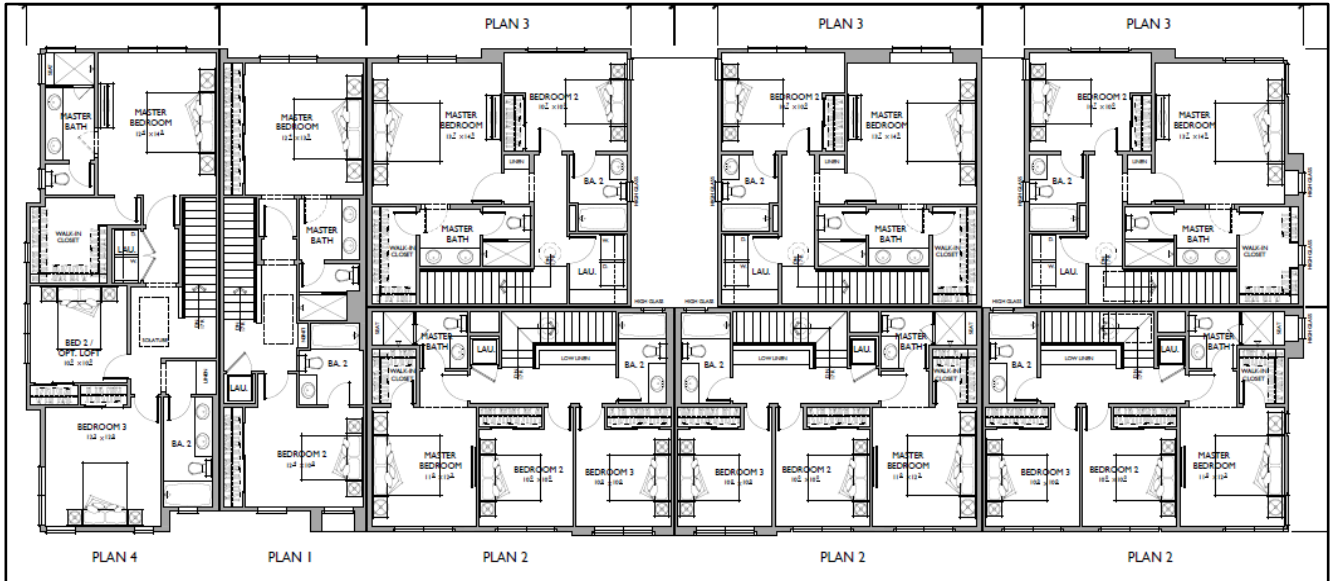


Exhibit S: PEDESTRIAN PASEO



Exhibit T—COMMUNITY PARK AMENITIES

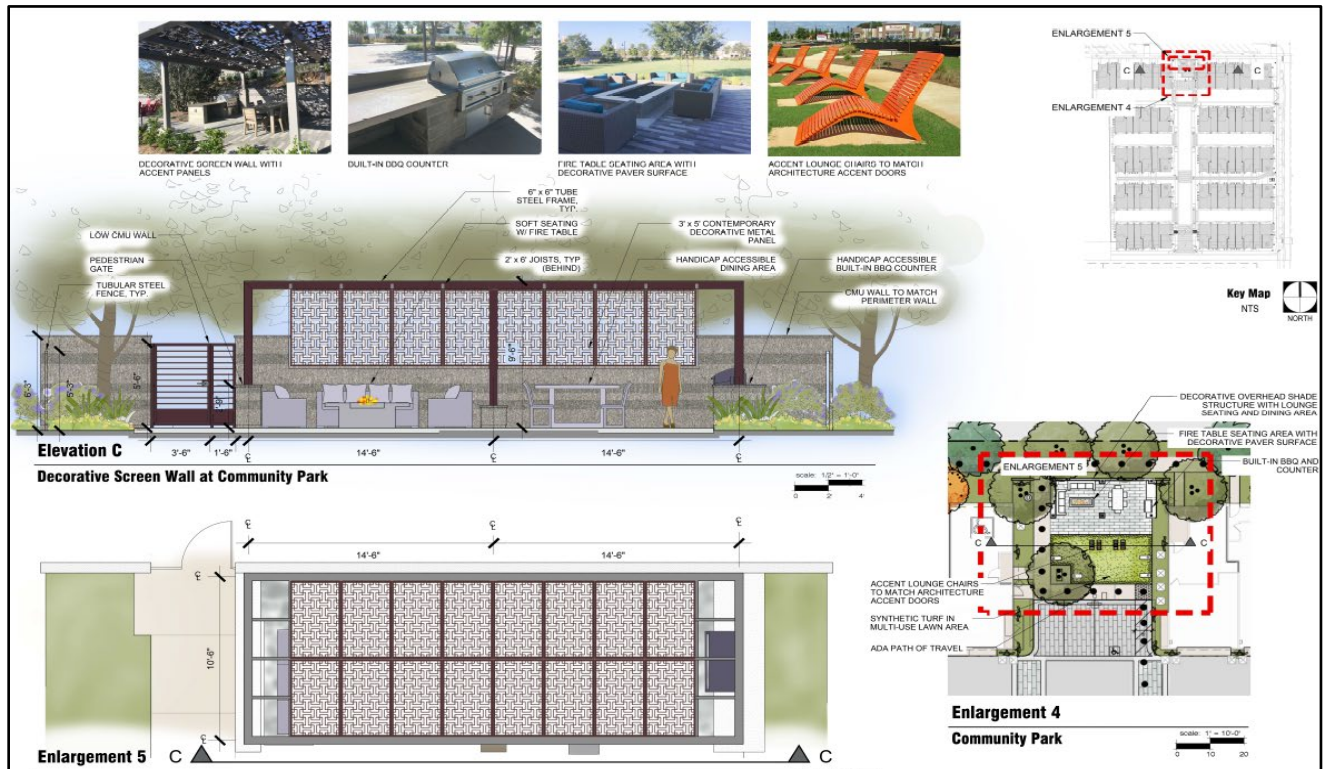


Exhibit U—COMMUNITY PARK

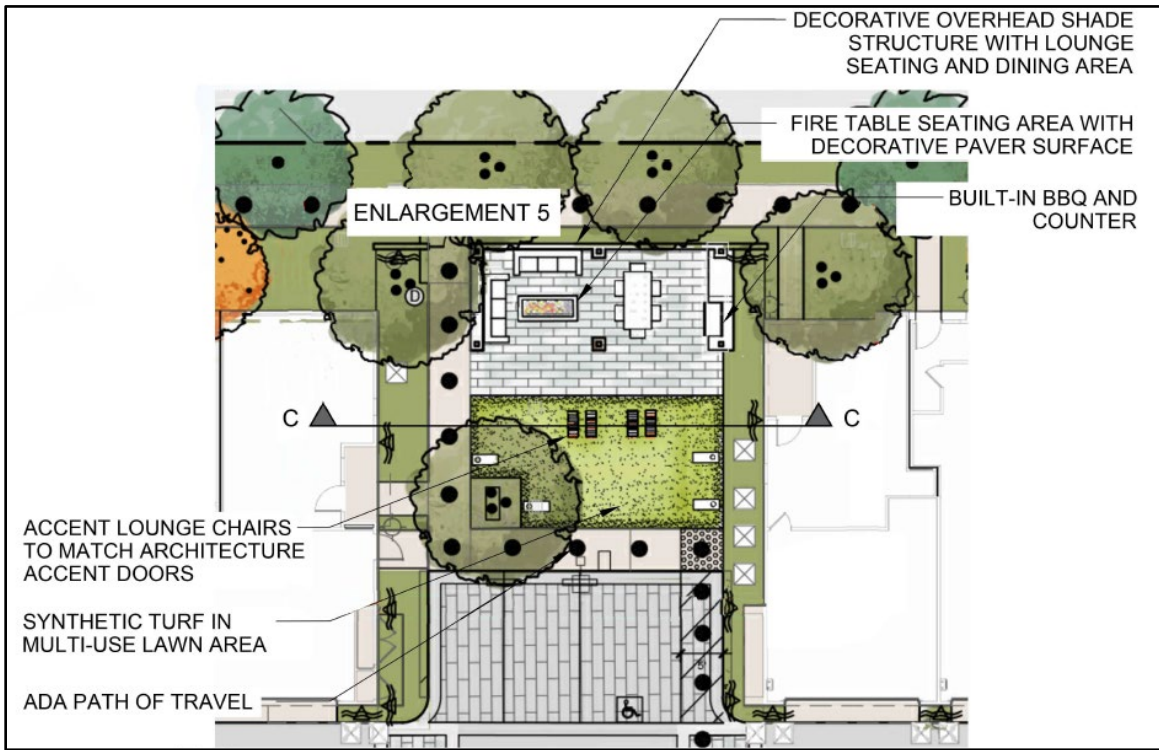


Exhibit V—TOTAL LANDSCAPED AREAS

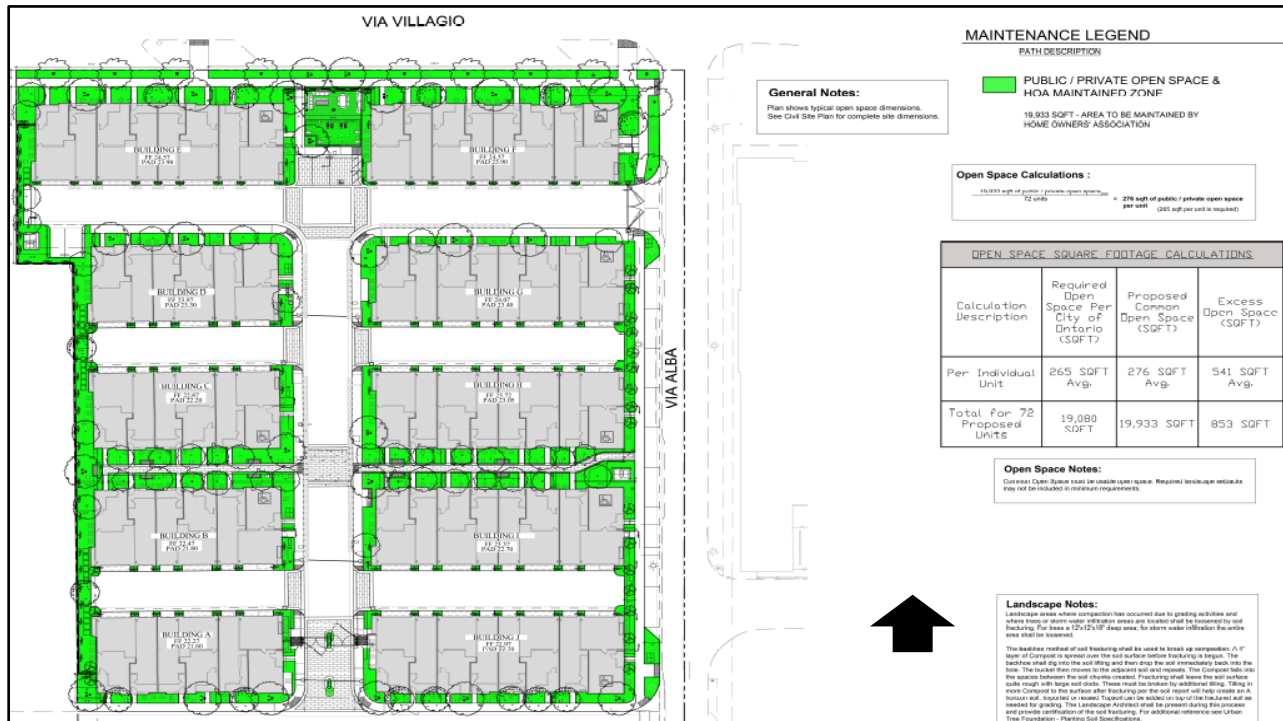


Exhibit W—PRIMARY VEHICULAR ENTRY GATES

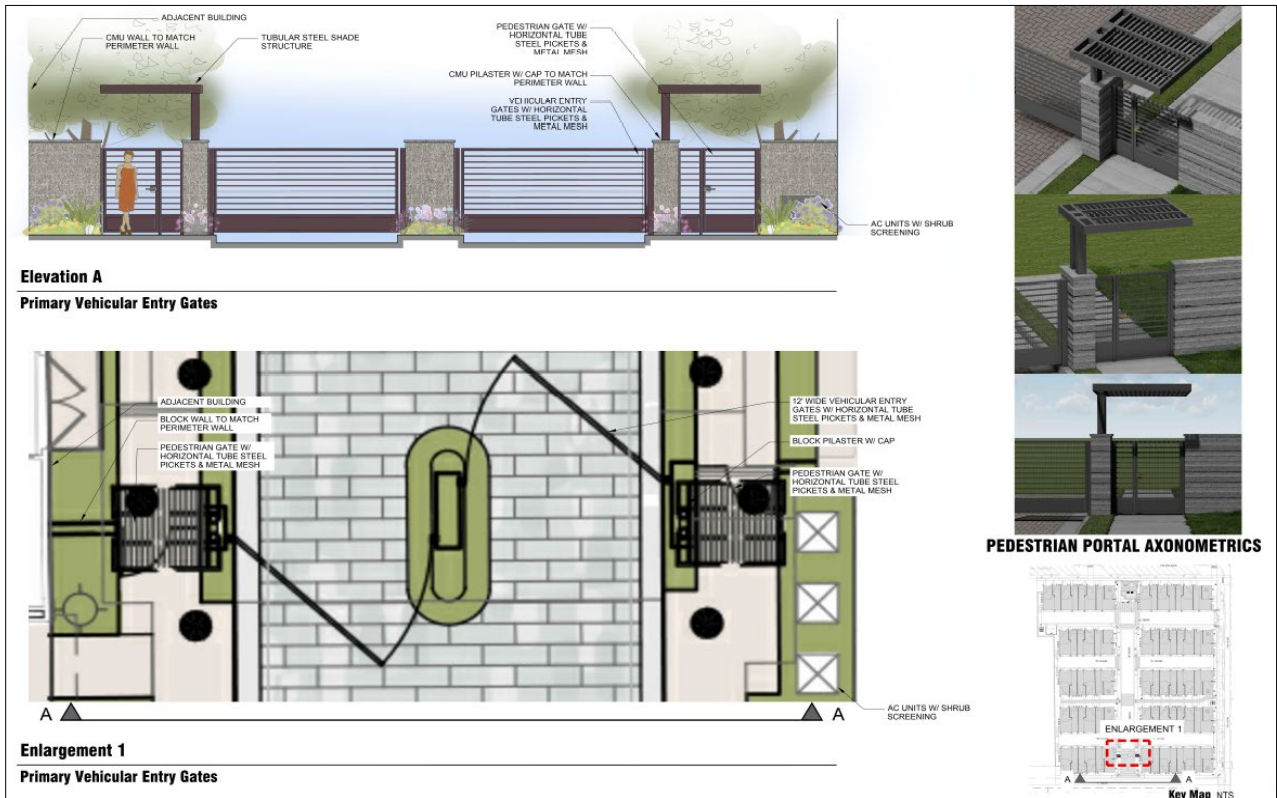
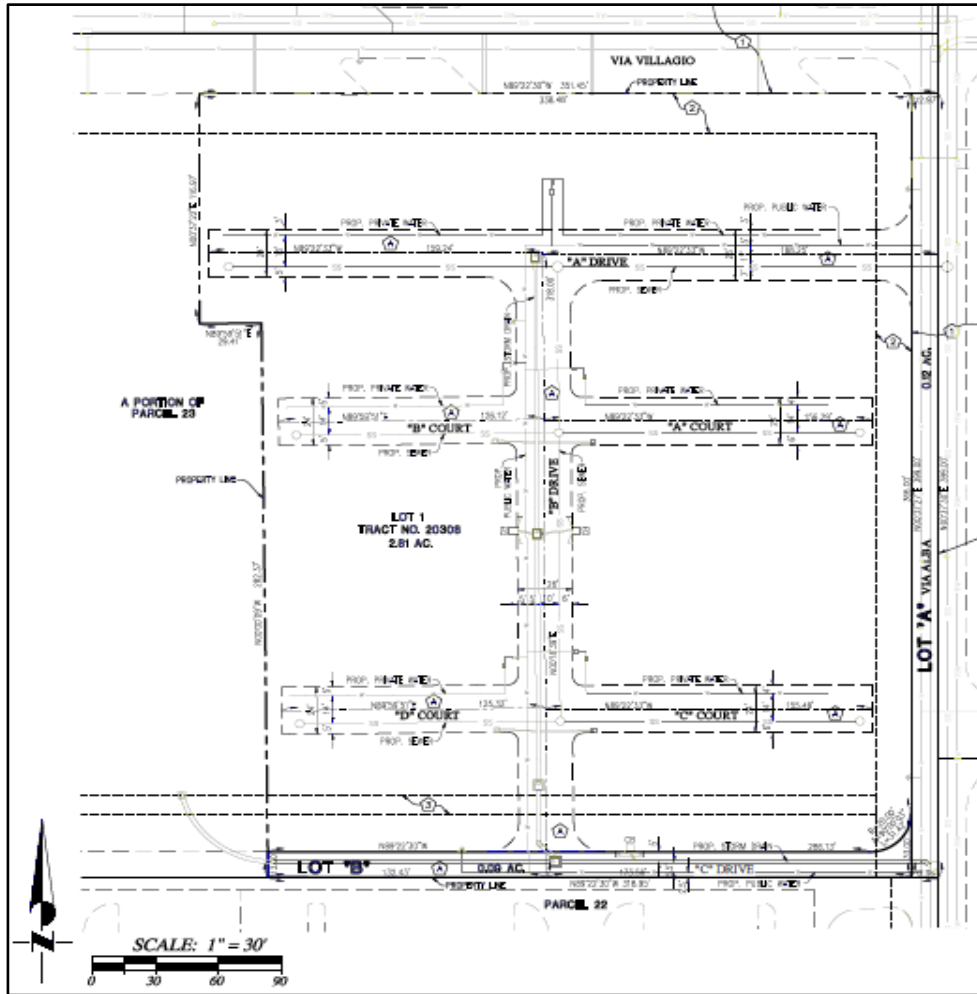


Exhibit X—TENTATIVE TRACT MAP



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PMTT19-016 (TT 20308), A TENTATIVE TRACT MAP TO SUBDIVIDE 3.02 ACRES OF LAND INTO ONE NUMBERED LOT AND TWO LETTERED LOTS FOR CONDOMINIUM PURPOSES, ON PROPERTY LOCATED AT THE SOUTHWEST CORNER OF VIA ALBA AND VIA VILLAGIO, WITHIN THE RESIDENTIAL LAND USE DISTRICT OF THE PIEMONTE OVERLAY OF THE ONTARIO CENTER SPECIFIC PLAN, AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0210-204-40.

WHEREAS, The New Home Company Southern California, LLC, ("Applicant") has filed an Application for the approval of a Tentative Tract Map, File No. PMTT19-016 (TT 20308), as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.02 acres of land generally located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay of The Ontario Center Specific Plan, and the site is presently vacant; and

WHEREAS, the property to the north of the project site is within the Entertainment/Retail land use district and is currently vacant. The property to the east is within the Commercial land use district and is developed with a Commercial Shopping Center. The property to the south is within the Special Use land use district and a new hotel (Element) is currently under construction. The property to the west is within the Entertainment/Retail land use district and is currently vacant; and

WHEREAS, in conjunction with a Tentative Tract Map, the applicant has also submitted a Development Plan (File No. PDEV19-054) to facilitate the construction of 72 multiple-family residential units (townhomes) on the subject property; and

WHEREAS, the proposed subdivision complies with the development standards and guidelines of the Piemonte Overlay district of The Ontario Center Specific Plan, as the Residential land use district does not have a minimum lot size requirement; and

WHEREAS, Lot 1 is being created to facilitate the individual sale of 72 multiple-family residential townhomes and Lots A and B provide for street dedications along Via Alba and a private drive along the southern project boundary (Drive "C"); and

WHEREAS, Covenants, Conditions and Restrictions (CC&Rs) are required for the proposed subdivision as a condition of project approval. The CC&Rs must be submitted, reviewed, and approved by the City, and will be recorded with the final map to ensure

ongoing maintenance of private roads, common landscape areas, amenities, and common drainage/easement areas; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration (hereinafter referred to as "MND") was adopted by the City Council on May 16, 2017, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 20, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-018, recommending the Planning Commission approve the Application; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous MND and supporting documentation. Based upon the facts and information contained in the previous MND and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference; and

(2) The previous MND contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous MND was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous MND reflects the independent judgment of the Planning Commission; and

(5) The proposed project will introduce no new significant environmental impacts beyond those previously analyzed in the previous MND, and all mitigation measures previously adopted with the MND, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the

preparation of a subsequent or supplemental MND is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the MND that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the MND was prepared, that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the MND was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the MND; or

(b) Significant effects previously examined will be substantially more severe than shown in the MND; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the MND would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the proposed project will not be consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The project site is listed in the Available Land Inventory of the Housing Element Technical Report (Parcel ID No. 125) and the proposed project is not consistent with the number of dwelling units (minimum 233 units required) and/or density specified (minimum 46 DUs/acre) in the Available Land Inventory; however, the removal will not impact the City's Regional Housing Needs Allocation obligations, as an amendment to the Available Land Inventory was previously made, which will offset the loss in units (161 unit difference) required on the project site. Recently added to the inventory is 22.39 acres of land located

at the southeast corner of Vineyard Avenue and Inland Empire Boulevard (APNs: 0110-311- 52, 0110-311-53, 0110-311-54, and 0110-311-55), which was approved by the Planning Commission on November 26, 2019 (File No. PDEV19-025/Resolution PC19-095) for the construction of 925 multiple-family dwellings, at a density of 47 DUs/acre. In addition, on March 3, 2020, The City Council Approved a General Plan Amendment (File No. PGPA20-001) to update the Future Buildout Table (Exhibit LU-03) to include the 925 multi-family units (Resolution No. 2020-016). Therefore, the subject property deficit of 161 residential units from the inventory will not impact the City’s Regional Housing Needs Allocation obligations, as there are an adequate number of sites identified in the Available Land Inventory to meet the RHNA obligation.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan (“ALUCP”) Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as “ONT”), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Tentative Tract is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable area and specific plans, and planned unit developments.*** The proposed Tentative Tract is located within the Mixed Use land use district of the Policy Plan Land Use Map, and the Residential District of the Piemonte Overlay Ontario Center Specific Plan. The proposed subdivision is

consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “a spectrum of housing types and price ranges that match the jobs in the City, and that make it possible for people to live and work in Ontario and maintain a quality of life” (Goal LU1). Furthermore, the project will promote the City’s policy to “incorporate a variety of land uses and building types that contribute to a complete community where residents of all stages of life, employers, workers, and visitors, have a wide spectrum of choices of where they can live, work, shop, and recreate within Ontario” (Policy LU1-6 *Complete Community*); and

(2) ***The design or improvement of the proposed Tentative Tract is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, and applicable specific plans and planned unit developments.*** The proposed Tentative Tract is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the Residential District of the Piemonte Overlay Ontario Center Specific Plan. The proposed design or improvement of the subdivision is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, as the project will contribute to providing “[a] high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct” (Goal CD2). Furthermore, the project will promote the City’s policy to “create distinct residential neighborhoods that are functional, have a sense of community, emphasize livability and social interaction, and are uniquely identifiable places through such elements as:

- A pattern of smaller, walkable blocks that promote access, activity and safety
- Variable setbacks and parcel sizes to accommodate a diversity of housing types
- Traffic calming measures to slow traffic and promote walkability while maintaining acceptable fire protection and traffic flows
- Floor plans that encourage views onto the street and de-emphasize the visual and physical dominance of garages (introducing the front porch as the “outdoor living room”), as appropriate; and Landscaped parkways, with sidewalks separated from the curb.” (Policy CD2-2 *Neighborhood Design*)

(3) ***The site is physically suitable for the type of development proposed.*** The project site meets the minimum lot area and dimensions of the Residential land use designation of the Piemonte Overlay-Ontario Center Specific Plan, and is physically suitable for the type of residential development proposed in terms of zoning, land use and development activity proposed, and existing and proposed site conditions; and

(4) ***The site is physically suitable for the density/intensity of development proposed.*** The project site is proposed for residential development at a density of 24.1 DUs/acre. The project site meets the minimum lot area and dimensions of the Residential District and is physically suitable for the proposed density; and

(5) ***The design of the subdivision or the proposed improvements thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat.*** The project site is not located in an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Wildlife or the U.S. Fish and Wildlife Service, nor does the site contain any riparian habitat or other sensitive natural community, and no wetland habitat is present on site; therefore, the design of the subdivision, or improvements proposed thereon, are not likely to cause substantial environmental damage, or substantially and avoidably injure fish or wildlife, or their habitat; and

(6) ***The design of the subdivision, or the type of improvements thereon, are not likely to cause serious public health problems.*** The design of the proposed subdivision, and the 72 multi-family residential units (townhomes) proposed for the project site, are not likely to cause serious public health problems. The project is not anticipated to involve the transport, use, or disposal of hazardous materials during either construction or project implementation, include the use of hazardous materials or volatile fuels, nor are there any known stationary commercial or industrial land uses within close proximity to the subject site that use/store hazardous materials to the extent that they would pose a significant hazard to visitors or occupants at the project site; and

(7) ***The design of the subdivision, or the type of improvements thereon, will not conflict with easements acquired by the public at large for access through, or use of property within, the proposed subdivision.*** The proposed subdivision has provided for all necessary public easements and dedications for access through, or use of property within, the proposed subdivision. Furthermore, all such public easements and dedications have been designed pursuant to: (a) the requirements of the Policy Plan component of The Ontario Plan and applicable area plans; (b) applicable specific plans or planned unit developments; (c) applicable provisions of the City of Ontario Development Code; (d) applicable master plans and design guidelines of the City; and (e) applicable Standard Drawings of the City.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 7: *Indemnification.* The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: *Custodian of Records.* The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: *Certification to Adoption.* The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PMTT19-016 (TT 20308)
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)




City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department
Land Development Division
Conditions of Approval

Meeting Date: April 28, 2020
File No: PMTT19-016 (TT 20308)
Related Files: PDEV19-054

Project Description: A Tentative Tract Map (File No. PMTT19-016/TT20308) to subdivide 3.02 acres of land into one numbered lot and two lettered lots for condominium purposes for property located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay-Ontario Center Specific Plan (APN: 0210-204-40); **submitted by The New Home Company Southern California LLC.**

Prepared By: Luis E. Batres, Senior Planner 
Phone: 909.395.2431
Email: Lbatres@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Tentative Tract Map approval shall become null and void 2 years following the effective date of application approval, unless the final parcel/tract map has been recorded, or a time extension has been approved by the Planning Commission pursuant to Development Code Section 2.02.025 (Time Limits and Extensions). This Permit does not supersede any individual time limits specified herein for performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

(d) A building permit will not be issued for this project unless the Specific Plan Amendment (File No. PSPA19-009) to allow tandem parking has been approved and adopted by the City Council.

2.3 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owners association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;

(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

(h) The applicant shall incorporate an approved Parking Enforcement Plan as part of the CC&R's for the project.

2.4 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

2.5 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.6 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.7 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

1.01 Dedicate to the City of Ontario, the right-of-way, described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____.

1.02 Dedicate to the City of Ontario, the following easement(s):
 1) Curb-to-curb public water easement for the public domestic water mains proposed within the private interior streets ("A" Drive [to prolongation of west curb line of "B" Drive] and "B" Drive) and curb-to-centerline of "C" Drive along the entire southerly frontage of the proposed tract in accordance with the approved tentative map ~~and the final Utilities Systems Map~~. Also, a minimum 10-foot wide easement for the master meter service lateral serving buildings "E" and "F" per said Utilities Systems Map. Additional easements to the City (Min. 5'x5') for maintenance and/or replacement of the master meters will be required. Ultimate location of the meters/easements will be determined prior to Final Map approval or the easements may be recorded by separate instrument.
 2) Curb-to-curb public sewer easement for the public sewer mains and manholes proposed within the private interior streets ("A" Drive, "B" Drive [to the south curb line of "C" and "D" Courts] and "A", "B", "C" and "D" Courts) of the proposed tract in accordance with the approved tentative map ~~and the final Utilities Systems Map~~.

1.03 Restrict vehicular access to the site as follows: _____

1.04 Vacate the following street(s) and/or easement(s):
 1) The south 20-feet of Via Villagio, dedicated to the City as a 102-foot wide easement for public utility and emergency access purposes.
 2) The south 20-feet of Via Villagio, previously reserved as a 102-foot wide private easement for street, private utility and maintenance purposes.
 3) The west 12-feet of Via Alba dedicated to the City as a 56-foot wide easement for public utility and emergency access purposes.
 4) The west 12-feet of Via Alba, previously reserved as a 56-foot wide private easement for street, private utility and maintenance purposes.
 5) The north 10-feet of the southerly 43-feet of Parcel 23 of Parcel Map No. 17550 dedicated to the City as an easement for pedestrian access purposes.

1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.

1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning



Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open spaces/easements **and shall clearly distinguish between public and private utilities within these spaces.** In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.

Note: The existing CC&Rs recorded with Parcel Map No. 17550 and on file with the Planning Department may require amendment to address the maintenance and other responsibilities of the new HOA as it relates to the existing overall Piemonte development.

- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure

Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.

- (1) _____
- (2) _____

- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required or complete all public improvements.

- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.

- 1.11 Provide a preliminary title report current to within 30 days.

- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.

- 1.13 New Model Colony (NMC) Developments:

- 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.

- 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm



Water Treatment Equivalents).

3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).

1.14 **Other conditions: 1) Developer/applicant shall ensure that the project is developed in accordance with all requirements of the underlying Parcel Map No. 17550 and the Ontario Center Specific Plan.**

2) Developer/applicant shall ensure that the tentative tract map and development plan conforms to the final Utility Systems Map, based on the conceptual map, dated 3/4/2020. The final Utility Systems Map shall be submitted for review and approval with the precise grading plan.

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

2.01 **Record Tract Map No. 20308 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.**

2.02 **Submit a duplicate photo mylar of the recorded map to the City Engineer's office.**

2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario Per Parcel 23 of Parcel Map No. 17550 as recorded in Book 216 of Parcel Maps, pages 7-20, inclusive in the Office of the County Recorder, C ounty of San Bernardino, California.**

2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____.

2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 Make a Dedication of Easement.

2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

2.08 Submit a soils/geology report.

2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:

- State of California Department of Transportation (Caltrans)
- San Bernardino County Road Department (SBCRD)



- San Bernardino County Flood Control District (SBCFCD)
- Federal Emergency Management Agency (FEMA)
- Cucamonga Valley Water District (CVWD) for sewer/water service
- United States Army Corps of Engineers (USACE)
- California Department of Fish & Game
- Inland Empire Utilities Agency (IEUA)
- Other: _____

- 2.10 Dedicate to the City of Ontario the right-of-way described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____.
- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.14 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.15 **Pay all Development Impact Fees (DIF) to the Building Department. Final fees shall be determined based on the approved site plan.**
- 2.16 Other conditions: _____

B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)



2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Interior Drives/Courts	Street 2	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace
Sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Laterals	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral



Water (see Sec. 2.D)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above: _____

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): _____
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.



- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Sewer plan bar codes: S13828 and S13829)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions: 1) Sewer mains within this development shall be designed to meet the requirements of Section 4-8 of the City's Master Plan of Sewer.**
2) The on-site sewer mains and manholes within this development shall be public, within a public utility easement and as such, shall be publicly maintained.

D. WATER

- 2.27 **A 12-inch water main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Water plan bar codes: W13050 and W13052)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions: 1) Developer/applicant shall submit an application for a Fire Flow Test to the Ontario Fire Department.**
2) All buildings (A, B, C, D, E, F, G, H, I & J) shall be served by master meters with backflow preventers and submetering for each residential unit.
3) The potable water mains, master meters and services within "A", "B" and "C" Drives of this development shall be public, within a public utility easement and as such, shall be publicly maintained. All potable water mains shall be ductile iron or CML&C.

E. RECYCLED WATER

- 2.30 **An 8-inch recycled water main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Recycled Water plan bar code: P10075)**
- 2.31 **Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.**
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.



- 2.33 **Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.**

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.34 **Other conditions: This development shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including, but not limited to landscaping irrigation. This includes separate recycled water services for each building's private landscape area.**

F. TRAFFIC / TRANSPORTATION

- 2.35 **Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:**
 1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer

- 2.36 **New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.**

- 2.37 **Other conditions: 1) Developer/applicant shall design and construct "C" Drive such that it will support two-way traffic (i.e. minimum width shall be 24-feet).**

2) Developer/applicant shall modify existing Via Alba such that the proposed improvements match the existing improvements along the street (i.e. new curb & gutter, relocation of street lights, etc.).

3) Developer/applicant shall modify the intersection of proposed "C" Drive and Via Alba to relocate the stop sign for northbound traffic to the proper location.

G. DRAINAGE / HYDROLOGY

- 2.38 **An existing 42-inch private HDPE storm drain main is available to accept flows from this project in Via Alba. This system connects to public system in Ontario Center Parkway. (Ref: Storm Drain plan bar code: N/A)**

- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study**

- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**

- 2.41 **Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.**

- 2.42 **Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.**

- 2.43 **Other conditions: _____**



H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at:
<http://www.sbcounty.gov/dpw/land/npdes.asp>.
- 2.46 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**
- 2.47 **Other conditions: 1) Activities resulting in land disturbance of one (1) or more acres, requires coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at:**
<https://smarts.waterboards.ca.gov/smarts/faces/SwSmartslogin.xhtml>

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.**
- 2.51 **Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.**



L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual located at: <http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions: 1) This site shall comply with the requirements of State Assembly Bill AB 1826 and AB 341, the Integrated Waste Department and the Refuse & Recycling Planning Manual located at: <https://www.ontarioca.gov/omuc/integrated-waste>
 - 2) A final Solid Waste Handling Plan (SWHP), demonstrating compliance with the SWHP requirements shall be submitted for review and approval with the precise grading plan.
 - 3) A final Integrated Waste Management Report (IWMR), demonstrating compliance with the IWMR requirements shall be submitted for review and approval with the precise grading plan.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.



- 4.03** The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.



EXHIBIT 'A'

**ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist**

Project Number: **PDEV19-054 and Tract Map No. 20308**

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. Three (3) sets of Public Street improvement plan with street cross-sections
7. Three (3) sets of Private Street improvement plan with street cross-sections
8. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. **Four (4) sets of Public Sewer improvement plan**
11. Five (5) sets of Public Storm Drain improvement plan
12. Three (3) sets of Public Street Light improvement plan
13. Three (3) sets of Signing and Striping improvement plan
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).
18. **One (1) copy of Hydrology/Drainage study**
19. One (1) copy of Soils/Geology report
20. **Payment for Final Map/Parcel Map processing fee**
21. **Three (3) copies of Final Map/Parcel Map**



- 22. One (1) copy of approved Tentative Map
- 23. One (1) copy of Preliminary Title Report (current within 30 days)
- 24. One (1) copy of Traverse Closure Calculations
- 25. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, APPROVING FILE NO. PDEV19-054, A DEVELOPMENT PLAN TO CONSTRUCT 72 MULTI-FAMILY RESIDENTIAL UNITS (TOWNHOMES). THE PROJECT IS LOCATED AT THE SOUTHWEST CORNER OF VIA ALBA AND VIA VILLAGIO, WITHIN THE RESIDENTIAL LAND USE DISTRICT OF THE PIEMONTE OVERLAY-ONTARIO CENTER SPECIFIC PLAN AND MAKING FINDINGS IN SUPPORT THEREOF—APN: 0210-204-40.

WHEREAS, The New Home Company Southern California, LLC. ("Applicant") has filed an Application for the approval of a Development Plan, File No. PDEV19-054, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 3.02 acres of land generally located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay of The Ontario Center Specific Plan, and is presently vacant; and

WHEREAS, the property to the north of the Project site is within the Entertainment/Retail land use district and is currently vacant. The property to the east is within the Commercial land use district and is developed with a Commercial Shopping Center. The property to the south is within the Special Use land use district and a new hotel (Element) is currently under construction. The property to the west is within the Entertainment/Retail land use district and is currently vacant; and

WHEREAS, in conjunction with the proposed Development Plan, the Applicant has submitted a Tentative Tract Map (File No. PMTT19-016/TT 20308) to subdivide the Project site into one numbered lot and two lettered lots for condominium purposes, to facilitate the development of 72 multiple-family dwelling units (townhomes); and

WHEREAS, in conjunction with the proposed Development Plan, the Applicant has submitted a Specific Plan Amendment (File No. PSPA19-009) for the purpose of modifying the Minimum Parking Requirements (Section 3.3.5.1) of the Piemonte Overlay of The Ontario Center Specific Plan to allow tandem parking to be counted toward a maximum of 50 percent of the dwelling unit parking space requirement. Moreover, Development Plan approval will not be final and conclusive until the Specific Plan Amendment is approved by the City Council; and

WHEREAS, the Development Plan is composed of ten buildings comprised of a total of 72 townhomes. All buildings are proposed at three stories, with an overall height of 38 feet. The development is proposed at a density of 24.1 dwellings per acre, consistent with the Piemonte Overlay-Ontario Center Specific Plan; and

WHEREAS, the Project has been designed as a gated community, with one point of vehicular access from a private drive that is common with the adjoining property to the south; and

WHEREAS, the Project provides four different floor plans range in size from 1,360 to 2,205 square feet, and are arranged in 6-Plex and 8-Plex row town products; and

WHEREAS, off-street parking in the form of enclosed garage spaces and uncovered surface parking is distributed throughout the Project site. A total of 156 off-street parking spaces are required for the Project, which includes 14 guest parking spaces. The Project provides 158 parking spaces, including the 14 guest parking spaces, and therefore, exceeds the minimum number of parking spaces required; and

WHEREAS, the Application is a Project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an initial study has been prepared to determine possible environmental impacts; and

WHEREAS, the environmental impacts of this Project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration (hereinafter referred to as "MND") was adopted by the City Council on May 16, 2017, and this Application introduces no new significant environmental impacts; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and act on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan

(hereinafter referred to as “ALUCP”), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, on April 20, 2020, the Development Advisory Board of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date, voting to issue Decision No. DAB20-019, recommending the Planning Commission approve the Application; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the information contained in the previous MND and supporting documentation. Based upon the facts and information contained in the previous MND and supporting documentation, the Planning Commission finds as follows:

(1) The environmental impacts of this Project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval and are incorporated herein by this reference; and

(2) The previous MND contains a complete and accurate reporting of the environmental impacts associated with the Project; and

(3) The previous MND was completed in compliance with CEQA and the Guidelines promulgated thereunder; and

(4) The previous MND reflects the independent judgment of the Planning Commission; and

(5) The proposed Project will introduce no new significant environmental impacts beyond those previously analyzed in the previous MND, and all mitigation measures previously adopted with the MND, are incorporated herein by this reference.

SECTION 2: Subsequent or Supplemental Environmental Review Not Required. Based on the information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental MND is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the MND that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the MND was prepared, that will require major revisions to the MND due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the MND was certified/adopted, that shows any of the following:

(a) The Project will have one or more significant effects not discussed in the MND; or

(b) Significant effects previously examined will be substantially more severe than shown in the MND; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the MND would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: Housing Element Compliance. Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the decision-making authority for the Project, the Planning Commission finds that based on the facts and information contained in the Application and supporting documentation, at the time of Project implementation, the proposed Project will not be consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan. The

Project site is listed in the Available Land Inventory of the Housing Element Technical Report (Parcel ID No. 125) and the proposed Project is not consistent with the number of dwelling units (minimum 233 units required) and/or density specified (minimum 46 DUs/acre) in the Available Land Inventory; however, the removal will not impact the City's Regional Housing Needs Allocation obligations, as an amendment to the Available Land Inventory was previously made, which will offset the loss in units (161 unit difference) required on the Project site. Recently added to the inventory is 22.39 acres of land located at the southeast corner of Vineyard Avenue and Inland Empire Boulevard (APNs: 0110-311- 52, 0110-311-53, 0110-311-54, and 0110-311-55), which was approved by the Planning Commission on November 26, 2019 (File No. PDEV19-025/Resolution PC19-095) for the construction of 925 multiple-family dwellings, at a density of 47 DUs/acre. In addition, on March 3, 2020, The City Council Approved a General Plan Amendment (File No. PGPA20-001) to update the Future Buildout Table (Exhibit LU-03) to include the 925 multi-family units (Resolution No. 2020-016). Therefore, the subject property deficit of 161 residential units from the inventory will not impact the City's Regional Housing Needs Allocation obligations, as there are an adequate number of sites identified in the Available Land Inventory to meet the RHNA obligation.

SECTION 4: *Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport (hereinafter referred to as "ONT"), which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the decision-making authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 5: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Section 1 through 4, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed development at the proposed location is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Project is located within the Mixed-Use land use district of the Policy Plan Land Use Map, and the Residential District of the Piemonte Overlay (The Ontario Center Specific Plan). The development standards and conditions under which the proposed Project will be constructed and maintained, is consistent with the goals, policies, plans, and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan. In addition, it meets goal LU1-6: Complete Community where we incorporate a variety of land uses and buildings types in our land use planning efforts that result in a complete community where residents at all stages of life, employers, workers, and visitors have a wide spectrum of choices of where they can live, work, shop and recreate within Ontario; and

(2) ***The proposed development is compatible with those on adjoining sites in relation to location of buildings, with particular attention to privacy, views, any physical constraint identified on the site and the characteristics of the area in which the site is located.*** The Project has been designed consistent with the requirements of the City of Ontario Development Code and the Residential land use district of the Piemonte Overlay district of The Ontario Center Specific Plan, including standards relative to the particular land use proposed, as-well-as building intensity, building and parking setbacks, building height, number of off-street parking and loading spaces, on-site and off-site landscaping, and fences, walls and obstructions. Approval of the project will result in the development of 72 multiple-family residential units on 3.02 acres of land. In addition, the project will include full on-site and off-site improvements that will also improve the immediate area; and

(3) ***The proposed development will complement and/or improve upon the quality of existing development in the vicinity of the project and the minimum safeguards necessary to protect the public health, safety and general welfare have been required of the proposed project.*** The Planning Commission has required certain safeguards, and impose certain conditions of approval, which have been established to ensure that: [i] the purposes of the Piemonte Overlay Ontario Center Specific Plan are maintained; [ii] the Project will not endanger the public health, safety or general welfare; [iii] the Project will not result in any significant environmental impacts; [iv] the Project will be in harmony with the area in which it is located; and [v] the Project will be in full conformity with the Vision, City Council Priorities and Policy Plan components of The Ontario Plan, and the Residential District of Piemonte Overlay-Ontario Center Specific Plan. In addition, the Project will provide much needed housing which will also allow the City to comply with our Housing Element needs; and

(4) ***The proposed development is consistent with the development standards and design guidelines set forth in the Development Code, or applicable specific plan or planned unit development.*** The proposed Project has been reviewed for consistency with the general development standards and guidelines of the Residential District of the Piemonte Overlay-Ontario Center Specific Plan, that are applicable to the proposed Project, including building intensity, building and parking setbacks, building height, amount of off-street parking and loading spaces, parking lot dimensions, design and landscaping, bicycle parking, on-site landscaping, and fences and walls, as-well-as those development standards and guidelines specifically related to the particular land use being proposed (72 multiple-family residential units). As a result of this review, the Planning Commission has determined that the Project, when implemented in conjunction with the conditions of approval is consistent with the development standards and guidelines described in the Residential land use of the Piemonte Overlay of The Ontario Center Specific Plan.

SECTION 6: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 5, above, the Planning Commission hereby APPROVES the herein described Application, subject to each and every condition set forth in the Department reports attached hereto as “Attachment A,” and incorporated herein by this reference.

SECTION 7: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 8: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East “B” Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 9: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PDEV19-054
Departmental Conditions of Approval**

(Departmental conditions of approval to follow this page)




City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

Planning Department Land Development Division Conditions of Approval

Meeting Date: April 28, 2020
File No: PDEV19-054
Related Files: PMTT19-016 (TT 20308)

Project Description: A Development Plan (File No. PDEV19-054) to construct 72 multi-family residential units (Townhomes) on 3.02 acres of land, on property located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay-Ontario Center Specific Plan (APN: 0210-204-40); **submitted by The New Home Company Southern California LLC.**

Prepared By: Luis Batres, Senior Planner 
Phone: 909.395.2431 (direct)
Email: Lbatres@ontarioca.gov

The Planning Department, Land Development Section, conditions of approval applicable to the above-described Project, are listed below. The Project shall comply with each condition of approval listed below:

1.0 Standard Conditions of Approval. The project shall comply with the *Standard Conditions for New Development*, adopted by City Council Resolution No. 2017-027 on April 18, 2017. A copy of the *Standard Conditions for New Development* may be obtained from the Planning Department or City Clerk/Records Management Department.

2.0 Special Conditions of Approval. In addition to the *Standard Conditions for New Development* identified in condition no. 1.0, above, the project shall comply with the following special conditions of approval:

2.1 Time Limits.

(a) Development Plan approval shall become null and void 2 years following the effective date of application approval, unless a building permit is issued and construction is commenced, and diligently pursued toward completion, or a time extension has been approved by the Planning Director. This condition does not supersede any individual time limits specified herein, or any other departmental conditions of approval applicable to the Project, for the performance of specific conditions or improvements.

2.2 General Requirements. The Project shall comply with the following general requirements:

(a) All construction documentation shall be coordinated for consistency, including, but not limited to, architectural, structural, mechanical, electrical, plumbing, landscape and irrigation, grading, utility and street improvement plans. All such plans shall be consistent with the approved entitlement plans on file with the Planning Department.

(b) The project site shall be developed in conformance with the approved plans on file with the City. Any variation from the approved plans must be reviewed and approved by the Planning Department prior to building permit issuance.

(c) The herein-listed conditions of approval from all City departments shall be included in the construction plan set for project, which shall be maintained on site during project construction.

2.3 Landscaping.

(a) The Project shall provide and continuously maintain landscaping and irrigation systems in compliance with the provisions of Ontario Development Code Division 6.05 (Landscaping).

(b) Comply with the conditions of approval of the Planning Department; Landscape Planning Division.

(c) Landscaping shall not be installed until the Landscape and Irrigation Construction Documentation Plans required by Ontario Development Code Division 6.05 (Landscaping) have been approved by the Landscape Planning Division.

(d) Changes to approved Landscape and Irrigation Construction Documentation Plans, which affect the character or quantity of the plant material or irrigation system design, shall be resubmitted for approval of the revision by the Landscape Planning Division, prior to the commencement of the changes.

2.4 Walls and Fences. All Project walls and fences shall comply with the requirements of Ontario Development Code Division 6.02 (Walls, Fences and Obstructions).

2.5 Block walls for the project shall be constructed of split face block and or slump stone. Walls shall also feature a decorative cap that projects 1 to 2 inches.

2.6 Parking, Circulation and Access.

(a) The Project shall comply with the applicable off-street parking, loading and lighting requirements of City of Ontario Development Code Division 6.03 (Off-Street Parking and Loading).

(b) All drive approaches shall be provided with an enhanced pavement treatment. The enhanced paving shall extend from the back of the approach apron, into the site, to the first intersecting drive aisle or parking space.

(c) Areas provided to meet the City's parking requirements, including off-street parking and loading spaces, access drives, and maneuvering areas, shall not be used for the outdoor storage of materials and equipment, nor shall it be used for any other purpose than parking.

(d) The required number of off-street parking spaces and/or loading spaces shall be provided at the time of site and/or building occupancy. All parking and loading spaces shall be maintained in good condition for the duration of the building or use.

(e) Parking spaces specifically designated and conveniently located for use by the physically disabled shall be provided pursuant to current accessibility regulations contained in State law (CCR Title 24, Part 2, Chapters 2B71, and CVC Section 22507.8).

(f) Bicycle parking facilities, including bicycle racks, lockers, and other secure facilities, shall be provided in conjunction with development projects pursuant to current regulations contained in CALGreen (CAC Title 24, Part 11).

2.7 Site Lighting.

(a) All off-street parking facilities shall be provided with nighttime security lighting pursuant to Ontario Municipal Code Section 4-11.08 (Special Residential Building Provisions) and Section 4-11.09 (Special Commercial/Industrial Building Provisions), designed to confine emitted light to the parking areas. Parking facilities shall be lighted from sunset until sunrise, daily, and shall be operated by a photocell switch.

(b) Unless intended as part of a master lighting program, no operation, activity, or lighting fixture shall create illumination on any adjacent property.

(c) Applicant shall work with staff during the plan check process to add decorative up-lighting to key locations of the project to enhance it during the evening hours.

2.8 Mechanical and Rooftop Equipment.

(a) All exterior roof-mounted mechanical, heating and air conditioning equipment, and all appurtenances thereto, shall be completely screened from public view by parapet walls or roof screens that are architecturally treated so as to be consistent with the building architecture.

(b) All ground-mounted utility equipment and structures, such as tanks, transformers, HVAC equipment, and backflow prevention devices, shall be located out of view from a public street, or adequately screened through the use of landscaping and/or decorative low garden walls.

2.9 Security Standards. The Project shall comply with all applicable requirements of Ontario Municipal Code Title 4 (Public Safety), Chapter 11 (Security Standards for Buildings).

2.10 Signs. All Project signage shall comply with the requirements of Ontario Development Code Division 8.1 (Sign Regulations).

2.11 Sound Attenuation. The Project shall be constructed and operated in a manner so as not to exceed the maximum interior and exterior noised levels set forth in Ontario Municipal Code Title 5 (Public Welfare, Morals, and Conduct), Chapter 29 (Noise).

2.12 Covenants, Conditions and Restrictions (CC&Rs)/Mutual Access and Maintenance Agreements.

(a) CC&Rs shall be prepared for the Project and shall be recorded prior to the issuance of a building permit.

(b) The CC&Rs shall be in a form and contain provisions satisfactory to the City. The articles of incorporation for the property owner's association and the CC&Rs shall be reviewed and approved by the City.

(c) CC&Rs shall ensure reciprocal parking and access between parcels.

(d) CC&Rs shall ensure reciprocal parking and access between parcels, and common maintenance of:

(i) Landscaping and irrigation systems within common areas;
(ii) Landscaping and irrigation systems within parkways adjacent to the project site, including that portion of any public highway right-of-way between the property line or right-of-way boundary line and the curb line and also the area enclosed within the curb lines of a median divider (Ontario Municipal Code Section 7-3.03), pursuant to Ontario Municipal Code Section 5-22-02;

(iii) Shared parking facilities and access drives; and

(iv) Utility and drainage easements.

(e) CC&Rs shall include authorization for the City's local law enforcement officers to enforce City and State traffic and penal codes within the project area.

(f) The CC&Rs shall grant the City of Ontario the right of enforcement of the CC&R provisions.

(g) A specific methodology/procedure shall be established within the CC&Rs for enforcement of its provisions by the City of Ontario, if adequate maintenance of the development does not occur, such as, but not limited to, provisions that would grant the City the right of access to correct maintenance issues and assess the property owners association for all costs incurred.

2.13 Disclosure Statements.

(a) A copy of the Public Report from the Department of Real Estate, prepared for the subdivision pursuant to Business and Professions Code Section 11000 et seq., shall be provided to each prospective buyer of the residential units and shall include a statement to the effect that:

(i) This tract is subject to noise from the Ontario International Airport and may be more severely impacted in the future.

(ii) Some of the property adjacent to this tract is zoned for agricultural uses and there could be fly, odor, or related problems due to the proximity of animals.

(iii) The area south of Riverside Drive lies within the San Bernardino County Agricultural Preserve. Dairies currently existing in that area are likely to remain for the foreseeable future.

(iv) This tract is part of a Landscape Maintenance District. The homeowner(s) will be assessed through their property taxes for the continuing maintenance of the district.

2.14 Environmental Review.

(a) The environmental impacts of this project were previously reviewed in conjunction with File No. PSPA16-003, for which a Mitigated Negative Declaration was adopted by the City Council on May 16, 2017. This application introduces no new significant environmental impacts, and all previously adopted mitigation measures are a condition of project approval

(b) If human remains are found during project grading/excavation/construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and Native American consultation has been completed (if deemed applicable).

(c) If any archeological or paleontological resources are found during project grading/excavation/construction, the area shall not be disturbed until the significance of the resource is determined. If determined to be significant, the resource shall be recovered by a qualified archeologist or paleontologist consistent with current standards and guidelines, or other appropriate measures implemented.

2.15 Indemnification. The applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void or annul any approval of the City of Ontario, whether by its City Council, Planning Commission or other authorized board or officer. The City of Ontario shall promptly notify the applicant of any such claim, action or proceeding, and the City of Ontario shall cooperate fully in the defense.

2.16 Additional Fees.

(a) Within 5 days following final application approval, the Notice of Determination (NOD) filing fee shall be provided to the Planning Department. The fee shall be paid by check, made payable to the "Clerk of the Board of Supervisors", which shall be forwarded to the San Bernardino County Clerk of the Board of Supervisors, along with all applicable environmental forms/notices, pursuant to the requirements of the California Environmental Quality Act (CEQA). Failure to provide said fee within the time specified may result in a 180-day extension to the statute of limitations for the filing of a CEQA lawsuit.

(b) After the Project's entitlement approval, and prior to issuance of final building permits, the Planning Department's Plan Check and Inspection fees shall be paid at the rate established by resolution of the City Council.

2.17 Additional Requirements.

(a) All parking spaces designated for guest parking inside of the development shall feature decorative paving, to match the rest of the project. Applicant shall work with staff during the plan check process to accomplish this.

(b) All interior perimeter block walls for the development shall feature attached vine pockets with guiding wire attached to the walls, so that vines grow on to the walls.

(c) All guest parking spaces shall be clearly marked as guest parking spaces for the development. Management and CC&R's shall clearly enforce that guest parking spaces shall not be used as permanent long-term parking by residents.

(d) All pedestrian gates shall feature a decorative pilaster on each side of the gate(s). Applicant shall work with staff during the plan check process to accomplish this.

(e) All metal/wrought iron fencing used on the project shall be powder coated to prevent rust.

(f) Applicant/developer shall work with staff prior to receiving a final for occupancy that all mechanical equipment (A/C Units, Double Detectors, etc..) have been carefully and adequately screened with landscaping and or other as determined by City.

(g) Applicant shall submit a Parking Enforcement Plan for review and approval by the Planning Department and once approved the plan shall become part of the CC&R's for the subject project.

(h) A building permit will not be issued for this project unless the Specific Plan Amendment (File No. PSPA19-009) to allow tandem parking has been approved and adopted by the City Council.



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner

FROM: Emily Hernandez, Police Department

DATE: October 15, 2019

SUBJECT: PDEV19-054 – A DEVELOPMENT PLAN TO CONSTRUCT 72 MULTI-FAMILY DWELLINGS ON THE SOUTHWEST CORNER OF VIA ALBA AND VIAL VILLAGIO OF THE PIEMONTE OVERLAY AND ONTARIO SPECIFIC PLAN.

The “Standard Conditions of Approval” contained in Resolution No. 2017-027 apply. The applicant shall read and be thoroughly familiar with these conditions, including, but not limited to, the requirements below.

- Required lighting for walkways, driveways, doorways, parking lots, hallways, stairwells, and other areas used by the public shall be provided. Lights shall operate via photosensor. Photometrics shall be provided to the Police Department and include the types of fixtures proposed and demonstrate that such fixtures meet the vandal-resistant requirement. Planned landscaping shall not obstruct lighting.
- Rooftop addresses shall be installed on the buildings as stated in the Standard Conditions. The numbers shall be at a minimum 3 feet tall and 1 foot wide, in reflective white paint on a flat black background, and oriented with the bottom of the numbers towards the addressed street.
- The Applicant shall comply with construction site security requirements as stated in the Standard Conditions.
- First floor stairwells shall be constructed so as to either allow for visibility through the stairwell risers or to prohibit public access to the areas behind stairwells.
- The development shall participate in the Crime-Free Multi Housing program offered by the Ontario Police Department COPS Division.

The Applicant is invited to contact Emily Hernandez at (909) 408-1755 with any questions or concerns regarding these conditions.



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: December 18, 2019

SUBJECT: PDEV19-054 – A Development Plan to construct a 72-unit residential condominium complex on 2.99 acres of land located at the southwest corner of Via Alba and Via Villagio, within the Residential land use district of the Piemonte Overlay of the Ontario Center Specific Plan (APN: 0210-204-40).

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply. See previous report dated 19-11-18.
-



CITY OF ONTARIO

MEMORANDUM

TO: Luis Batres, Senior Planner
Planning Department

FROM: Paul Ehrman, Deputy Fire Chief/Fire Marshal
Fire Department

DATE: November 18, 2019

SUBJECT: PDEV19-054 – A Development Plan to construct 72 residential condominium units on 2.99 acres of land located at the southwest corner of Via Alba and Vial Villagio, within the Residential land use district of the Piemonte Overlay of the Ontario Center Specific Plan (APN: 0210-204-40).

-
- The plan **does** adequately address Fire Department requirements at this time.
- Standard Conditions of Approval apply, as stated below.

SITE AND BUILDING FEATURES:

- A. 2016 CBC Type of Construction: Type V-B
- B. Type of Roof Materials: Ordinary
- C. Ground Floor Area(s): Varies
- D. Number of Stories: 3
- E. Total Square Footage: Varies, 13,443 to 17,807 Sq. Ft.
- F. 2016 CBC Occupancy Classification(s): R-2

CONDITIONS OF APPROVAL:

1.0 GENERAL

- ☒ 1.1 The following are the Ontario Fire Department ("Fire Department") requirements for this development project, based on the current edition of the California Fire Code (CFC), and the current versions of the Fire Prevention Standards ("Standards.") It is recommended that the applicant or developer transmit a copy of these requirements to the on-site contractor(s) and that all questions or concerns be directed to the Bureau of Fire Prevention, at (909) 395-2029. For copies of Ontario Fire Department Standards please access the City of Ontario web site at www.ontarioca.gov, click on "Fire Department" and then on "Standards and Forms."
- ☒ 1.2 These Fire Department conditions of approval are to be included on any and all construction drawings.

2.0 FIRE DEPARTMENT ACCESS

- ☒ 2.1 Fire Department vehicle access roadways shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved. Roadways shall be paved with an all-weather surface and shall be a minimum of twenty-four (24) ft. wide. See Standard #B-004.
- ☒ 2.2 In order to allow for adequate turning radius for emergency fire apparatus, all turns shall be designed to meet the minimum twenty five feet (25') inside and forty-five feet (45') outside turning radius per Standard #B-005.
- ☒ 2.3 Fire Department access roadways that exceed one hundred and fifty feet (150') in length shall have an approved turn-around per Standard #B-002.
- ☒ 2.4 Access drive aisles which cross property lines shall be provided with CC&Rs, access easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of building plan check.
- ☒ 2.5 "No Parking-Fire Lane" signs and /or red painted curbs with lettering are required to be installed in interior access roadways, in locations where vehicle parking would obstruct the minimum clear width requirement. Installation shall be per Standard #B-001.
- ☒ 2.6 Security gates or other barriers on fire access roadways shall be provided with a Knox brand key switch or padlock to allow Fire Department access. See Standards #B-003, B-004 and H-001.
- ☒ 2.7 Any time PRIOR to on-site combustible construction and/or storage, a minimum twenty-four (24) ft. wide circulating all weather access roads shall be provided to within 150 ft. of all portions of the exterior walls of the first story of any building, unless specifically approved by fire department and other emergency services.

3.0 WATER SUPPLY

- 3.1 The required fire flow per Fire Department standards, based on the 2016 California Fire Code, Appendix B, is 1750 gallons per minute (g.p.m.) for 2 hours at a minimum of 20 pounds per square inch (p.s.i.) residual operating pressure.
- 3.2 Off-site (public) fire hydrants are required to be installed on all frontage streets, at a minimum spacing of three hundred foot (300') apart, per Engineering Department specifications.
- 3.4 The water supply, including water mains and fire hydrants, shall be tested and approved by the Engineering Department and Fire Department prior to combustible construction to assure availability and reliability for firefighting purposes.

4.0 FIRE PROTECTION SYSTEMS

- 4.1 On-site private fire hydrants are required per Standard #D-005, and identified in accordance with Standard #D-002. Installation and location(s) are subject to the approval of the Fire Department. An application with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.2 Underground fire mains which cross property lines shall be provided with CC & R, easements, or reciprocating agreements, and shall be recorded on the titles of affected properties, and copies of same shall be provided at the time of fire department plan check. The shared use of private fire mains or fire pumps is allowable only between immediately adjacent properties and shall not cross any public street.
- 4.3 An automatic fire sprinkler system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard. All new fire sprinkler systems, except those in single family dwellings, which contain twenty (20) sprinkler heads or more shall be monitored by an approved listed supervising station. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.
- 4.4 Wood frame buildings that are to be sprinkled shall have these systems in service (but not necessarily finalized) before the building is enclosed.
- 4.5 Fire Department Connections (FDC) shall be located on the address side of the building within one hundred fifty feet (150') of a public fire hydrant on the same side of the street. Provide identification for all fire sprinkler control valves and fire department connections per Standard #D-007. Raised curbs adjacent to Fire Department connection(s) shall be painted red, five feet either side, per City standards.
- 4.6 A fire alarm system is required. The system design shall be in accordance with National Fire Protection Association (NFPA) Standard 72. An application along with detailed plans shall be submitted, and a construction permit shall be issued by the Fire Department, prior to any work being done.

- ☒ 4.7 Portable fire extinguishers are required to be installed prior to occupancy per Standard #C-001. Please contact the Fire Prevention Bureau to determine the exact number, type and placement required.

5.0 BUILDING CONSTRUCTION FEATURES

- ☒ 5.1 The developer/general contractor is to be responsible for reasonable periodic cleanup of the development during construction to avoid hazardous accumulations of combustible trash and debris both on and off the site.
- ☒ 5.2 Approved numbers or addresses shall be placed on all new and existing buildings in such a position as to be plainly visible and legible from the street or road fronting the property. Multi-tenant or building projects shall have addresses and/or suite numbers provided on the rear of the building. Address numbers shall contrast with their background. See Section 9-1 6.06 of the Ontario Municipal Code and Standards #H-003 and #H-002.
- ☒ 5.3 Single station smoke alarms and carbon monoxide alarms are required to be installed per the California Building Code and the California Fire Code.
- ☒ 5.4 Multiple unit building complexes shall have building directories provided at the main entrances. The directories shall be designed to the requirements of the Fire Department, see Section 9-1 6.06 of the Ontario Municipal Code and Standard #H-003.
- ☒ 5.5 All residential chimneys shall be equipped with an approved spark arrester meeting the requirements of the California Building Code.
- ☒ 5.6 Knox ® brand key-box(es) shall be installed in location(s) acceptable to the Fire Department. All Knox boxes shall be monitored for tamper by the building fire alarm system. See Standard #H-001 for specific requirements.

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT



Project File No.: PMTT19-016 & PDEV19-054
 Address: SWC Via Villagio & Via Alba
 APN: 210-204-40
 Existing Land Use: Vacant
 Proposed Land Use: A Tentative Tract Map for condominium purposes and Development Plan to construct 72 multi-family residential units
 Site Acreage: 2.9 acres Proposed Structure Height: 39 FT
 ONT-IAC Project Review: n/a
 Airport Influence Area: ONT

Reviewed By: Lorena Mejia
 Contact Info: 909-395-2276
 Project Planner: Luis Batres
 Date: 12/17/19
 CD No.: 2019-071
 PALU No.: n/a

The project is impacted by the following ONT ALUCP Compatibility Zones:

- | Safety | Noise Impact | Airspace Protection | Overflight Notification |
|-------------------------------|---------------------------------------|---|--|
| <input type="radio"/> Zone 1 | <input type="radio"/> 75+ dB CNEL | <input checked="" type="checkbox"/> High Terrain Zone | <input type="radio"/> Avigation Easement Dedication |
| <input type="radio"/> Zone 1A | <input type="radio"/> 70 - 75 dB CNEL | <input checked="" type="checkbox"/> FAA Notification Surfaces | <input type="radio"/> Recorded Overflight Notification |
| <input type="radio"/> Zone 2 | <input type="radio"/> 65 - 70 dB CNEL | <input checked="" type="checkbox"/> Airspace Obstruction Surfaces | <input checked="" type="checkbox"/> Real Estate Transaction Disclosure |
| <input type="radio"/> Zone 3 | <input type="radio"/> 60 - 65 dB CNEL | <input type="radio"/> Airspace Avigation Easement Area | |
| <input type="radio"/> Zone 4 | | Allowable Height: 70 ft | |
| <input type="radio"/> Zone 5 | | | |

The project is impacted by the following Chino ALUCP Safety Zones:

- Zone 1 Zone 2 Zone 3 Zone 4 Zone 5 Zone 6

Allowable Height: _____

CONSISTENCY DETERMINATION

This proposed Project is: Exempt from the ALUCP Consistent Consistent with Conditions Inconsistent

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan (ALUCP) for ONT.

Real Estate Transaction Disclosure Required.

Airport Planner Signature: _____

AIRPORT LAND USE COMPATIBILITY PLANNING

CONSISTENCY DETERMINATION REPORT

CD No.: 2019-071
PALU No.: _____

PROJECT CONDITIONS

The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the Airport Land Use Compatibility Plan (ALUCP) for ONT. The applicant is required to meet the Real Estate Transaction Disclosure in accordance with California Codes (Business and Professions Code Section 11010-11024). New residential subdivisions within an Airport Influence Area are required to file an application for a Public Report consisting of a Notice of Intention (NOI) and a completed questionnaire with the Department of Real Estate and include the following language within the NOI:

NOTICE OF AIRPORT IN VICINITY

This property is presently located in the vicinity of an airport, within what is known as an airport influence area. For that reason, the property may be subject to some of the annoyances or inconveniences associated with proximity to airport operations (for example: noise, vibration, or odors). Individual sensitivities to those annoyances can vary from person to person. You may wish to consider what airport annoyances, if any, are associated with the property before you complete your purchase and determine whether they are acceptable to you.

CITY OF ONTARIO MEMORANDUM

TO: PLANNING DEPARTMENT, Luis Batres
FROM: BUILDING DEPARTMENT, Kevin Shear
DATE: October 17, 2019
SUBJECT: PDEV19-054

-
- The plan **does** adequately address the departmental concerns at this time.
- No comments
- Report below.

Conditions of Approval


1. Standard Conditions of Approval apply.

KS:lr

CITY OF ONTARIO
LANDSCAPE PLANNING DIVISION
 303 East "B" Street, Ontario, CA 91764

DAB CONDITIONS OF APPROVAL

Sign Off


 Jamie Richardson, Sr. Landscape Planner

1/15/20
 Date

Reviewer's Name:
Jamie Richardson, Sr. Landscape Planner

Phone:
(909) 395-2615

D.A.B. File No.:
 PDEV19-054

Case Planner:
 Luis Batres

Project Name and Location:
 Nuvo Piemonte
 SW Corner of Via Alba and Via Villagio

Applicant/Representative:
 The New Home Company of Southern California, LLC
 85 Enterprise, Ste. 450
 Aliso Viejo, CA 92656

A Preliminary Landscape Plan (dated 12/11/19) meets the Standard Conditions for New Development and has been approved with the consideration that the following conditions below be met upon submittal of the landscape construction documents.

A Preliminary Landscape Plan (dated) has not been approved. Corrections noted below are required prior to Preliminary Landscape Plan approval.

A RESPONSE SHEET IS REQUIRED WITH RESUBMITTAL OR PLANS WILL BE RETURNED AS INCOMPLETE.
 Landscape construction plans with plan check number may be emailed to: landscapeplancheck@ontarioca.gov
DIGITAL SUBMITTALS MUST BE 10MB OR LESS.

Civil/ Site Plans

- Storm water infiltration devices located in landscape areas shall be reviewed and plans approved by the Landscape Planning Division prior to permit issuance. Any storm water devices in parkway areas shall not displace street trees.
- Locate utilities including light standards, fire hydrants, water, drain and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.

Landscape Plans

- Show/Note backflow devices with 36" high strappy leaf shrub screening. Do not encircle utility, show as masses and duplicate masses in other locations on regular intervals.
- Locate light standards, fire hydrants, water and sewer lines to not conflict with required tree locations. Coordinate civil plans with landscape plans.
- Show all utilities on the landscape plans. Coordinate so utilities are clear of tree locations.
- Provide larger trees where space allows along Via Alba.
- Show 6' (smaller trees) or 8' (larger trees) diameter of mulch only at new trees, Detail irrigation dripline outside of mulched root zone.
- Designer or developer to provide agronomical soil testing and include report on landscape construction plans. For phased projects, a new report is required for each phase or a minimum of every 6 homes in residential developments.
- Show minimum on-site tree sizes per the Landscape Development standards, see the Landscape Planning website. 5% 48" box, 10% 36 box, 30% 24" box, 55% 15 gallon.
- Show 25% of trees as California native (Platanus racemosa, Quercus agrifolia, Quercus wislizenii, Quercus douglasii, Cercis occidentalis etc.) in appropriate locations.

11. After a project's entitlement approval, the applicant shall pay all applicable fees for landscape plan check and inspections at a rate established by resolution of the City Council. Fees are:

Plan Check—less than 5 acres	\$1,301.00
Inspection—Construction (up to 3 inspections per phase).....	\$278.00
Total.....	\$1,579.00
Inspection—Field – any additional.....	\$83.00

Landscape construction plans with building permit number for plan check may be emailed to:
landscapeplancheck@ontarioca.gov



THIS PROJECT SHALL COMPLY WITH THE REQUIREMENTS SET FORTH IN THE GENERAL STANDARD CONDITIONS OF APPROVAL ADOPTED BY THE CITY COUNCIL (RESOLUTION NO. 2017-027) AND THE PROJECT SPECIFIC CONDITIONS OF APPROVAL SPECIFIED IN HEREIN. ONLY APPLICABLE CONDITIONS OF APPROVAL ARE CHECKED. THE APPLICANT SHALL BE RESPONSIBLE FOR THE COMPLETION OF ALL APPLICABLE CONDITIONS OF APPROVAL PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, ISSUANCE OF PERMITS AND/OR OCCUPANCY CLEARANCE, AS SPECIFIED IN THIS REPORT.

1. PRIOR TO FINAL MAP OR PARCEL MAP APPROVAL, APPLICANT SHALL: Check When Complete

- 1.01 Dedicate to the City of Ontario, the right-of-way, described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____.
- 1.02 Dedicate to the City of Ontario, the following easement(s): 1) Curb-to-curb public water easement for the public domestic water mains proposed within the private interior streets ("A" Drive [to prolongation of west curb line of "B" Drive] and "B" Drive) and curb-to-centerline of "C" Drive along the entire southerly frontage of the proposed tract in accordance with the approved tentative map ~~and the final Utilities Systems Map~~. Also, a minimum 10-foot wide easement for the master meter service lateral serving buildings "E" and "F" per said Utilities Systems Map. Additional easements to the City (Min. 5'x5') for maintenance and/or replacement of the master meters will be required. Ultimate location of the meters/easements will be determined prior to Final Map approval or the easements may be recorded by separate instrument.

 2) Curb-to-curb public sewer easement for the public sewer mains and manholes proposed within the private interior streets ("A" Drive, "B" Drive [to the south curb line of "C" and "D" Courts] and "A", "B", "C" and "D" Courts) of the proposed tract in accordance with the approved tentative map ~~and the final Utilities Systems Map~~.
- 1.03 Restrict vehicular access to the site as follows: _____
- 1.04 Vacate the following street(s) and/or easement(s): 1) The south 20-feet of Via Villagio, dedicated to the City as a 102-foot wide easement for public utility and emergency access purposes.

 2) The south 20-feet of Via Villagio, previously reserved as a 102-foot wide private easement for street, private utility and maintenance purposes.

 3) The west 12-feet of Via Alba dedicated to the City as a 56-foot wide easement for public utility and emergency access purposes.

 4) The west 12-feet of Via Alba, previously reserved as a 56-foot wide private easement for street, private utility and maintenance purposes.

 5) The north 10-feet of the southerly 43-feet of Parcel 23 of Parcel Map No. 17550 dedicated to the City as an easement for pedestrian access purposes.
- 1.05 Submit a copy of a recorded private reciprocal use agreement or easement. The agreement or easement shall ensure, at a minimum, common ingress and egress and joint maintenance of all common access areas and drive aisles.
- 1.06 Provide (original document) Covenants, Conditions and Restrictions (CC&Rs) as applicable to the project and as approved by the City Attorney and the Engineering and Planning



Departments, ready for recordation with the County of San Bernardino. The CC&Rs shall provide for, but not be limited to, common ingress and egress, joint maintenance responsibility for all common access improvements, common facilities, parking areas, utilities, median and landscaping improvements and drive approaches, in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project. The CC&Rs shall also address the maintenance and repair responsibility for public improvements/utilities (sewer, water, storm drain, recycled water, etc.) located within open spaces/easements **and shall clearly distinguish between public and private utilities within these spaces.** In the event of any maintenance or repair of these facilities, the City shall only restore disturbed areas to current City Standards.

Note: The existing CC&Rs recorded with Parcel Map No. 17550 and on file with the Planning Department may require amendment to address the maintenance and other responsibilities of the new HOA as it relates to the existing overall Piemonte development.

- 1.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.
- 1.08 File an application for Reapportionment of Assessment, together with payment of a reapportionment processing fee, for each existing assessment district listed below. Contact the Management Services Department at (909) 395-2124 regarding this requirement.
 - (1) _____
 - (2) _____
- 1.09 Prepare a fully executed Subdivision Agreement (on City approved format and forms) with accompanying security as required or complete all public improvements.
- 1.10 Provide a monument bond (i.e. cash deposit) in an amount calculated by the City's approved cost estimate spreadsheet (available for download on the City's website: www.ci.ontario.ca.us) or as specified in writing by the applicant's Registered Engineer or Licensed Land Surveyor of Record and approved by the City Engineer, whichever is greater.
- 1.11 Provide a preliminary title report current to within 30 days.
- 1.12 File an application, together with an initial deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community Facilities District Act of 1982. The application and fee shall be submitted a minimum of three (3) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact Management Services at (909) 395-2353 to initiate the CFD application process.
- 1.13 New Model Colony (NMC) Developments:
 - 1) Provide evidence of final cancellation of Williamson Act contracts associated with this tract, prior to approval of any final subdivision map. Cancellation of contracts shall have been approved by the City Council.
 - 2) Provide evidence of sufficient storm water capacity availability equivalents (Certificate of Storm



Water Treatment Equivalents).

3) Provide evidence of sufficient water availability equivalents (Certificate of Net MDD Availability).

1.14 **Other conditions: 1) Developer/applicant shall ensure that the project is developed in accordance with all requirements of the underlying Parcel Map No. 17550 and the Ontario Center Specific Plan.**

2) Developer/applicant shall ensure that the tentative tract map and development plan conforms to the final Utility Systems Map, based on the conceptual map, dated 3/4/2020. The final Utility Systems Map shall be submitted for review and approval with the precise grading plan.

2. PRIOR TO ISSUANCE OF ANY PERMITS, APPLICANT SHALL:

**A. GENERAL
 (Permits includes Grading, Building, Demolition and Encroachment)**

2.01 **Record Tract Map No. 20308 pursuant to the Subdivision Map Act and in accordance with the City of Ontario Municipal Code.**

2.02 **Submit a duplicate photo mylar of the recorded map to the City Engineer's office.**

2.03 **Note that the subject parcel is a recognized parcel in the City of Ontario Per Parcel 23 of Parcel Map No. 17550 as recorded in Book 216 of Parcel Maps, pages 7-20, inclusive in the Office of the County Recorder, C ounty of San Bernardino, California.**

2.04 Note that the subject parcel is an 'unrecognized' parcel in the City of Ontario and shall require a Certificate of Compliance to be processed unless a deed is provided confirming the existence of the parcel prior to the date of _____.

2.05 Apply for a: Certificate of Compliance with a Record of Survey; Lot Line Adjustment
 Make a Dedication of Easement.

2.06 Provide (original document) Covenants, Conditions and Restrictions (CC&R's), as applicable to the project, and as approved by the City Attorney and the Engineering and Planning Departments, ready for recordation with the County of San Bernardino. The CC&R's shall provide for, but not be limited to, common ingress and egress, joint maintenance of all common access improvements, common facilities, parking areas, utilities and drive approaches in addition to maintenance requirements established in the Water Quality Management Plan (WQMP), as applicable to the project.

2.07 For all development occurring south of the Pomona Freeway (60-Freeway) and within the specified boundary limits (per Boundary Map found at <http://tceplumecleanup.com/>), the property developer/owner is made aware of the South Archibald Trichloroethylene (TCE) Plume "Disclosure Letter". Property owner may wish to provide this Letter as part of the Real Estate Transfer Disclosure requirements under California Civil Code Section 1102 et seq. This may include notifications in the Covenants, Conditions and Restrictions (CC&Rs) or other documents related to property transfer and disclosures. Additional information on the plume is available from the Santa Ana Regional Water Quality Control Board at http://geotracker.waterboards.ca.gov/profile_report?global_id=T10000004658.

2.08 Submit a soils/geology report.

2.09 Other Agency Permit/Approval: Submit a copy of the approved permit and/or other form of approval of the project from the following agency or agencies:

- State of California Department of Transportation (Caltrans)
- San Bernardino County Road Department (SBCRD)



- San Bernardino County Flood Control District (SBCFCD)
- Federal Emergency Management Agency (FEMA)
- Cucamonga Valley Water District (CVWD) for sewer/water service
- United States Army Corps of Engineers (USACE)
- California Department of Fish & Game
- Inland Empire Utilities Agency (IEUA)
- Other: _____

- 2.10 Dedicate to the City of Ontario the right-of-way described below:
 _____ feet on _____
 Property line corner 'cut-back' required at the intersection of _____
 and _____.
- 2.11 Dedicate to the City of Ontario the following easement(s): _____

- 2.12 New Model Colony (NMC) Developments:
 - 1) Submit a copy of the permit from the San Bernardino County Health Department to the Engineering Department and the Ontario Municipal Utilities Company (OMUC) for the destruction/abandonment of the on-site water well. The well shall be destroyed/abandoned in accordance with the San Bernardino County Health Department guidelines.
 - 2) Make a formal request to the City of Ontario Engineering Department for the proposed temporary use of an existing agricultural water well for purposes other than agriculture, such as grading, dust control, etc. Upon approval, the Applicant shall enter into an agreement with the City of Ontario and pay any applicable fees as set forth by said agreement.
 - 3) Design proposed retaining walls to retain up to a maximum of three (3) feet of earth. In no case shall a wall exceed an overall height of nine (9) feet (i.e. maximum 6-foot high wall on top of a maximum 3-foot high retaining wall).
- 2.13 **Submit a security deposit to the Engineering Department to guarantee construction of the public improvements required herein valued at 100% of the approved construction cost estimate. Security deposit shall be in accordance with the City of Ontario Municipal Code. Security deposit will be eligible for release, in accordance with City procedure, upon completion and acceptance of said public improvements.**
- 2.14 **The applicant/developer shall submit all necessary survey documents prepared by a Licensed Surveyor registered in the State of California detailing all existing survey monuments in and around the project site. These documents are to be reviewed and approved by the City Survey Office.**
- 2.15 **Pay all Development Impact Fees (DIF) to the Building Department. Final fees shall be determined based on the approved site plan.**
- 2.16 Other conditions: _____

B. PUBLIC IMPROVEMENTS
 (See attached Exhibit 'A' for plan check submittal requirements.)



2.17 Design and construct full public improvements in accordance with the City of Ontario Municipal Code, current City standards and specifications, master plans and the adopted specific plan for the area, if any. These public improvements shall include, but not be limited to, the following (checked boxes):

Improvement	Interior Drives/Courts	Street 2	Street 3	Street 4
Curb and Gutter	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New; ___ ft. from C/L <input type="checkbox"/> Replace damaged <input type="checkbox"/> Remove and replace
AC Pavement	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions	<input type="checkbox"/> Replacement <input type="checkbox"/> Widen ___ additional feet along frontage, including pavm't transitions
PCC Pavement (Truck Route Only)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Drive Approach	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace <input type="checkbox"/> replace
Sidewalk	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
ADA Access Ramp	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Parkway	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)	<input type="checkbox"/> Trees <input type="checkbox"/> Landscaping (w/irrigation)
Raised Landscaped Median	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace	<input type="checkbox"/> New <input type="checkbox"/> Remove and replace
Fire Hydrant	<input checked="" type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Sewer (see Sec. 2.C)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Laterals	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral



Water (see Sec. 2.D)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Recycled Water (see Sec. 2.E)	<input checked="" type="checkbox"/> Main <input checked="" type="checkbox"/> Services	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service	<input type="checkbox"/> Main <input type="checkbox"/> Service
Traffic Signal System (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Traffic Signing and Striping (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Street Light (see Sec. 2.F)	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation	<input type="checkbox"/> New / Upgrade <input type="checkbox"/> Relocation
Bus Stop Pad or Turn-out (see Sec. 2.F)	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing	<input type="checkbox"/> New <input type="checkbox"/> Modify existing
Storm Drain (see Sec. 2.G)	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral	<input type="checkbox"/> Main <input type="checkbox"/> Lateral
Fiber Optics (see Sec. 2K)	<input checked="" type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances	<input type="checkbox"/> Conduit / Appurtenances
Overhead Utilities	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate	<input type="checkbox"/> Underground <input type="checkbox"/> Relocate
Removal of Improvements	_____	_____	_____	_____
Other Improvements	_____	_____	_____	_____

Specific notes for improvements listed in item no. 2.17, above: _____

- 2.18 Construct a 2" asphalt concrete (AC) grind and overlay on the following street(s): _____
- 2.19 Reconstruction of the full pavement structural section, per City of Ontario Standard Drawing number 1011, may be required based on the existing pavement condition and final street design. Minimum limits of reconstruction shall be along property frontage, from street centerline to curb/gutter.
- 2.20 Make arrangements with the Cucamonga Valley Water District (CVWD) to provide water service sewer service to the site. This property is within the area served by the CVWD and Applicant shall provide documentation to the City verifying that all required CVWD fees have been paid.



- 2.21 Overhead utilities shall be under-grounded, in accordance with Title 7 of the City's Municipal Code (Ordinance No. 2804 and 2892). Developer may pay in-lieu fee, approximately _____, for undergrounding of utilities in accordance with Section 7-7.303.e of the City's Municipal Code.
- 2.22 Other conditions: _____

C. SEWER

- 2.23 **An 8-inch sewer main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Sewer plan bar codes: S13828 and S13829)**
- 2.24 Design and construct a sewer main extension. A sewer main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.25 Submit documentation that shows expected peak loading values for modeling the impact of the subject project to the existing sewer system. The project site is within a deficient public sewer system area. Applicant shall be responsible for all costs associated with the preparation of the model. Based on the results of the analysis, Applicant may be required to mitigate the project impact to the deficient public sewer system, including, but not limited to, upgrading of existing sewer main(s), construction of new sewer main(s) or diversion of sewer discharge to another sewer.
- 2.26 **Other conditions: 1) Sewer mains within this development shall be designed to meet the requirements of Section 4-8 of the City's Master Plan of Sewer.**
2) The on-site sewer mains and manholes within this development shall be public, within a public utility easement and as such, shall be publicly maintained.

D. WATER

- 2.27 **A 12-inch water main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Water plan bar codes: W13050 and W13052)**
- 2.28 Design and construct a water main extension. A water main is not available for direct connection. The closest main is approximately _____ feet away.
- 2.29 **Other conditions: 1) Developer/applicant shall submit an application for a Fire Flow Test to the Ontario Fire Department.**
2) All buildings (A, B, C, D, E, F, G, H, I & J) shall be served by master meters with backflow preventers and submetering for each residential unit.
3) The potable water mains, master meters and services within "A", "B" and "C" Drives of this development shall be public, within a public utility easement and as such, shall be publicly maintained. All potable water mains shall be ductile iron or CML&C.

E. RECYCLED WATER

- 2.30 **An 8-inch recycled water main is available for connection by this project within a public utility easement in both Via Villagio and Via Alba, private streets. (Ref: Recycled Water plan bar code: P10075)**
- 2.31 **Design and construct an on-site recycled water system for this project. A recycled water main does exist in the vicinity of this project.**
- 2.32 Design and construct an on-site recycled water ready system for this project. A recycled water main does not currently exist in the vicinity of this project but is planned for the near future. If Applicant would like to connect to this recycled water main when it becomes available, the cost for the connection shall be borne solely by the Applicant.



- 2.33 **Submit two (2) hard copies and one (1) electronic copy, in PDF format, of the Engineering Report (ER), for the use of recycled water, to the OMUC for review and subsequent submittal to the California Department of Public Health (CDPH) for final approval.**

Note: The OMUC and the CDPH review and approval process will be approximately three (3) months. Contact the Ontario Municipal Utilities Company at (909) 395-2647 regarding this requirement.

- 2.34 **Other conditions: This development shall comply with City Ordinance 2689 and make use of recycled water for all approved uses, including, but not limited to landscaping irrigation. This includes separate recycled water services for each building's private landscape area.**

F. TRAFFIC / TRANSPORTATION

- 2.35 **Submit a focused traffic impact study, prepared and signed by a Traffic/Civil Engineer registered in the State of California. The study shall address, but not be limited to, the following issues as required by the City Engineer:**
 1. On-site and off-site circulation
 2. Traffic level of service (LOS) at 'build-out' and future years
 3. Impact at specific intersections as selected by the City Engineer

- 2.36 **New traffic signal installations shall be added to Southern California Edison (SCE) customer account number # 2-20-044-3877.**

- 2.37 **Other conditions: 1) Developer/applicant shall design and construct "C" Drive such that it will support two-way traffic (i.e. minimum width shall be 24-feet).**

2) Developer/applicant shall modify existing Via Alba such that the proposed improvements match the existing improvements along the street (i.e. new curb & gutter, relocation of street lights, etc.).

3) Developer/applicant shall modify the intersection of proposed "C" Drive and Via Alba to relocate the stop sign for northbound traffic to the proper location.

G. DRAINAGE / HYDROLOGY

- 2.38 **An existing 42-inch private HDPE storm drain main is available to accept flows from this project in Via Alba. This system connects to public system in Ontario Center Parkway. (Ref: Storm Drain plan bar code: N/A)**

- 2.39 **Submit a hydrology study and drainage analysis, prepared and signed by a Civil Engineer registered in the State of California. The study shall be prepared in accordance with the San Bernardino County Hydrology Manual and City of Ontario standards and guidelines. Additional drainage facilities, including, but not limited to, improvements beyond the project frontage, may be required to be designed and constructed, by Applicant, as a result of the findings of this study**

- 2.40 **An adequate drainage facility to accept additional runoff from the site does not currently exist downstream of the project. Design and construct a storm water detention facility on the project site. 100 year post-development peak flow shall be attenuated such that it does not exceed 80% of pre-development peak flows, in accordance with the approved hydrology study and improvement plans.**

- 2.41 **Submit a copy of a recorded private drainage easement or drainage acceptance agreement to the Engineering Department for the acceptance of any increase to volume and/or concentration of historical drainage flows onto adjacent property, prior to approval of the grading plan for the project.**

- 2.42 **Comply with the City of Ontario Flood Damage Prevention Ordinance (Ordinance No. 2409). The project site or a portion of the project site is within the Special Flood Hazard Area (SFHA) as indicated on the Flood Insurance Rate Map (FIRM) and is subject to flooding during a 100 year frequency storm. The site plan shall be subject to the provisions of the National Flood Insurance Program.**

- 2.43 **Other conditions: _____**



H. STORM WATER QUALITY / NATIONAL POLLUTANT DISCHARGE AND ELIMINATION SYSTEM (NPDES)

- 2.44 401 Water Quality Certification/404 Permit – Submit a copy of any applicable 401 Certification or 404 Permit for the subject project to the City project engineer. Development that will affect any body of surface water (i.e. lake, creek, open drainage channel, etc.) may require a 401 Water Quality Certification from the California Regional Water Quality Control Board, Santa Ana Region (RWQCB) and a 404 Permit from the United States Army Corps of Engineers (USACE). The groups of water bodies classified in these requirements are perennial (flow year round) and ephemeral (flow during rain conditions, only) and include, but are not limited to, direct connections into San Bernardino County Flood Control District (SBCFCD) channels.
If a 401 Certification and/or a 404 Permit are not required, a letter confirming this from Applicant's engineer shall be submitted.
Contact information: USACE (Los Angeles District) (213) 452-3414; RWQCB (951) 782-4130.
- 2.45 Submit a Water Quality Management Plan (WQMP). This plan shall be approved by the Engineering Department prior to approval of any grading plan. The WQMP shall be submitted, utilizing the current San Bernardino County Stormwater Program template, available at:
<http://www.sbcounty.gov/dpw/land/npdes.asp>.
- 2.46 **Design and construct a Connector Pipe Trash Screen or equivalent Trash Treatment Control Device, per catch basin located within or accepting flows tributary of a Priority Land Use (PLU) area that meets the Full Capture System definition and specifications and is on the Certified List of the State Water Resources Control Board. The device shall be adequately sized per catch basin and include a deflector screen with vector control access for abatement application, vertical support bars, and removable component to facilitate maintenance and cleaning.**
- 2.47 **Other conditions: 1) Activities resulting in land disturbance of one (1) or more acres, requires coverage under the Construction General Permit (CGP). The owner is the legally responsible person (LRP) of the site and shall have a Stormwater Pollution Prevention Plan (SWPPP) developed and submitted through the SMARTS website at:**
<https://smarts.waterboards.ca.gov/smarts/faces/SwSmartslogin.xhtml>

J. SPECIAL DISTRICTS

- 2.48 File an application, together with an initial payment deposit (if required), to establish a Community Facilities District (CFD) pursuant to the Mello-Roos Community facilities District Act of 1982. The application and fee shall be submitted a minimum four (4) months prior to final subdivision map approval, and the CFD shall be established prior to final subdivision map approval or issuance of building permits, whichever occurs first. The CFD shall be established upon the subject property to provide funding for various City services. An annual special tax shall be levied upon each parcel or lot in an amount to be determined. The special tax will be collected along with annual property taxes. The City shall be the sole lead agency in the formation of any CFD. Contact the Management Services Department at (909) 395-2353 to initiate the CFD application process.
- 2.49 Other conditions: _____

K. FIBER OPTIC

- 2.50 **Design and construct fiber optic system to provide access to the City's conduit and fiber optic system per the City's Fiber Optic Master Plan. Building entrance conduits shall start from the closest OntarioNet hand hole constructed along the project frontage in the ROW and shall terminate in the main telecommunications room for each building. Conduit infrastructure shall interconnect with the primary and/or secondary backbone fiber optic conduit system at the nearest OntarioNet hand hole.**
- 2.51 **Refer to the City's Fiber Optic Master Plan for design and layout guidelines. Contact the Information Technology Department at (909) 395-2000, regarding this requirement.**



L. Solid Waste

- 2.52 Onsite solid waste shall be designed in accordance with the City's Solid Waste Manual located at: <http://www.ontarioca.gov/municipal-utilities-company/solid-waste>
- 2.53 Other conditions: 1) This site shall comply with the requirements of State Assembly Bill AB 1826 and AB 341, the Integrated Waste Department and the Refuse & Recycling Planning Manual located at: <https://www.ontarioca.gov/omuc/integrated-waste>
 - 2) A final Solid Waste Handling Plan (SWHP), demonstrating compliance with the SWHP requirements shall be submitted for review and approval with the precise grading plan.
 - 3) A final Integrated Waste Management Report (IWMR), demonstrating compliance with the IWMR requirements shall be submitted for review and approval with the precise grading plan.

3. PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY, APPLICANT SHALL:

- 3.01 Set new monuments in place of any monuments that have been damaged or destroyed as a result of construction of the subject project. Monuments shall be set in accordance with City of Ontario standards and to the satisfaction of the City Engineer.
- 3.02 Complete all requirements for recycled water usage.
 - 1) Procure from the OMUC a copy of the letter of confirmation from the California Department of Public Health (CDPH) that the Engineering Report (ER) has been reviewed and the subject site is approved for the use of recycled water.
 - 2) Obtain clearance from the OMUC confirming completion of recycled water improvements and passing of shutdown tests and cross connection inspection, upon availability/usage of recycled water.
 - 3) Complete education training of on-site personnel in the use of recycled water, in accordance with the ER, upon availability/usage of recycled water.
- 3.03 The applicant/developer shall submit all final survey documents prepared by a Licensed Surveyor registered in the State of California detailing all survey monuments that have been preserved, revised, adjusted or set along with any maps, corner records or Records of Survey needed to comply with these Conditions of Approvals and the latest edition of the California Professional Land Survey Act. These documents are to be reviewed and approved by the City Survey Office.
- 3.04 NMC Projects: For developments located at an intersection of any two collector or arterial streets, the applicant/developer shall set a monument if one does not already exist at that intersection. Contact the City Survey office for information on reference benchmarks, acceptable methodology and required submittals.
- 3.05 Confirm payment of all Development Impact Fees (DIF) to the Building Department.
- 3.06 Submit electronic copies (PDF and Auto CAD format) of all approved improvement plans, studies and reports (i.e. hydrology, traffic, WQMP, etc.).

4. PRIOR TO FINAL ACCEPTANCE, APPLICANT SHALL:

- 4.01 Complete all Conditions of Approval listed under Sections 1-3 above.
- 4.02 Pay all outstanding fees pursuant to the City of Ontario Municipal Code, including but not limited to, plan check fees, inspection fees and Development Impact Fees.



- 4.03** The applicant/developer shall submit a written request for the City's final acceptance of the project addressed to the City Project Engineer. The request shall state that all Conditions of Approval have been completed and shall be signed by the applicant/developer. Upon receipt of the request, review of the request shall be a minimum of 10 business days. Conditions of Approval that are deemed incomplete by the City will cause delays in the acceptance process.



EXHIBIT 'A'

**ENGINEERING DEPARTMENT
First Plan Check Submittal Checklist**

Project Number: **PDEV19-054 and Tract Map No. 20308**

The following items are required to be included with the first plan check submittal:

1. **A copy of this check list**
2. **Payment of fee for Plan Checking**
3. **One (1) copy of Engineering Cost Estimate (on City form) with engineer's wet signature and stamp.**
4. **One (1) copy of project Conditions of Approval**
5. Two (2) sets of Potable and Recycled Water demand calculations (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size).
6. Three (3) sets of Public Street improvement plan with street cross-sections
7. Three (3) sets of Private Street improvement plan with street cross-sections
8. **Four (4) sets of Public Water improvement plan (include water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size)**
9. Four (4) sets of Recycled Water improvement plan (include recycled water demand calculations showing low, average and peak water demand in GPM for the proposed development and proposed water meter size and an exhibit showing the limits of areas being irrigated by each recycled water meter)
10. **Four (4) sets of Public Sewer improvement plan**
11. Five (5) sets of Public Storm Drain improvement plan
12. Three (3) sets of Public Street Light improvement plan
13. Three (3) sets of Signing and Striping improvement plan
14. **Three (3) sets of Fiber Optic plan (include Auto CAD electronic submittal)**
15. Three (3) sets of Dry Utility plans within public right-of-way (at a minimum the plans must show existing and ultimate right-of-way, curb and gutter, proposed utility location including centerline dimensions, wall to wall clearances between proposed utility and adjacent public line, street work repaired per Standard Drawing No. 1306. Include Auto CAD electronic submittal)
16. Three (3) sets of Traffic Signal improvement plan and One (1) copy of Traffic Signal Specifications with modified Special Provisions. Please contact the Traffic Division at (909) 395-2154 to obtain Traffic Signal Specifications.
17. Two (2) copies of Water Quality Management Plan (WQMP), including one (1) copy of the approved Preliminary WQMP (PWQMP).
18. **One (1) copy of Hydrology/Drainage study**
19. One (1) copy of Soils/Geology report
20. **Payment for Final Map/Parcel Map processing fee**
21. **Three (3) copies of Final Map/Parcel Map**



- 22. One (1) copy of approved Tentative Map
- 23. One (1) copy of Preliminary Title Report (current within 30 days)
- 24. One (1) copy of Traverse Closure Calculations
- 25. One (1) set of supporting documents and maps (legible copies): referenced improvement plans (full size), referenced record final maps/parcel maps (full size, 18"x26"), Assessor's Parcel map (full size, 11"x17"), recorded documents such as deeds, lot line adjustments, easements, etc.
- 26. Two (2) copies of Engineering Report and an electronic file (include PDF format electronic submittal) for recycled water use
- 27. Other: _____



PLANNING COMMISSION STAFF REPORT

April 28, 2020

FILE NO.: PSPA19-004

SUBJECT: An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway.

PROPERTY OWNER: Toyota Motor Sales USA, Inc.

RECOMMENDED ACTION: That the Planning Commission consider and recommend the City Council adopt an Addendum to The Ontario Plan Environmental Impact Report and approve File No. PSPA19-004 pursuant to the facts and reasons contained in the staff report and attached resolutions.

PROJECT SETTING: The Project site (depicted in Figure 1: Project Location, right) is comprised of 95.35 acres of land generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. Planning Area 1, which is the subject of the proposed land use change, is a triangular shaped, unimproved site located at the northwest corner of the Specific Plan area, consisting of approximately 14.4 acres of land that is bordered by Jurupa Street to the north, Milliken Avenue to the west, and Toyota Way (a private street) to the southeast. The remaining Planning Areas 2 and 3 currently contain an approximate 857,000 square foot warehouse and distribution building that is owned and operated by Toyota Motor Sales USA, Inc.

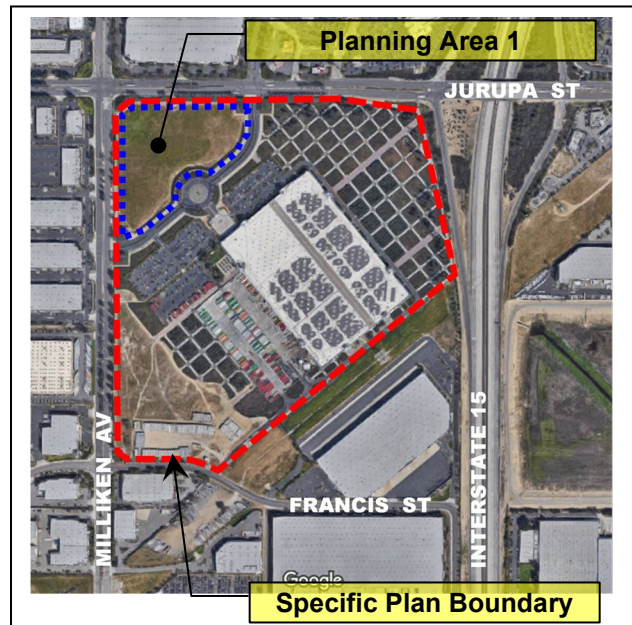


Figure 1: Project Location

Case Planner:	Charles Mercier
Planning Director Approval:	
Submittal Date:	

Hearing Body	Date	Decision	Action
DAB	N/A	N/A	N/A
PC	4/28/2020		Recommend
CC	6/2/2020		Final

The areas surrounding the Project site are characterized by industrial land uses to the north, west and south, and Interstate 15 to the west. The California Commerce Center Specific Plan Light Industrial land use district abuts the Project to the north and west. The area to the south of the Project site includes IG (General Industrial) and UC (Utility Corridor) zoned properties. The area east of the Project site, beyond Interstate 15, is within the California Commerce Center Specific Plan Light Industrial land use district and includes a mix of fast food and retail uses. The surrounding existing land uses, Policy Plan (General Plan) and zoning information are tabulated in the Technical Appendix Section of this report.

PROJECT ANALYSIS: The Applicant is requesting a change in land use affecting Planning Area 1, from Office/Research & Development (“Office/R&D”) to Industrial Mixed Use. The proposed change in land use designation will allow for a mix of Office/R&D, warehouse/distribution, and manufacturing land uses within the Planning Area, consistent with uses allowed in the surrounding California Commerce Center Specific Plan Light Industrial land use district and the IG (General Industrial) zoning district. Planning Areas 2 and 3 are unaffected by the Specific Plan Amendment and will continue to be designated Warehouse/Distribution (see Figure 2: Proposed Toyota Ontario Business Park Specific Plan Land Use Map, right, and Exhibit A—Proposed Land Use Change, attached).

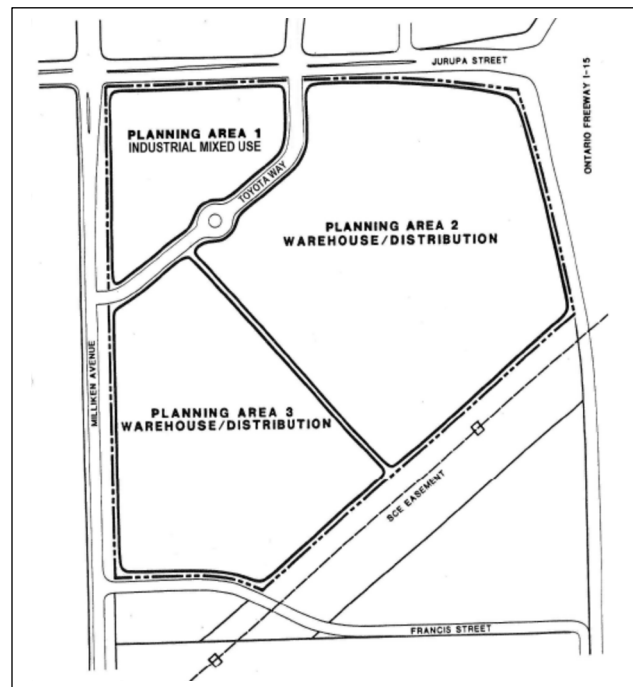


Figure 2: Proposed Toyota Ontario Business Park Specific Plan Land Use Map

In addition to the land use designation change to Planning Area 1, the Specific Plan Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices, which will apply to any future development projects throughout the Specific Plan area. Furthermore, the private street bordering Planning Area 1 on the southwest side, which the Specific Plan currently designates as Rockefeller Drive, will be corrected to reflect the street’s actual assigned name: Toyota Way. A Specific Plan Amendment is not required to make this correction; however, it will be accomplished with this Amendment.

COMPLIANCE WITH THE ONTARIO PLAN: The proposed Project is consistent with the principles, goals and policies contained within the Vision, Governance, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan (TOP). More

specifically, the goals and policies of TOP that are furthered by the proposed Project are as follows:

[1] City Council Goals.

- Invest in the Growth and Evolution of the City's Economy
- Operate in a Businesslike Manner

[2] Policy Plan (General Plan)

Land Use Element:

- Goal LU2: Compatibility between a wide range of uses.
 - LU2-6: Infrastructure Compatibility: We require infrastructure to be aesthetically pleasing and in context with the community character.

Community Economics Element:

- Goal CE2: A City of distinctive neighborhoods, districts, and corridors, where people choose to be.
 - CE2-1 Development Projects. We require new development and redevelopment to create unique, high-quality places that add value to the community.
 - CE2-2 Development Review. We require those proposing new development and redevelopment to demonstrate how their projects will create appropriately unique, functional and sustainable places that will compete well with their competition within the region.
 - CE2-4 Protection of Investment. We require that new development and redevelopment protect existing investment by providing architecture and urban design of equal or greater quality.
 - CE2-5 Private Maintenance. We require adequate maintenance, upkeep, and investment in private property because proper maintenance on private property protects property values.

Community Design Element:

- Goal CD1: A dynamic, progressive city containing distinct neighborhoods and commercial districts that foster a positive sense of identity and belonging among residents, visitors, and businesses.

➤ CD1-1 City Identity. We take actions that are consistent with the City being a leading urban center in Southern California while recognizing the diverse character of our existing viable neighborhoods.

➤ CD1-3 Neighborhood Improvement. We require viable existing residential and non-residential neighborhoods to be preserved, protected, and enhanced in accordance with our land use policies.

▪ Goal CD2: A high level of design quality resulting in public spaces, streetscapes, and developments that are attractive, safe, functional and distinct.

➤ CD2-9 Landscape Design. We encourage durable landscaping materials and designs that enhance the aesthetics of structures, create and define public and private spaces, and provide shade and environmental benefits.

➤ CD2-10 Surface Parking Areas. We require parking areas visible to or used by the public to be landscaped in an aesthetically pleasing, safe and environmentally sensitive manner. Examples include shade trees, pervious surfaces, urban run-off capture and infiltration, and pedestrian paths to guide users through the parking field.

➤ CD2-13 Entitlement Process. We work collaboratively with all stakeholders to ensure a high degree of certainty in the efficient review and timely processing of all development plans and permits.

▪ Goal CD3: Vibrant urban environments that are organized around intense buildings, pedestrian and transit areas, public plazas, and linkages between and within developments that are conveniently located, visually appealing and safe during all hours.

➤ CD3-6 Landscaping. We utilize landscaping to enhance the aesthetics, functionality and sustainability of streetscapes, outdoor spaces and buildings.

▪ Goal CD5: A sustained level of maintenance and improvement of properties, buildings and infrastructure that protects the property values and encourages additional public and private investments.

➤ CD5-1 Maintenance of Buildings and Property. We require all public and privately owned buildings and property (including trails and easements) to be properly and consistently maintained.

➤ CD5-2 Maintenance of Infrastructure. We require the continual maintenance of infrastructure.

HOUSING ELEMENT COMPLIANCE: The Project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the Project

site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

AIRPORT LAND USE COMPATIBILITY PLAN (ALUCP) COMPLIANCE: The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of Ontario approved and adopted the Ontario International Airport Land use Compatibility Plan (“ALUCP”), establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. The proposed Project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the ALUCP. Any special conditions of approval associated with uses in close proximity to the airport are included in the conditions of approval provided with the attached Resolution.

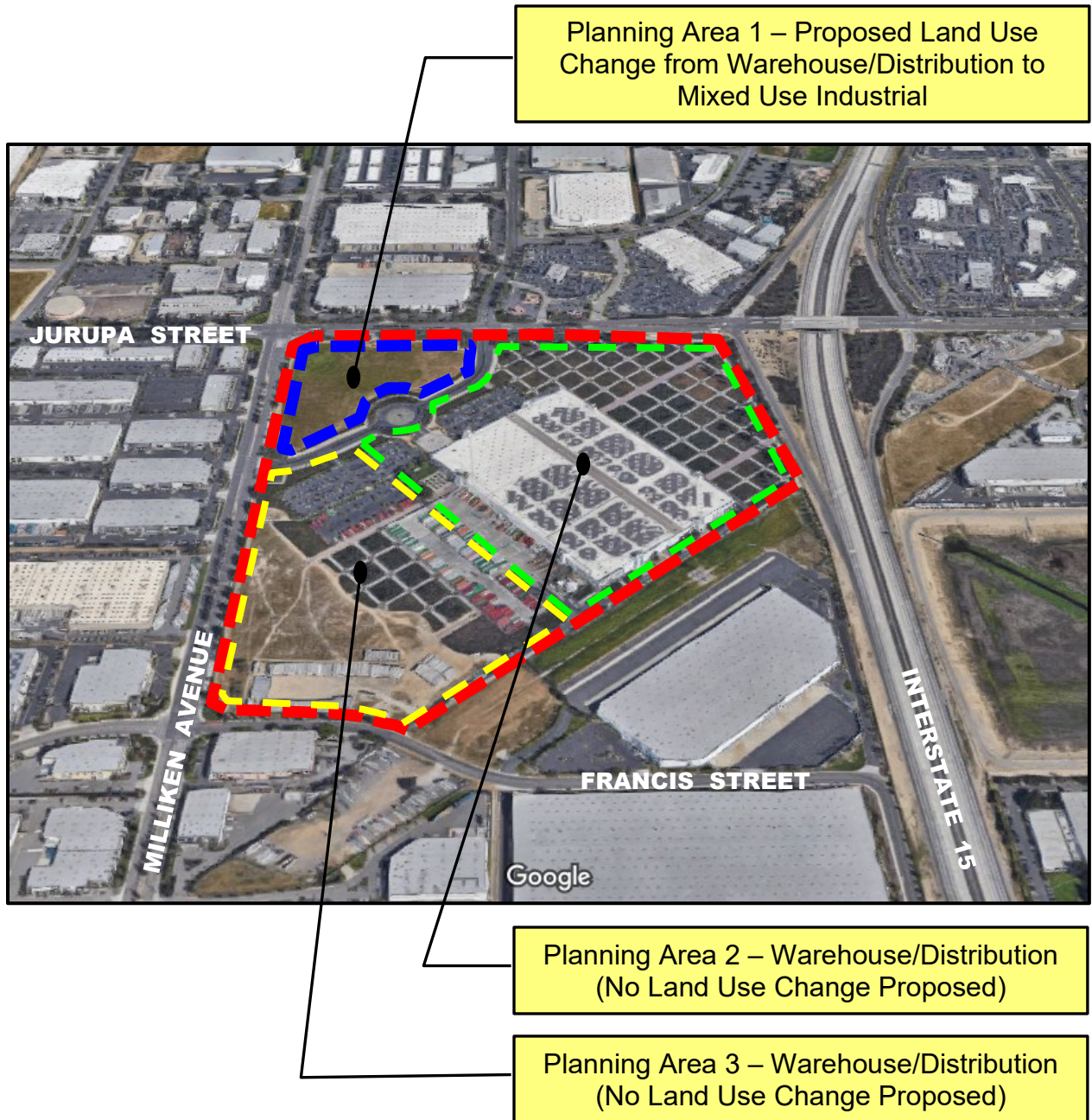
ENVIRONMENTAL REVIEW: On January 27, 2010, the City Council certified The Ontario Plan Environmental Impact Report (State Clearinghouse No. 200405115), in conjunction with File No. PGPA06-001. Staff prepared an Addendum to The Ontario Plan Environmental Impact Report (“Certified EIR”) for the proposed Project, which found that all potential adverse direct, indirect, and cumulative environmental impacts were thoroughly analyzed and discussed in the Certified EIR, including, but not limited to, potential aesthetic, air quality, greenhouse gas emissions, noise, transportation, and tribal cultural resources impacts, and all feasible mitigation has been identified and will be incorporated into the proposed Project. This Project does not contemplate any actions that would require the preparation of a subsequent or supplemental environmental document under State CEQA Guidelines Sections 15162 or 15163, as it is consistent with the development scenario identified within the Certified EIR. Furthermore, this Project introduces no new significant environmental impacts and no further environmental review is required. A copy of the Addendum has been included with the environmental action resolution provided with this report.

TECHNICAL APPENDIX:

Surrounding Zoning and Land Use:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
<i>Site</i>	Warehouse/Distribution & Vacant	Industrial	SP (Specific Plan)	Office/R&D & Warehouse/Distribution (Toyota Business Park SP)
<i>North</i>	Warehouse/Distribution & Motor Vehicle Sales	Industrial	SP	Light Industrial (California Commerce Center SP)
<i>South</i>	Manufacturing & SCE Easement	Industrial & Open Space – Non-Recreational	IG (General Industrial) & UC (Utility Corridor)	N/A
<i>East</i>	Fast Food, Retail & SCE Easement	Industrial & Open Space – Non-Recreational	IG (General Industrial) & UC (Utility Corridor)	Light Industrial (California Commerce Center SP)
<i>West</i>	Warehouse/Distribution & Offices	Industrial	SP	Light Industrial (California Commerce Center SP)

Exhibit A—PROPOSED LAND USE CHANGE



RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO, CALIFORNIA, RECOMMENDING THE CITY COUNCIL APPROVE FILE NO. PSPA19-004, AN AMENDMENT TO THE TOYOTA ONTARIO BUSINESS PARK SPECIFIC PLAN, GENERALLY LOCATED SOUTH OF JURUPA STREET, EAST OF MILLIKEN AVENUE, NORTH OF FRANCIS STREET, AND WEST OF THE I-15 FREEWAY, REVISING THE LAND USE DESIGNATION FOR PLANNING AREA 1 FROM OFFICE/RESEARCH AND DEVELOPMENT TO INDUSTRIAL MIXED USE, AND UPDATING THE LANDSCAPE PALETTE TO CONFORM TO CURRENT CALIFORNIA FRIENDLY LANDSCAPE PRACTICES, AND MAKING FINDINGS IN SUPPORT THEREOF — APN: 0238-121-75.

WHEREAS, MIG (hereinafter referred to as "Applicant") has filed an Application for the approval of a Specific Plan Amendment, File No. PSPA19-004, as described in the title of this Resolution (hereinafter referred to as "Application" or "Project"); and

WHEREAS, the Application applies to 95.35 acres of land generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. Planning Area 1, which is the subject of the proposed land use change, is a triangular shaped, unimproved site located at the northwest corner of the Specific Plan area, consisting of approximately 14.4 acres of land that is bordered by Jurupa Street to the north, Milliken Avenue to the west, and Toyota Way (a private street) to the southeast. Planning Areas 2 and 3 currently contain an approximate 857,000 square foot warehouse/distribution building that is owned and operated by Toyota Motor Sales USA, Inc.; and

WHEREAS, the Applicant is requesting a change in land use affecting Planning Area 1, from Office/Research & Development ("Office/R&D") to Industrial Mixed Use. The proposed change in land use designation will allow for a mix of Office/R&D, warehouse/distribution, and manufacturing land uses within the Planning Area, consistent with uses allowed in the surrounding Specific Plans and the IG (General Industrial) zoning district; and

WHEREAS, Planning Areas 2 and 3 are unaffected by the Specific Plan Amendment and will continue to be designated Warehouse/Distribution; and

WHEREAS, in addition to the land use designation change to Planning Area 1, the Specific Plan Amendment will update the Specific Plan's landscape palette to conform to current California friendly landscape practices, which will apply to any future development projects throughout the Specific Plan area; and

WHEREAS, the private street bordering Planning Area 1 on the southwest side, which the Specific Plan currently designates as Rockefeller Drive, will be corrected to reflect the street's actual assigned name: Toyota Way. A Specific Plan Amendment is not required to make this correction; however, it will be accomplished with this Amendment; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2020 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario prepared and approved for attachment to the Certified EIR, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the environmental impacts of this project were thoroughly analyzed in the EIR Addendum, which concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, the City's "Local Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental assessment in situations where the impacts of subsequent projects are adequately analyzed; and

WHEREAS, the Application is a project pursuant to the California Environmental Quality Act — Public Resources Code Section 21000 et seq. — (hereinafter referred to as "CEQA") and an EIR Addendum has been prepared to determine possible environmental impacts; and

WHEREAS, Ontario Development Code Table 2.02-1 (Review Matrix) grants the Planning Commission the responsibility and authority to review and make recommendation to the City Council on the subject Application; and

WHEREAS, the Project has been reviewed for consistency with the Housing Element of the Policy Plan component of The Ontario Plan, as State Housing Element law (as prescribed in Government Code Sections 65580 through 65589.8) requires that development projects must be consistent with the Housing Element, if upon consideration of all its aspects, it is found to further the purposes, principals, goals, and policies of the Housing Element; and

WHEREAS, the Project is located within the Airport Influence Area of Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and is subject to, and must be consistent with, the policies and criteria set forth in the Ontario International Airport Land Use Compatibility Plan (hereinafter referred to as "ALUCP"), which applies only to jurisdictions within San Bernardino County, and addresses the noise, safety, airspace protection, and overflight impacts of current and future airport activity; and

WHEREAS, City of Ontario Development Code Division 2.03 (Public Hearings) prescribes the manner in which public notification shall be provided and hearing procedures to be followed, and all such notifications and procedures have been completed; and

WHEREAS, as the first action on the Project, on April 28, 2020, the Planning Commission issued a Resolution recommending the City Council adopt the EIR Addendum, finding that the proposed Project introduces no new significant environmental impacts and applying all previously adopted mitigation measures to the Project, which were incorporated by reference; and

WHEREAS, on April 28, 2020, the Planning Commission of the City of Ontario conducted a hearing to consider the Project, and concluded said hearing on that date; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: *Housing Element Compliance.* Pursuant to the requirements of California Government Code Chapter 3, Article 10.6, commencing with Section 65580, as the recommending body for the Project, the Planning Commission finds that based upon the facts and information contained in the Application and supporting documentation, the project is consistent with the Housing Element of the Policy Plan (General Plan) component of The Ontario Plan, as the project site is not one of the properties in the Available Land Inventory contained in Table A-3 (Available Land by Planning Area) of the Housing Element Technical Report Appendix.

SECTION 2: *Ontario International Airport Land Use Compatibility Plan ("ALUCP") Compliance.* The California State Aeronautics Act (Public Utilities Code Section 21670 et seq.) requires that an Airport Land Use Compatibility Plan be prepared for all public use airports in the State; and requires that local land use plans and individual development proposals must be consistent with the policies set forth in the adopted Airport Land Use Compatibility Plan. On April 19, 2011, the City Council of the City of

Ontario approved and adopted the ALUCP, establishing the Airport Influence Area for Ontario International Airport, which encompasses lands within parts of San Bernardino, Riverside, and Los Angeles Counties, and limits future land uses and development within the Airport Influence Area, as they relate to noise, safety, airspace protection, and overflight impacts of current and future airport activity. As the recommending authority for the Project, the Planning Commission has reviewed and considered the facts and information contained in the Application and supporting documentation against the ALUCP compatibility factors, including [1] Safety Criteria (ALUCP Table 2-2) and Safety Zones (ALUCP Map 2-2), [2] Noise Criteria (ALUCP Table 2-3) and Noise Impact Zones (ALUCP Map 2-3), [3] Airspace protection Zones (ALUCP Map 2-4), and [4] Overflight Notification Zones (ALUCP Map 2-5). As a result, the PLANNING COMMISSION, therefore, finds and determines that the Project, when implemented in conjunction with the conditions of approval, will be consistent with the policies and criteria set forth within the ALUCP.

SECTION 3: *Concluding Facts and Reasons.* Based upon the substantial evidence presented to the Planning Commission during the above-referenced hearing, and upon the specific findings set forth in Sections 1 and 2, above, the Planning Commission hereby concludes as follows:

(1) ***The proposed Specific Plan Amendment is consistent with the goals, policies, plans and exhibits of the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan.*** The proposed Specific Plan Amendment is consistent with the Vision, Policy Plan (General Plan), and City Council Priorities components of The Ontario Plan, in that the Specific Plan Amendment contributes toward the legislative framework for the implementation of The Ontario Plan's allowed land uses, guiding growth and development within the Toyota Ontario Business Park Specific Plan, and achieving optimum results from the City's physical, economic, environmental, and human resources.

(2) ***The proposed Specific Plan Amendment would not be detrimental to the public interest, health, safety, convenience, or general welfare of the City.*** The proposed land uses and corresponding standards and guidelines established by the Specific Plan have been created with the intent to safeguard and further the public interest, health, safety, convenience, and general welfare, and to ensure that the purposes of The Ontario Plan are maintained.

(3) ***In the case of an application affecting specific property, the proposed will not adversely affect the harmonious relationship with adjacent properties and land uses.*** The locations of the proposed Specific Plan Amendment, and the conditions under which it will be implemented and maintained, is consistent with the Policy Plan component of The Ontario Plan and the City's Development Plan, and, therefore, will not adversely affect the harmonious relationship with adjacent properties and land uses.

(4) ***In the case of an application affecting specific property, the subject site is physically suitable, including, but not limited to, parcel size, shape, access, and availability of utilities, for the request and anticipated development.*** The proposed Specific Plan Amendment has been thoroughly evaluated by City agencies and departments, which have established that the affected property is physically suitable for the proposed land use changes in terms of parcel size, shape, access, and availability of utilities.

SECTION 4: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 through 3, above, the Planning Commission hereby RECOMMENDS THE CITY COUNCIL APPROVE the herein described Specific Plan Amendment. A copy of the Toyota Ontario Business Park Specific Plan containing proposed additions and deletions is attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 5: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 6: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 7: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. ____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:

**File No. PSPA19-004 —
Toyota Ontario Business Park Specific Plan
Showing Proposed Additions and Deletions**

(Departmental conditions of approval to follow this page)

Red-Lined Copy

**Toyota Ontario Business Park
Specific Plan**

(City File No. 4621-SP)



Prepared for:
City of Ontario

Prepared by
Toyota Motor Sales, Inc.

Draft SPA February 2020

DRAFT
**Toyota/Ontario Business Park
Specific Plan**

(City File No. 4621-SP)

Prepared for:
City of Ontario

Original Prepared By:
Toyota Motor Sales Inc.

August 1993

**SPA Prepared By:
MIG**

**For:
Toyota Motor Sales, Inc.**

(Draft) February 2020

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Specific Plan Amendment PSPA 19-004

Applicant:

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Jim Bowman
Rudy Favila
Gary Ovitt

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Kate Cerda-Nunez
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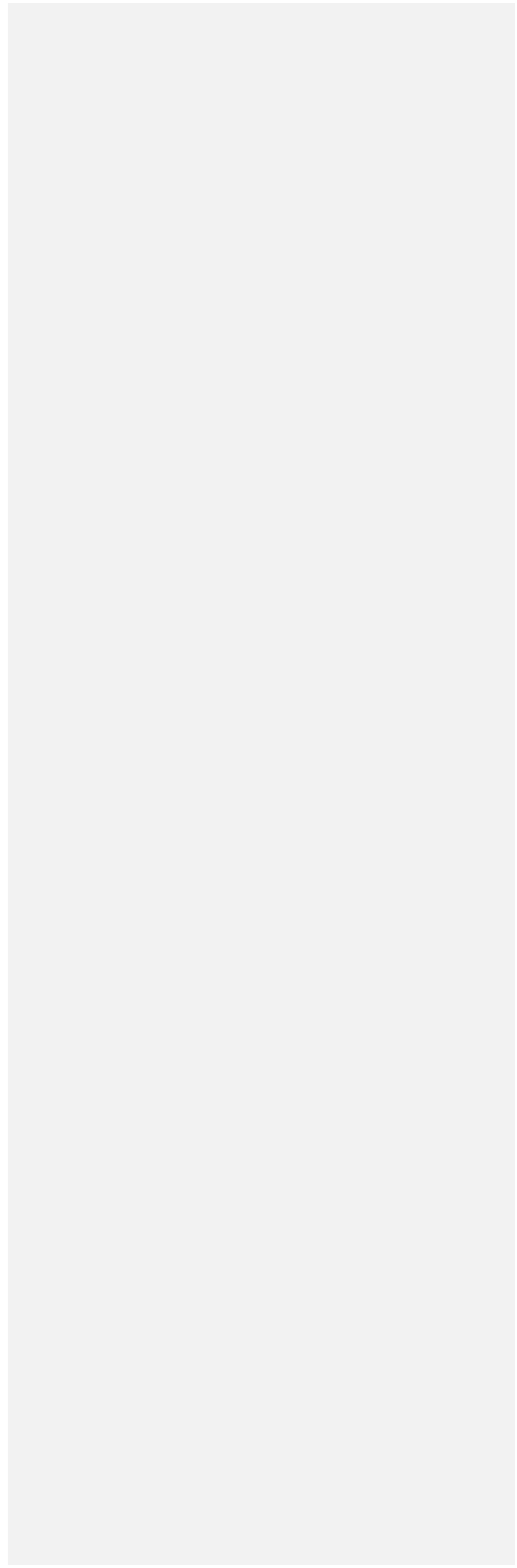
ONTARIO CITY MANAGER

G. Michael Milheiser

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1.0 Introduction

1.1 Purpose of the Specific Plan

This Specific Plan document assures the systematic implementation of the goals and policies contained in the Ontario General Plan. In addition, it provides a framework outlining the logical and comprehensive development of the subject property governed by the Specific Plan. Land use designations, development standards and design guidelines contained within this document shall govern all territory known as the Toyota/Ontario Business Park (T/OPB) project and all previous specific plans that may have governed the subject property are superseded.

The Specific Plan contains development standards for various classifications of land use within the project site expressed in both text and graphic formats. In addition to land use, major components of the plan include transportation and circulation, streetscape and landscape guidelines, infrastructure, and public services.

1.2 Project Location

The T /OBP project includes approximately 94 acres of land located south of Jurupa Street, east of Milliken Avenue, west of the Ontario (1-15) Freeway and north of Francis Street and a Southern California Edison (SCE) easement in Ontario, California.

Exhibit 1 shows the regional location of the project in relation to surrounding communities and **Exhibit 2** depicts the local context of the project site.

The project is centrally located within Southern California, approximately 40 miles east of downtown Los Angeles, 20 miles east of downtown San Bernardino and 30 miles northeast of Orange County.

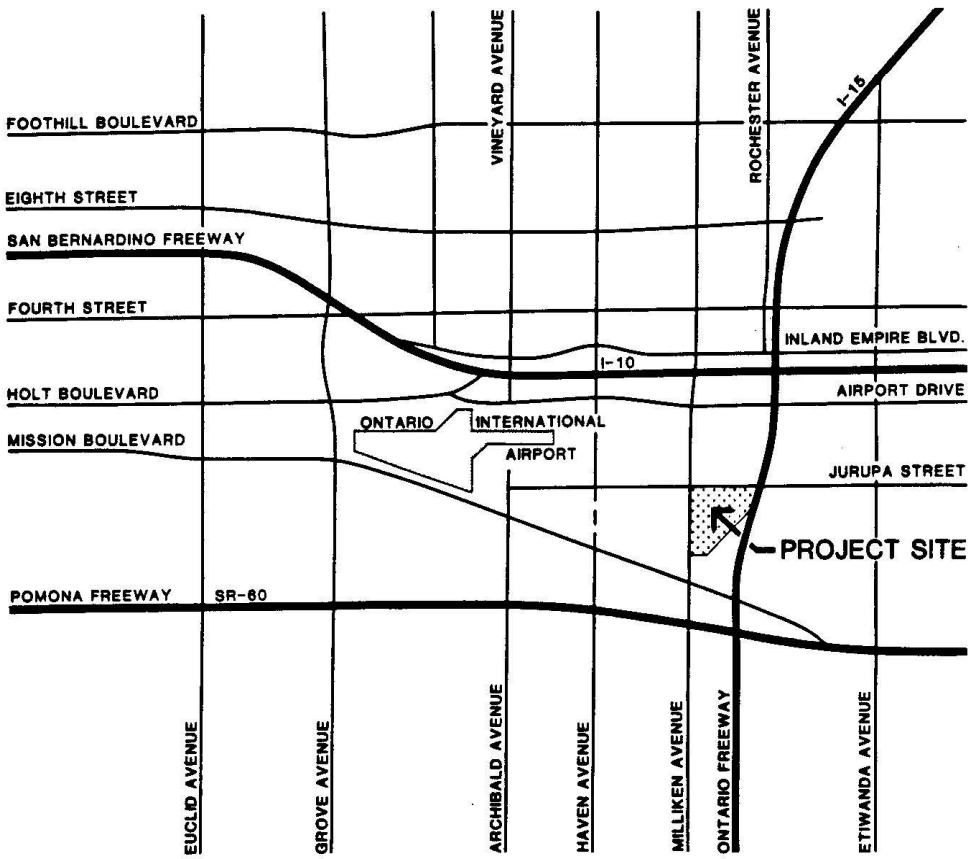
1.3 Project Site Conditions

The project site consists of approximately 94 acres of land within eastern Ontario. The site is generally rectangular in shape with a triangular cutoff to the southeast to accommodate an existing SCE easement for high-voltage transmission lines. Topographically, the site has a distinct slope to the southeast, with a total "fall" of approximately thirty to thirty-five feet over the run of the site.

Existing site uses include a City of Ontario water well facility near the intersection of Jurupa Street and Milliken Avenue and a small number of mature trees (fewer than a dozen) just north of Francis Street.

A legal description of the site is found in the Appendix of this Specific Plan.

Toyota Ontario Business Park

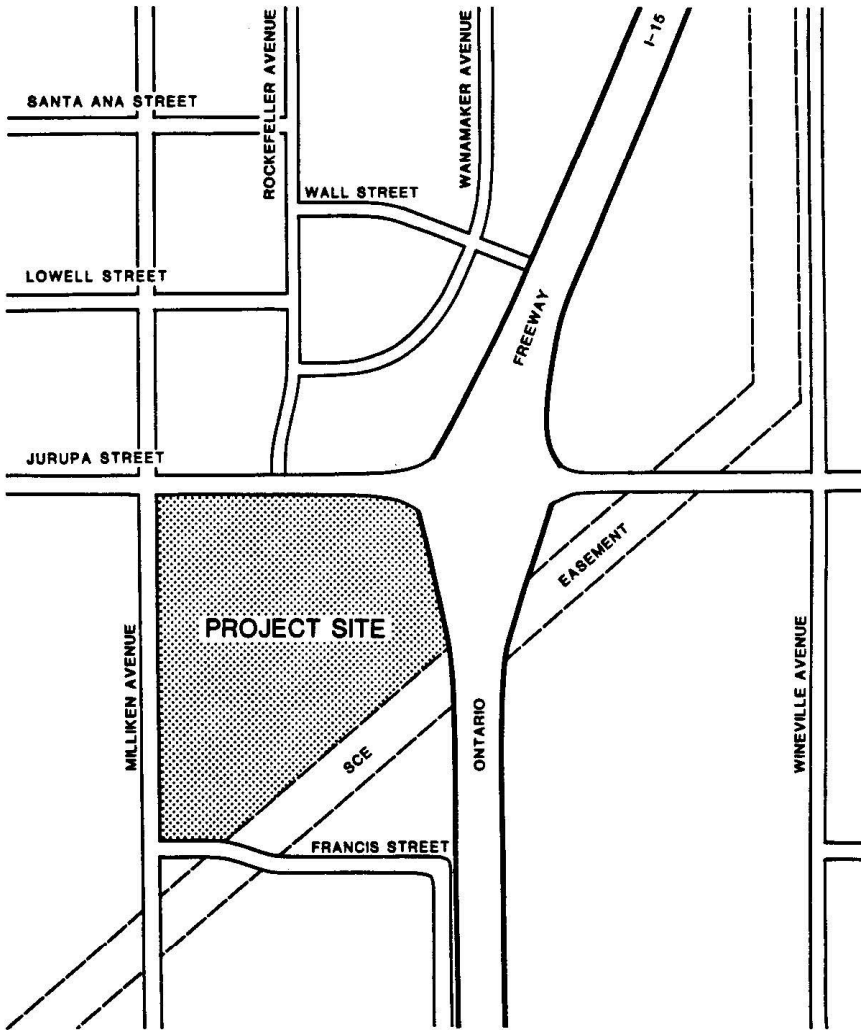


No Scale 
2

Exhibit 1
Regional Location



Toyota Ontario Business Park



No Scale 

Exhibit 2
Vicinity Map



1.4 Authority for Specific Plans

Ordinance No. 2124, adopted by the Ontario City Council on March 16, 1981, allows for the creation, adoption and implementation of Specific Plans in the City. Specific Plans are governed by Section 65450 et. seq. of the California Government Code.

1.5 Relationship to the Ontario General Plan, Municipal Code, and Related Policies

Development standards or land use regulations not specifically addressed in this document shall be governed by appropriate City codes and standards. Unless otherwise noted in the Specific Plan, all off-site improvements are subject to City of Ontario policies, standards and specifications in effect at the time of submittal of improvement plans.

1.6 Severability

If any term, provision, condition or requirement of this Specific Plan shall be held invalid or unenforceable, the remainder of the Specific Plan or the application of such term, provision, condition, or requirement to circumstances other than those in which it was held invalid or unenforceable shall not be affected thereby; and each term, provision, condition or requirement of the Specific Plan shall be valid and unenforceable to the fullest extent permitted by law.

1.7 Specific Plan Organization

The Specific Plan is organized as follows:

- The *Introduction* section provides background information regarding the opportunities and objectives of the Plan as well as describing the purpose;
- *Project Characteristics* and *Objectives* describe the scope and intent of the project;
- The *Development Program* section describes the manner in which the site is to be developed and includes a land use concept plan, a streetscape and landscape plan, a circulation and transportation plan, a transportation demand management plan, a grading concept, a plan to provide infrastructure, public services and community facilities;
- A set of *comprehensive Design Guidelines* to provide direction relative to architectural treatment of buildings and ancillary structures on the project site;
- *Development Standards*, which establish minimum site development requirements, including permitted land uses, setbacks, building heights and related topics;
- *Specific Plan Administration*, which describe the manner in which the Plan will be implemented and, if necessary, amended to reflect changing conditions.

2.0 Project Characteristics

2.1 Project Characteristics and Objectives

The project includes the construction of a combination ~~of~~ warehouse and distribution ~~uses with potential and~~ office ~~building(s) complex for Toyota Motor Sales U.S.A., Inc. (TMS).~~

Land Uses, which are described more fully in Chapter 3.0 of the Specific Plan, include two large warehouse and distribution structures. The larger of the two will be Toyota's NAPLD (North American Parts and Logistics Division) building, which will receive bulk auto parts from overseas and North American suppliers, sorted via manual and automated materials handling system (described below), and then distributed to smaller, regional warehouse facilities throughout North America, Hawaii, and the South Pacific. Parts will arrive and be shipped via tractor-trailer trucks with no use of on-site rail transit anticipated. A second, smaller warehouse and distribution facility will be built on the site which will be a regional facility to supply retail Toyota dealers throughout the Western United States. The warehouse and distribution buildings will also contain related administrative offices.

The materials handling component of the facility is described as follows. Automotive parts will be received in sea or land containers at the NAPLD facility on a scheduled basis. Containers will be unloaded, the contents broken down and moved by conveyor to areas designated for immediate shipping, repacking or storage. Parts will then be moved to their final destination by conveyor, automatic guided vehicle, or forklift. The fastest moving (in terms of time in the warehouse) small parts will be stored in a system that will allow automated retrieval and storage. Slower moving large parts will be stored and retrieved by warehouse personnel using wire-guided pickers or reach trucks. Such parts will then be transported to packing areas or staging areas by conveyor to shipping docks.

The entire inventory system will be paperless in that all parts will be received and shipped using bar code and computer scanner technology to verify quantities and order accuracy.

Other land uses ~~will may~~ include freestanding office buildings and Research and Development (R&D) facilities. ~~Office and R&D intended users may include administrative and corporate offices for Toyota, office and research space for the use of Toyota suppliers or establishments desiring to locate near Toyota's Ontario facility.~~ Non-auto related businesses may also select to locate within the Toyota/Ontario Business Park due to the geographic location or quality of development. ~~The Office and R&D component is envisioned to consist of multi-story buildings grouped around one or several courtyards or plazas.~~ ~~Other ancillary or complementary uses are also allowed.~~ Ample landscaping and related amenities will be provided throughout the site as described in ~~Section 3.3 of the Specific Plan and a landscaped parking area will be sited near the office complex.~~

The project will be built in multiple phases. Phasing is discussed in Section 3.8.

Project objectives are to:

- Permit the construction of a national auto parts warehouse and distribution center for [Toyota Motor Sales, U.S.A., Inc. \(TMS\)](#) relatively close to a major port facility to be able to serve the needs of their national dealer network in an economic and efficient manner.
- Provide for the construction of a regional auto parts warehouse on the same site to serve Toyota dealers in the western United States.
- Permit the construction of office and research and development space, ~~including offices related to the warehouse use and offices for other occupants desiring a high visibility site.~~
- Construct the above land uses in a high-quality, [campus-like project](#) setting which will allow on-site uses to be complimentary and which will present a positive image of the project and the City.
- Provide a range of employment opportunities for local residents, including managerial, technical, administrative, and general labor.
- Link the construction of major facilities to the provision of required infrastructure and community facilities.
- Comply with local and regional programs and policies to improve air quality within the South Coast Air Basin.

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Toyota Ontario Business Park



Scale 1"=300'

Exhibit 3
Land Use Concept



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3.0 Development Program

3.1 Development Plan Overview

The type, intensity, and character of anticipated development on the site are described in Chapter 2.0, Project Characteristics. To support this level of development, the Specific Plan provides for:

- A comprehensive transportation and circulation component, governing vehicular and non-vehicular modes of traffic generated by the project, including truck traffic and a plan for transportation demand management;
- A streetscape and landscape component to guide the aesthetic and functional treatment of adjacent street frontages, other project edges and interior landscaping;
- An infrastructure, public service and community facility component describing required improvements to water, sewer, storm drainage systems, as well as police protection, fire protection, solid waste disposal, and maintenance of public and private facilities;
- Phasing of on-site facilities and any off-site facilities which may be required.

3.2 Land Use Plan

The overall concept for the Toyota/Ontario Business Park Specific Plan is to permit the construction of a mixed use warehouse/distribution/office complex for TMS [and future owners](#) on the site. Land uses will include the NAPLD building, previously described, ~~a smaller regional warehouse and distribution facilities~~ [ies and/or other and complementary complex of office and research and development buildings, which have also been previously described. A trip generation comparison was conducted by Ganddini Group and reviewed by the City's Traffic Engineering Division that compared an Office use versus a Warehouse at a FAR of 0.48.](#)

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Exhibit 3 depicts the land use plan for the project. The exhibit divides the site into three Planning Areas based on anticipated, optimum land uses within each of these areas. In addition to buildings, each planning area will contain parking and landscaped areas. A description of uses within each planning areas follows.

- Planning Area 1, located directly on the northwest corner of the site, will [provide the opportunity to develop warehouse/distribution, manufacturing, office, research and development](#) ~~contain research and development~~ facilities or similar [offices-uses](#) which may be occupied by Toyota facilities or other users desiring a high visibility site near Toyota's distribution facility. Maximum development within Planning Area 1 will not exceed 300,000 gross square feet ~~of office and research and development uses.~~
- Planning Area 2 will house the NAPLD warehouse and distribution facility, consisting of 1.2 million square feet of gross floor space to be built in at least two phases. As a part of the NAPLD building, approximately 65,000 square feet of internal office space will be included to house administrative offices related to the warehouse function.
- Planning Area 3 will contain a regional distribution facility, consisting of a maximum of 700,000 square feet of gross floor area.

Land uses which are permitted within each of the Planning Areas are described in Section 5.2, Permitted Uses.

Table 1 below summarizes land use, development intensity, planning area size and Floor Area Ratio (FAR for the Toyota/Ontario Business Park).

Table 1 Toyota/Ontario Business Park Land Use Summary

Planning Area	Size (Ac.)	Land Use	Max. Dev. Intensity (Sq. Ft.)	Maximum FAR
1	14.4	Office/R&D, Warehouse, and Distribution	300,000	0.48
2	50.6	Warehouse/ Distribution	1,200,000	0.54
3	29.0	Warehouse/ Distribution	700,000	0.55
Total	94.0		2,200,200	0.54

It is anticipated that all or a portion of the site may be subdivided into smaller parcels, consistent with the City's subdivision ordinance and provisions of the Specific Plan for purposes of financing or utility provision. Minimum lot size shall be consistent with all Development Standards set forth in Chapter 5.0 of the plan.

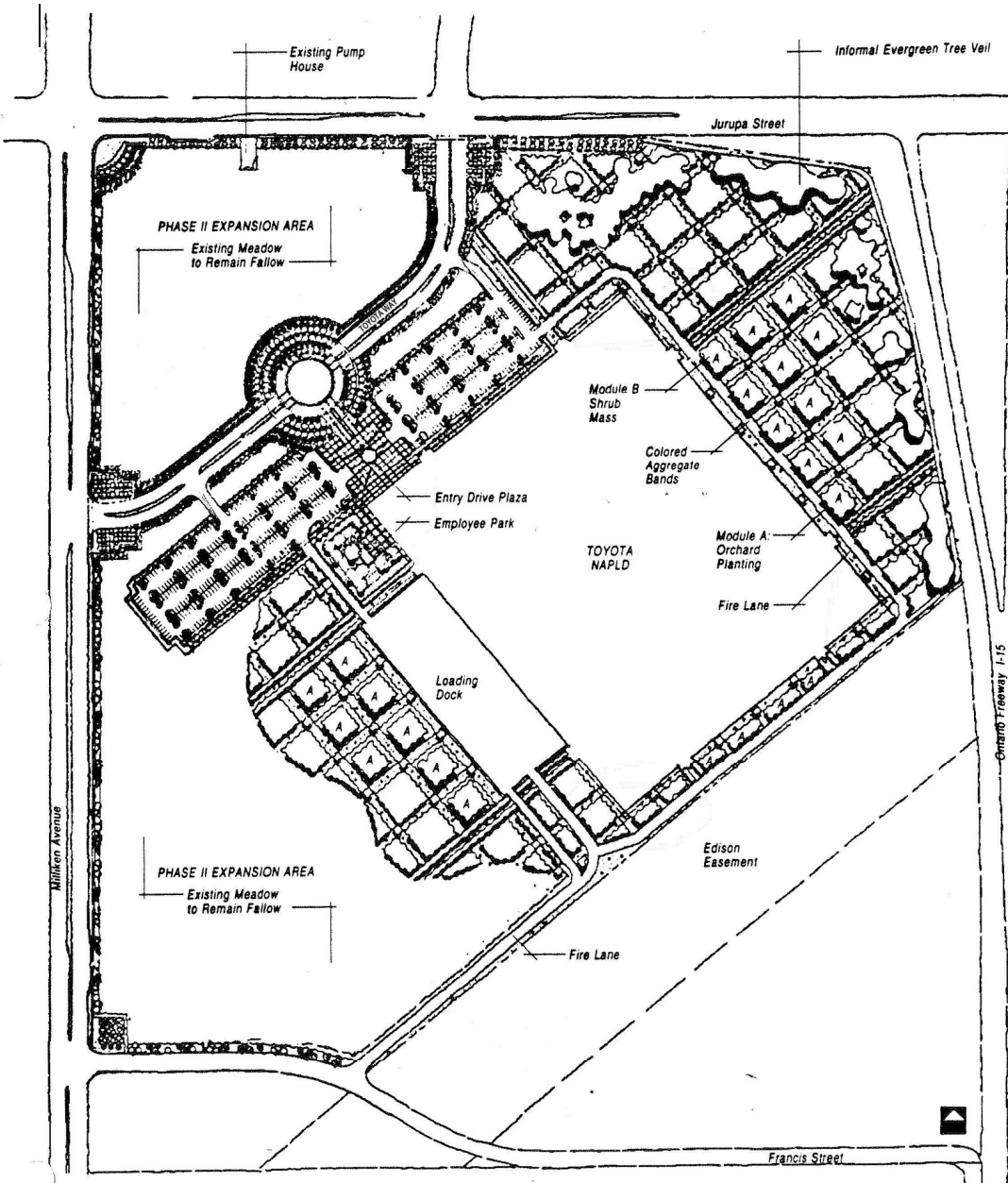
3.3 Streetscape and Landscape Concepts

The general location and extent of landscape and streetscape elements of the project site are depicted on **Exhibit 4**. Landscape and streetscape design objectives include:

- To integrate building architecture and landscape architectural design providing a [harmonious-complementary](#) appearance;
- To use a plant palette appropriate to Ontario's climatic condition;
- To provide an [environmentally](#) responsible design solution consistent with the water constraints in southern California and the necessity to reduce greenwaste;
- To provide a landscape concept requiring low maintenance;
- To provide a solution for the temporary landscape areas which meet the above objectives as well as providing dust and weed control.

Principal landscape components for the Toyota/Ontario Business Park consist of a landscape grid, shrub dots, tower shrub bands, orchards and parking lot trees which are all design extensions of the architectural elements of the [first building on Planning Area 2-façade](#). The role of the landscape is to complement and enhance buildings comprising the Business Park, not to hide these buildings. Perimeter landscape elements consist of streetscape plantings and an evergreen tree veil, which provides a transition zone between the context of the surrounding area and the project site.

Toyota Ontario Business Park



Note
 1. This plan is a diagrammatic concept plan only. Actual numbers and location of plant material may change during the design process.
 2. See Exhibit 20 for Recommended Plant Palette.

Exhibit 4
Landscape Concept

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 11



A key component of the landscape concept is the landscape module, reminiscent of agricultural patterns found in Southern California. The orchard concept has been developed not to replicate, but to pay homage to Ontario's agricultural history and the natural beauty of the landscape. Plantings create a rhythm with the building façade harmonizing with the architecture. The height of orchard trees has been selected with the intent of allowing views to accent "dot" windows, which together with the trees, appear as a "necklace" around the top of the building. **Orchards have been located only between the building towers to focus views to the accented corners of the buildings.** The shrub dots are repeating forms found on the building and integrated into the landscape. Shrub tower bands are an extension of the vertical architectural element into the horizontal plane of the landscape. The parking lot trees are also sequenced with building elevations.

Streetscapes along Jurupa Street and Milliken Avenue have been designed to complement existing landscape forms on the opposite sides of the street, providing a unified public appearance. [Reckefeller Avenue Toyota Way](#), the private drive within the project, is accented with special landscape treatments at the intersections with Jurupa Street and Milliken Avenue, as well as [Reckefeller Avenue Toyota Way](#). No street trees are planted adjacent to [Reckefeller Avenue Toyota Way](#).

An evergreen tree veil with a hydroseeded meadow mix below functions as a transitional element between adjacent streets and the Toyota/Ontario Business Park. This treatment is not contiguous, which allows for views into the project site so that individual buildings can be seen.

A crushed aggregate mulch will be placed on portions of the project site slated for future construction. This will provide temporary dust and weed control prior to permanent construction and landscaping.

The irrigation system for the project reflects water conservation elements consistent with the overall landscape theme and plant palette. Conventional spray irrigation will be minimized. An individual basin watering strategy will be incorporated which uses water efficiently and minimizes weed growth.

Exhibit 5 shows the location of the various landscape zones within the project area. These are further detailed on exhibits later in the Specific Plan Document.

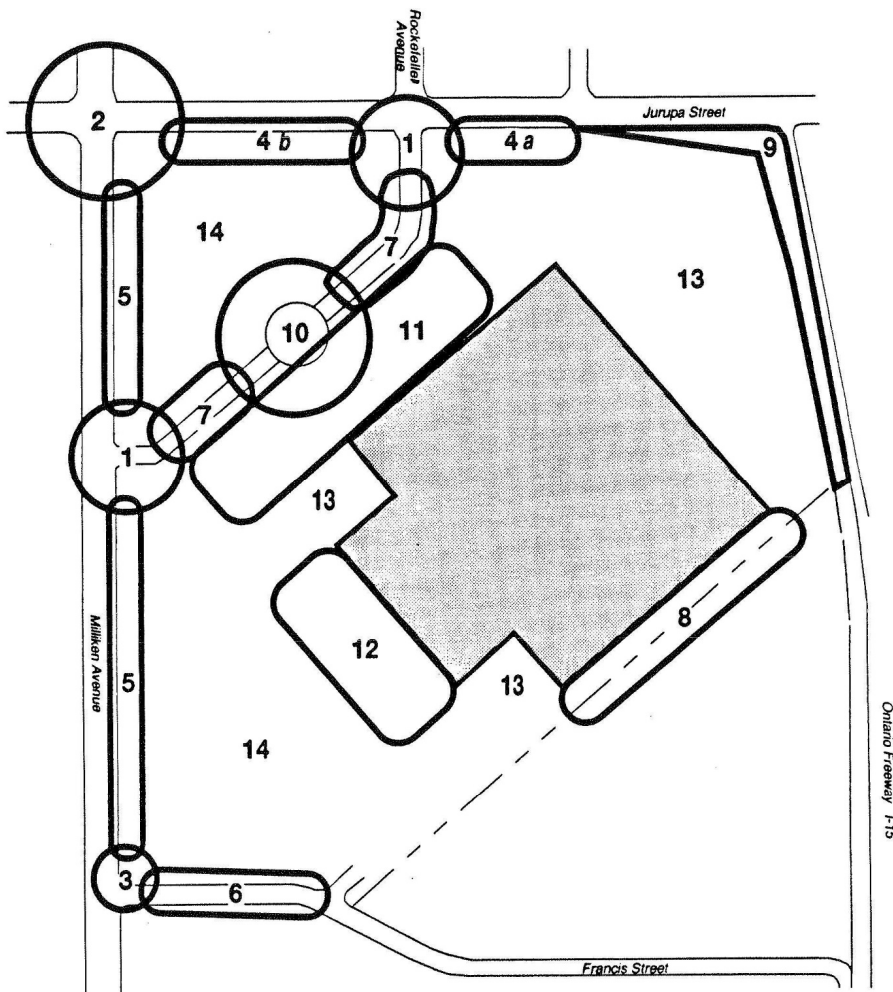
A small number of trees are currently growing on site which are perhaps the remnants of a windrow or windbreak associated with the previous agricultural use of the property. Prior to commencement of construction on the site, the applicant shall complete an arborist report to determine the health and status of these trees and, based on the arborist report, will prepare a mitigation plan to deal with trees. The mitigation plan may recommend that the trees be relocated on site. If the existing trees are not healthy or are too large to be safely relocated, they may be removed, ~~and compensatory tree plantings accomplished elsewhere on the project site.~~ [Replacement and mitigation for removed trees shall be equal to trunk diameter of heritage trees removed per the Development Code Tree Preservation Policy and Protection Measures, section 6.05.020. Add tree protection notes on construction and demolition plans to protect trees to remain.](#)

Following is a description of key streetscape and landscape elements.

3.3.1 Project Entrances

Two project entrances areas are planned, one at the intersection of Jurupa Street and Rockefeller Avenue Toyota Way and the other at the intersection of Milliken Avenue and Rockefeller Avenue Toyota Way. The project entrance design is shown on **Exhibit 6**. Each

Toyota Ontario Business Park



LEGEND

- | | |
|--|--|
| 1 Project Entrances (Exhibit 6) | 7 Toyota Way Streetscape Treatment (Exhibit 12) |
| 2 Milliken / Jurupa Intersection Treatment (Exhibit 7) | 8 Southeast Property Line Treatment (Exhibit 13) |
| 3 Francis Intersection Treatment (Exhibit 8) | 9 Freeway Edge Treatment (Exhibit 14) |
| 4a Jurupa Streetscape Treatment - East (Exhibit 9a) | 10 Toyota Way Circle Treatment (Exhibit 15) |
| 4b Jurupa Streetscape Treatment - West (Exhibit 9b) | 11 Parking Lot (Exhibit 16) |
| 5 Milliken Streetscape Treatment (Exhibit 10) | 12 Loading Dock Screening (Exhibit 19) |
| 6 Francis Streetscape Treatment (Exhibit 11) | 13 Permanent Landscaping |
| | 14 Existing Meadow to Remain Fallow |

Exhibit 5

Landscape Zones



Scale: 1" = 400'

8/23/93

the recommended plant palette (**Exhibit 19**). Public and private sidewalks will be integrated into the design of the entrance areas. Special paving material will be installed at project entrances, which will likely be exposed concrete aggregate to match accent material elsewhere in the Business Park.

The minimum dimension for primary entry treatments is ninety (90) feet.

3.3.2 Milliken Avenue/Jurupa Street and Francis Street Intersection Treatments

Exhibit 7 depicts landscaping improvements which will be installed on the southeast corner of Milliken Avenue and Jurupa Street. Design of the intersection will be complementary with similar intersection treatments constructed by California Commerce Center. Improvements will consist of a concentric, semi-circular ring of Crape Myrtle and Italian Stone Pines with the centerpiece being a number of Mexican Fan Palms sited in a turf area adjacent to the roadway.

Exhibit 8 illustrates the project entrance treatment at the intersection of Francis Street and Milliken Avenue. The concept is to provide a geometric-shaped landscaped area adjacent to Francis Street with a minimum width of ninety (90) feet. Features within this area include a formal planting of an accent tree (Chinese Flame Tree) in a bed of hydroseeded turf and framed by shrub plantings of New Zealand Flax on the perimeter of the intersection treatment.

Plant selection may vary somewhat from the above, but all plantings will conform with the Recommended Plant Palette, **Exhibit 20**.

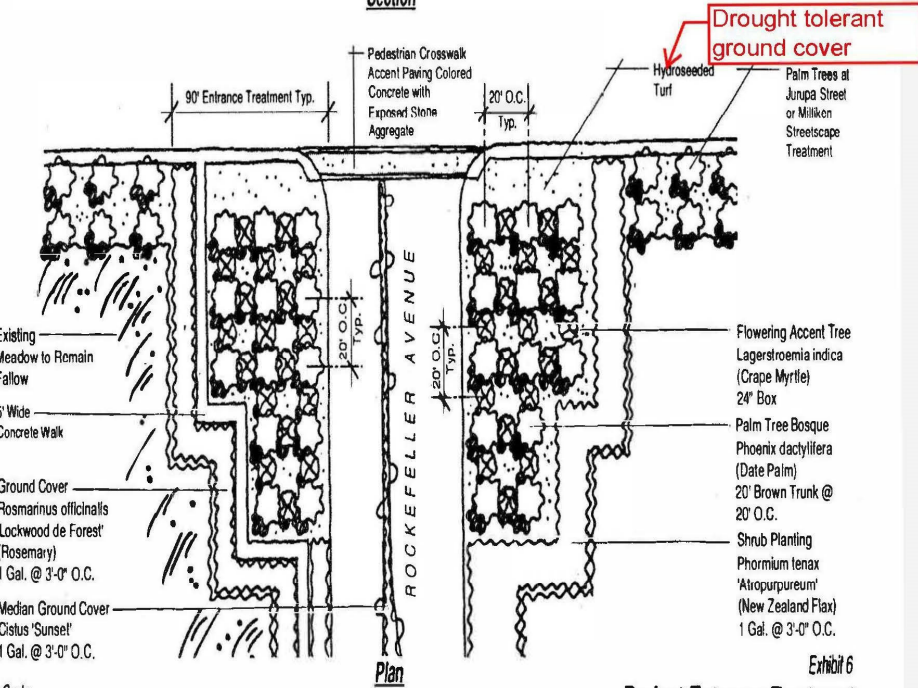
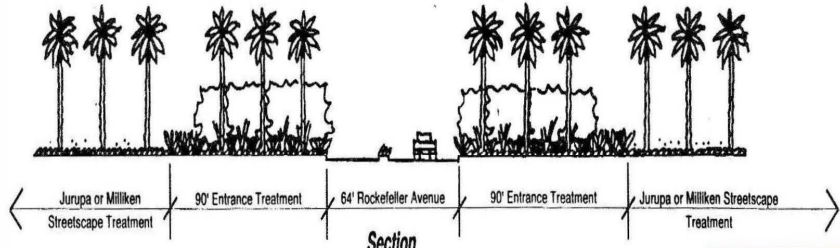
3.3.3 Streetscape Treatments

Exhibit 9a depicts the streetscape treatment which will be characteristic of Jurupa Street east of ~~Rockefeller Avenue~~[Toyota Way](#). Features include a five (5) foot wide public sidewalk with a landscaped parkway of forty-one (41) feet in back of the sidewalk which will contain a double row of Date Palms within a ~~turfed~~[ground cover](#) area. A forty (40) foot storm drain easement east of ~~Rockefeller-Toyota Way~~ overlays the landscaped parkway, which contains a ninety-six (96) inch storm drain facility.

West of ~~Rockefeller Avenue~~[Toyota Way](#) no storm drain facility or easement exists. **Exhibit 9b** depicts the streetscape condition in this location, which includes plantings of Date Palms with a closer spacing, twenty feet as opposed to twenty-two feet, since the existing storm drain facility does not need to be avoided.

Exhibit 10 illustrates streetscape conditions adjacent to Milliken Avenue. Improvements will consist of an eight (8) foot wide swath of landscaping immediately adjacent to the street, followed by a five (5) foot wide public sidewalk with a twelve (12) foot bermed landscape parkway located behind the sidewalk. Street trees will consist of informal groupings of Ginkgo (Maiden Hair) trees and Italian Stone Pine with a rosemary ground cover.

Toyota Ontario Business Park



Drought tolerant ground cover

Not to Scale

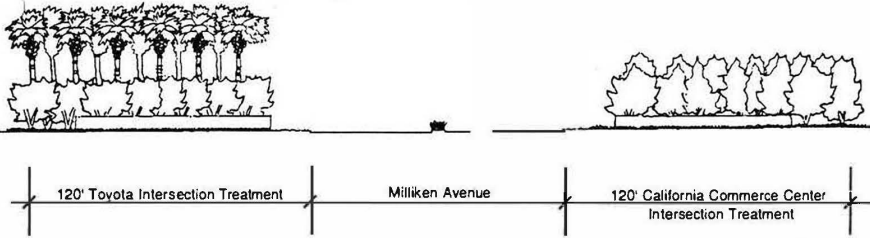
Project Entrance Treatment

Exhibit 6

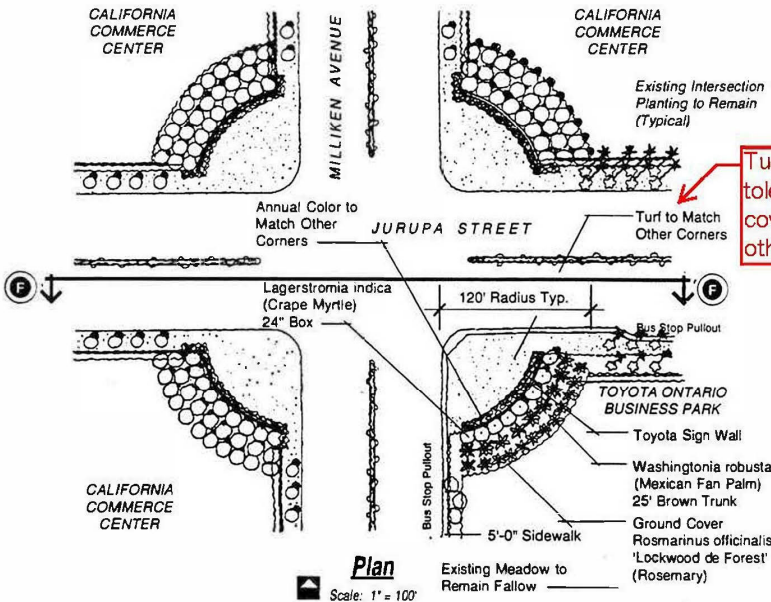
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Toyota Ontario Business Park



Section
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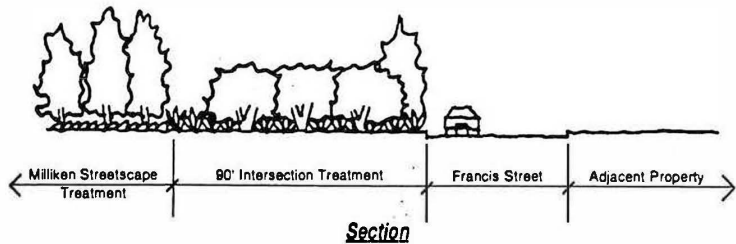


Turf or drought tolerant ground cover to match other corners

Exhibit 7
Milliken / Jurupa Intersection Treatment



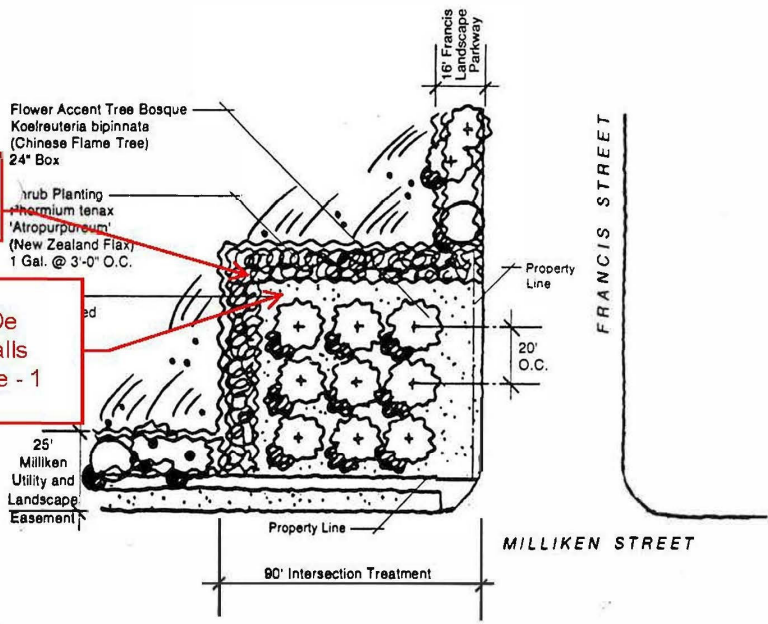
Exhibit 8 Franci



Section

Drought tolerant shrubs 5 Gal. @ 4' O.C.

Rosemary
Lockwood De
Forest or Halls
Honeysuckle - 1
Gal. 3' O.C.



Plan

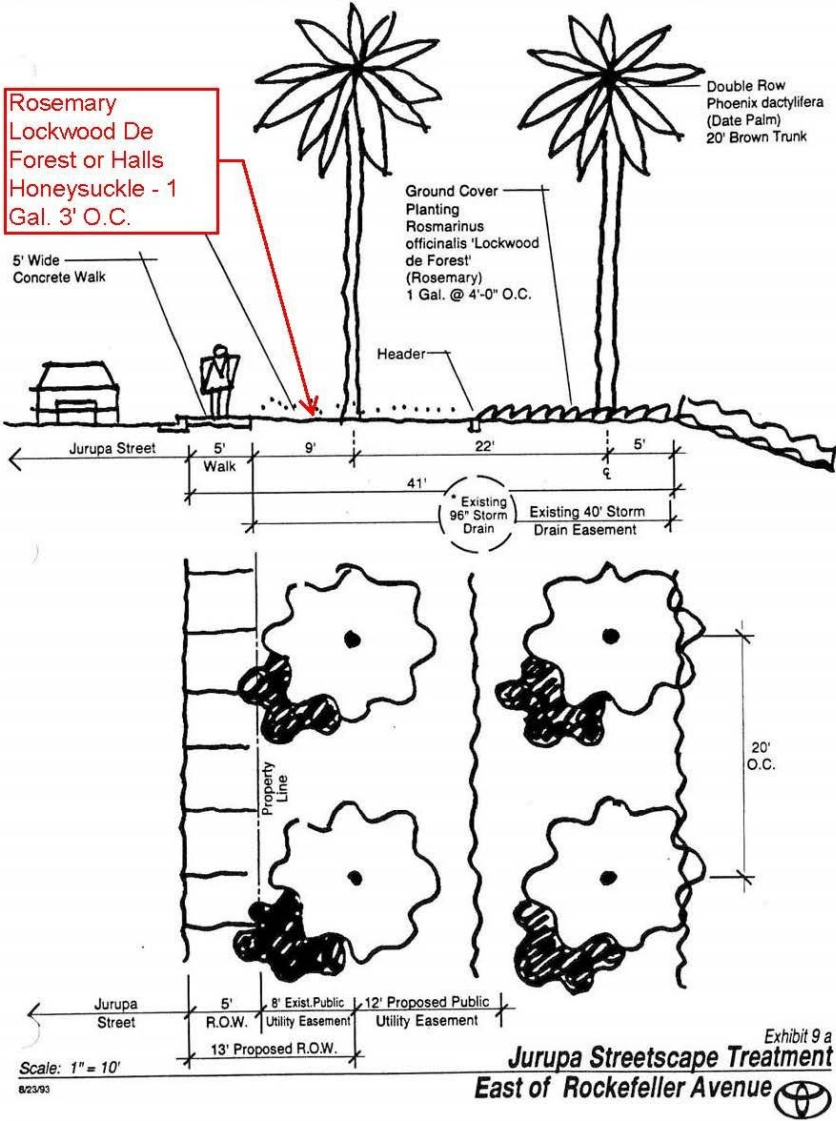
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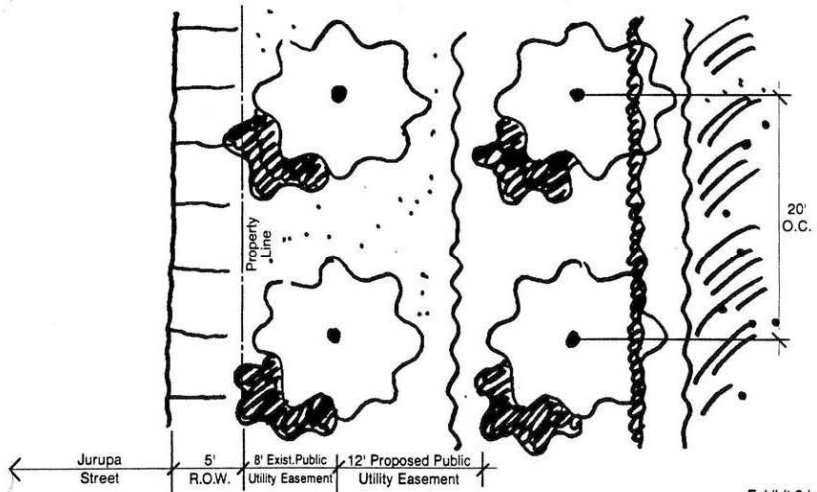
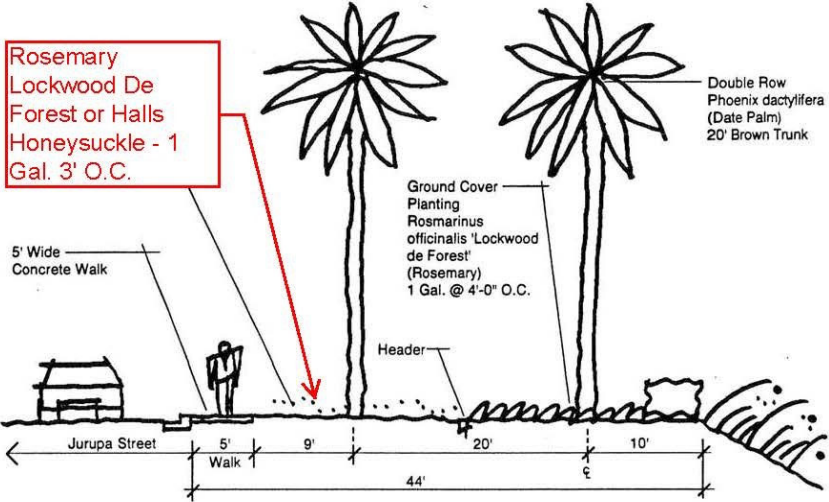
Francis Street Intersection Treatment

Exhibit 8



Toyota Ontario Business Park





Scale: 1" = 10'

8/23/93

Exhibit 9 b

Jurupa Streetscape Treatment
West of Rockefeller Avenue

Exhibit 10 Millik

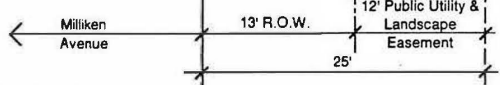
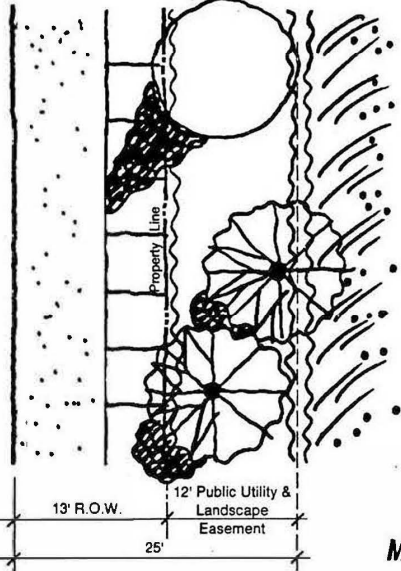
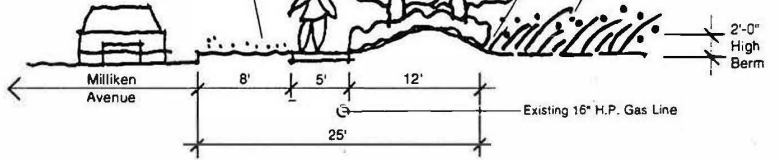
Rosemary
Lockwood De
Forest or Halls
Honeysuckle - 1
Gal. 3' O.C.

Informal Planting
Ginkgo biloba
(Maiden Hair Tree)
and
Pinus canariensis
(Canary Island Pine)
24" Box @
3 / 1,000 S.F.
Turf
Hydroseeded

Low growing
drought tolerant
shrub 5 Gal. @ 4'
O.C.

Low Growing Shrub
Pittosporum tobira
'Wheeler's Dwarf'
(Dwarf Tobira)
1 Gal. @ 3'-0" O.C.

Existing Meadow to
Remain Fallow



Scale: 1" = 10'
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Exhibit 10
Milliken Streetscape
Treatment



The Francis Street streetscape treatment, illustrated in **Exhibit 11**, will consist of informal groupings of Chinese Flame trees and Italian Stone Pine trees planted in a fifteen (15) foot wide public utility easement.

Exhibit 12 shows the treatment along both sides of [Rockefeller Avenue Toyota Way](#), which will be a private street. **No trees will be planted in this location, so as not to obscure the visibility of surrounding buildings and landscaping. Instead, the streetscape treatment will consist of a low-growing groundcover within the median strip (*Hypericum*) and New Zealand Flax on the perimeter of the roadway.**

3.3.4 Southeast Property Line and Ontario Freeway Landscape Treatment

Special attention will be given the southeastern property line edge of the project site, since this will be visible from the nearby Ontario Freeway. **Exhibit 13** shows this treatment. A private driveway will run along a portion of the southeasterly property line, primarily for truck access to loading areas, but also to provide emergency access around nearby buildings. Adjacent to the driveway will be a building setback varying in width from approximately 70'0" to 92'6" in width. A grove of olive trees will be planted within the building setback area.

Landscaping will be provided along the easterly boundary of the project site as depicted on **Exhibit 14**. Major features of this treatment will consist of a row of Date Palms on Caltrans property integrated with slope bank landscaping installed by Caltrans as part of the I-15 / Jurupa interchange project. Immediately adjacent to the site property line will be a solid planting of shrubs followed by a band of rock aggregate. A tree veil of Red Gum eucalyptus trees, which are tall and columnar, will frame the NAPLD building and provide a contrasting element to the horizontal features of the building.

3.3.5 On-Site Landscaping

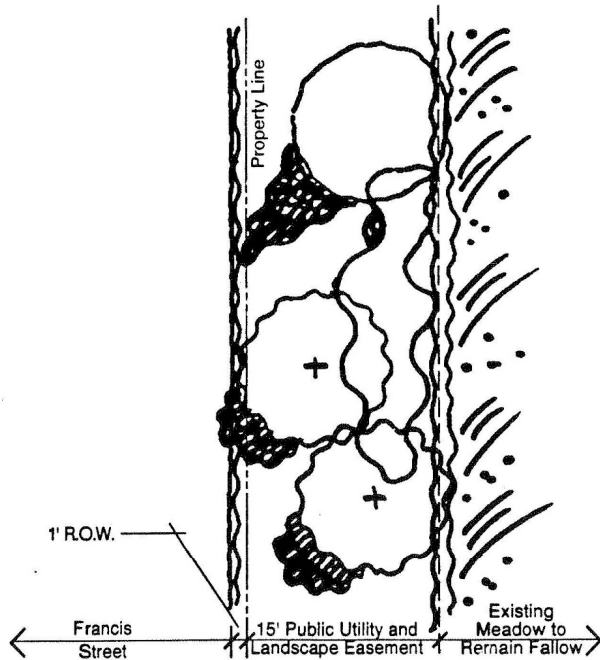
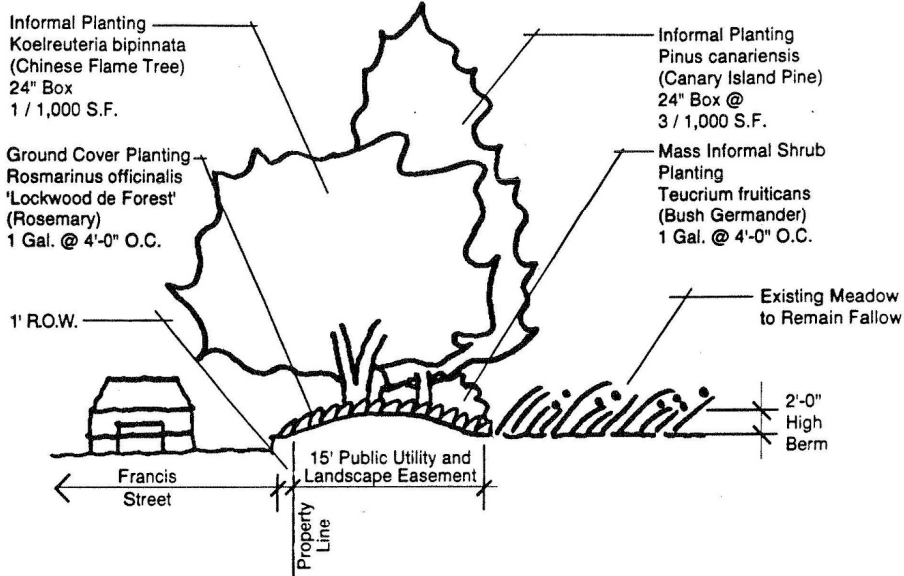
Treatment of the special landscape visual feature along [Rockefeller Avenue Toyota Way](#) is shown on **Exhibit 15**. This will consist of a circular planter intersecting [Rockefeller Avenue Toyota Way](#) at the convergence of the three Planning Areas. The primary visual element in this planter will be a row of Mexican Fan Palm and a row of Crape Myrtle trees with the foundation planting of [New Zealand Flax Shrubs drought tolerant shrubs](#) behind the trees. Interspersed within the planter area will be a low-growing ground cover of [Hypericum Halls Honeysuckle](#).

Exhibit 16 depicts typical parking lot landscape conditions within the [Toyota/Ontario Business Park Specific Plan](#) both in plain view and section. Future development **will may** incorporate a similar grid theme, although the modules may be scaled down to more appropriately integrate future building modules.

Landscaping adjacent to buildings within the [Business Park Specific Plan](#) is depicted on **Exhibit 17**. Landscape standards are further described in Section 5.7 of this specific plan document. Landscape planters and walkways adjacent to parking lots have taken into consideration overhangs from parked vehicles.

Plant material within parking lots are listed on the plant palette contained in the next section of the Specific Plan.

Toyota Ontario Business Park

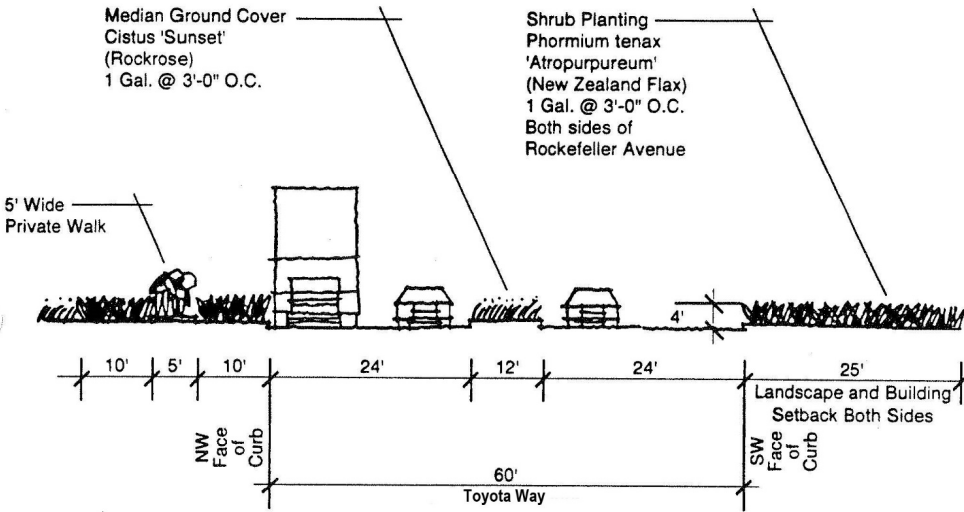


Scale: 1" = 10'

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Exhibit 11
Francis Streetscape Treatment





Scale: 1" = 20'

8/23/93

Exhibit 12
Toyota Way Streetscape Treatment



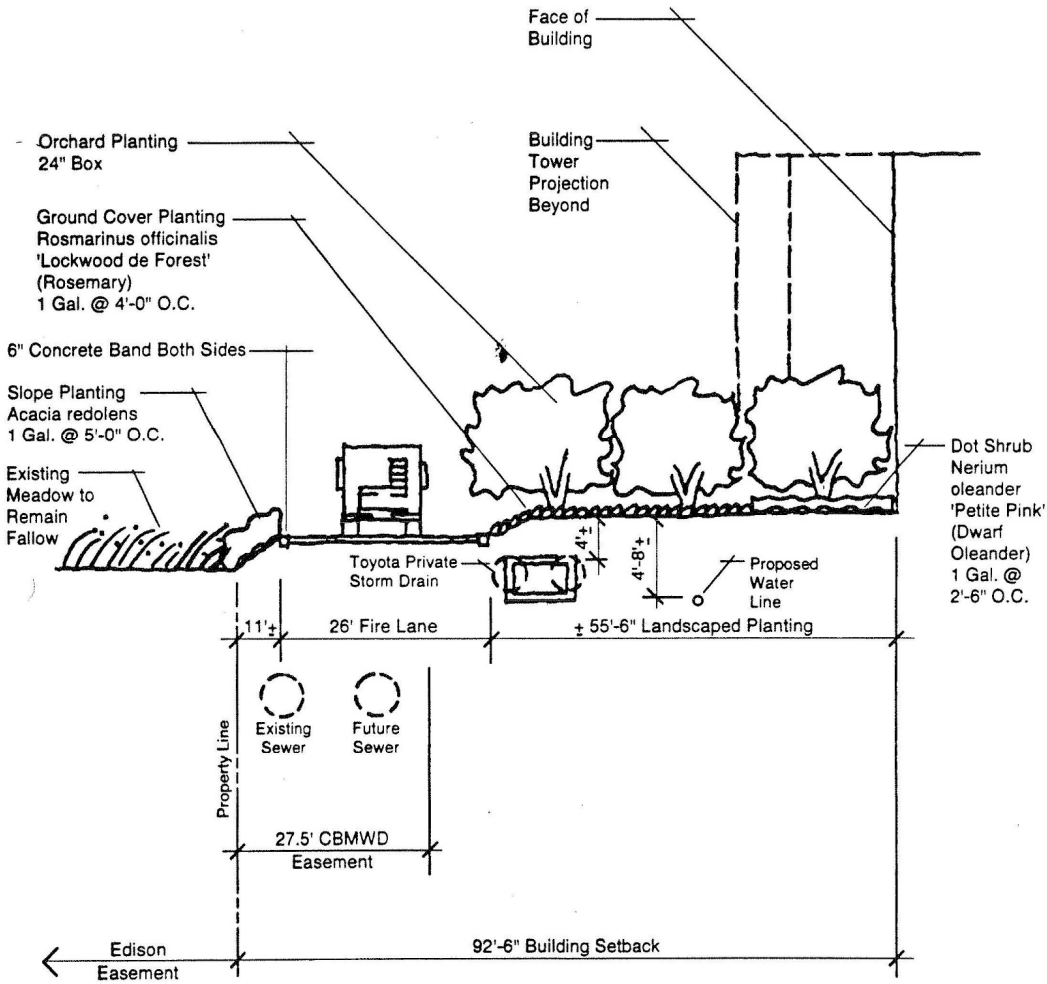


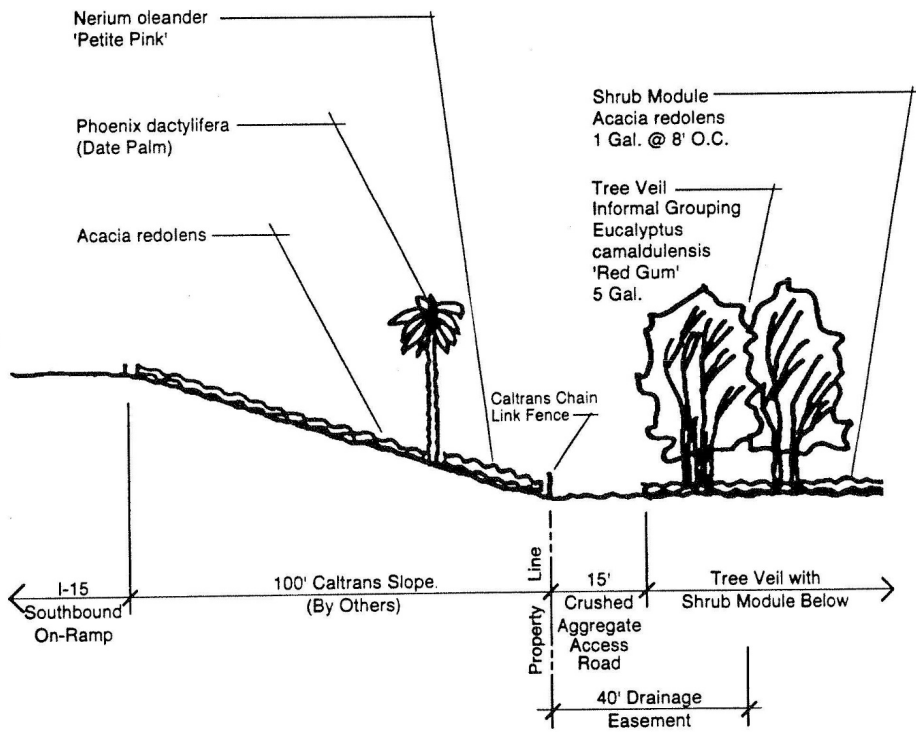
Exhibit 13

Southeast Property Line Treatment

Scale: 1" = 20'

8/23/93



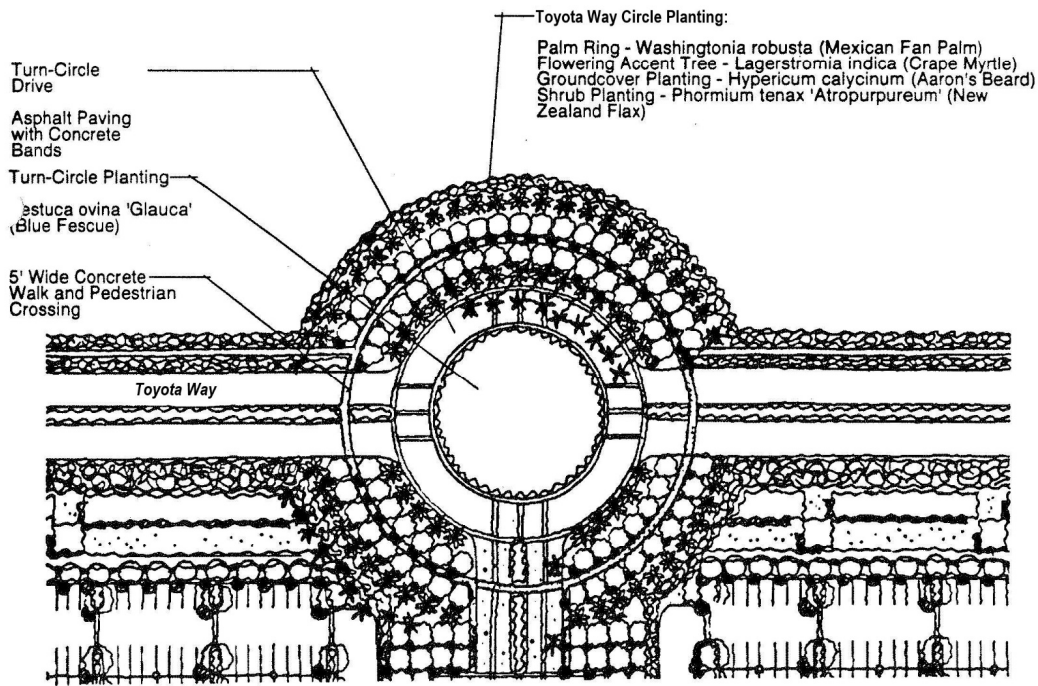
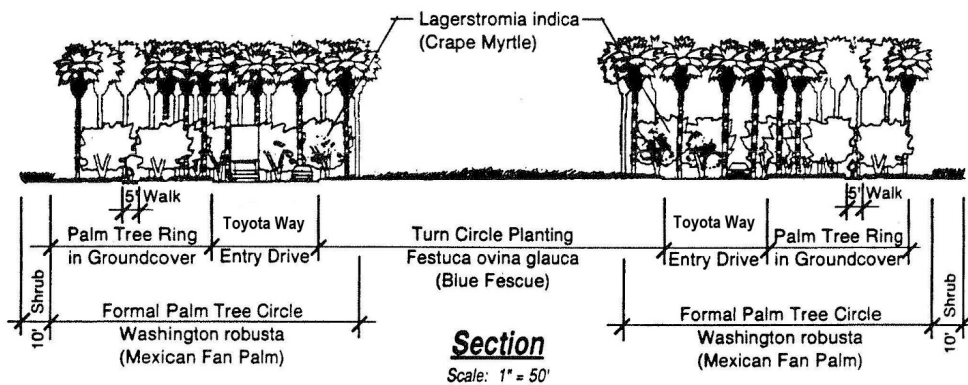


Scale: 1" = 30'

8/23/93

Exhibit 14
Freeway Edge Treatment





Plan

Scale: 1" = 100'

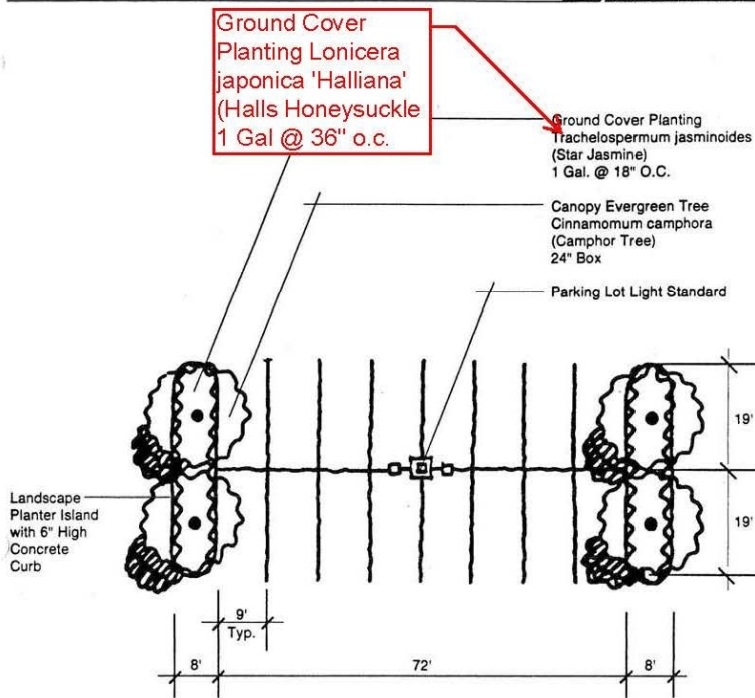
Exhibit 15

Toyota Way Circle Treatment

8/23/93



Exhibit 16 Typi



Ground Cover
Planting Lonicera
japonica 'Halliana'
(Halls Honeysuckle)
1 Gal @ 36" o.c.

Ground Cover Planting
Trachelospermum jasminoides
(Star Jasmine)
1 Gal. @ 18" O.C.

Canopy Evergreen Tree
Cinnamomum camphora
(Camphor Tree)
24" Box

Parking Lot Light Standard

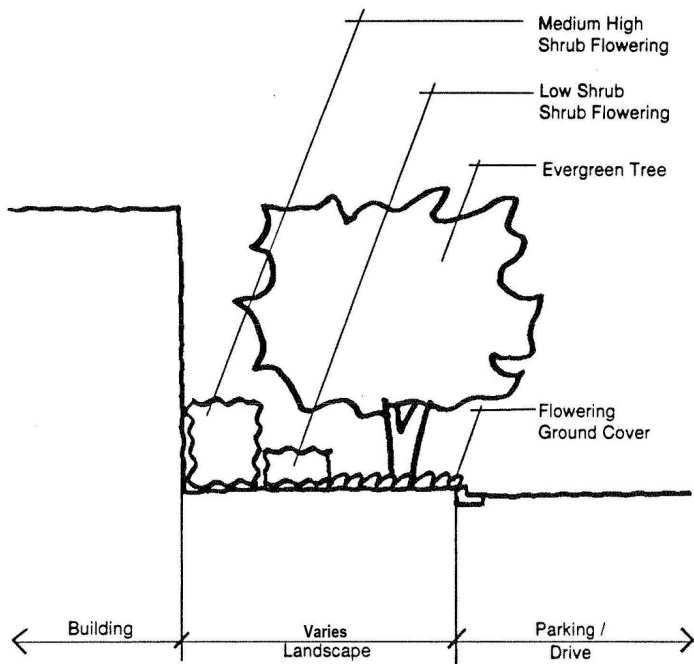
Landscape
Planter Island
with 6" High
Concrete
Curb

Note:
Minimum one landscape
finger per 8 parking stalls.

Scale: 1" = 20'
8/23/93

Exhibit 16
Typical Parking Bay





Scale: 1" = 10'

8/23/83

Typical Office and Warehouse Building Landscape

Exhibit 17



Exhibit 18 shows typical landscape modules which will be installed throughout the project. Such features include the orchard modules which form the primary element for the landscaping of the project site, shrub modules and the colored aggregate field, which constitutes the landscaping treatment on portions of the site which are slated for construction in future phases.

Finally, **Exhibit 19** depicts how landscape and structural elements will be used to screen loading docks and loading areas from adjacent streets [on the NAPLD Building, Planning Area 2](#). Screening consists of a ten (10) foot high block wall immediately in front of the truck loading and maneuvering area. A 120-foot area landscaped with a grove of olive trees will be planted in front of the wall, which will ensure that views of the loading area will be obscured from passing motorists. In addition, a sight line analysis shall accompany each site plan submittal to the City in order to demonstrate that sufficient screening has been provided to obscure truck docks and loading areas from nearby streets.

[Following installation of on- and off-site landscape with the initial phase of development, any proposed revisions to the landscape will be required to meet the City landscape requirements in effect at the time of Site Plan review and/or the updated landscape palette on Exhibit 20.](#)

3.3.6 Plant Palette

The recommended plant palette for the Toyota/Ontario Business Park is shown on **Exhibit 20**. As noted previously, plant material has been chosen for drought tolerance, which is compatible with the local climate and which is readily available from local suppliers.

3.4 Circulation and Transportation Concepts

3.4.1 Regional Issues

Vehicular traffic in Southern California and the Inland Empire in particular has been steadily increasing over the past fifteen to twenty years, generally outstripping the capacity of the surface transportation network to accommodate demand. This has resulted in high levels of traffic congestion on freeways, at freeway interchanges, and on arterial highways, especially during peak commute times.

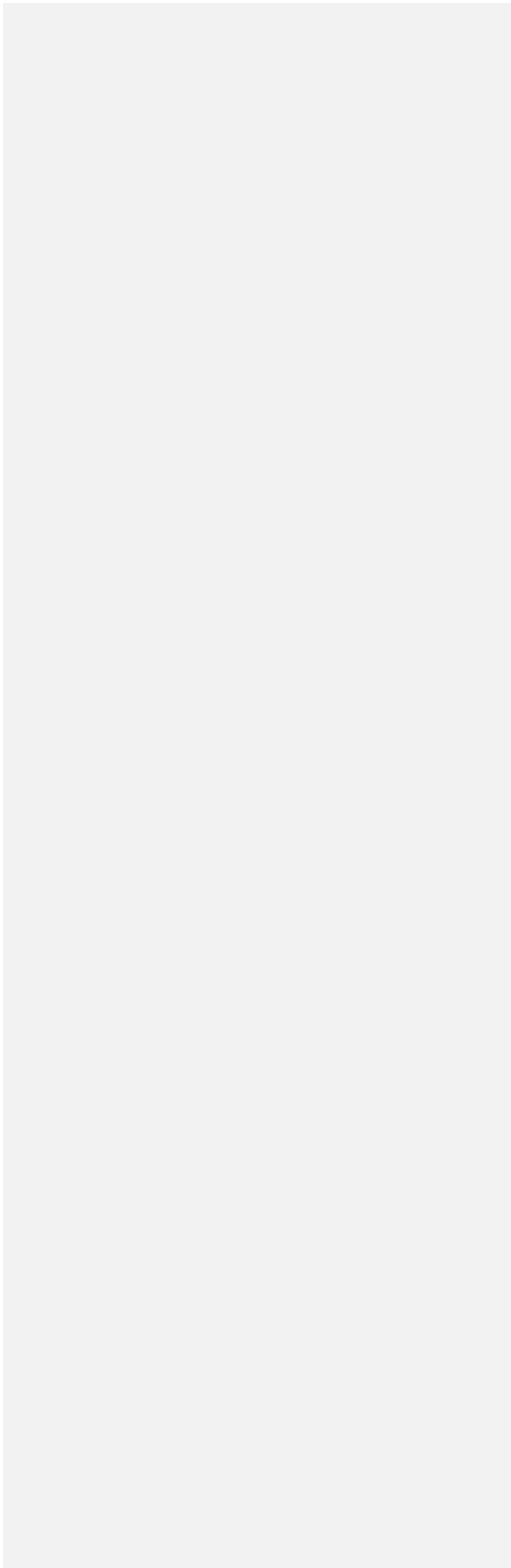
To improve mobility within the region, a number of governmental agencies have undertaken programs to upgrade the surface street system, to provide for alternative transportation modes, including the expansion of bus transportation opportunities and the addition of mass transit facilities, including commuter rail and light rail projects.

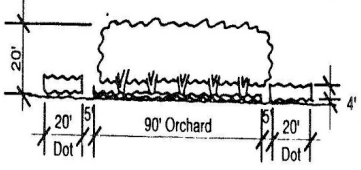
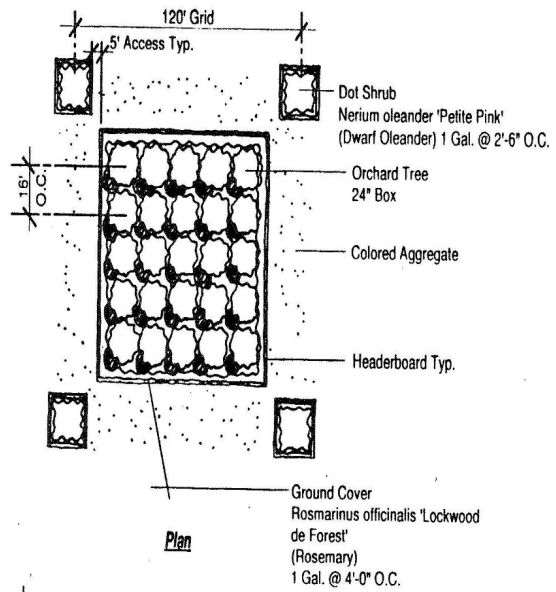
Strongly linked to regional transportation and mobility improvements is a concerted regional effort to improve air quality through increased reliance on carpooling, bus transportation, and mass transit, all of which decrease the use of single-occupant automobile traffic.

On November 4, 1992, the San Bernardino County Congestion Management Agency (CMA) adopted a Congestion Management Plan (CMP) for the County, which will affect the T/OBP project. Pursuant to the CMP, the traffic and circulation analysis for this project must comply with CMP guidelines.

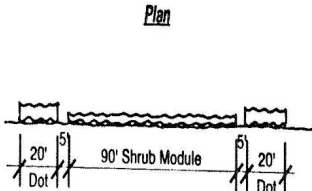
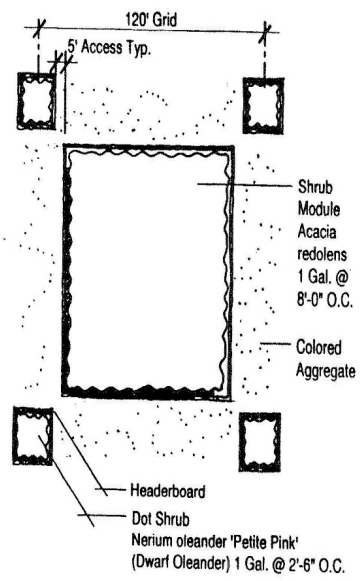
Other regional transportation issues which have been considered in the development of this Specific Plan include: the Ground Access Program for Ontario International Airport, which

includes the expansion and construction of arterial highways and freeway interchanges in the community to accommodate the planned enlargement of the terminal for Ontario International Airport and Metrolink, which is a commuter rail link between Riverside and downtown Los Angeles with at least one stop in Ontario.





Section
Module A: The Orchard



Section
Module B: Shrubs

Scale: 1" = 60'

8/23/93

Exhibit 18
Typical Landscape Modules



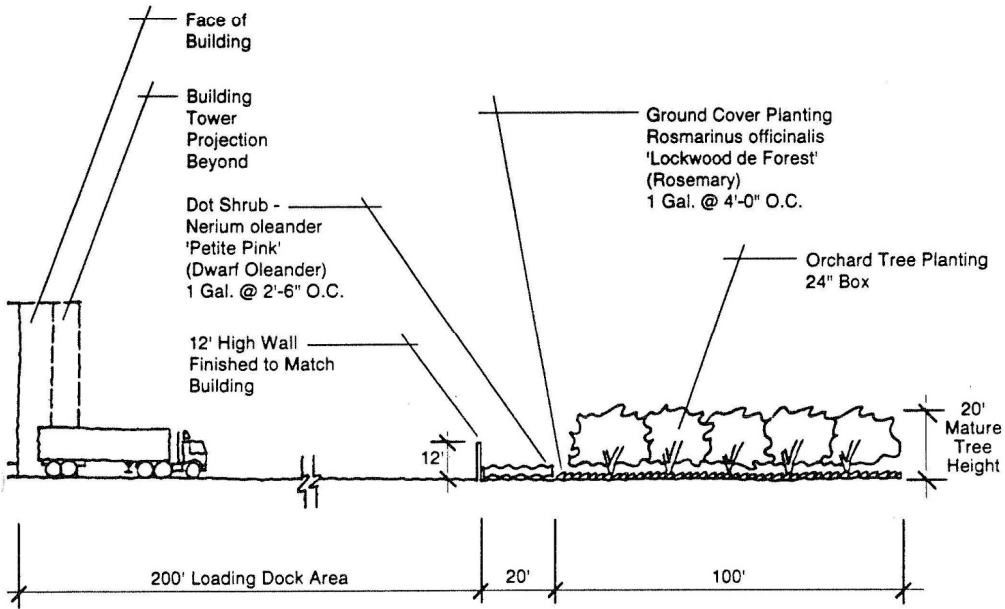


Exhibit 19

Scale: 1" = 40'

Toyota NAPLD Loading Dock Screening

8/23/93



Toyota Ontario Business Park

BOTANICAL NAME	COMMON NAME	SIZE
JURUPA STREETSCAPE		
TREES		
Phoenix dactylifera	Date Palm	20' Brown Trunk
SHRUBS		
None		
GROUND COVERS		
Turf	Fescue-blend	Hydroseeded
Rosmarinus officinalis	Prostrate	1 Gal. @ 4'-0" O.C.
'Lockwood de Forest'	Rosemary	
MILLIKEN STREETSCAPE		
TREES (Informally grouped canopy trees)		
Pinus canariensis	Canary Island Pine	24" Box
Ginkgo biloba	Maidenhair Tree	24" Box
GROUND COVERS		
Rosmarinus officinalis	Prostrate	1 Gal. @ 4'-0" O.C.
'Lockwood de Forest'	Rosemary	
FRANCIS STREETSCAPE		
TREES (Informally grouped)		
Pinus canariensis	Canary Island Pine	24" Box
Koeleruteria bipinnata	Chinese Flame Tree	24" Box
GROUND COVERS		
Turf	Fescue-blend	Hydroseeded
ROCKEFELLER STREETSCAPE -- TOYOTA STREETSCAPE		
TREES		
None		
SHRUBS		
3 Phormium tenax	New Zealand Flax	1 Gal. @ 3'-0" O.C.
'Atropurpureum'		
GROUND COVERS (in median)		
Cistus 'Sunset'	Rockrose	1 Gal. @ 3'-0" O.C.
ROCKEFELLER CIRCLE -- TOYOTA CIRCLE		
TREES		
1 Washingtonia robusta	Mexican Fan Palm	25' Brown Trunk
Lagerstroemia indica	Crape Myrtle	24" Box
SHRUBS		
3 Phormium tenax	New Zealand Flax	1 Gal. @ 3'-0" O.C.
'Atropurpureum'		
GROUND COVERS		
2 Hypericum calycinum	Aaron's Beard	Flats @ 12" O.C.
4 Festuca ovina glauca	Blue Fescue	
MILLIKEN/JURUPA INTERSECTION		
TREES		
1 Washingtonia robusta	Mexican Fan Palm	25' Brown Trunk
SHRUBS		
None		
GROUND COVERS		
2 Hypericum calycinum	Aaron's Beard	Flats @ 12" O.C.
7 Felargonium peltatum	Ivy Geranium	1 Gal. @ 8" O.C.
Turf	Fescue-blend	Hydroseeded
PROJECT ENTRANCE (Rockefeller @ Milliken and Jurupa)		
TREES		
Phoenix dactylifera	Date Palm	20' Brown Trunk
Lagerstroemia indica	Crape Myrtle	24" Box
SHRUBS		
3 Phormium tenax	New Zealand Flax	5 Gal. @ 3'-0" O.C.
'Atropurpureum'		
Turf	Fescue-blend	Hydroseeded

BOTANICAL NAME	COMMON NAME	SIZE
TEMPORARY LANDSCAPE AREAS		
TREES (Informally grouped veil)		
Eucalyptus camaldulensis	Red Gum	5 Gal.
SHRUBS		
6 Nerium oleander 'Petite Pink'	Dwarf Oleander	1 Gal. @ 2'-6" O.C.
Existing Meadow to Remain Fallow		
PARKING AREAS		
TREES		
Cinnamomum camphora	Camphor Tree	24" Box
Gejera parviflora	Australian Willow	24" Box
SHRUBS (as windscreen)		
Ligustrum japonicum 'Texanum'	Texas Privet	1 Gal. @ 3'-0" O.C.
GROUND COVERS		
2 Hypericum calycinum	Aaron's Beard	Flats @ 12" O.C.
OFFICE / RESEARCH AND DEVELOPMENT AREA -- MIXED USE INDUSTRIAL AREA (Building edges and frontages and entry courtyards)		
TREES		
Eucalyptus rudis	Desert Gum	15 Gal.
Lagerstroemia indica	Crape Myrtle	24" Box
1 Washingtonia robusta	Mexican Fan Palm	20' Brown Trunk
Phoenix dactylifera	Date Palm	25' Brown Trunk
Magnolia grandiflora	Southern Magnolia	24" Box
Ginkgo biloba	Maidenhair Tree	24" Box
Pinus pinea	Italian Stone Pine	24" Box
SHRUBS		
6 Nerium oleander 'Petite Pink'	Oleander	5 Gal.
Pittosporum tobira	Tobira	5 Gal.
'Wheeler's Dwarf'	Tobira	5 Gal.
Raphiolepis indica	Indian Hawthorn	5 Gal.
Xylosma congestum 'Compacta'	Shiny Xylosma	5 Gal.
5 Photinia fraseri	Photinia	5 Gal.
Juniperus sabina 'Tamariscifolia'	Tamarix Juniper	5 Gal.
GROUND COVERS		
Rosmarinus officinalis	Rosemary	1 Gal. @ 4'-0" O.C.
2 Hypericum calycinum	Aaron's Beard	Flats @ 12" O.C.
Trachelospermum jasminoides	Star Jasmine	1 Gal. @ 2'-0" O.C.
Turf	Fescue-blend	Hydroseeded
INTERIOR SITE		
MODULE A - THE ORCHARD		
Olea europaea 'Wilsonii'	Wilson's Olive	24" Box
Rhus lancea	African Sumac	24" Box
Schinus molle	California Pepper	24" Box
GROUND COVER BELOW		
Rosmarinus officinalis	Rosemary	1 Gal. @ 4'-0" O.C.
Vinca major	Periwinkle	Flats @ 12" O.C.
2 Hypericum calycinum	Aaron's Beard	Flats @ 12" O.C.
MODULE B - SHRUBS		
Acacia redolens		1 Gal. @ 8'-0" O.C.
MODULE C		
Crushed Colored Aggregate		2" Deep
THE DOT		
6 Nerium Oleander 'Petite Pink'	Dwarf Oleander	1 Gal. @ 2'-6" O.C.
TOWER SHRUB BANDS		
3 Phormium tenax	New Zealand Flax	1 Gal. @ 3'-0" O.C.
'Atropurpureum'		

**Exhibit 20
Recommended Plant Palette**



<u>CHANGE</u>		<u>TO</u>		
1	<u>Washingtonia Robusta</u>	<u>Mexican Fan Palm</u>	<u>Washingtonia Filifera</u>	<u>California Fan Palm</u>
2	<u>Hypericum Calycinum</u>	<u>Aaron's Beard</u>	<u>Lonicera Japonica</u>	<u>Halls Honeysuckle</u>
3	<u>Phormium Tenax 'Atropurpureum'</u>	<u>New Zealand Flax</u>	<u>Dianella Revoluta</u>	<u>Baby Bliss Flax Lily</u>
4	<u>Festuca Ovina Glauca</u>	<u>Blue Fescue</u>	<u>Curio Repens</u>	<u>Blue Chalksticks</u>
5	<u>Photinia Fraseri</u>	<u>Photinia</u>	<u>Ligustrum</u>	<u>Privet</u>
6	<u>Nerium Oleander 'Petite Pink'</u>	<u>Dwarf Oleander</u>	<u>Rhaphiolepis Indica</u>	<u>India Hawthorn</u>
7	<u>Pelargonium Peltatum</u>	<u>Ivy Gernium</u>	<u>Rosmarinus Officinallis 'Lockwood De Forest'</u>	<u>Rosemary</u>

3.4.2 Circulation and Transportation Overview

Exhibit 21 depicts the major components of the circulation and transportation system to support the level of development intensity on the project site. These elements include an expanded and enlarged Jurupa Street/I-15 Freeway interchange; arterial streets Milliken Avenue, Jurupa Street, and Francis Street, a new private local street, known as [Rockefeller AvenueToyota Way](#) and links to the regional public transportation system, OmniTrans. The proposed location of bus bays adjacent to the project is shown on **Exhibit 24**.

3.4.3 Jurupa Street, Milliken Avenue and Francis Street Improvements

3.4.3.1 Existing Conditions

Jurupa Street, which forms the northerly boundary for the project site, presently has a total right-of-way width of 112 feet with a curb-to-curb width of 108 feet, and is improved as a four-lane divided arterial highway with curb and gutter on both the north and south sides of the roadway. A raised median has been constructed within Jurupa Street.

Milliken Avenue has a dedicated right-of-way width of 120 feet (94 feet curb-to-curb) and has been configured as a four lane arterial highway with full curb and gutter improvements.

Francis Street has a right-of-way width of 50 feet with a 48 foot Curb-to-Curb width.

No sidewalks have been built in the vicinity of the project.

3.4.3.2 Planned Improvements

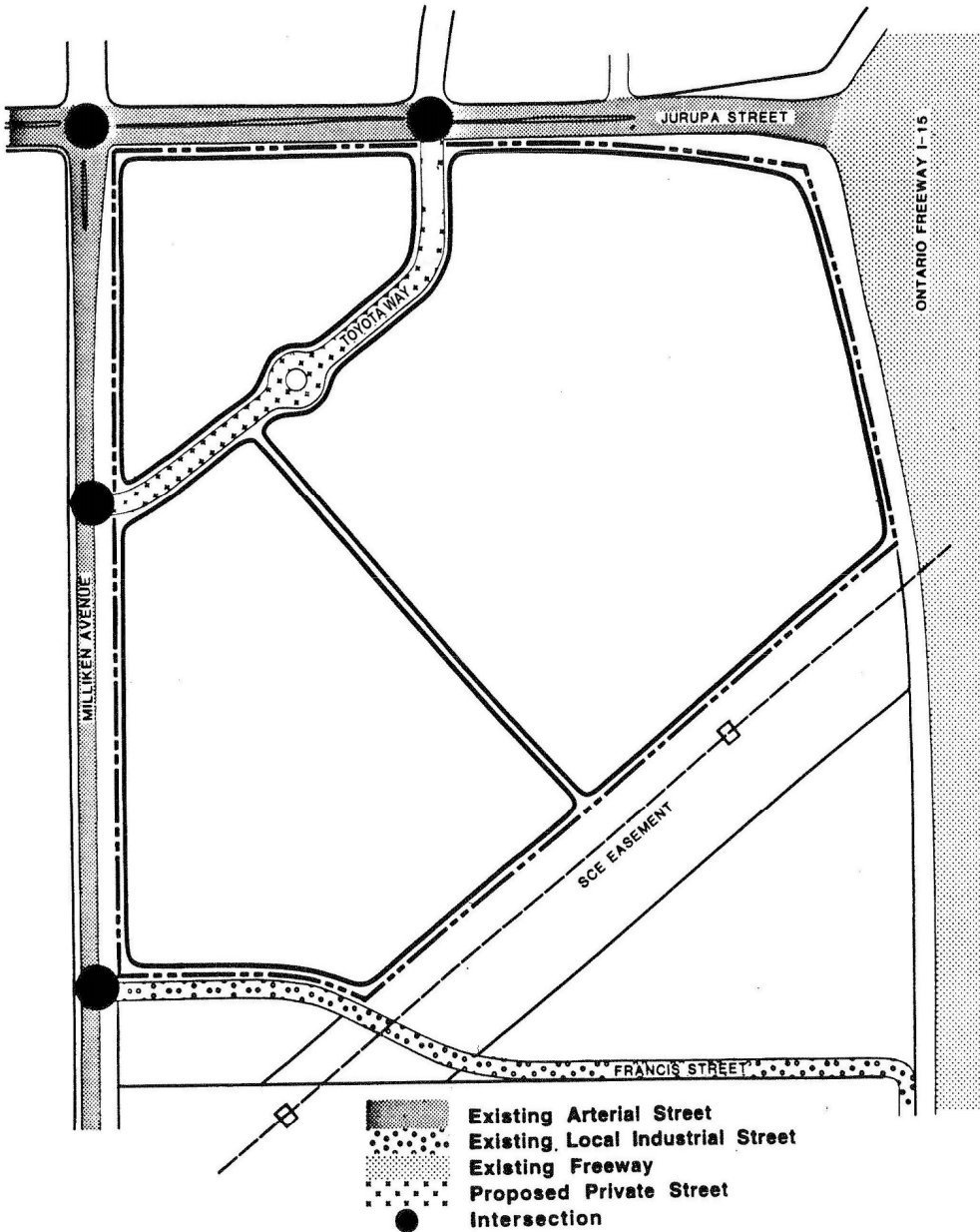
Exhibit 22a shows, in cross-section format, the ultimate mid-block configurations of Jurupa Street and Milliken Avenue.

A new private street will also be constructed, known as a [Rockefeller AvenueToyota Way](#), which will link Jurupa Street and Milliken Avenue, and which will also provide primary vehicular access into the Business Park. The curb-to-curb width of [Rockefeller AvenueToyota Way](#) will be sixty (60) feet. The cross-section design for both [Rockefeller AvenueToyota Way](#) and Francis Street are shown on **Exhibit 22b**.

In each instance, these improvements are consistent with the City of Ontario's Master Plan of Streets and Highways.

In conjunction with the T/OBP project, traffic signals will be constructed at the intersections of Milliken Avenue/[Rockefeller AvenueToyota Way](#) and Jurupa Street/[Rockefeller AvenueToyota Way](#). A traffic signal has previously been approved for construction at the intersection of Francis and Milliken and is not part of this project. The existing signal at Milliken Avenue/Jurupa Street will be modified to accommodate a new right turn lane.

Toyota Ontario Business Park

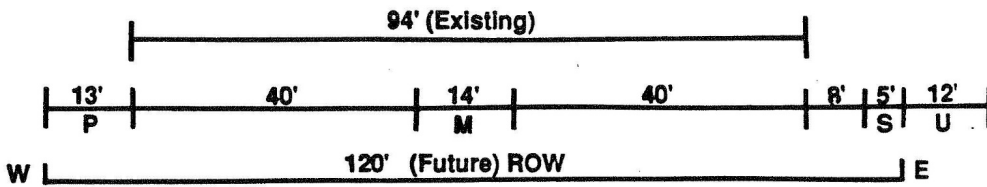


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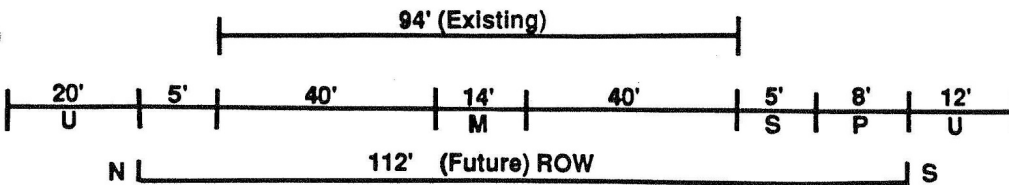
Exhibit 21
Circulation Concept



Milliken Avenue



Jurupa Street



Legend

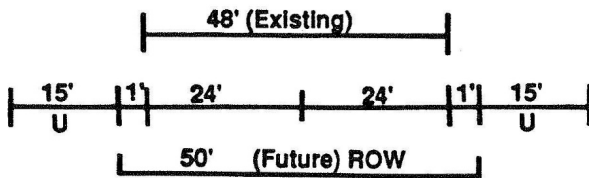
- M Median
- U Utility and Landscape Easement
- S Sidewalk
- W Private Walkway

No Scale

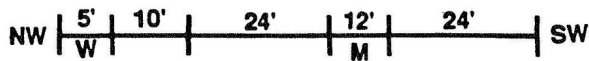
Exhibit 22a
Milliken / Jurupa
Cross Sections



Francis Street



Toyota Way (Private)



Legend

- M Median
- U Utility and Landscape Easement
- S Sidewalk
- W Private Walkway

No Scale

Exhibit 22b
Francis / Toyota
Cross Sections



All of these improvements will be incorporated into and financed by Assessment District 106:

- Milliken Avenue:
 - The addition of a 250-ft. long right turn pocket on northbound Milliken Avenue to eastbound Jurupa Street.
 - A median break to allow a left turn pocket for southbound Milliken Avenue to eastbound [Rockefeller Avenue Toyota Way](#).
 - Construction of two bus turnouts, one just north of Francis Street and one north of Rockefeller Street.
 - Construction of a new traffic signal at the intersection of Milliken Avenue and [Rockefeller Avenue Toyota Way](#) and the modification of the existing signal at Milliken Avenue and Jurupa Street.
- Jurupa Street:
 - Construction of a traffic signal at the intersection of Jurupa Street and [Rockefeller Avenue Toyota Way](#).

3.4.4 I-15/Jurupa Interchange

Caltrans has prepared plans for the improvement of the existing Jurupa Street/I-15 interchange. The existing diamond interchange configuration is planned to be retained. Primary improvements will include the widening of Jurupa Street to three lanes in each direction with dual left turn lanes accessing the I-15. The existing freeway on-ramps will be widened to three lanes.

The above interchange improvements are anticipated to be completed by late 1994. Funding is to be provided by a combination of Federal grants and the State of California sources, and are not part of the T/OPB.

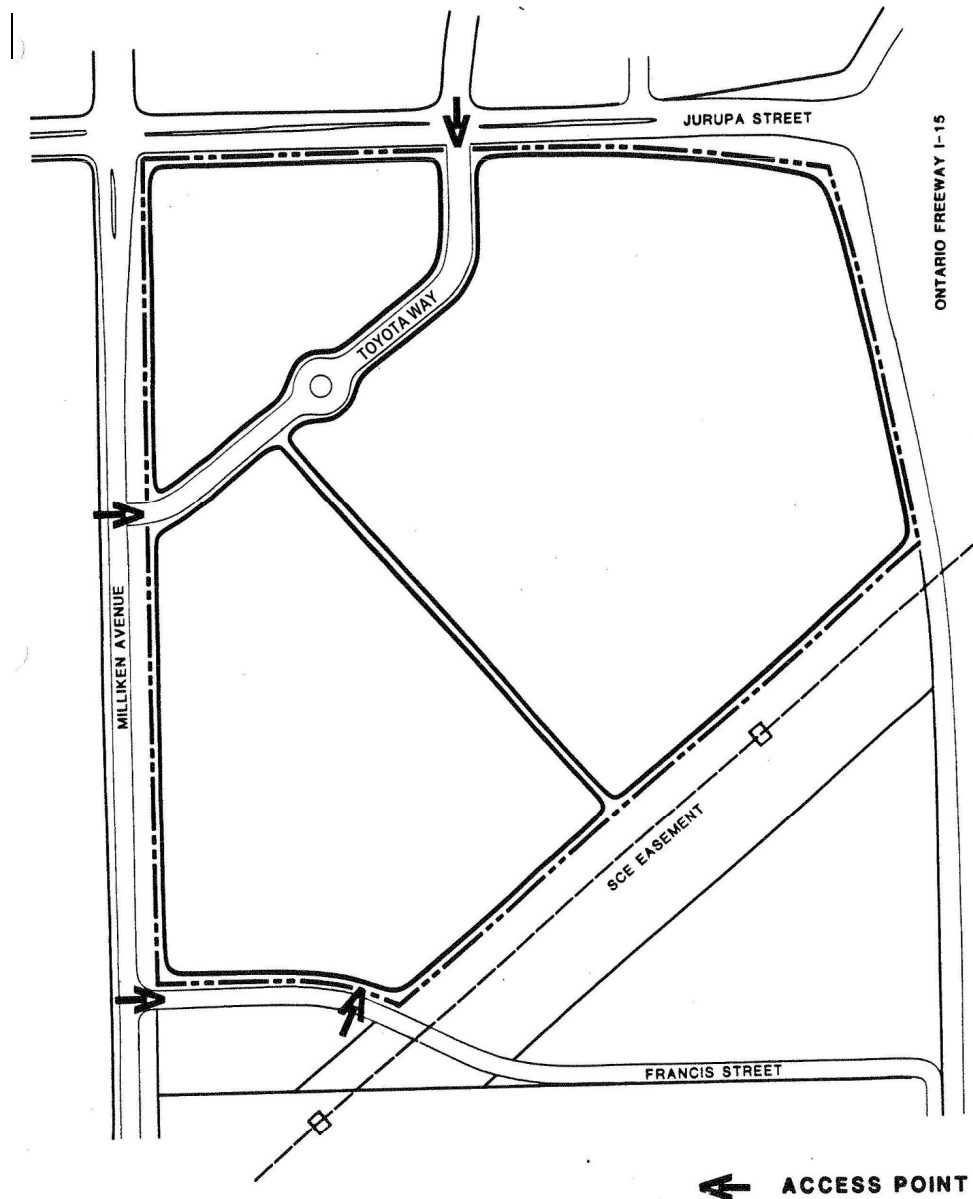
3.4.5 Access Points

Exhibit 23 shows the approximate location of primary vehicular entry points into the project. The precise location of access points will be indicated on site plans for individual projects within the T/OPB project. Additional access points, if desired, shall be subject to the approval of the Planning and Engineering Departments.

3.4.6 Truck Routes

The NAPLD facility will be served by tractor-trailer trucks originating at the Ports of Los Angeles and Long Beach. The primary truck routing paths include the use of the SR-60 (Pomona) Freeway exiting at Milliken Avenue, a right turn onto Francis Street and a left turn into the project site. It is estimated that some truck travel paths would include use of the I-15 (Ontario) Freeway, exiting at Jurupa Street, a left turn on Milliken, left turn onto Francis Street and a final left turn into the project site. A number of the trucks exiting the site would use I-15 and I-10 freeways. Other trucks leaving the site, would use the SR-60 freeway to return to the ports area or to serve auto dealers in the Southern California area. In the future, truck travel paths, origins and destinations, are expected to become more diverse.

Toyota Ontario Business Park



Scale 1"=300'

Exhibit 23
Access Points



It is anticipated that a majority of the truck trips will take place during non-peak commute hours.

On-site rail service will not be used.

3.4.7 Public Transportation and Pedestrian Facilities

At the present time, there is no public transit service to the project site. Future transit service is anticipated by OmniTrans based on the relatively large size of the facility and the number of employees to be located within the complex. **Exhibit 24** shows the approximate location of bus turnouts adjacent to the project. The precise design of each bus turnout facility and timing of installation will be mutually agreed by the City of Ontario, OmniTrans staff and landowner.

Planned pedestrian facilities are also shown on **Exhibit 24**. Public sidewalks are anticipated near the intersection of Jurupa Street and Milliken Avenue which are planned to be integrated with private walkways serving the office complexes and the entries to warehouse and distribution buildings.

3.4.8 Transportation Demand Management

An important component of the project is the incorporation of Transportation Demand Management (TDM) concepts. The goal of TDM is to reduce the number of automobiles entering and leaving the site at peak travel times which will, in turn, reduce traffic congestion within the region and thereby reduce emission of air pollutants.

Strategies to achieve this goal includes use of carpooling and/or vanpooling, use of public transit opportunities, alternative work hours and reliance upon alternative transportation modes, such as bicycling.

Objectives of TDM include:

- Increased traffic levels generated by new development on the site will be mitigated through TDM strategies aimed at reducing the number of peak hour trips.
- Requirements of the South Coast Air Quality Management District to achieve regional air quality standards are to be partially addressed through implementation of TDM requirements.

Toyota Ontario Business Park

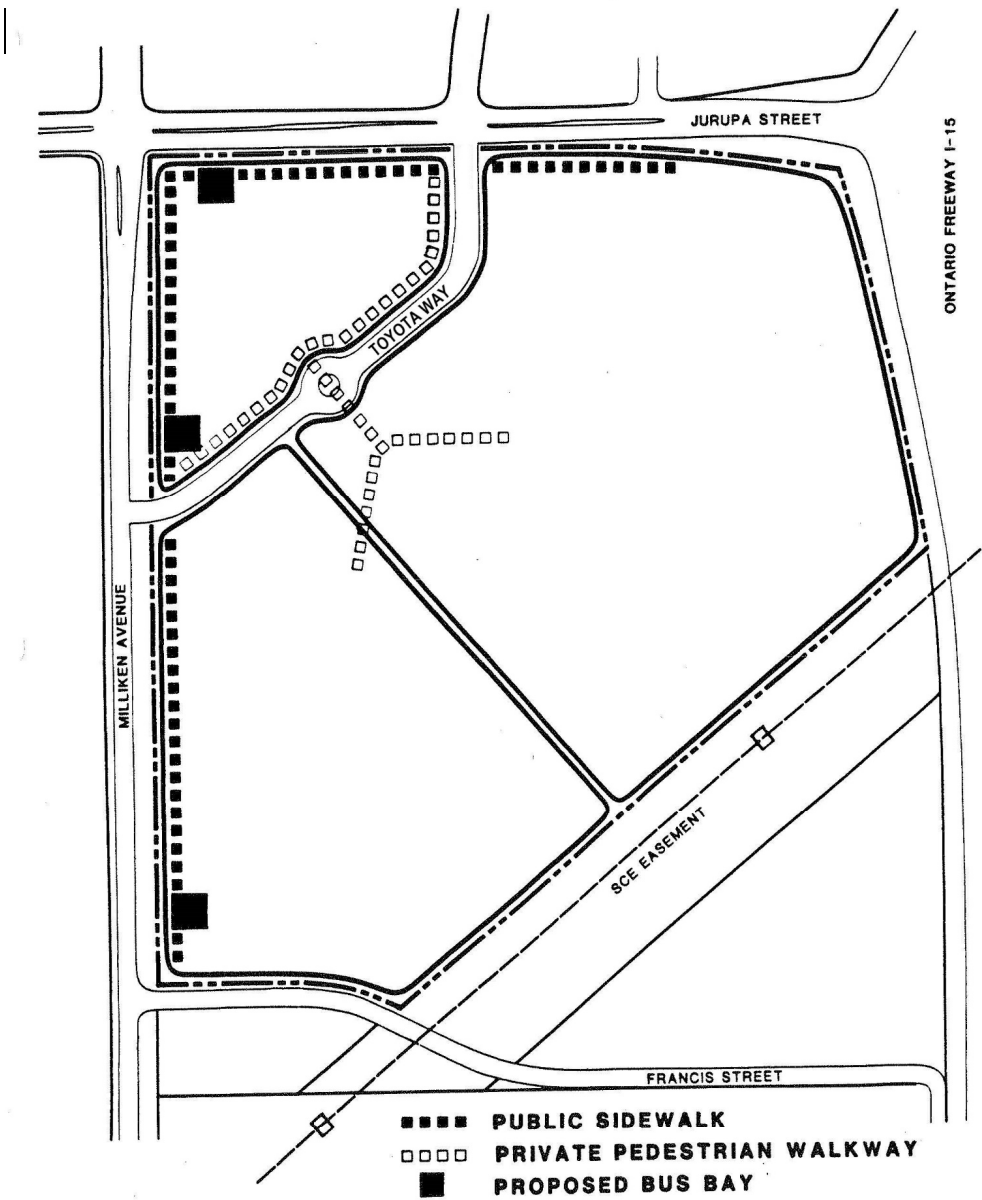


Exhibit 24
Pedestrian Paths and Bus Bays

Scale 1"=300'



3.4.8.1 Role of Property Owner, City of Ontario and South Coast Air Quality Management District

The individual property owner of any property owned within the specific plan area, ~~Toyota Motor Sales~~, or its designated agent, will be the responsible party for designing and implementing specific TDM methods within the project for that property. These methods are described in the following section. The City of Ontario will review individual site plans within the project for consistency with TDM elements as outlined in the Specific Plan. The South Coast Air Quality Management District reviews and approves trip reduction plans for facilities having 100 or more people.

~~The property owner shall also become a member of the Airport Communities Transportation Management Agency (TMA), which is administered by the Inland Empire Economic Council. The TMA will assist in the implementation of various TDM methods to be employed.~~

Commented [PS1]: This organization no longer exists – that I could find. Pam

3.4.8.2 Trip Reduction Plan

Consistent with the provisions established by the South Coast Air Quality Management District Regulation XV, a comprehensive Trip Reduction Plan (TRP) shall be completed and submitted to the City of Ontario and the South Coast Air Quality Management District prior to or subsequent with the first site plan to be filed within the T/OB Project, and shall apply to all additional site plans approved by the City. The Trip Reduction Plan consists of specific measures to be taken to ensure that an Average Vehicle Ridership of 1.5 is achieved and maintained. Techniques and strategies which can be employed to reach this goal are listed in Section 3.4.8.3. Compliance with this requirement will be monitored through annual reporting and updates.

If a portion of the project site is later sold to another owner by Toyota Motor Sales, the new owner shall have the responsibility of submitting a separate Trip Reduction Plan should a minimum of 100 employees be located on the site.

The Trip Reduction Plan shall contain, at minimum, the following information:

- Proposed land use or uses;
- Gross building square footages and site address;
- Number of employees at the site (by shift) and methods used to determine employee population;
- Hours of operations, work hour shifts and related information which may assist in developing TRP;
- A listing of nearby businesses which have or plan to have a TRP in place;
- A listing of specific trip reduction techniques to be used to achieve trip reduction goals and estimated time frames for installation of physical improvements (such as bike racks) or start dates for trip reduction programs (such as carpooling);

- The name of the Employee Transportation Coordinator (ETC) who will be responsible for developing, implementing, and evaluating the effectiveness of the TRP;
- A discussion of estimated costs of funding sources necessary to implement the TRP;
- Provision for annual monitoring and updating of the TRP, including information as to achievement of trip reduction goals for the year. If reduction goals are not met, a specific listing of additional methods which will be implemented over the next year must be included.

3.4.8.3 Trip Reduction Techniques

Individual site plans within the project will contain some or all of the following trip reduction techniques. Such techniques will be specified upon submittal of site plans to the City of Ontario.

Facility Improvements (on-site):

- Provision of bicycle lockers;
- Provision of on-site bus benches, bus shelters and bus turnouts;
- Reservation of high occupancy vehicle parking areas in convenient locations;

Employee Benefits:

- Preferential parking for carpools/vanpools;
- Distribution of incentives and subsidies, such as bus passes and similar incentives;
- Prizes for participation in carpools/vanpools;
- Alternative work hours and flex time options;
- Provision for telecommuting.

Alternative Transportation Modes:

- Bus (public or private transit);
- Train (when and if available);
- Carpooling/vanpooling;
- Bicycling.

As part of the Trip Reduction plan, the transportation coordinator shall ensure that all employees are aware of the Trip Reduction Plan and the various incentives and programs available.

3.5 Grading Concept

Existing site conditions consist of undeveloped land, gently sloping in a southeasterly direction. Consisting of loose soil, the site is covered with seasonal vegetation and a few trees that may be removed during grading operations. Drainage for the site consists generally of sheet flow in a southeasterly direction towards Francis Street, where it is collected in existing City storm drain systems.

Grading for the project will consist of both mass and precise grading. The site is to be developed in multiple phases, with large areas landscaped to create a visual statement that will also accommodate future phased construction.

The initial phase of the grading concept calls for the construction of phase I of the NAPLD facility, along with roadways connecting to Jurupa Street, Milliken Avenue and Francis Street as well as truck loading areas and parking areas. Small amounts of grading will also occur adjacent to Milliken Avenue and Jurupa Street to allow for installation of streetscape improvements and sidewalks. This area, identified as Planning Areas 1 and 2 on **Exhibit 25**, will be designed such that no importation of soil from outside of the site will be required.

The remaining phased construction of the regional distribution facility (~~in Planning Area 3~~) and the ~~research and development offices~~[development of Planning Area 1](#) will proceed as the need is encountered and will utilize on-site materials when possible. The preliminary earthwork estimates for Phase 1 of development (Planning Areas 1 and 2) will require approximately 290,000 cubic yards of material to be handled. The future of development of Planning Area 1 will require approximately 30,000 cubic yards and Planning Area 3 will require approximately 140,000 cubic yards. The total estimated quantity of soil to be handled is approximately 460,000 cubic yards.

Prior to issuance of building permits, the developer must obtain permits for dust control from the City of Ontario as well as the San Bernardino County Department of Agriculture. The notice of intent for the NPDES permit will also be required. Grading operations shall meet or exceed all Federal, State, and local NPDES requirements.

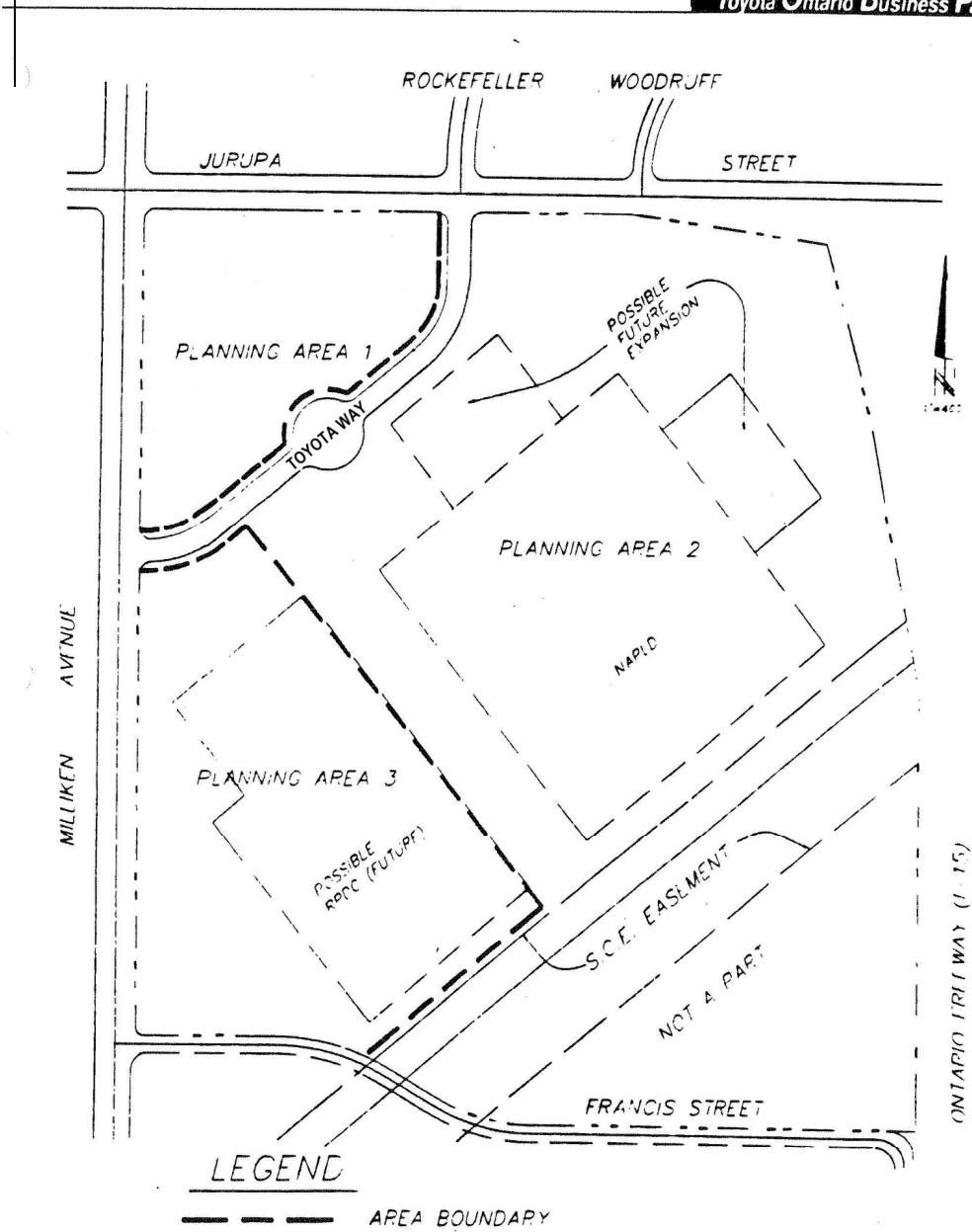
The Chino Basin Municipal Water District has a large Interceptor Relief Sewer line in an easement along the south easterly boundary of the site. Their review and approval will be required prior to the start of any of the grading operations.

3.6 Infrastructure and Public Services

Presented below are descriptions of the existing water, sewer, and storm drain systems in the vicinity of the project site as well as the improvements that will be required to accommodate the proposed project. Technical master plans for infrastructure components for T/OBP have been printed separately.

So that improvements will be sized conservatively, the water and sewer master plans assume that the facilities built will have an industrial use rather than a mix of industrial and office/research and development uses. The industrial land use designation yields higher values for water demand as well as for wastewater generation.

Toyota Ontario Business Park



Scale 1"=300'

Exhibit 25
Concept Grading



December of 1989). However, several factors make it likely that the assumptions and conclusions contained in this earlier study may be subject to modification. First, the City's current study of the entire water system could result in new data for existing system segments. In addition, changes to the siting of buildings have required the reconfiguration of the previously designed system for the site.

3.6.1.1 Existing Water Systems

The City of Ontario currently serves the site with a 16" line in both Milliken Avenue and Jurupa Street, as well as a 12" line north of the street centerline in Francis Street. These lines are within the City of Ontario's Eighth Street System.

There is also an 18" line in Milliken Avenue and a 12" line south of the street centerline in Francis Street that are within the City's proposed Phillips Street System. This system operates at a lower pressure than the Eighth Street System and will ultimately be supplied by a reservoir to be located near the Interstate 10 Freeway and Milliken Avenue. Currently, this system is connected to the Eighth Street System and is supplied by a pressure reducing station located approximately 250' south of Francis Street.

Although the City of Ontario is attempting to provide non-reclaimable water lines for landscape and irrigation uses, no such lines are currently in the vicinity of this project. Therefore, services for landscape and irrigation will be provided by the existing lines bordering the site. Final design of the system will permit connection to any future non-reclaimable water lines.

3.6.1.2 Water System Requirements

A Water Master Plan for T/OBP has been prepared to be used as a guide for the design and construction of the water system for the project. The Water Master Plan also indicates the water facilities necessary to provide service and meet fire flow requirements.

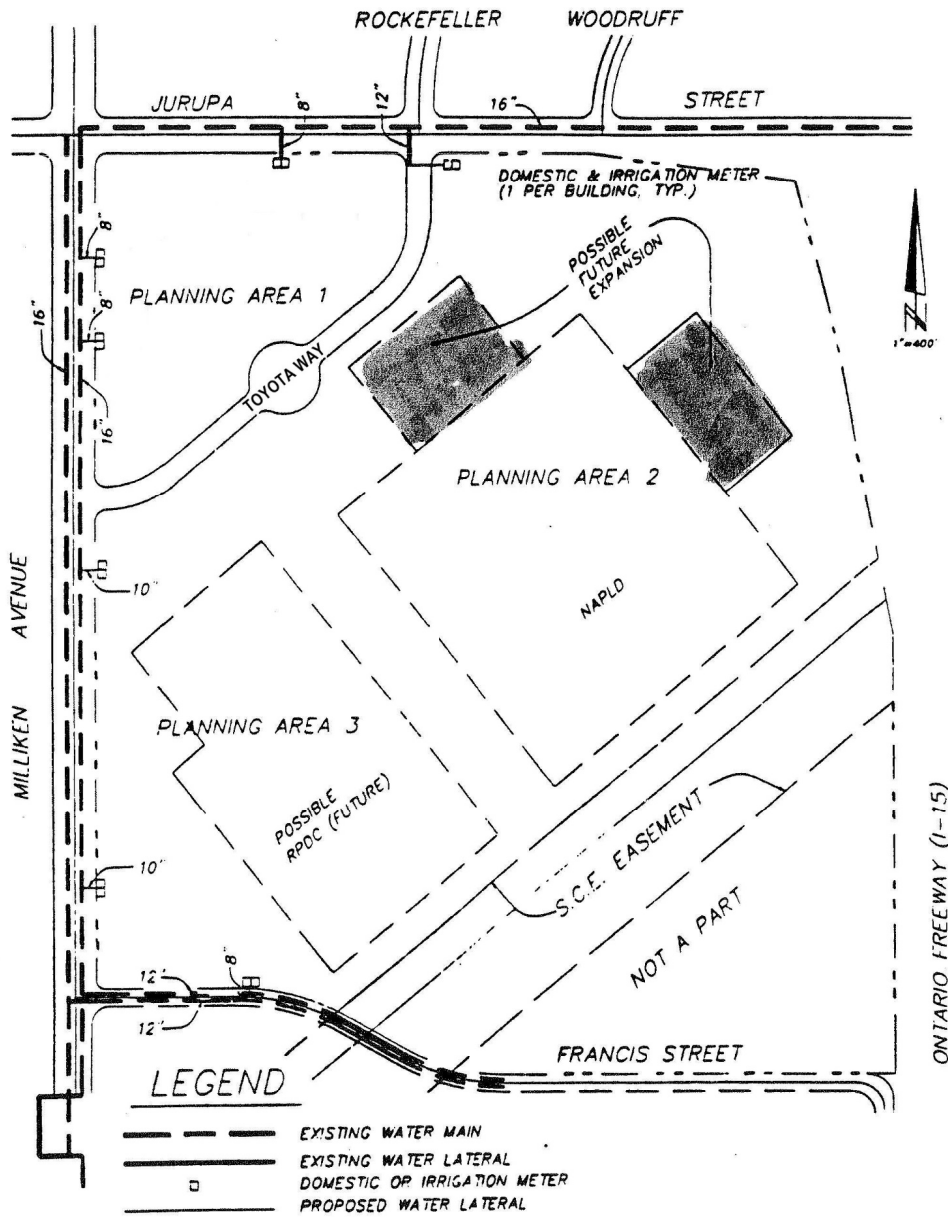
All water facility improvements will be constructed in accordance with the requirements of the City of Ontario. Minimum pressures should be normally above 40 pounds per square inch (PSI) under maximum day conditions. The maximum velocities should not exceed 10 feet per second and the minimum residual pressure allowed is 20 psi for fire flow conditions. Fire hydrant spacing will generally be between 300' and 350'. Fire hydrants will be located in accordance with Ontario Fire Department recommendations.

Fire flow demands of approximately 3,000 gallons per minute (gpm) shall be assumed at two fire hydrant locations (for a total of 6,000 gpm demand) spaced no more than 300 feet apart.

3.6.1.3 Water Master Plan

The services for domestic and landscape/irrigation uses will be provided by the existing Eighth Street system facilities bordering the site. Existing laterals will be utilized when practical and, if none exists where needed, new laterals will be constructed. Each building will be separately metered.

Toyota Ontario Business Park



LEGEND

- EXISTING WATER MAIN
- EXISTING WATER LATERAL
- DOMESTIC OR IRRIGATION METER
- - - - PROPOSED WATER LATERAL

Scale 1"=300'

**Exhibit 26
Concept Water System**



~~If future development of the results in building(s) that The research and development offices located in the northwest corner of the site may be multi-story. If the buildings exceed two stories, booster pumps may be required to insure adequate pressures at the upper stories.~~

Exhibit 26 shows the proposed ultimate water system, in concept format, for domestic and landscape/irrigation service.

The fire protection system will be independent of the domestic system. The proposed system utilizes the 12" Phillips Street System line in Francis Street as its source. The proposed system is looped; the plan will call for a secondary storage tank to be constructed in the southeast portion of the site rather than connecting to an existing main as a second source. Pumps are proposed at both sources to boost the pressures so that adequate fire protection will be provided.

The fire protection system is connected to the Phillips Street system because of the uncertain reliability of the Eighth Street System to provide adequate supplies for use in fire protection. The fire protection system for this site may be revised when the City's on-going study of the entire water system has been completed.

Prior to the issuance of occupancy permits, future applicants shall submit documentation to the City of Ontario Building Department that all appropriate water conservation measures have been incorporated into building and site designs. Compliance with all relevant State laws will be demonstrated, including Title 20 and Title 24. Drought tolerant landscaping, efficient irrigation, and mulching shall be employed where appropriate.

3.6.2 Sewer Master Plan

This site was included in the "Master Plan of Sewer and Water for the Entratter Property" (prepared by Williamson and Schmid in December 1989). However, the development of this site has changed considerably since preparation of that report with the addition of approximately 8 acres in the northwest and southwest corners and the construction of Francis Street along the southerly boundary. The current plan both responds to the current site configuration and complies with restrictions on utilities crossing the Southern California Edison right-of-way.

3.6.2.1 Existing Sewage Conveyance System

The City of Ontario provides sewage collection for this site. The Chino Basin Municipal Water District (CBMWD) accepts the flow from the City lines into their interceptor and relief sewers, which is then conveyed to the CBMWD Regional Treatment Plant 1.

Major existing sewer lines in the project vicinity include an 18" line in Jurupa Street and Milliken Avenues, and a 10" line in Francis Street. The 18" line in Jurupa Street is connected to the 18" line in Milliken Avenue in the intersection of the two streets. However, the manhole was rechannelized when the line in Jurupa Street was extended westerly. Flows that previously flowed southerly in Milliken Avenue now flow westerly in Jurupa Street; only the flows generated by properties fronting Milliken Avenue contribute flow into the 18" line in Milliken Avenue.

The CBMWD had a 66" Interceptor Relief Sewer and a 36" Interceptor Sewer. The 66" line runs adjacent to the Southern California Edison Easement along the southeasterly border of the property, while the 36" line runs along the southerly edge of Francis Street.

A metering manhole located approximately 190 feet south of the centerline of Francis Street limits the capacity of the 18" sewer in Milliken Avenue to 1.28 cfs. The existing 18" sewer south of Francis Street turns easterly through a manhole and decreases to 8". The sewer turns southerly, and increases to 10" to the upstream end of the metering manhole. Upon leaving the metering manhole, the pipe size is again 8" until it connects to a manhole at the intersection with the 66" CBMWD Fontana Interceptor Relief Sewer.

3.6.2.2 Sewage Flows

Average wastewater flows differ by type of land use and by overall building coverage. Unit flow factors used in previous California Commerce Center sewer master plans and previous infrastructure master plans for this site were based on gross acreage. This approach has been used in calculating flows for this site.

In addition, a peaking equation presented in the previous sewer master plans was used to calculate peak flows and form the basis of design for sizing collection sewers. The wastewater flow factors and peaking equations are shown in **Tables 2** and **3**, respectively.

Table 2 Average Wastewater Flow Factors

City of Ontario Sewer Master Plan (gal/ac-day)	California Commerce Center (gal/ac-day)	Entratter/Baxter Site (gal/ac-day)	Toyota NAPLD (gal/ac-day)
4,000	1,950	4,000	4,000

Source: Williamson & Schmid, *Draft- Master Plan of Water and Sewer for the Toyota NAPLD (Ontario)*, 3-22-93.

Table 3 Wastewater Peak Flow Equation

Peaking Equation	Units
$Q_{pk} = 2.42 - 0.235 \ln Q_{avg}$	CFS

Source: Williamson & Schmid, *Draft- Master Plan of Water and Sewer for the Toyota NAPLD (Ontario)*.

3.6.2.3 System Requirements

Design and construction of the sewer system will be completed in accordance with the standards and specifications of the City of Ontario. After calculating peak flows, the sewer lines were sized based on maximum 50 percent full of 8" diameter line and maximum 75 percent full for sizes greater than 8" at peak flow with a minimum velocity of 2 feet per second.

The estimated pipe slope used was based on preliminary finish grade and proposed street alignment. The minimum depth of sewer will be 6 to 7 feet below finished grade. Manholes will be spaced at 300 to 400 feet. All facilities will be built in locations approved by the City. The sewer system has been laid out so that all portions of the site can be served by public sewers, in public rights of way or in easements, without the necessity of pumping.

3.6.2.4 Sewer Plan

Applying the peaking factor to the total average flow for this site yields 1.83 cubic feet per second (cfs). The site configuration allows for the flow to be divided and distributed into two different sewer systems. Each of the systems ultimately flows to CBMWD's Regional Treatment Plant No. 1.

Planning Area 1 was analyzed to consider the development of -Research and Development Offices as a maximum use. Located at ~~occupying~~ the northwest corner of the site, this use would ~~will~~ generate 0.29 cfs, and ~~will~~ flow into the existing 18" sewer in Milliken Avenue. A triangular shaped portion of the southwest corner of the site, although containing only open space at this time, as well as an adjacent area just south of Francis Street has been tabulated to generate 0.14 cfs, and ~~will~~ flow into the existing 10" sewer line in Francis Street. Both of these areas could contain buildings in the future and would best be served by the existing 10" sewer in Francis Street. Should a warehouse building(s) be developed in Planning Area 1 instead of offices, the sewer demand would be less. At the time of entitlement, the specific project will be analyzed to confirm anticipated sewer flow.

The existing 10" line in Francis Street connects to the existing 18" line in Milliken Avenue in the intersection of the two streets. Therefore, the total flow from this site entering the existing 18" line in Milliken Avenue is 0.43 cfs.

In addition to these areas of the project site, the area bounded on the north by Jurupa Street, on the south by the future extension of Francis Street, on the east by Milliken Avenue, and on the west at the midpoint between Milliken and Dupont Avenues was calculated to flow into the existing 18" line in Milliken. Applying the generation factor to the area of 39.4 acres, yields a peaked flow of 0.66 cfs. This flow, combined with the 0.43 cfs generated from this project site, totals 1.09 cfs.

The proposed NAPLD and regional distribution facility will flow into the existing 66" CBMWD Fontana Interceptor Relief Sewer. The total flow from all of these facilities will be 1.35 cfs.

A request for service to connect a new facility to an interceptor or relief sewer is initiated by the municipality being served rather than individual users. The City of Ontario must submit a formal letter to the CBMWD Board of Directors requesting this type of connection.

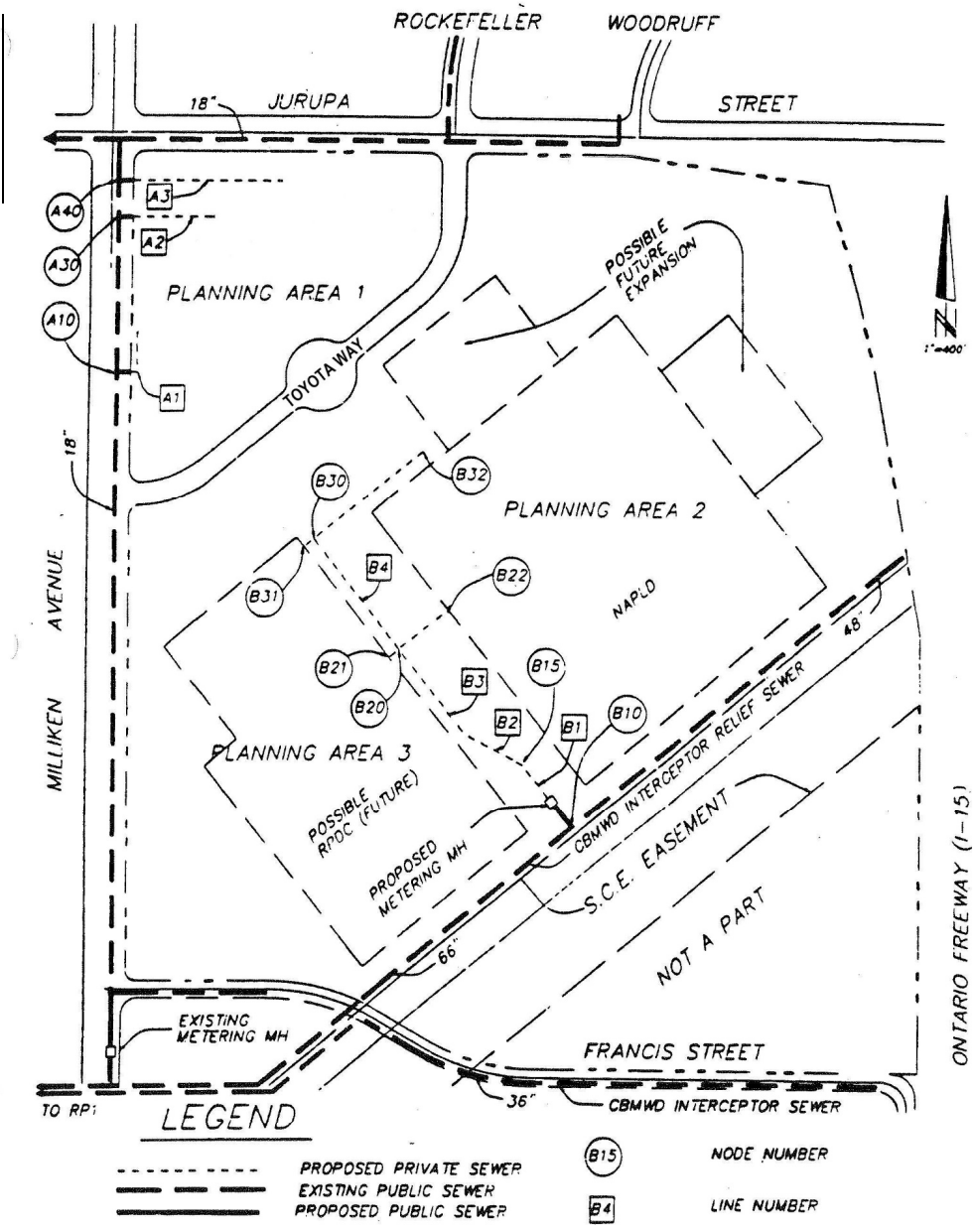
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Toyota Ontario Business Park



LEGEND

	PROPOSED PRIVATE SEWER		NODE NUMBER
	EXISTING PUBLIC SEWER		LINE NUMBER
	PROPOSED PUBLIC SEWER		

Scale 1"=300'

**Exhibit 27
Concept Sewer System**



3.6.2.5 Sewer Master Plan

Exhibit 27 shows the proposed sewer concept plan for the Toyota/Ontario Business Park, including existing facilities, pipe sizes for the backbone system and related components. The limited capacity of the existing 18" line in Milliken Avenue has not been exceeded.

3.6.3 Storm Drainage System

This master plan serves as a guide for design and construction of drainage systems for this project. This master plan considers the existing and future regional drainage facilities as well as the drainage from this site and its effect on existing master planned facilities.

3.6.3.1 Methodology

This drainage study follows the procedures outlined in the 1986 San Bernardino County Hydrology Manual. All of the watersheds studied for this site encompass less than one square mile in area and therefore the rational method has been used for the hydrologic analysis. Based on a 1985 rainfall study in the City of Ontario, a slope of 0.55 for the intensity-duration curve will be used in lieu of the 0.60 slope provided in the Hydrology Manual.

3.6.3.2 Existing Storm Drain Systems

The existing storm drain facilities, as shown on **Exhibit 2728**, will be utilized to drain the project site. The facilities consist of the following:

A. Existing 90" Reinforced Concrete Pipe

This system is in [Rockefeller Avenue Toyota Way](#), crosses Jurupa Street and turns easterly, then it runs easterly along the southerly side of Jurupa Street before turning southerly along the Interstate 15 Freeway, then it runs southerly parallel to the Interstate 15 Freeway before turning easterly at the Southern California Edison easement, it then continues easterly, across the Interstate 15 Freeway as an 8'x8' reinforced concrete box, where it terminates at the County of San Bernardino's Wineville Detention Basin.

B. The Baxter Storm Drain System-West

This system consists of 48" reinforced concrete pipe in Francis Street that drains ultimately to the Philadelphia Street Master Planned Storm Drain System.

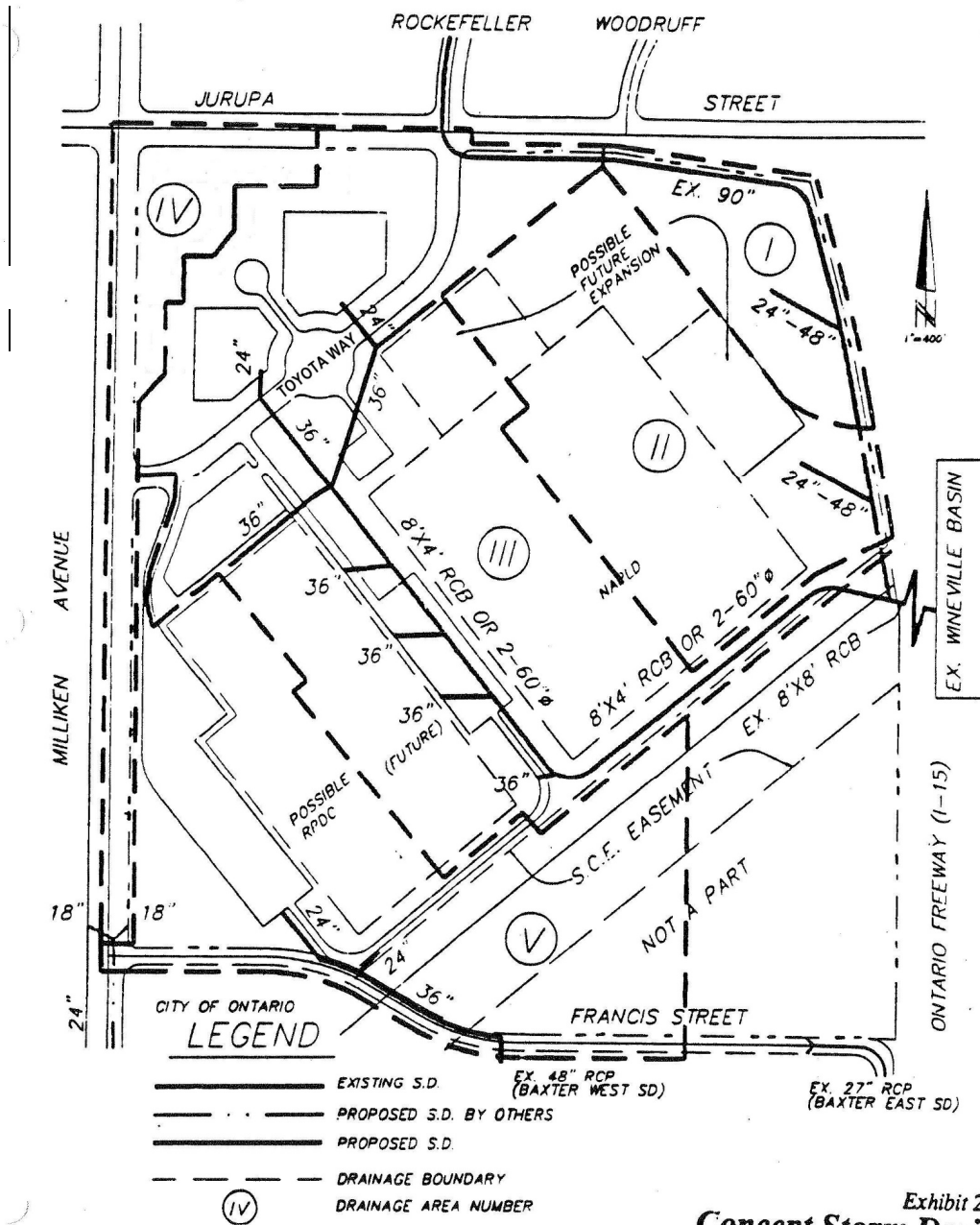
C. The Baxter Storm Drain System-East

This system consists of a 27" reinforced concrete pipe in Francis Street that ultimately drains to the Philadelphia Master Planned Storm Drain System.

D. The City of Ontario AD 106 Storm Drain System

This proposed system consists of two 18" reinforced concrete pipes connecting to a 24" reinforced concrete main line in Milliken Avenue at Francis Street. this system will ultimately connect to the storm drain facility in Philadelphia Street.

Toyota Ontario Business Park

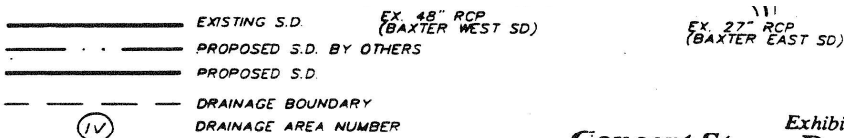


Scale 1"=300'

Exhibit 28
Concept Storm Drain System



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Scale 1"=300'

Exhibit 28
Concept Storm Drain System



3.6.3.3 Storm Drain Master Plan

The site has been segmented into five major contributing drainage areas. These areas, as shown on **Exhibit 28**, will drain to the existing or proposed facilities as described in Section 3.6.3.2. Areas I, II, and III will drain to the existing 90" RCP that runs along the Interstate 15 Freeway. The total tributary acreages in the 1981 drainage report was approximately 73 acres. It is anticipated that a total of 73 acres will now drain into the 90" RCP. Area IV will drain to the proposed City of Ontario AD 16 storm drain system in Milliken Avenue. This site will contribute 6.7 acres or 13.8 cfs for Q100, which is less than the Q100 value of 19.0 cfs for which the system has been designed.

Area V, as shown on **Exhibit 28** will drain to the Baxter Storm Drain System-West. This system is 48" and has been designed to accept 89 cfs. The new tributary area is approximately 38 acres with a peak flow at Q100 of 86 cfs.

The site grading and ultimate location of the buildings may require a minor adjustment to these five tributary areas; however, they generally conform with the existing or proposed system's design capacities. The Baxter Storm Drain System-East will not be utilized for any of this site's drainage needs.

3.7 Community Facilities

3.7.1 Fire Protection

Fire protection service to the site is provided by the Ontario Fire Department, which is headquartered at 425 East "B" Street. The Department also currently maintains six other fire stations throughout the community. In addition to fire suppression, the Department offers emergency medical and rescue services, fire code compliance, and inspection services.

The closest fire station to the project site is located at 5400 East Jurupa Street, east of the I-15 freeway, which is equipped with one engine/paramedic company and one ladder company.

Prior to the approval of the individual site plans within the project area, a Master Fire Protection Plan shall be submitted to and approved by the Ontario Fire Department which will detail specific fire protection measures to be included within the site plan. The Master Fire Protection Plan shall address:

- Adequate interior sprinkler systems, smoke detectors or other fire suppression systems.
- Location and testing of fire hydrants and fire extinguishers.
- Identification of and methods for handling and storage of potentially hazardous materials.
- Adequate access and turning radii for emergency vehicles

Individual site plans within the T/OBP will be subject to City Ordinance No. 2491, requiring the payment of fire facility and fire equipment impact fees.

3.7.2 Police Protection

Police Protection is provided by the Ontario Police Department which is headquartered at 200 North Cherry Street in the Ontario Civic Center. The Police Department presently employs a force of 268 personnel, of which 186 are sworn officers. All future construction within the project site will comply with applicable provisions of City of Ontario Ordinance No. 2482, Security Standards for Buildings.

3.7.3 Solid Waste Disposal

The City of Ontario provides solid waste disposal services to the site, which includes periodic pick-up of waste material and transportation to the County's Milliken Landfill facility. Trash enclosures will be constructed within the project area, with the number, location, and size of the enclosures to be determined by the City of Ontario Public Service Agency at the time of the site plan review.

The Toyota/Ontario Business Park will be subject to solid waste reduction programs currently being developed by the City pursuant to the requirements of AB 939.

3.7.4 Maintenance

Maintenance of utilities and related facilities within public rights-of-way, including traffic signalization, street paving, lane striping, street signs, and street lights is the responsibility of the City of Ontario.

Landscape and hardscape features, both on private property and within adjacent street rights-of-way are maintained by the property owner.

Major water, sewer and storm drainage facilities within public rights-of-way or within dedicated easements are maintained by the City of Ontario. Telephone, electric and natural gas facilities are to be maintained by their respective providers.

3.8 Phasing

The Toyota/Ontario Business Park is anticipated to be built in multiple phases. The initial phase will consist of approximately 872,000 square feet as the first increment of the NAPLD project within Planning Area 2 (shown on **Exhibit 3**). Additional phases will be built, but no projections exist as to the timing or the square footages of the increments to be built.

It is anticipated that all of the off-site improvements, including street improvements, traffic control devices, major utilities, including fire hydrants, sidewalks, and streetscape improvements will be installed in conjunction with the initial phase of project construction. On-site facilities to be built as the first phase includes a portion of the NAPLD building, described above, parking lots, building, and parking lot landscaping and utility connections to major utility systems (sewer, water, storm drainage etc.)

Minor extensions to on-site improvements, including water, sewer, storm drains, sidewalks, and building landscaping, will be constructed as additional buildings are constructed.

4.0 Design Guidelines

4.1 Overview

This portion of the plan sets forth guidelines to assure aesthetically pleasing and functional design for all on-site improvements, including main and accessory buildings and related improvements, including lighting and similar amenities.

4.2 Urban Design Concepts

Included in this section are guidelines for construction within the [Office/R&D-Industrial Mixed Use](#) and the Warehouse/Distribution land use classifications.

4.2.1 Warehouse/Distribution

4.2.1.1 Material

Consistent with the need to provide tall, high-volumetric clear span areas to maximize storage, warehousing and distribution buildings will consist of tilt-up concrete construction with minimally sloping roofs. Building entrances and office frontages will be highlighted with accent material which could include architectural panels and expanses of window and glass areas painted, extruded aluminum frames. Architectural panels will consist of composite aluminum, steel, painted or textured concrete, cement plaster, or similar products. Building entrance and office frontage architectural treatment will be enhanced with enriched landscaping, which is described in Section 3.3 of the Specific Plan.

Materials which will not be used include exposed wood, brick, or stucco.

4.2.1.2 Building Design

A significant effort will be expended to minimize large, flat expanses of unarticulated or undifferentiated wall surfaces. To achieve this objective, distinctive architectural reveals and recesses will be integrated into walls and/or architectural panels. Similar elements or other treatments could be placed at strategic locations to create visual interest and scale to the buildings. Architectural elements or landscape masses will be used to break up or soften large expanses of unarticulated wall surfaces.

All exterior walls and surfaces will either be painted, sandblasted, or the concrete tinted or dyed.

Parapets will extend above the rooflines, unless such elements are treated to create an architectural statement. All other roof-mounted mechanical equipment will be screened as noted in the Specific Plan.

Office components and primary entrances of warehouse and distribution buildings will receive special architectural and landscape treatment to differentiate these particular areas and to direct visitor traffic to these points.

Consistent with Ontario Planning Commission Resolution No. 2392, special attention will be given to the design and treatment of warehouse elevations fronting the I-15 Freeway. Such special treatment will consist of detailed facades, use of texturing, trellises or other architectural or graphic design elements together with enriched landscaping and similar features. Although the Resolution requires the placement of building entries to front on the freeway, such an orientation is not feasible for this site given the location of the Jurupa overcrossing, which essentially obscures much of the site from the freeway with changes of grade, and that the site has limited amount of frontage adjacent to the I-15 Freeway.

A copy of Planning Commission Resolution No 2393 is located in the Appendix.

4.2.1.3 Colors and Textures

Primary building colors will be determined by the intrinsic qualities of the building finish material. Appropriate complementary colors may be used as accents for reveals, window, and door trim and similar features. Accent colors may be used at project entries, the warehouse office area and at other locations requiring special treatment, such as freeway elevations.

The color palette chosen for warehouse and distribution buildings shall be complementary with other buildings on the project site, including fences and walls, light standards, accessory buildings, sign structures and other structures.

Texturing may be used to achieve the desired level of building articulation, including sandblasting, "ribbing," and use of exposed aggregate material. The scale of these elements will be appropriate for the design.

4.2.2 Office/Research and Development-Industrial Mixed Use

4.2.2.1 Material

Similar to warehouse and distribution buildings, offices and R&D buildings could be built of tilt-up concrete, precision block, architectural metal panel systems, cement plaster, or pre-cast concrete. Flat roofs with parapets could also be employed. Exposure of sloping metal roofs, when integral to the design, may also be used. Unlike warehouse building, a predominant feature of office structures will be windows and expanses of plate glass in extruded aluminum, painted frames. Special accent material will be used at office entrances and to enhance frontages adjacent to Milliken Avenue, Jurupa Street and the I-15 Freeway. [Warehouse and distribution buildings in this district will follow the guidelines under 4.2.1.](#)

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Exposed wood and brick are prohibited building materials.

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4.2.2.2 Building Design

[If a complex of Office and Research and Development buildings will be developed to be consistent with a campus-like setting, is proposed in this district,](#) dominant design elements will likely be horizontal, including appearance of the ground floor level and building entrances through the use of recessed entries, architectural panels, canopies, and enriched landscaped treatment.

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All exterior surfaces will be painted, sandblasted, finished metal or the concrete will be treated as noted in the next section.

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Similar to warehouse structures, parapets will extend above the roofline. Rooftop mounted mechanical and plumbing equipment will be appropriately screened.

4.2.2.3 Colors and Textures

Use of color in the office buildings as in the warehouse structures will be complementary, although not necessarily identical to other buildings within the T/OBP project, to create a harmonious effect. Special texturing may be used, such as sandblasting and exposed aggregate.

4.3 Lighting

A Master Lighting Plan will be submitted to the City of Ontario for review and approval prior to building permit issuance for the first phase of building construction on the project site. The Master Plan shall contain criteria and standards governing lighting along [Reckefeller Avenue](#) [Toyota Way](#) parking lot lighting, lighting within parking lots and access drives and lighting improvements for pedestrian walkways. The Master Plan will also establish minimum illumination criteria consistent with City of Ontario policies on exterior illumination.

Site lighting will be directed inward and downward, to avoid spill over of light and glare onto the adjacent freeway, nearby public streets or onto adjacent properties.

4.4 Fences and Walls

Fences and walls are an integral part of the overall project design. They will be articulated in a manner consistent with building architecture in terms of proportion, materials color and texture. Painted concrete with reveals, metal fencing, both solid or transparent, may be used with the concrete block, when appropriate, and shall be articulated or screened from view by landscape materials.

4.5 Public Art and On-Site Amenities

Construction within the project site shall comply with applicable City of Ontario General Plan policies and implementing ordinances regarding public art. Plans to provide public art will be reviewed and approved by the City of Ontario Development Director prior to issuance of a Certificate of Occupancy for any major building within the Business Park.

Each Building within the Toyota/Ontario Business Park will have access to an outdoor [employee break area](#) [plaza or central courtyard](#).

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5.0 Development Standards

5.1 Overview

This portion of the Specific Plan establishes minimum standards governing development on the project site, including, but not limited to setbacks, Floor Area Ratios, building heights, parking requirements and similar elements.

5.2 Permitted Uses

The following land uses are permitted within the T/OBP Specific Plan, based upon land use category:

5.2.1 ~~Office and Research and Development~~ **Industrial Mixed Use Permitted Uses**

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- Administrative, business, corporate, and professional offices.
- Banks and financial institutions.
- Business services.
- Child care facilities, only for use of on-site employees.
- Communication services.
- Health clubs and spas, as ancillary uses to other permitted uses.
- Manufacturing that is within enclosed building(s).
- Medical clinics.
- Research, testing, assembly and service of components, devices and similar equipment. Research laboratories, development laboratories. And related uses.
- Restaurants, cafeterias and eating establishments, including outdoor eating areas.
- Security facilities intended to limit access to the site.
- Warehousing, storage, and distribution within enclosed buildings or fully screened from a public right-of-way.
- Wholesale establishments.
- All other uses, unless specifically listed above are prohibited unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted use.

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5.2.2 Warehouse/Distribution Permitted Uses

- Administrative and managerial offices as ancillary to other permitted uses.
- Child care facilities, only for use of on-site employees.
- Health clubs and spas, as ancillary uses to other permitted uses.
- Restaurants, cafeterias and eating establishments, only as ancillary to other permitted uses, including outdoor dining areas.
- Warehousing, storage, distribution and classification of parts and materials within enclosed buildings.

- Wholesale establishments.
- All other uses are prohibited ~~unless specifically listed above, are prohibited unless approved by the Zoning Administrator in accordance with the requirements of Ontario Development Code Section 1.02.010 (Interpretations and Land Use Determinations), unless a finding is made by the Planning Commission that the use is similar to and no more objectionable than a permitted use.~~

5.3 Maximum Floor Area Ratio (FAR)

Pursuant to the Ontario General Plan, the maximum Floor Area Ratio (FAR) within the Toyota/Ontario Business Park shall not exceed 0.55, as approved by both the Planning Commission and City Council. Although the FAR on individual parcels may be somewhat higher than this figure, FAR shall be calculated and regulated over the entire property contained within the Specific Plan site.

5.4 Building Height

Maximum building height above finished grade shall not exceed sixty-five (65) feet. Certain encroachments, such as communication antennae, water tanks which are architecturally integrated into the design of the building and similar roof-mounted equipment, shall be allowed with the written approval of the Ontario City Planner.

5.5 Minimum Parcel Size

All parcels or lots created within the Specific Plan shall have a minimum size of one (1) acre.

5.6 Building and Parking Setbacks

5.6.1 Building Setbacks

The following building setbacks shall be maintained within the Specific Plan area.

- Adjacent to Jurupa Street: Forty (40) feet
- Adjacent to Milliken Avenue: Forty-Five (45) feet
- Adjacent to Francis Street: Forty-Five (45) feet
- Adjacent to ~~Rockefeller Avenue~~ Toyota Way: Thirty-Five (35) feet
- Along Interior Property Lines: Five (5) feet

These setbacks apply to front, side, and rear yard conditions. Setbacks shall be measured from the nearest property line with the exception of ~~Rockefeller Avenue~~ Toyota Way which is a private street. Setbacks along ~~Rockefeller Avenue~~ Toyota Way shall be measured from the curb line adjacent to the roadway. Setback shall be measured to the face of building and shall exclude minor architectural features such as awnings, downspouts, and similar ancillary features.

No structures, other than the following exceptions, shall be permitted to exist within the building setback area. Exceptions include drive approaches, vehicular parking (subject to

parking setback standards, listed in the next section), public or quasi-public utility improvements, project entry signs and improvements (as described in Section 3.3 of the Specific Plan), public transit facilities, utility and infrastructure appurtenances, temporary real estate signs, pedestrian walkways and ancillary facilities such as guard houses and water pump houses not exceeding a height of twelve (12) feet and a floor area of four hundred (400) square feet.

5.6.2 Parking Setbacks

The following parking setbacks shall be maintained:

- Jurupa Street: Twenty-five (25) feet
- Milliken Avenue: Twenty-five (25) feet
- Francis Street: Twenty-five (25) feet
- Toyota Way: Twenty-five (25) feet
- Along Interior Property Line: Five (5) feet

The same uses as noted in section 4.6.1 are permitted within parking setback areas as well, with the exception of vehicular parking.

5.6.3 Freeway Setback

Adjacent to the Ontario (I-15) Freeway, a minimum twenty (20) foot parking and building setback shall be established and maintained.

5.7 On-Site Landscaping

All parcels within the Toyota/Ontario Business Park Specific Plan area shall contain a minimum of ten (10) percent on-site landscaping, which shall include:

- All required parking setbacks;
- A minimum of five (5) percent landscaping within parking areas;
- A minimum of twelve (12) feet of landscaping adjacent to warehousing and distribution buildings, except at loading docks and service areas. (Note: minor architectural features may project no greater than two feet into the required landscaped area).

Pedestrian walkways, minor utility installations and drive accessways may be located within these areas and shall be counted as part of the landscaped area.

5.8 Parking and Loading

Parking and loading within the project site shall comply with City of Ontario standards in effect at the time individual site plans are submitted for City review and approval.

5.9 Signs

A Master Sign Plan will be submitted to the City of Ontario for review and approval prior to or concurrently with the submittal of the first site plan on the project site. The Signage Master Plan shall contain criteria and standards governing the number, size, height and placement of both temporary and permanent signs within the complex, including signs for both warehouse and distribution uses and for the industrial mixed use portion of the project.

5.7 — On Site Landscaping

5.8 — Parking and Loading

5.9 — Signs

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5.10 Outdoor Storage and Screening Requirements

Outdoor storage is not permitted within the project. This does not include truck trailers.

All rooftop equipment, such as primary HVAC equipment, shall be fully screened from adjacent streets and from the I-15 Freeway with building parapets, architectural screening or shall be expressed as an overall part of the design concept. Screening of mechanical equipment shall be architecturally integrated with the design and materials of the project. Other, similar methods may also be approved by the Ontario Planning Department.

5.11 Performance Standards

5.11.1 Air Quality

No operation or activity shall cause the emission of smoke, fly ash, fumes, dust, vapors, gasses, or other forms of air pollution which has the probability of damage to human health, vegetation, or other forms of property or which can cause excessive soiling to adjacent properties. No emission shall be permitted which exceeds the requirements of the South Coast Air Quality Management District or any related requirements adopted by the City of Ontario.

5.11.2 Electrical or Electrical Interference

No operation or activity shall cause any source of electrical or electronic disturbance that adversely affects persons or the operation of any equipment on adjacent parcels of land that is not in conformance with FCC regulations.

5.11.3 Light and Glare

No lighting fixture shall create any illumination which exceeds five foot candles on adjacent parcels of land, whether such illumination is direct or indirect. Glare levels shall be measured with a photoelectric photometer following standard spectral luminous efficiency curves adopted by the International Commission of Illumination.

5.11.4 Mechanical and Electrical Equipment

All mechanical and electrical equipment, such as air conditioners, antennas, pumps, transformers, heating and ventilating equipment, and similar equipment, shall be located and operated in a manner that does not disturb adjacent uses and activities.

5.11.5 Noise and Sound

Unless otherwise specified, loudspeakers, bells, gongs, buzzers, or other noise attention or attracting devices shall not exceed 60 decibels at any one time beyond the boundaries of the subject property.

5.11.6 Airport Noise

A portion of the project site lies within the 65 CNEL noise contour limit generated by Ontario International Airport. Prior to issuance of building permits on any parcel of land within the 65 CNEL contour level, an acoustical report shall be prepared and submitted to the City along with final building plans. The acoustical report shall contain specific recommendations to reduce interior noise within all buildings to acceptable levels as identified in the Noise Element of the Ontario General Plan.

5.11.7 Odors

No operation or activity shall be permitted which emits odorous gasses or other odorous matter in such quantities as to be dangerous, injurious, noxious, or otherwise objectionable to a level that is detectable with or without the aid of instruments at or beyond the property within which odor is detected.

5.11.8 Vibration

No operation or activity shall be permitted to cause a steady, earth-borne oscillation which is continuous and occurring more than 100 times per minute beyond the property within which the vibration was created. Ground vibration caused by moving vehicles, trains, aircraft, or temporary construction or demolition is exempted.

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6.0 Administration

6.1 Overview

This Chapter Describes how the Specific Plan is to be implemented and amended, if necessary.

6.2 Site Plans and Parcel Maps

The two primary methods for implementing the Toyota/Ontario Business Park Specific Plan are site plans and parcel maps. Applications for site plans shall be prepared for each individual building or structure within the project area. The format, content and submittal requirements for site plans shall be determined by the City of Ontario Planning Department and shall be accompanied by appropriate fees and necessary environmental documentation. In addition, a sight line analysis shall accompany submittals for warehouse and distribution building site plans to ensure that appropriate screening is provided to obscure views to truck docks and loading areas.

Parcel maps, or subdivision maps, may also be permitted for land division purposes. Applications for parcel map approval shall also be submitted to the Ontario Planning Department in a form prescribed by the City along with required and other supporting documentation required by the City. All new parcels created within the project area shall comply with minimum lot size requirements and other standards set forth in this Specific Plan.

Both site plans and parcel maps shall be reviewed by the Development Advisory Board (DAB). If approved by the DAB, site plans shall not require additional review by the City of Ontario unless an appeal is filed in the prescribed manner. Parcel maps shall be reviewed by the Planning Commission following DAB review. Once Planning Commission approval is received, no further City review is required unless an appeal is filed in the prescribed manner.

6.3 Environmental Review

An Environmental Impact Report (EIR) is in process of being prepared to assess the environmental impacts of this project (City of Ontario EIR NO. 93-1). Once a Final EIR is certified by the City of Ontario, no additional EIRs will be required for implementing site plans or parcel maps, so long as such plans and maps are consistent with the Specific Plan. A Notice of Intent may be required to be filled with site plans and parcel maps, as determined by the Ontario Planning Department.

6.4 Substantial Conformance

Substantial Conformance is a process established in the Specific Plan to allow a limited degree of flexibility for both the applicant and City of Ontario in the implementation of the project. Through this process, minor modifications may be made in certain technical components of the Specific Plan. Substantial Conformance may include, but is not limited to, modifications to infrastructure improvements, public service and facility improvements, landscape materials, location of signs and similar issues. The Substantial Conformance process shall not be used to modify development regulations, basic design concepts or to exceed the maximum development intensity cap established in the Specific Plan.

Determination of Substantial Conformance may be made by the DAB when the following findings can be made:

- The proposed modification complies with the goals and intent of the Specific Plan;
- The modification does not create adverse impacts on infrastructure such as sewer, water, storm drainage, or circulation Systems;
- The modification does not create adverse impacts on public facilities or community services;
- The modification does not represent an increase in density or intensity allowed in the Specific Plan;
- Subsequent technical studies and analysis substantiate the need for the modification;
- Other changes have occurred either on- or off-site which necessitate a minor modification.

6.5 Specific Plan Amendments

Amendments to the Specific Plan may be submitted to the City of Ontario which exceed the scope and intent of the Substantial Conformance Process. Specific Plan Amendments are governed by Section 65500 of the California Government Code.

RESOLUTION NO.

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF ONTARIO APPROVING AN ADDENDUM TO THE ONTARIO PLAN ENVIRONMENTAL IMPACT REPORT, PURSUANT TO THE REQUIREMENTS OF THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AS AMENDED, FOR FILE NO PSPA19-004

WHEREAS, MIG (hereinafter referred to as "Applicant") has filed an Application for the approval of a Specific Plan Amendment, File No. PSPA19-004, which consists of An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway., in the City of Ontario, California (hereinafter referred to as "Application" or "Project"). The Specific Plan Amendment will revise the current land use district covering Planning Area 1, from Office/Research and Development ("Office/R&D") to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in addition to the currently allowed Office/R&D land uses. Planning Area 1 is approximately 14.4 acres in size and is located at the northwest corner of the Specific Plan area. Furthermore, the proposed Amendment will update the Specific Plan's landscape palette to conform to current California friendly landscape practices, which will apply to any future development within the Specific Plan Area; and

WHEREAS, The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140) was certified on January 27, 2010 (hereinafter referred to as "Certified EIR"), in which development and use of the Project site was discussed; and

WHEREAS, the Planning Director of the City of Ontario has prepared and approved for attachment to the certified Environmental Impact Report, an Addendum to the Certified EIR (hereinafter referred to as "EIR Addendum") in accordance with the requirements of the California Environmental Quality Act of 1970, together with State and local guidelines implementing said Act, all as amended to date (collectively referred to as "CEQA"); and

WHEREAS, the EIR Addendum concluded that implementation of the Project could result in a number of significant effects on the environment that were previously analyzed in the Certified EIR, and that the Certified EIR identified mitigation measures that would reduce each of those significant effects to a less-than-significant level; and

WHEREAS, pursuant to State CEQA Guidelines Section 15164(a), a lead agency shall prepare an addendum to a previously certified EIR if some changes or additions are necessary to a project, but the preparation of a subsequent or supplemental EIR is not required; and

WHEREAS, the City determined that none of the conditions requiring preparation of a subsequent or supplemental EIR would occur from the Project, and that preparation of an Addendum to the Certified EIR was appropriate; and

WHEREAS, the City of Ontario is the lead agency on the Project, and the Planning Commission is the recommending authority for the requested approval to construct and otherwise undertake the Project; and

WHEREAS, the Planning Commission has reviewed and considered the EIR Addendum for the Project, has concluded that none of the conditions requiring preparation of a subsequent or supplemental EIR have occurred, and intends to take actions on the Project in compliance with CEQA and state and local guidelines implementing CEQA; and

WHEREAS, the EIR Addendum for the Project are on file in the Planning Department, located at 303 East B Street, Ontario, CA 91764, are available for inspection by any interested person at that location and are, by this reference, incorporated into this Resolution as if fully set forth herein; and

WHEREAS, all legal prerequisites to the adoption of this Resolution have occurred.

NOW, THEREFORE, IT IS HEREBY FOUND, DETERMINED, AND RESOLVED by the Planning Commission of the City of Ontario, as follows:

SECTION 1: Environmental Determination and Findings. As the recommending authority for the Project, The Planning Commission has reviewed and considered the information contained in the administrative record for the Project. Based upon the facts and information contained in the administrative record, including all written and oral evidence presented to the Planning Commission, the Planning Commission finds as follows:

(1) The environmental impacts of this project were reviewed in conjunction with an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), certified by the Ontario City Council on January 27, 2010 in conjunction with File No. PGPA16-001.

(2) The EIR Addendum and administrative record have been completed in compliance with CEQA, the State CEQA Guidelines, and the City of Ontario Local CEQA Guidelines; and

(3) The City's "Guidelines for the Implementation of the California Environmental Quality Act (CEQA)" provide for the use of a single environmental

assessment in situations where the impacts of subsequent projects are adequately analyzed. This Application introduces no new significant environmental impacts; and

(4) All previously adopted mitigation measures shall be a condition of project approval, as they are applicable to the Project, and are incorporated herein by this reference; and

(5) The EIR Addendum contains a complete and accurate reporting of the environmental impacts associated with the Project, and reflects the independent judgment of the Planning Commission; and

(6) There is no substantial evidence in the administrative record supporting a fair argument that the project may result in significant environmental impacts.

SECTION 2: Additional Environmental Review Not Required. Based on the Addendum, all related information presented to the Planning Commission, and the specific findings set forth in Section 1, above, the Planning Commission finds that the preparation of a subsequent or supplemental Environmental Impact Report is not required for the Project, as the Project:

(1) Does not constitute substantial changes to the Certified EIR that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; and

(2) Does not constitute substantial changes with respect to the circumstances under which the Certified EIR was prepared, that will require major revisions to the Certified EIR due to the involvement of new significant environmental effects or a substantial increase in the severity of the previously identified significant effects; and.

(3) Does not contain new information of substantial importance that was not known and could not have been known with the exercise of reasonable diligence at the time the Certified EIR was certified/adopted, that shows any of the following:

(a) The project will have one or more significant effects not discussed in the Certified EIR; or

(b) Significant effects previously examined will be substantially more severe than shown in the Certified EIR; or

(c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the Project, but the City declined to adopt such measures; or

(d) Mitigation measures or alternatives considerably different from those analyzed in the Certified EIR would substantially reduce one or more significant effects on the environment, but which the City declined to adopt.

SECTION 3: Planning Commission Action. Based upon the findings and conclusions set forth in Sections 1 and 2, above, the Planning Commission hereby recommends the City Council find that based upon the entire record of proceedings before it, and all information received, that there is no substantial evidence that the Project will constitute substantial changes to the Certified EIR, and does hereby approve the EIR Addendum, attached hereto as "Attachment A," and incorporated herein by this reference.

SECTION 4: Indemnification. The Applicant shall agree to defend, indemnify and hold harmless, the City of Ontario or its agents, officers, and employees from any claim, action or proceeding against the City of Ontario or its agents, officers or employees to attack, set aside, void, or annul this approval. The City of Ontario shall promptly notify the applicant of any such claim, action, or proceeding, and the City of Ontario shall cooperate fully in the defense.

SECTION 5: Custodian of Records. The documents and materials that constitute the record of proceedings on which these findings have been based are located at the City of Ontario City Hall, 303 East "B" Street, Ontario, California 91764. The custodian for these records is the City Clerk of the City of Ontario.

SECTION 6: Certification to Adoption. The Secretary shall certify to the adoption of the Resolution.

The Secretary Pro Tempore for the Planning Commission of the City of Ontario shall certify as to the adoption of this Resolution.

I hereby certify that the foregoing Resolution was duly and regularly introduced, passed and adopted by the Planning Commission of the City of Ontario at a regular meeting thereof held on the 28th day of April 2020, and the foregoing is a full, true and correct copy of said Resolution, and has not been amended or repealed.

Jim Willoughby
Planning Commission Chairman

ATTEST:

Cathy Wahlstrom
Planning Director and
Secretary to the Planning Commission

STATE OF CALIFORNIA)
COUNTY OF SAN BERNARDINO)
CITY OF ONTARIO)

I, Gwen Berendsen, Secretary Pro Tempore of the Planning Commission of the City of Ontario, DO HEREBY CERTIFY that foregoing Resolution No. _____ was duly passed and adopted by the Planning Commission of the City of Ontario at their regular meeting held on April 28, 2020, by the following roll call vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAIN:

Gwen Berendsen
Secretary Pro Tempore

ATTACHMENT A:
**Addendum to The Ontario Plan
Environmental Impact Report**

(Attachment follows this page)



City of Ontario
Planning Department
303 East B Street
Ontario, California 91764
Phone: 909.395.2036
Fax: 909.395.2420

California Environmental Quality Act Addendum to The Ontario Plan Environmental Impact Report

Project Title/File No.: PSPA19-004

Lead Agency: City of Ontario, 303 East "B" Street, Ontario, California 91764, (909) 395-2036

Contact Person: Charles Mercier, Senior Planner, 909-395-2425

Project Sponsor: City of Ontario, 303 East B Street, Ontario, CA 91764

Project Location: The project site is located in southwestern San Bernardino County, within the City of Ontario. The City of Ontario is located approximately 40 miles from downtown Los Angeles, 20 miles from downtown San Bernardino, and 30 miles from Orange County. As illustrated on Figures 1 through 3, below, the project site is located on Assessor Parcel Number (APN): 0238-121-75, which is comprised of 95.35 gross acres south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of Interstate 15.

Figure 1: REGIONAL LOCATION MAP

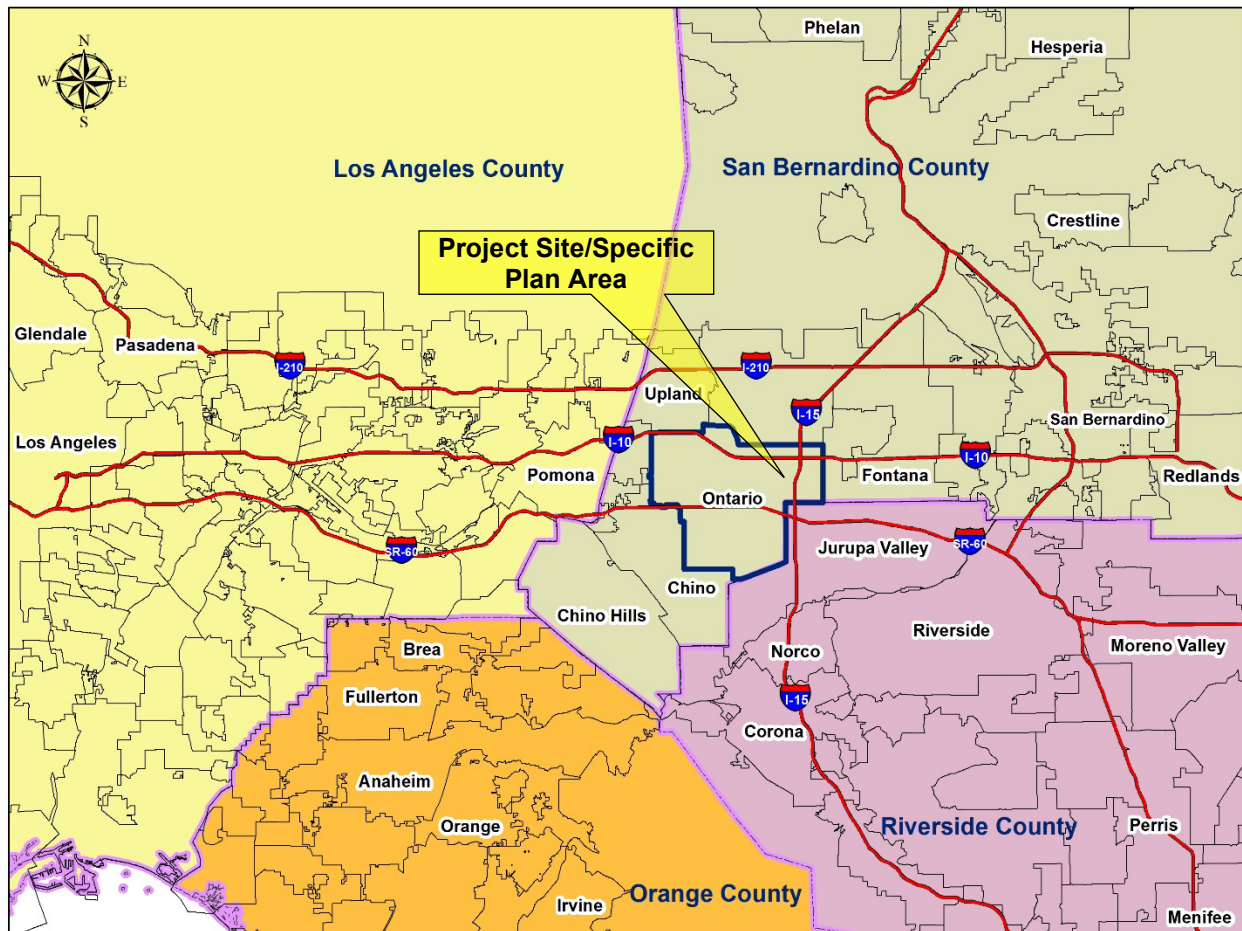


Figure 2: VICINITY MAP

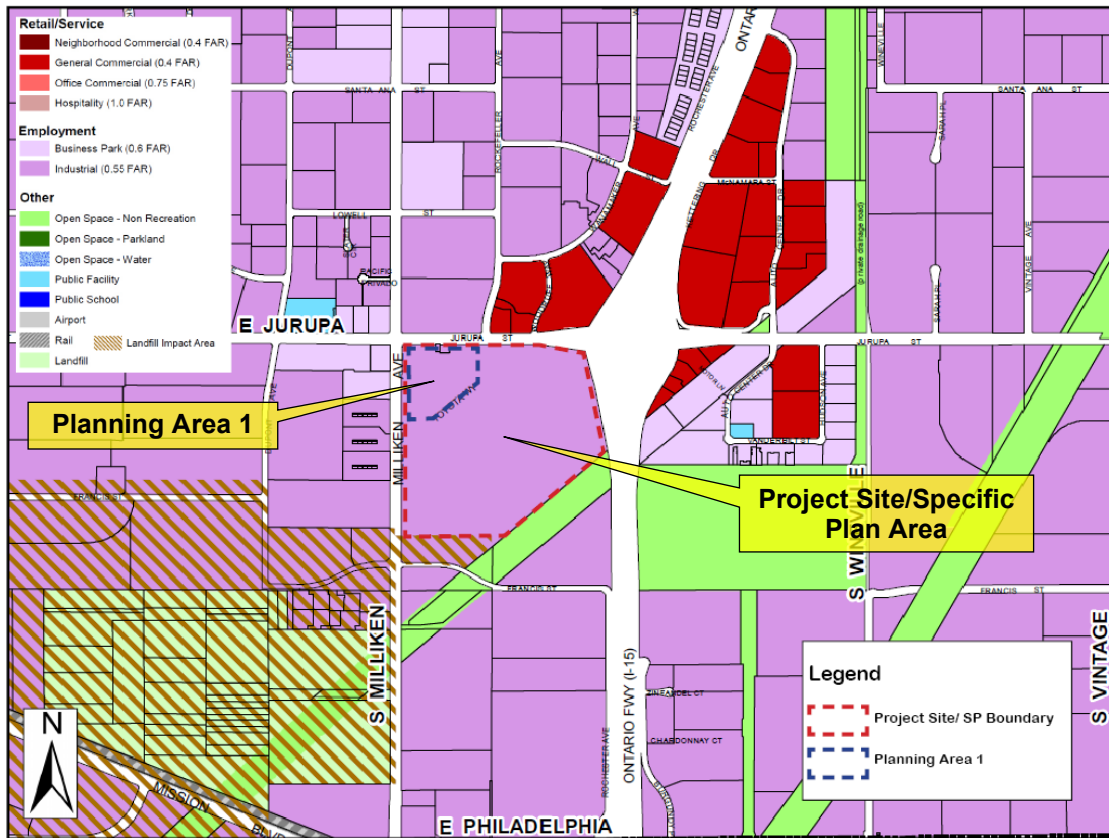
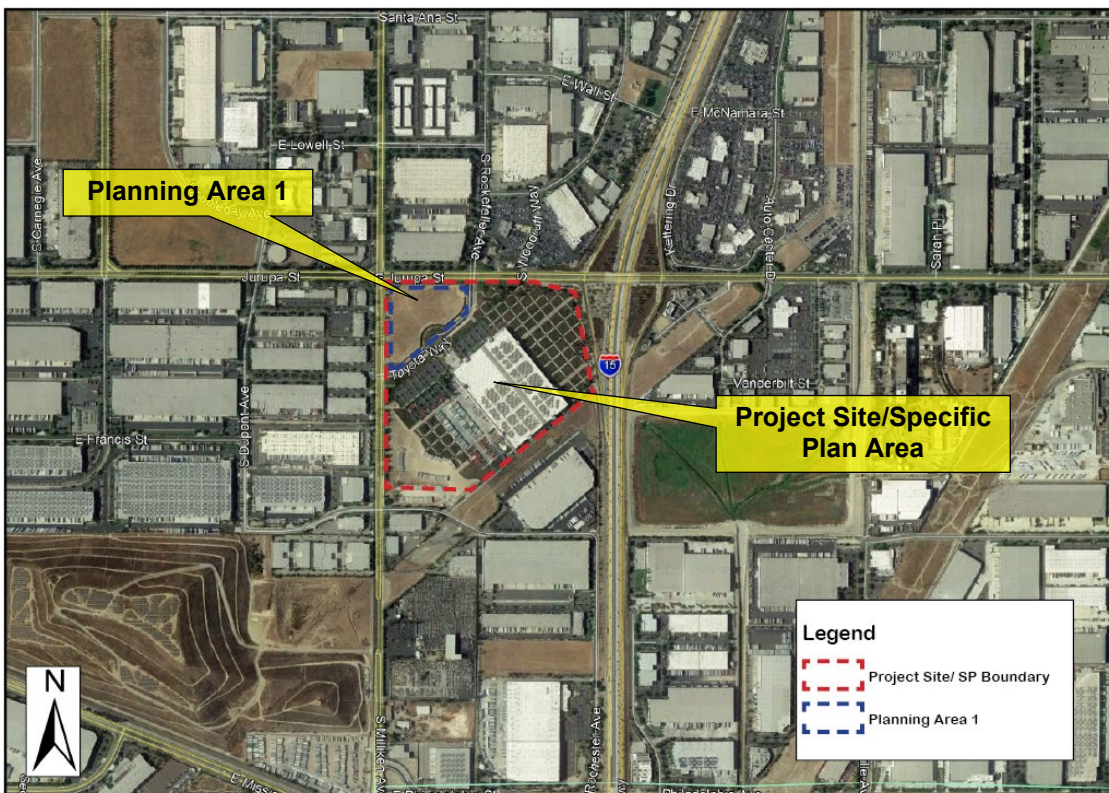


Figure 3: AERIAL PHOTOGRAPH



General Plan Designation: Existing – Industrial; Proposed – Industrial.

Zoning: Existing - Office/Research & Development (Office/R&D); Planning Area 1 of the Toyota/Ontario Business Park Specific Plan. Warehouse/ Distribution; Planning Areas 2 and 3. Proposed - Industrial Mixed Use; Planning Area 1. No change for Planning Areas 2 and 3.

Description of Project: An Amendment to the Toyota Ontario Business Park Specific Plan, which was adopted in August 1993. The Specific Plan is located on 95.35 gross acres of land generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway, in the City of Ontario. The Applicant is requesting a change in land use affecting Planning Area 1, from *Office/Research & Development (Office/R&D)* to *Industrial Mixed Use*, which would allow for both Office/R&D and warehouse/distribution/manufacturing land uses on the site. Planning Area 1 consists of approximately 14.4 acres of land located at the northwest corner of the Specific Plan area. Planning Areas 2 and 3 are unaffected by the Specific Plan Amendment (hereinafter referred to as “SPA”) and will remain Warehouse/Distribution. The SPA also updates the landscape palette to conform to current California friendly landscape practices, which will apply to any future development within the Specific Plan. The Specific Plan will also be revised to show the actual street name, *Toyota Way*, within the Specific Plan boundaries, in place of *Rockefeller Drive*, as incorrectly shown in the original Specific Plan document. The revision to the street name, from *Rockefeller Drive* to *Toyota Way*, is simply a correction and is, therefore, not analyzed in this Addendum. The update of the landscape palette and graphics is to bring the Specific Plan into compliance with the City’s drought tolerant requirements and is also not analyzed in this Addendum.

Revisions to the Specific Plan include:

- Any graphic that currently identifies Planning Area 1 zoning as *Office/R&D*, will be changed to *Industrial Mixed Use*;
- Text in the Specific Plan that refers to the *Office/R&D* land use designation, will be revised to reflect the new *Industrial Mixed Use* land use designation;
- Permitted uses within the *Industrial Mixed Use* land use district will be updated to include warehouse and distribution, and manufacturing land uses;
- Graphic and text references to Rockefeller Drive within the Toyota Ontario Business Park Specific Plan will be changed to reflect the actual street name: Toyota Way;
- The landscape palette and graphic representations of landscape will be updated to reflect more drought tolerant, California friendly materials; and
- Specific Design Guidelines for the *Office/R&D* land use district will be retitled to *Industrial Mixed Use*.

Project Setting: The project site consists of approximately 95.35 acres of land within eastern Ontario. The site is generally pentagonal in shape, with an existing Southern California Edison (“SCE”) easement containing high-voltage transmission lines located along the southeasterly edge of the. Topographically, the site has a distinct slope to the southeast, with a total “fall” of approximately thirty to thirty-five feet over the run of the site. Existing site uses include a City of Ontario water well facility near the intersection of Jurupa Street and Milliken Avenue and a small number of mature trees (fewer than a dozen) just north of Francis Street. Planning Area 1, which is the subject of the proposed land use change, is a triangular shaped site located in the northwest corner of the Specific Plan area. Planning Area 1 consists of approximately 14.4 acres and is bound by Jurupa Street to the north, Milliken Avenue to the west, and Toyota Way to the southeast. Planning Areas 2 and 3 currently contain a warehouse/distribution building owned and operated by Toyota.

Background: On January 27, 2010, the Ontario City Council adopted The Ontario Plan (“TOP”). TOP serves as the framework for the City’s business plan and provides a foundation for the City to operate as a municipal corporation that consists of six (6) distinct components: 1) Vision; 2) Governance Manual; 3) Policy Plan; 4) Council Priorities; 5) Implementation; and 6) Tracking and Feedback. The Policy Plan component of TOP meets the functional and legal mandate of a General Plan and contains nine elements: Land Use, Housing, Parks and Recreation, Environmental Resources, Community Economics, Safety, Mobility, Community Design and Social Resources.

An Environmental Impact Report (“EIR”) was prepared for TOP (State Clearinghouse No. 2008101140) and certified by the City Council on January 27, 2010 (hereinafter referred to as “Certified EIR”), which included Mitigation, Findings and a Statement of Overriding Considerations pursuant to the requirements of the California Environmental Quality Act, commencing with Public Resources Code Section 21000 (“CEQA”). The Certified EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan, and in the Policy Plan and impacts resultant of population and employment growth in the City. The subject site was analyzed in the Certified EIR as industrial (See Exhibit A) to be consistent with the industrial uses to the north, west, and south of the subject site, Interstate I-15 freeway to the east, and the subject site’s location under the landing path of the Ontario International Airport. The significant unavoidable adverse impacts that were identified in Certified EIR included agriculture resources, air quality, cultural resources, greenhouse gas emissions, noise, and transportation/traffic.

Analysis: According to the California Environmental Quality Act Guidelines Section 15164, an Addendum to a previously certified EIR may be used if some changes or additions are necessary, but none of the conditions described in Section 15162 requiring the preparation of a subsequent Negative Declaration or EIR have occurred. The CEQA Guidelines require that a brief explanation be provided to support the findings that no subsequent EIR or Negative Declaration are needed for further discretionary approval. These findings are described below:

1) *Required Finding: Substantial changes are not proposed for the project that will require major revisions of the previous EIR due to the involvement of new, significant environmental effects or a substantial increase in the severity of previously identified effects.*

Substantial changes are not proposed by the project and its implementation will not require revisions to the Certified EIR. The Certified EIR analyzed the direct and physical changes in the environment that would be caused by TOP; focusing on changes to land use associated with the buildout of the proposed land use plan. The project site is located in the Toyota/Ontario Business Park Specific Plan, with a zone change to Planning Area 1, from Office/R&D to Industrial Mixed Use, to permit warehouse/distribution uses along with the already allowed Office/R&D uses. As described in the Specific Plan and, therefore, analyzed in the Certified EIR, maximum development within Planning Area 1 would not exceed 300,000 gross square feet. The proposed SPA will allow for warehouse/distribution and/or manufacturing uses, as well as the existing Office/R&D uses that are allowed in Planning Area 1. The SPA would allow for a maximum of 300,000 gross square feet of development on Planning Area 1; therefore, the proposed amendment to the site will result in the same or less development than TOP EIR analyzed at buildout.

Since the anticipated buildout resulting from the proposed SPA will be the same or less than that originally analyzed in the Certified EIR, no revisions to the Certified EIR are required. Based on an allowable 0.48 floor area ratio (FAR), the proposed zone change would replace 300,000 square feet of potential office space with approximately 173,247 square feet of proposed warehouse/distribution uses. A trip generation comparison was conducted by Ganddini Group and reviewed by the City’s Traffic Engineering Division that compared an Office use versus a Warehouse use at a FAR of 0.48 (See Appendix A). As shown below, the proposed zone change is forecast to result in 2,528 fewer daily PCE trips compared to the maximum allowable development under existing zoning, including 299 fewer PCE trips during the AM peak hour and 293 fewer PCE trips during the PM peak hour.

Trips Generated									
Zoning	Quantity	Unit ¹	AM Peak Hour			PM Peak Hour			Daily
			In	Out	Total	In	Out	Total	
Existing (Office)	300.000	TSF	299	49	348	55	290	345	2,922
Proposed (Warehouse)	173.247	TSF	35	14	49	16	36	52	394
Trip Generation Comparison (Proposed - Existing)			-264	-35	-299	-39	-254	-293	-2,528

Notes:

(1) TSF = Thousand Square Feet

In addition, all previously adopted mitigation measures of the Certified EIR are conditions of project approval or mitigation measures and are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

2) *Required Finding: Substantial changes have not occurred with respect to the circumstances under which the project is undertaken, that would require major revisions of the previous Environmental Impact Report due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects.*

Substantial changes have not occurred with respect to the circumstances under which the project was undertaken, that would require major revisions to the Certified EIR in that the proposed changes would be in keeping with the surrounding area. The Certified EIR evaluated the site as Industrial, with a maximum FAR of 0.55 (see attached Exhibit "A"- Certified EIR Figure 1-3, Proposed Land Use Plan), consistent with the surrounding industrial properties to the north, west, and south. A future warehouse/distribution land use for the site now requires a zone change to achieve consistency between TOP's Land Use Plan and the industrial land use designation of the Toyota/Ontario Business Park Specific Plan. The proposed Specific Plan Amendment is implementing the Certified EIR's industrial land use designation for the site and is consistent with the previous 1992 General Plan land use designation of Planned Industrial for the site; therefore, no proposed changes or revisions to the Certified EIR are required. In addition, all previously adopted mitigation measures of the Certified EIR are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

3) *Required Finding: No new information has been provided that would indicate that the proposed project would result in one or more significant effects not discussed in the previous EIR.*

No new information has been provided that would indicate the proposed project would result in any new significant effects not previously discussed in the Certified EIR. As stated above in Section 2, no substantial changes have occurred with respect to the circumstances under which the Project was undertaken. TOP EIR evaluated the site as Industrial, with a maximum FAR of 0.55 (see attached Exhibit "A"- Certified EIR Figure 1-3, Proposed Land Use Plan), consistent with the surrounding industrial properties to the north, west, and south. Since the 1992 General Plan and the 2010 Certified EIR, the subject site and surrounding area have been planned for and remained industrial (see attached Exhibit "B"- 1992 General Plan Land Use Map); therefore, no proposed changes or revisions to the Certified EIR are required. In addition, all previously adopted mitigation measures are incorporated herein by reference. The attached Initial Study provides an analysis of the Project and verification that the Project will not cause environmental impacts such that any of the circumstances identified in State CEQA Guidelines Section 15162 are present.

CEQA Requirements for an Addendum: If changes to a project or its circumstances occur or new information becomes available after adoption of a negative declaration, the lead agency may: (1) prepare a subsequent EIR if the criteria of State CEQA Guidelines Section 15162(a) are met, (2) prepare a subsequent negative declaration, (3) prepare an addendum, or (4) prepare no further documentation. (State CEQA Guidelines Section 15162(b)). When only minor technical changes or additions to the negative declaration are necessary and none of the conditions described in section 15162 calling for the preparation of a subsequent EIR or negative declaration have occurred, CEQA allows the lead agency to prepare and adopt an addendum. (State CEQA Guidelines Section 15164(b).)

Under Section 15162, a subsequent EIR or negative declaration is required only when:

1) Substantial changes are proposed in the project which will require major revisions of the previous negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;

2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the negative declaration due to the involvement of any new significant

environmental effects or a substantial increase in the severity of previously identified significant effects; or

3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the negative declaration was adopted, shows any of the following:

a) The project will have one or more significant effects not discussed in the previous negative declaration;

b) Significant effects previously examined will be substantially more severe than shown in the previous EIR;

c) Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or

d) Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

Thus, if the Project does not result in any of the circumstances listed in Section 15162 (i.e., no new or substantially greater significant impacts), the City may properly adopt an addendum to the Certified EIR.

Conclusion: TOP EIR, certified by City Council on January 27, 2010, was prepared as a Program EIR in accordance with CEQA, the State CEQA Guidelines, and the City's Rules for the Implementation of CEQA and in accordance with Section 15121(a) of the State CEQA Guidelines (California Code of Regulations, Title 14, Division 6, Chapter 3). TOP EIR considered the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, TOP EIR focused on impacts from changes to land use associated with buildout of the City's Land Use Plan, within the Policy Plan, and impacts from the resulting population and employment growth in the City. The proposed Specific Plan Amendment coordinates with the existing uses of the properties and uses within the surrounding areas. As described on page 4, the amount of development anticipated at buildout will be equal to or lower for Planning Area 1 than the Certified EIR analyzed. Subsequent activities within TOP Program EIR have been evaluated to determine whether an additional CEQA document needs to be prepared.

Accordingly, and based on the findings and information contained in the Certified EIR, the analysis above, the attached Initial Study, and CEQA statute and State CEQA Guidelines, including Sections 15164 and 15162, the Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary, nor is there a need for any additional mitigation measures; therefore, pursuant to State CEQA Guidelines Section 15164, the Ontario City Council hereby adopts this Addendum to the Certified EIR.

Surrounding Land Uses:

	<i>Existing Land Use</i>	<i>General Plan Designation</i>	<i>Zoning Designation</i>	<i>Specific Plan Land Use</i>
Site:	Vacant	Industrial	Toyota (4621-SP)	Office/R&D
North:	Warehouse/Distribution	Industrial	California Commerce Center (2591-SP)	Light Industrial
South:	Toyota Warehouse	Industrial	Toyota (4621-SP)	Warehouse/Distribution
East:	Interstate 15 Freeway	Interstate 15 Freeway	Interstate 15 Freeway	Interstate 15 Freeway
West:	Office/Business Park	Industrial	California Commerce Center (2591-SP)	Light Industrial Commercial/Food/Hotel

Other public agencies whose approval is required (e.g., permits, financing approval or participation agreement): None

Tribal Consultation: Have California Native American tribes traditionally and culturally affiliated with the project area requested consultation pursuant to Public Resources Code section 21080.3.1? Yes No

If "yes," has consultation begun? Yes No Completed

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

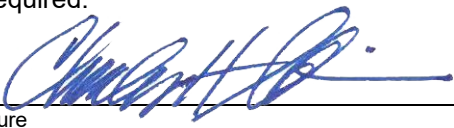
The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a "Potentially Significant Impact" as indicated by the checklist on the following pages.

- | | | |
|--|---|---|
| <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Agriculture/Forestry Resources | <input type="checkbox"/> Air Quality |
| <input type="checkbox"/> Biological Resources | <input type="checkbox"/> Cultural Resources | <input type="checkbox"/> Geology / Soils |
| <input type="checkbox"/> Greenhouse Gas Emissions | <input type="checkbox"/> Hazards & Hazardous Materials | <input type="checkbox"/> Hydrology / Water Quality |
| <input type="checkbox"/> Land Use / Planning | <input type="checkbox"/> Mineral Resources | <input type="checkbox"/> Noise |
| <input type="checkbox"/> Population / Housing | <input type="checkbox"/> Public Services | <input type="checkbox"/> Recreation |
| <input type="checkbox"/> Transportation | <input type="checkbox"/> Utilities / Service Systems | <input type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Tribal Cultural Resources | <input type="checkbox"/> Wildfire | <input type="checkbox"/> Energy |

DETERMINATION (To be completed by the Lead Agency)

On the basis of this initial evaluation:

- I find that the proposed project COULD NOT have a significant effect on the environment, and a NEGATIVE DECLARATION will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A MITIGATED NEGATIVE DECLARATION will be prepared.
- I find that the proposed project MAY have a significant effect on the environment, and an ENVIRONMENTAL IMPACT REPORT is required.
- I find that the proposed project MAY have a "potentially significant" or "potentially significant unless mitigated" impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An ENVIRONMENTAL IMPACT REPORT is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.


Signature

3/17/2020
Date

Charles H. Mercier, Principal Planner
Printed Name and Title

City of Ontario
For

EVALUATION OF ENVIRONMENTAL IMPACTS

1) A brief explanation is required for all answers except "No Impact" answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A "No Impact" answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g. the project falls outside a fault rupture zone). A "No Impact" answer should be explained where it is based on project-specific factors as well as general standards (e.g. the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, then the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. "Potentially Significant Impact" is appropriate if there is substantial evidence that an effect is significant. If there are one or more "Potentially Significant Impact" entries when the determination is made, an EIR is required.

4) "Negative Declaration: Less Than Significant with Mitigation Incorporated" applies where the incorporation of mitigation measures has reduced an effect from "Potentially Significant Impact" to a "Less than Significant Impact." The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from the "Earlier Analyses" Section may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a) Earlier Analyses Used. Identify and state where they are available for review.

b) Impacts Adequately Addressed. Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c) Mitigation Measures. For effects that are "Less than Significant with Mitigation Measures Incorporated," describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g. general plans, zoning ordinances). Reference to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) Supporting Information Sources. A source list should be attached, and other sources used, or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project's environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

a) The significance criteria or threshold, if any, used to evaluate each question; and

b) The mitigation measure identified, if any, to reduce the impact to less than significance.

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
1. AESTHETICS. Would the project:				
a. Have a substantial adverse effect on a scenic vista?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest protocols adopted by the California Air Resources Board. Would the project:				
a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:				
a. Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in other emissions (such as those leading to odors adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. BIOLOGICAL RESOURCES. Would the project:				
a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. CULTURAL RESOURCES. Would the project:				
a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Disturb any human remains, including those interred outside of dedicated cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. ENERGY. Would the project:				
a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. GEOLOGY AND SOILS. Would the project:				
a. Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on expansive soil, as defined in Table 18 1 B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8. GREENHOUSE GAS EMISSIONS. Would the project:				

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emission of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9. HAZARDS AND HAZARDOUS MATERIALS. Would the project:				
a. Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10. HYDROLOGY AND WATER QUALITY. Would the project:				
a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
ii. substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or offsite;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
11. LAND USE AND PLANNING. Would the project:				
a. Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
12. MINERAL RESOURCES. Would the project:				
a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
13. NOISE. Would the project result in:				
a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
14. POPULATION AND HOUSING. Would the project:				
a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
15. PUBLIC SERVICES. Would the project:				
a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:				
i. Fire protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii. Police protection?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii. Schools?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv. Parks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
v. Other public facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
16. RECREATION. Would the project:				
a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project include recreational facilities or require the construction or expansion of recreational facilities which have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
17. TRANSPORTATION. Would the project:				
a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Would the project conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is				

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
19. UTILITIES AND SERVICE SYSTEMS. Would the project:				
a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
20. WILDFIRES. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a. Substantially impair an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Issues	Potentially Significant Impact	Less Than Significant with Mitigation	Less Than Significant Impact	Impacts Previously Analyzed in TOP EIR
d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
21. MANDATORY FINDINGS OF SIGNIFICANCE.				
a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Note: Authority cited: Public Resources Code sections 21083, 21083.05, 21083.09.</p> <p>Reference: Gov. Code section 65088.4; Public Resources Code sections 21073, 21074, 21080(c), 21080.1, 21080.3, 21080.3.1, 21080.3.2, 21082.3, 21083, 21083.3, 21083.5, 21084.2, 21084.3, 21093, 21094, 21095 and 21151; <i>Sundstrom v. County of Mendocino</i> (1988) 202 Cal.App.3d 296; <i>Leonoff v. Monterey Board of Supervisors</i> (1990) 222 Cal.App.3d 1337; <i>Eureka Citizens for Responsible Govt. v. City of Eureka</i> (2007) 147 Cal.App.4th 357; <i>Protect the Historic Amador Waterways v. Amador Water Agency</i> (2004) 116 Cal.App.4th 1099, 1109; <i>San Franciscans Upholding the Downtown Plan v. City and County of San Francisco</i> (2002) 102 Cal.App.4th 656.</p>				

EXPLANATION OF ISSUES

1. AESTHETICS. Would the project:

a. Have a substantial adverse effect on a scenic vista?

Discussion of Effects: The Policy Plan (General Plan) does not identify scenic vistas within the City. However, the Policy Plan (Policy CD1-5) requires all major north-south streets be designed and redeveloped to feature views of the San Gabriel Mountains. The project site is located at the southeast corner of Milliken Avenue and Jurupa Street, both Principal Arterials, as identified in the Functional Roadway Classification Plan (Figure M-2) of the Mobility Element within the Policy Plan. The proposed Specific Plan Amendment to permit warehouse, distribution, and/or manufacturing with the office/research and development uses that are already allowed will not result in adverse environmental impacts with regard to views of the San Gabriel Mountains. Therefore, no adverse impacts are anticipated in relation to the project.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings and historic buildings within a state scenic highway?

Discussion of Effects: The City of Ontario is served by three freeways: I-10, I-15, and SR-60. I-10 and SR-60 traverse the northern and central portion of the City, respectively, in an east–west direction. I-15 traverses the northeastern portion of the City in a north–south direction. These segments of I-10, I-15, and SR-60 have not been officially designated as scenic highways by the California Department of Transportation. In addition, there are no historic buildings, or any scenic resources identified on or in the vicinity of the project site. Therefore, it will not result in adverse environmental impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

c. In non-urbanized areas, substantially degrade the existing visual character or quality of public views of the site and its surroundings (Public views are those that are experienced from publicly accessible vantage point). If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Discussion of Effects: The Project would not degrade the existing visual character or quality of the site or its surroundings. The project site is located in an area that is characterized by industrial development and is surrounded by urban land uses.

Any development proposals that would subsequently occur from the proposed Project will be required to be in accordance with the policies of the Community Design Element of the Policy Plan (General Plan) and zoning designations on the property. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Create a new source of substantial light or glare that would adversely affect day or nighttime views in the area?

Discussion of Effects: The proposed land use change itself will not cause lighting to be installed in the Project. New lighting will be introduced to the site with the development of the project. Pursuant to the requirements of the City’s Development Code, on-site lighting will be shielded, diffused or indirect, to avoid glare to pedestrians or motorists. In addition, lighting fixtures will be selected and located to confine the area of illumination to within the project site and minimize light spillage.

Site lighting plans will be subject to review by the Planning Department and Police Department prior to issuance of building permits (pursuant to the City’s Building Security Ordinance). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

2. AGRICULTURE AND FOREST RESOURCES. In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state’s inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. Would the project:

a. Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?

Discussion of Effects: The site is presently vacant and does not contain any agricultural uses. Further, the site is identified as Urban and Built-up Land on the map prepared by the California Resources

Agency, pursuant to the Farmland Mapping and Monitoring Program. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict with existing zoning for agricultural use, or a Williamson Act contract?

Discussion of Effects: The project site is not zoned for agricultural use. The project proposes to change the allowed lands uses in Planning Area 1 of the Toyota/Ontario Business Park Specific Plan, to allow warehousing, distribution, and manufacturing uses in addition to office and research and development uses, which are already permitted in Planning Area 1. Future development will be consistent with the development standards and allowed land uses. Furthermore, there are no Williamson Act contracts in effect on the subject site. Therefore, no impacts to agricultural uses are anticipated, nor will there be any conflict with Williamson Act contracts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104)?

Discussion of Effects: The project proposes to change the allowed land uses in Planning Area 1 of the Toyota/Ontario Business Park Specific Plan, to allow for warehousing, distribution, and manufacturing uses in addition to the office and research and development uses currently allowed. This would not result in the rezoning of forest land, timberland, or timberland zoned Timberland Production because such land use designations do not exist within the City of Ontario. Therefore, no impacts to forest or timberland are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in the loss of forest land or conversion of forest land to non-forest use?

Discussion of Effects: There is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, the proposed project would not result in the loss or conversion of forest land.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Involve other changes in the existing environment, which, due to their location or nature, could individually or cumulatively result in loss of Farmland to non-agricultural use or conversion of forest land to non-forest use?

Discussion of Effects: Implementation of the Project would not result in changes to the existing environment other than those previously addressed in the Certified EIR. While conversion of farmland increases the potential for adjacent areas to also be converted from farmland to urban uses, there are no agricultural uses occurring onsite or in the vicinity and the Project does not directly or indirectly result in conversion of farmland. No new cumulative impacts beyond those identified in the Certified EIR would result from Project implementation. As a result, the project will not result in loss of Farmland to non-agricultural use.

Additionally, there is currently no land in the City of Ontario that qualifies as forest land as defined in Public Resources Code Section 12220(g). Neither TOP nor the City's Zoning Code provide designations for forest land. Consequently, to the extent that the proposed project would result in changes to the existing environment, those changes would not impact forest land.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

3. AIR QUALITY. Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. Would the project:

a. Conflict with or obstruct implementation of the applicable air quality plan?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and is surrounded on the north, west, and south by industrial uses and on the east by the Interstate 15 freeway. The project will not conflict with or obstruct implementation of any air quality plan. As noted in the Certified EIR (Section 5.3), pollutant levels in the Ontario area already exceed Federal and State standards. To reduce pollutant levels, the City of Ontario is actively participating in efforts to enhance air quality by implementing Control Measures in the Air Quality Management Plan for local jurisdictions within the South Coast Air Basin.

The proposed project is consistent with The Ontario Plan, for which the EIR was prepared and impacts evaluated. Furthermore, the project is consistent with the City's participation in the Air Quality Management Plan and, because of the project's limited size and scope, will not conflict with or obstruct implementation of the plan. Mitigation (Mitigation Measure 5.3-2) has been adopted by the City that requires fugitive dust control measures pursuant to SCAQMD's Rule 403, use of Tier 3 construction equipment, proper service and maintenance of construction equipment, limiting nonessential idling of construction equipment, and use of Super-Compliant VOC paints for coating and architectural surfaces. Any future development proposals on the project site will be required to comply with Mitigation Measure 5.3-2. No new impacts beyond those identified in the Certified EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Discussion of Effects: The project will not result in a cumulatively considerable net increase of any criteria pollutant for which the region is in non-attainment under an applicable federal or state ambient air quality because the Project is a Specific Plan Amendment that adds permitted industrial uses to property specified for industrial land uses by the Official Land Use Map (Exhibit LU-01) of the Policy Plan component of TOP. Mitigation (Mitigation Measure 5.3-1) has already been adopted by the City that would facilitate continued City cooperation with the SCAQMD and SCAG to achieve regional air quality improvement goals, promote energy conservation design and development techniques, encourage alternative modes of transportation, and implement transportation demand strategies. The project will comply with the air quality standards of the Certified EIR and the SCAQMD resulting in impacts that are less than significant [please refer to Sections 3(a) and 3(b)].

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Expose sensitive receptors to substantial pollutant concentrations?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an industrial use and is surrounded on the north, west, and south by industrial uses and on the east by the Interstate 15 Freeway. As discussed in Section 5.3 of the Certified EIR, the proposed Project is within a non-attainment region of the South Coast Air Basin (SCAB). The proposed Specific Plan Amendment closely correlates with the land use designations of the surrounding area and will not generate significant new or greater air quality impacts than identified in the Certified EIR. Adequate mitigation (Mitigation Measure 5.3-5) has already been adopted by the City that would require new developments to be consistent with recommended buffer distances of the California Air Resources Board's Air Quality and Land Use Handbook: A Community Health Perspective (April 2005). Under this mitigation, new development that is inconsistent with the recommended buffer distances shall only be approved if all feasible mitigation measures, such as high

efficiency Minimum Efficiency Reporting Value filters, have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources. No new impacts beyond those identified in the Certified EIR would result from Project implementation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an industrial use and is surrounded on the north, west, and south by industrial uses and on the east by the Interstate 15 freeway. The uses proposed on the subject site, as well as those permitted within the Mixed Use Industrial land use district, do not create objectionable odors. Further, the project shall comply with the policies of the Ontario Municipal Code and the Policy Plan (General Plan). Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified EIR analyses are necessary.

4. BIOLOGICAL RESOURCES. Would the project:

a. Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. According to the Certified EIR, the project site is located within an area that has been identified as containing species identified as a candidate, sensitive, or special status species in local or regional plans, policies or regulations or by the California Department of Fish and Game or the U.S. Fish and Wildlife Service. These species include: Delhi Sands Flower Loving Fly, Coast Horned Lizard, Burrowing Owl, and various species of nesting birds protected under the Migratory Bird Treaty Act. However, the proposed Project, a Specific Plan Amendment that adds additional zoning uses to Planning Area 1, would not have an impact on these species because no development project is proposed at this time that would have the ability to disturb the site. Any future development of Planning Area 1 would be required to conduct focused USFWS protocol surveys for Delhi Sands Flower Loving Fly, pre-construction surveys for Coast Horned Lizards and Burrowing Owls, and focused surveys for migratory bird nests. If the surveys identify any of these species on the Project site, the proper measures must be undertaken by the future project applicant to ensure that these impacts are reduced to less than significant. Therefore, because the proposed Project is a Specific Plan Amendment and zone change, and no development of the site is proposed at this time, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified TOP EIR. No changes or additions to the Certified analyses are necessary.

b. Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The site does not contain any riparian habitat or other sensitive natural community identified by the Department of Fish & Game or Fish & Wildlife Service. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

Discussion of Effects: No wetland habitat is present on site. Therefore, project implementation would have no impact on these resources.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

Discussion of Effects: The site is a vacant property that is bounded on all four sides by development. As a result, there are no wildlife corridors connecting this site to other areas. Therefore, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

Discussion of Effects: The City of Ontario does not have any specific policies or ordinances protecting biological resources. Further, Planning Area 1 does not contain any mature trees necessitating the need for preservation. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Conflict with the provisions of an adopted Habitat Conservation Plan (HCP), Natural Community Conservation Plan (NCCP), or other approved local, regional, or state habitat conservation plan?

Discussion of Effects: The site is not part of an adopted HCP, NCCP or other approved habitat conservation plan. As a result, no adverse environmental impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

5. CULTURAL RESOURCES. Would the project:

a. Cause a substantial adverse change in the significance of a historical resource pursuant to Section 15064.5?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an industrial use. The subject site is vacant and does not contain any buildings or structures constructed more than 50 years ago and cannot be considered for eligibility for listing in the California Register of Historic Resources. A Cultural Resources Records Search was conducted on July 16, 2019 at the California Historic Resource Inventory System at the South Central Coastal Information Center (CHRIS-SCCIC). The records search indicated that there are no cultural resources (prehistoric, historic, or built environments) recorded within the Project boundaries. There was one (1) historic resource (CA-SBR-008857H) located within a one-half mile radius of the Project Site. The historic resource is a section of the Southern California Edison Company's Lugo-Mira No. 1 500kv Transmission Line. The transmission line was determined eligible for listing in the National Register of Historic Place (NRHP) under Criteria A and C, and therefore, is eligible for listing in the California Register of Historic Resources. This historic resource will not be impacted (directly or indirectly) by the proposed Project. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a substantial adverse change in the significance of an archaeological resource pursuant to Section 15064.5?

Discussion of Effects: The Certified EIR (Section 5.5) indicates no archeological sites or resources have been recorded in the City with the Archeological Information Center at San Bernardino County Museum. Figure 5.5-2 of the Certified EIR shows that the Project site has not been surveyed for archeological resources. Further, the CHRIS-SCCIC records search noted in subsection “a” above failed to identify prehistoric, historic, or historic built environments within or adjacent to the Project boundaries. The NAHC Sacred Lands File search also failed to indicate archaeological resources or artifacts associated with Tribal Cultural Resources (TCRs) within the Project site. The Project site has been highly disturbed by modern human activities to include agricultural use from the 1940’s through the 1960’s and the development and construction of the Toyota Motors North American Parts Center and supporting infrastructure that would have displaced potential surface and subsurface archaeological resources.¹ Therefore, the proposed Project will not impact cultural (prehistoric, historic, or historic built environments) resources and no mitigation measures are recommended. While no adverse impacts to archeological resources are anticipated at this site due to its urbanized nature, standard conditions have been imposed on the project that in the event of unanticipated archeological discoveries, construction activities will not continue or will moved to other parts of the project site and a qualified archaeologist shall be contacted to determine significance of these resources. If the find is discovered to be historical or unique archaeological resources, as defined in Section 15064.5 of the CEQA Guidelines, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Disturb any human remains, including those interred outside of formal cemeteries?

Discussion of Effects: The proposed project is in an area that has been previously disturbed by human activity. No known religious or sacred sites exist within the Project area. Thus, human remains are not expected to be encountered during any construction activities. However, in the unlikely event that human remains are discovered, existing regulations, including the California Public Resources Code Section 5097.98, would afford protection for human remains discovered during development activities. Furthermore, standard conditions have been imposed on the project that in the event of unanticipated discoveries of human remains are identified during excavation, construction activities, the area shall not be disturbed until any required investigation is completed by the County Coroner and/or Native American consultation has been completed, if deemed applicable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

6. ENERGY Would the project:

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Discussion of Effects: Energy was not analyzed in the Certified EIR but has been included as part of the 2019 revisions to the State CEQA Guidelines. Implementation of the Project would increase the demand for electricity and natural gas at the Project site and gasoline consumption in the region during construction and operation. A detailed discussion is provided below.

¹ Historic Aerial Photographs. 1948-1964. Toyota Ontario Business Park Specific Plan Project. Prepared by MIG, Inc. Riverside California 92507; prepared for Toyota Motor Sales, USA (Mr. C. McMorris), Plano Texas, 75024. Electronically available at: <https://www.historicaerials.com/>

Electricity

Construction. Temporary electric power would be required for lighting and electronic equipment (e.g., computers) located in trailers used by the construction crew. However, the electricity used for such activities would be temporary and would have a negligible contribution to the project's overall energy consumption.

Operational. The proposed zone change would allow additional industrial uses in the Planning Area that were not previously permitted. However, these uses will be similar to the already allowed uses and will have similar energy use during operation. The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a warehouse, distribution, or manufacturing use would require electricity for multiple purposes, such as: building heating and cooling, lighting, appliances, and electronics. Any future development on the site would be required to comply with the CALGreen Building Code requirements in effect at the time of development, which are more efficient than the 2016 standards. Moreover, the Project includes a sample Greenhouse Gas Reduction Measure Screening Table for Commercial and Industrial Development. The Screening Table includes measures energy efficient development, indoor space efficiency measures, building efficiency measures, renewable energy measures, and water conservation measures. Measures that would reduce electricity consumption include, but are not limited to: greatly enhanced window insulation, an enhanced cool-roof, an improved efficiency heating, ventilation, and air conditioning ("HVAC") system, blower doors HERS verified Envelope leakage or equivalent, enhanced duct insulation, Energy Star commercial appliances, water efficient landscaping and irrigation systems, and water-efficient toilets and faucets. Although electricity consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would also be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce electricity consumption. Electricity that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the electricity that would be consumed by the Project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Natural Gas

Construction. Natural gas consumption is not anticipated during construction of the Project. Fuels used for construction would generally consists of diesel and gasoline, which are discussed in the next subsection. Any amounts of natural gas that may be consumed during project construction would be nominal and would have a negligible contribution to the project's overall energy consumption

Operational. The proposed zone change would allow additional industrial uses in the Planning Area that were not previously permitted. However, these uses will be similar to the already-allowed uses and will have similar natural gas use during operation. The Project does not include construction of an actual development. However, during hypothetical operation of the Project, a warehouse, distribution, or manufacturing use would require natural gas consumption for various purposes, such as building heating and cooling. Any future structure developed on the site would be built to the 2016 Title 24 CALGreen efficiency requirements or the code in effect at the time of development. In addition, measures will be applied based on the information contained in the County's GHG DRP checklist. These measures include, but are not limited to: enhanced wall, attic, and window insulation; high efficiency water heater, and optimized building orientation. Although natural gas consumption would increase at the site under implementation of the Project, the building envelope, HVAC, lighting, and other systems, would be designed to maximize energy performance. The project would be subject to statewide mandatory energy requirements as outlined in the CALGreen Code. In addition, the project would implement additional measures, as detailed in the GHG reduction measures screening table, that would further reduce natural gas consumption. Natural gas that would be consumed by the Project would also be subject to the cap-and-trade regulation. For these reasons, the natural gas that would be consumed by the project is not considered to be inefficient or wasteful, and impacts would be less than significant.

Diesel and Gasoline Fuel

Construction. Diesel and gasoline fuels, also referred to as petroleum in this subsection, would be consumed throughout construction of the Project. Fuel consumed by construction equipment would be the primary energy resource consumed over the course of construction, and vehicle miles traveled ("VMT") associated with the transportation of construction materials (e.g., deliveries to the site) and worker trips to

and from the site would also result in petroleum consumption. Whereas on-site, heavy-duty construction equipment and delivery trucks would predominantly use diesel fuel, construction workers would generally rely on gasoline-powered vehicles. Any future development would be required to comply with CARB's Airborne Toxic Control Measures, which restricts heavy-duty diesel vehicle idling to five minutes. Since petroleum use during construction would be temporary and required to conduct development activities, it would not be wasteful or inefficient, and impacts would be less than significant.

Operational. Fuel consumption associated with development pursuant to the Project's operational phase would primarily be attributable to workers commuting to and from the Project and the operation of large, diesel-powered trucks (e.g., semi-trucks) needed to transport goods. Over the lifetime of the Project, the fuel efficiency of the vehicles being used by the employees is expected to increase. As such, the amount of petroleum consumed as a result of vehicular trips to and from the Project site during operation is anticipated to decrease over time. There are numerous regulations in place that require and encourage fuel efficiency. For example, CARB has adopted an approach to passenger vehicles by combining the control of smog-causing pollutants and GHG emissions into a single, coordinated package of standards. The approach also includes efforts to support an accelerate the number of plug-in hybrids and ZEVs in California. In addition, per the requirements identified in SB 375, CARB adopted a regional goal for the SCAG region of reducing per-capita GHG emissions from 2005 levels by 8 percent by 2020 and 19 percent by 2035 for light-duty passenger vehicles. Accordingly, operation of the Project is expected to decrease the amount of petroleum it consumes in the future due to advances in fuel economy. Although the Project would increase petroleum use in the region during construction and operation, the use would be a small fraction of the statewide use and, due to efficiency increases, would diminish over time. As such, petroleum consumption associated with the Project would not be considered inefficient or wasteful and would result in a less-than-significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Discussion of Effects: The Project would not conflict with or obstruct a state or local plan adopted for the purposes of increasing the amount of renewable energy or energy efficiency. The California Title 24 Building Code contains energy efficiency standards for non-residential buildings. These standards address electricity and natural gas efficiency in lighting, water, heating, and air conditioning, as well as the effects of the building envelope (e.g., windows, doors, walls and rooves, etc.) on energy consumption. As described above, the Project would be required to comply with the 2019 Title 24 CALGreen standards and would implement additional measures as identified in the County's GHG DRP checklist. Since the Project would comply with applicable State standards and adhere to the County's energy reductions measures identified in the GHG Emissions Reduction Plan, the Project would not conflict with nor obstruct a state or local plan for renewable energy or energy efficiency. This impact would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts. No changes or additions to the Certified EIR analyses are necessary.

7. GEOLOGY & SOILS. Would the project:

a. Expose people or structures to potential substantial adverse effects, including the risk of loss, injury or death involving:

i. Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42.

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. Given that the closest fault zone is located more than ten miles from the project site, fault rupture within the project area is not likely. All development will comply with the Uniform Building Code seismic design standards to reduce geologic hazard susceptibility. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Strong seismic ground shaking?

Discussion of Effects: There are no active faults known on the site and the project site is located outside the Fault Rapture Hazard Zone (formerly Alquist-Priolo Zone). The Certified EIR (Section 5.7/Figure 5.7-2) identifies eight active or potentially active fault zones near the City. The closest fault zone is located more than ten miles from the project site. The proximity of the site to the active faults will result in ground shaking during moderate to severe seismic events. All construction will comply with the California Building Code, the Ontario Municipal Code, The Ontario Plan and all other ordinances adopted by the City related to construction and safety. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Seismic-related ground failure, including liquefaction?

Discussion of Effects: As identified in the Certified EIR (Section 5.7), groundwater saturation of sediments is required for earthquake induced liquefaction. In general, groundwater depths shallower than 10 feet to the surface can cause the highest liquefaction susceptibility. Depth to ground water at the project site during the winter months is estimated to be between 250 to 450 feet below ground surface. Therefore, the liquefaction potential within the project area is minimal. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Landslides?

Discussion of Effects: The project would not expose people or structures to potential adverse effects, including the risk of loss, injury, or death involving landslides because the relatively flat topography of the project site (less than 2 percent slope across the City) makes the chance of landslides remote. Amending the Specific Plan Planning Area 1 to allow warehousing, distribution, and/or manufacturing uses will not create greater landslide potential impacts than were identified in the Certified EIR. Implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal Code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in substantial soil erosion or the loss of topsoil?

Discussion of Effects: Amending the Specific Plan Planning Area 1 to allow warehousing, distribution, and/or manufacturing uses will not create greater erosion impacts than were identified in the Certified TOP EIR. Impacts will be less than significant with mitigation.

The project will not result in significant soil erosion or loss of topsoil because of the previously disturbed nature of the Project site and the limited size and scope of the Project. Grading increases the potential for erosion by removing protective vegetation, changing natural drainage patterns, and constructing slopes. However, compliance with the California Building Code and review of grading plans by the City Engineer will ensure no significant impacts will occur. In addition, the City requires an erosion/dust control plan for projects located within this area. Implementation of a NPDES program, the Environmental Resource Element of the Policy Plan (General Plan) strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Discussion of Effects: Amending the Specific Plan Planning Area 1 to allow warehousing, distribution, and/or manufacturing uses will not create greater landslide potential impacts than were identified in the Certified EIR. In addition, the associated projects would not result in the location of development on a geologic unit or soil that is unstable, or that would become unstable because as previously discussed, the potential for liquefaction and landslides associated with the project is less than significant. The Certified EIR (Section 5.7) indicates that subsidence is generally associated with large decreases or withdrawals of water from the aquifer. The project would not withdraw water from the existing aquifer. Further, implementation of The Ontario Plan strategies, Uniform Building Code and Ontario Municipal code would reduce impacts to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

Discussion of Effects: The majority of Ontario, including the project site, is located on alluvial and eolian soil deposits. These types of soils are not considered to be expansive. Therefore, no adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?

Discussion of Effects: The area is served by the local sewer system and the use of alternative systems is not necessary. There will be no impact to the sewage system.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

Discussion of Effects: The City of Ontario is underlain by deposits of Quaternary and Upper-Pleistocene sediments deposited during the Pliocene and early Pleistocene time, Quaternary Older Alluvial sediments may contain significant, nonrenewable, paleontological resources and are, therefore, considered to have high sensitivity at depths of 10 feet or more below ground surface. In addition, the Certified EIR (Section 5.5) indicates that one paleontological resource has been discovered in the City. Moreover, Results of the paleontological resources records search through the Natural History Museum of Los Angeles County (NHMLAC) indicate that there are no known vertebrate fossil localities or unique geological features that have been previously identified within the Project area or within a one-mile radius. The results of the literature review and the search at the NHMLAC indicate that the Project site has surficial sediments composed of younger Quaternary Alluvium, derived as alluvial fan deposits from the San Gabriel Mountains to the north or as dune sands. These deposits typically do not contain significant vertebrate fossils, at least in the uppermost layers, but they may be underlain by older sedimentary materials at estimated depths greater than 9 feet (McLeod 2019). Therefore, it is concluded that the proposed Project will not impact paleontological resources or unique geological features and as such no mitigation measures are recommended. While no adverse impacts are anticipated, standard conditions have been imposed on the Project that in the event of unanticipated paleontological resources are identified during excavation, construction activities will not continue or will be moved to other parts of the Project site and a qualified paleontologist shall be contacted to determine significance of these resources. If the find is determined to be significant, avoidance or other appropriate measures shall be implemented.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

8. GREENHOUSE GAS EMISSIONS. Would the project:

a. Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. Additionally, the impact of buildout of The Ontario Plan on the environment due to the emission of greenhouse gases ("GHGs") was analyzed in the Certified EIR. According to the EIR, this impact would be significant and unavoidable (Re-circulated Portions of the Ontario Plan Draft Environmental Impact Report, p. 2-118.) This EIR was certified by the City on January 27, 2010, at which time a statement of overriding considerations was also adopted for The Ontario Plan's significant and unavoidable impacts, including that concerning the emission of greenhouse gases. Amending the Specific Plan Planning Area 1 to allow additional industrial uses on the site will not create significantly greater impacts than were identified in the Certified EIR. The Project includes a sample GHG Reduction Measures Screening Threshold Table, which provides guidance in measuring the reduction of greenhouse gas ("GHG") emissions attributable to certain design and construction measures incorporated into development projects. The analysis, methodology, and significance determination (thresholds) are based upon the City's Climate Action Plan ("CAP"), which includes GHG emission inventories (2008 and 2020 forecasts), a year 2020 emission reduction target, the goals and policies to reach the target, together with the Addendum prepared for the CAP. The Screening Table assigns points for each option incorporated into a project as mitigation or a project design feature (collectively referred to as "feature"). The point values correspond to the minimum emissions reduction expected from each feature. The menu of features allows maximum flexibility and options for how development projects can implement the GHG reduction measures. The point levels are based upon improvements compared to 2008 emission levels of efficiency. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the City's CAP. As such, those projects that garner a total of 100 points or greater would not require quantification of project specific GHG emissions. Consistent with CEQA Guidelines, such projects would be determined to have a less than significant individual and cumulative impact for GHG emissions. As shown in the Project GHG Reduction Measures Screening Table, the Project garners a total of 103 points, and is therefore consistent with the reduction quantities anticipated in the City's CAP. Therefore, quantification of Project-specific GHG emissions is not required.

Additionally, pursuant to Public Resources Code Section 21083.3, this impact need not be analyzed further, because (1) the proposed project would result in an impact that was previously analyzed in the Certified EIR, which was certified by the City; (2) the proposed project would not result in any greenhouse gas impacts that were not addressed in the Certified EIR; (3) the proposed project is consistent with The Ontario Plan. The proposed impacts of the project were already analyzed in the Certified EIR and the project will be built to current energy efficient standards. Potential impacts of project implementation will be less than significant with mitigation already required under the Certified EIR and current energy efficiency standards. No changes or additions to the Certified EIR analyses are necessary.

Mitigation Required: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to TOP EIR analyses are necessary. The mitigation measures adopted as part of TOP EIR adequately address any potential significant impacts and there is no need for any additional mitigation measures. The City has reviewed the emission reduction measures and concepts in The Ontario Plan EIR's MM 6-2 and 6-3, and has determined that the following actions apply and shall be undertaken by the applicant in connection with the project: energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations.

b. Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The proposed Project is consistent with The Ontario Plan Goal ER 4 of improving air quality by, among other things, implementation of Policy ER4-3, regarding the reduction of greenhouse gas emissions in

accordance with regional, State, and federal regulations. In addition, the proposed Project is consistent with the policies outlined in Section 5.6.4 of the Environmental Impact Report for The Ontario Plan, which aims to reduce the City's contribution of greenhouse gas emissions at build-out by fifteen (15 percent), because the project is upholding the applicable City's adopted mitigation measures as represented in 6-1 through 6-6 and energy efficient design, efficient irrigation systems, electric vehicle charging stations, and compliance with Title 24 of the California Code of Regulations. The Project is consistent with the City's Climate Action Plan. Therefore, the proposed Project does not conflict with an applicable plan, policy or regulation adopted for the purpose of reducing emissions of greenhouse gases.

Mitigation Required: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary

9. HAZARDS & HAZARDOUS MATERIALS. Would the project:

a. Create a significant hazard to the public or the environment through the routine transport, use or disposal of hazardous materials?

Discussion of Effects: The subject site was previously analyzed by TOP EIR as an industrial use. The project is not anticipated to involve the transport, use or disposal of hazardous materials during either construction or project implementation. Therefore, no adverse impacts are anticipated. However, in the unlikely event of an accident, implementation of the strategies included in The Ontario Plan will decrease the potential for health and safety risks from hazardous materials to a less than significant impact.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. The proposed project does not include the use of hazardous materials or volatile fuels. In addition, there are no known stationary commercial or industrial land uses within close proximity to the subject site, which use/store hazardous materials to the extent that they would pose a significant hazard to visitors/occupants to the subject site, in the event of an upset condition resulting in the release of a hazardous material.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances or waste within one-quarter mile of an existing or proposed school?

Discussion of Effects: The proposed project does not include the use, emissions or handling of hazardous or acutely hazardous materials, substances, or waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

Discussion of Effects: The proposed project site is not listed on the hazardous materials site compiled pursuant to Government Code Section 65962.5. Therefore, the project would not create a hazard to the public or the environment and no impact is anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. For a project located within the safety zone of the airport land use compatibility plan for ONT or Chino Airports, would the project result in a safety hazard for people residing or working in the project area?

Discussion of Effects: The proposed project was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The subject site is required to file and record an Avigation Easement with the Ontario International Airport Authority prior to obtaining a Certificate of Occupancy. The site is located within the airport influence area but outside the airport safety zones. The proposed Specific Plan Amendment to allow warehousing, distribution, and/or manufacturing uses in Planning Area 1 is compatible with the ALUCP. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

f. Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: The City's Safety Element, as contained within The Ontario Plan, includes policies and procedures to be administered in the event of a disaster. The Ontario Plan seeks interdepartmental and inter-jurisdictional coordination and collaboration to be prepared for, respond to and recover from every day and disaster emergencies. In addition, the project will comply with the requirements of the Ontario Fire Department and all City requirements for fire and other emergency access. Because the project is required to comply with all applicable City codes, any impacts would be reduced to a less than significant level.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

g. Expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires?

Discussion of Effects: The project site is not located in or near wildlands. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

10. HYDROLOGY & WATER QUALITY. Would the project:

a. Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

Discussion of Effects: The project site is served by City water and sewer service and will not affect water quality standards or waste discharge requirements. Discharge of storm water pollutants from areas of materials storage, vehicle or equipment fueling, vehicle or equipment maintenance (including washing, waste handling, hazardous materials handling or storage, delivery areas or loading docks, or other outdoor work) areas could result in a temporary increase in the amount of suspended solids, trash and debris, oil and grease, organic compounds, pesticides, nutrients, heavy metals and bacteria pathogens in surface flows during a concurrent storm event, thus resulting in surface water quality impacts. The site is required to comply with the statewide National Pollutant Discharge Elimination System ("NPDES") General Industrial Activities Stormwater Permit, the San Bernardino County Area-Wide Urban Runoff Permit (MS4 permit) and the City of Ontario's Municipal Code (Title 6, Chapter 6 (Stormwater Drainage System)). This would reduce any impacts to below a level of significance. Furthermore, any future applicant to develop the site would be required to submit a Preliminary Water Quality Management Plan ("PWQMP"), which would establish the site's compliance with storm water discharge and water quality management requirements. The PWQMP will include site design measures that capture runoff and pollutant transport by minimizing impervious surfaces and maximizes low impact development ("LID") best management practices ("BMPs"),

such as retention and infiltration, biotreatment and evapotranspiration. The PWQMP would include the use of an underground stormwater infiltration system for the site. Any overflow drainage from future development of the site will be conveyed to the public street by way of parkway culverts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. No increases in the current amount of water flow to the project site are anticipated, and the proposed project will not deplete groundwater supplies, nor will it interfere with recharge. The water use associated with the proposed use of the property was included in the Certified EIR analysis. The development of the site will require the grading of the site and excavation is expected to be less than three feet and would not affect the existing aquifer, estimated to be about 230 to 250 feet below the ground surface. No adverse impacts are anticipated.

Mitigation: No additional mitigation required.

c. Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would:

i. Result in substantial erosion or siltation on- or off-site?

Discussion of Effects: It is not anticipated that the Project would alter the drainage pattern of the site or area, in a manner that would result in erosion, siltation or flooding on-or-off site, nor will the proposed Project increase the erosion of the subject site or surrounding areas. The existing drainage pattern of the site will not be altered, and it will have no significant impact on downstream hydrology. Stormwater generated by the project will be discharged in compliance with the statewide NPDES General Construction Activities Stormwater Permit and San Bernardino County MS4 permit requirements. With the full implementation of a Storm Water Pollution Prevention Plan developed in compliance with the General Construction Activities Permit requirements, the Best Management Practices included in the SWPPP, and a stormwater monitoring program would reduce any impacts to below a level of significance. No streams or streambeds are present on the site. No changes in erosion off-site are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Substantially increase the rate or amount of surface runoff water in a manner which would result in flooding on- or off-site?

Discussion of Effects: The proposed Project is not anticipated to increase the flow velocity or volume of storm water runoff to cause environmental harm from the site and will not create a burden on existing infrastructure. Furthermore, with the implementation of an approved Water Quality Management Plan developed for the site, in compliance with the San Bernardino County MS4 Permit requirements, stormwater runoff volume shall be reduced to below a level of significance.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?

Discussion of Effects: It is not anticipated that the project would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems or create or contribute stormwater runoff pollutants during construction and/or post-construction activity. The stormwater flows will enter an existing storm pipe in Jurupa Street. Pursuant to the requirements of The Ontario Plan, the City's Development Code, and the San Bernardino County MS4 Permit's "Water Quality Management Plan" ("WQMP"), individual developments must provide site drainage and WQMP plans

according to guidelines established by the City's Engineering Department. If master drainage facilities are not in place at the time of project development, then standard engineering practices for controlling post-development runoff may be required, which could include the construction of on-site storm water detention and/or retention/infiltration facilities. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Impede or redirect flood flows?

Discussion of Effects: Urbanization in the areas surrounding the project site have resulted in increased responsiveness of the basin to rainfall. The increase in impervious surfaces such as roofs, roads, and parking lots has resulted in a decrease in groundwater infiltration and larger storm surges. The Project site currently slopes southeast, and the existing drainage pattern is characterized by sheet flows that follow the slope to the northwest. The project site is not impacted by offsite flows. The project site is not located in a FEMA Firm Panel designated Flood Zone Risk, and according to the United States Fish and Wildlife Service National Wetlands Inventory ("NWI") no wetlands exist on the property. The Project could lead to the conversion of permeable surfaces to impermeable surfaces such as parking areas and building foundation areas. Any future development on the Project site would discharge onsite flows into an existing storm drain facility. As such, the proposed project would not impede or redirect flood flows. With adherence to existing federal, state, and local regulation no changes to the existing flood flows would occur.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?

Discussion of Effects: Impacts associated with flooding are primarily related to the construction or placement of structures in areas prone to flooding including within an unprotected 100-year flood zone, and in areas susceptible to high tides, tsunamis, seiches, mudflows or sea level rise. Specifically, structures placed in flood prone areas, if flooded, would be damaged, and could subject people to injury or death. The National Flood Insurance Act of 1968 requires the identification of floodplain areas and establishment of flood-risk zones within those areas. FEMA administers the programs and coordinates with communities to establish effective floodplain management standards. According to FEMA, the Project is not located in a known floodplain. Furthermore, this area is not known to flood and is not typically subjected to flooding. The Project site is not located in a floodplain as shown in Figure S-2 of TOP. The Project site is dominated by Agricultural fallow fields and does not contain any vegetation associated with riparian features. No wetlands have been mapped on the project site according to the NWI. According to the FEMA, the Project is not located in an area that is subject to flood hazard, tsunami, or seiche zones. The project site is located over 60 miles east of the Pacific Ocean and is not located in a mapped tsunami zone. Therefore, the project would not have a significant risk of flood hazard, tsunami, seiche zones, release of pollutants due to project inundation.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

Discussion of Effects: The Regional Water Quality Control Board's Basin Plan is designed to preserve and enhance water quality and protect the beneficial uses of all regional waters. Specifically, the Basin Plan (i) designates beneficial uses for surface and ground waters, (ii) sets narrative and numerical objectives that must be attained or maintained to protect the designated beneficial uses and conform to the state's anti-degradation policy, and (iii) describes implementation programs to protect all waters in the region. Development allowed by the Project would be required to adhere to requirements of the water quality control plan, including all existing regulation and permitting requirements. This would include the incorporation of best management practices ("BMPs") to protect water quality during construction and operational periods. Development of the Project would be subject to all existing water quality regulations

and programs, as described in the regulatory section above, including all applicable construction permits. Existing General Plan policies related to water quality would also be applicable to the Project. Implementation of these policies, in conjunction with compliance with existing regulatory programs, would ensure that water quality impacts related to the Project would be less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

11. LAND USE & PLANNING. Would the project:

a. Physically divide an established community?

Discussion of Effects: The project site is located in an area that is currently developed with urban land uses. This project will be of similar design and size to surrounding development. No adverse impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use. Planning Area 1 is zoned Office/R&A in applicable zoning. Amending the Specific Plan to allow industrial uses on Planning Area 1 will not create greater impacts than were identified in the Certified EIR. The proposed project does not interfere with any policies for environmental protection. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

12. MINERAL RESOURCES. Would the project:

a. Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?

Discussion of Effects: The Project site is located within a mostly developed area surrounded by urban land uses. There are no known mineral resources in the area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?

Discussion of Effects: There are no known mineral resources in the area. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

13. NOISE. Would the project result in:

a. Generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and the Project will not expose people to or generate noise levels in excess of standards as established in the Certified EIR (Section 5.12). The subject site is surrounded on the north, west, and south sides by industrial uses, to the east of the subject site is the Interstate 15 freeway, and the subject site is not within

the landing approach of the Ontario International Airport. No additional analysis will be required at the time of site development review.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Generation of excessive groundborne vibration or groundborne noise levels?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and the uses associated with this project normally do not induce groundborne vibrations. As such, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. For a project located within the vicinity of a private airstrip or the noise impact zones of the airport land use compatibility plan for ONT and Chino Airports, would the project expose people residing or working in the project area to excessive noise levels?

Discussion of Effects: The proposed Amendment was reviewed and found to be located within the Airport Influence Area of Ontario International Airport ("ONT") and was evaluated and found to be consistent with the policies and criteria of the Airport Land Use Compatibility Plan ("ALUCP") for ONT. The project site is located outside of the Safety, Noise Impact and Airspace Protection Zones. A portion of the project site is located within the 65-70 dB CNEL Noise Impact Zones; however, the proposed zone change is a compatible land use. In addition, the project site lies outside the boundaries of the Chino Airport Influence Area. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

14. POPULATION & HOUSING. Would the project:

a. Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of road or other infrastructure)?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and changing the zoning on Planning Area 1 consistent with the Industrial General Plan designation would not induce significant population growth. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Displace substantial numbers of existing people or housing, necessitating the construction of replacement housing elsewhere?

Discussion of Effects: The project site does not contain existing housing. Changing the permitted zoning on the site will not create existing housing impacts.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

15. PUBLIC SERVICES. Would the project:

a. Result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services:

i. Fire protection?

Discussion of Effects: The site is in a developed area currently served by the Ontario Fire Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

ii. Police protection?

Discussion of Effects: The site is in a developed area, currently served by the Ontario Police Department. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iii. Schools?

Discussion of Effects: Upon development, the Project will be required to pay school fees as prescribed by state law prior to the issuance of building permits. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

iv. Parks?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

v. Other public facilities?

Discussion of Effects: The site is in a developed area, currently served by the City of Ontario. The Project will not require the construction of any new facilities or alteration of any existing facilities or cause a decline in the levels of service, which could cause the need to construct new facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

16. RECREATION. Would the project:

a. Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would cause an increase in the use of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project include recreational facilities or require the construction or expansion of recreational facilities that have an adverse physical effect on the environment?

Discussion of Effects: This project is not proposing any new housing or large employment generator that would require the construction of neighborhood parks or other recreational facilities. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

17. TRANSPORTATION. Would the project:

a. Conflict with a program plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and is surrounded on the north, west, and south by industrial uses and on the east by the Interstate 15 freeway. The project is in an area that is mostly developed with all street improvements existing. As previously mentioned, a Trip Generation Analysis (See Appendix A) was performed to compare the number of trips forecast to be generated by the existing zoning and by the proposed Project. The Toyota-Ontario Business Park Specific Plan currently establishes the project site (Planning Area 1) with a maximum of 300,000 gross square feet of office and research and development land uses. Existing zoning trip generation forecast is based upon trip generation rates obtained from the Institute of Transportation Engineers, Trip Generation Manual, 10th Edition, 2017. Trip generation rates were determined for daily trips and AM/PM peak hour trips based on the General Office land use (ITE Land Use Code 710). The number of trips forecast to be generated by the existing zoning is determined by multiplying the trip generation rates by the maximum allowable land use quantity under existing zoning. As shown in the Trip Generation Comparison table on Page 4 of this document, maximum development under existing zoning is forecast to generate approximately 2,922 daily trips, including 348 trips during the AM peak hour and 345 trips during the PM peak hour. However as shown in Table 3 of the Trip Generation Analysis, the proposed Project is forecast to result in 2,528 fewer daily PCE trips compared to the maximum allowable development under existing zoning, including 299 fewer PCE trips during the AM peak hour and 293 fewer PCE trips during the PM peak hour. As such, the proposed Project is forecast to generate fewer trips than the existing zoning. Therefore, the traffic impacts will be consistent with and less than the traffic impacts projected and analyzed under the Certified EIR. The project will not create a substantial increase in the number of vehicle trips, traffic volume or congestion at intersections. Less than significant impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?

Discussion of Effects: CEQA Guidelines Section 15064.3 subdivision (b) has been included in the 2018 CEQA Guidelines as part of the implementation of SB 743 which requires local jurisdictions to use Vehicle Miles Travelled (VMT) instead of Level of Service (LOS) methodologies for the purpose of determining the significance of traffic impacts under CEQA. Also, as part of the implementation of SB 743 local jurisdiction are given until July 1, 2020 to develop and implement thresholds of significance criteria and methodologies for evaluating VMT under the new SB 743 requirements. The City of Ontario has not

yet established a VMT analysis threshold or analysis methodology. Therefore, impacts with respect to CEQA Guidelines Section 15064.3(b) are less than significant.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

Discussion of Effects: The Project is in an area that is mostly developed. All street improvements are complete, and no alterations are proposed for adjacent intersections or arterials. The Project will, therefore, not create a substantial increase in hazards due to a design feature. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Result in inadequate emergency access?

Discussion of Effects: Development of the Project will be designed to provide access for all emergency vehicles and will therefore not create an inadequate emergency access. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

e. Result in inadequate parking capacity?

Discussion of Effects: The zone change does not affect the parking capacity in the Specific Plan. Future development of the site will be required to meet parking standards established by the Ontario Development Code and will therefore not create an inadequate parking capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

18. TRIBAL CULTURAL RESOURCES. Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k)?

Discussion of Effects: The subject site was previously analyzed by the Certified EIR as an industrial use and is not listed in the California Register of Historic Resources or local register of historical resources. Amending the Specific Plan to include additional permitted industrial uses will not create greater impacts than were identified in the Certified EIR. In addition, the results of the records research compiled from the CHRIS-SCCIC, the Sacred Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (“TCR”) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (PRC): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. AB 52 (Gatto, 2014) is clear in stating that it is the responsibility of the Public Agency (e.g. Lead Agency) to consult with Native American tribes early in the CEQA process to allow tribal governments, lead agencies, and project proponents to discuss the appropriate level of environment review, identify and address potential adverse impacts to TCRs, and reduce the potential for delay and conflict in the environmental review process (see PRC Section 2108.3.2). Specifically, government-to-government consultation may provide “tribal knowledge” of the Project Area that can be used in identifying TCRs that cannot be obtained through other investigative means. The Project Site has been highly disturbed by modern human activities to include agricultural use from the 1940’s through the 1960’s and the development and construction of the Toyota Motors North American Parts Center and supporting infrastructure that would have displaced surface and

subsurface archaeological resources. Therefore, it is concluded that the proposed Project will not impact Tribal Cultural Resources or Native America artifacts relating to TCRs and as such, no mitigation measures are recommended.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Discussion of Effects: The subject site is not listed in the California Register of Historic Resources. It is anticipated that during the application process the Lead Agency will notify the tribes of the proposed Specific Plan Amendment and will commence AB 52 Consultations as specified in the regulations. In addition, the results of the records research compiled from the CHRIS-SCCIC, the Scared Lands File Search (commissioned through the NAHC) failed to indicate known Tribal Cultural Resources (“TCR”) within the Project boundaries or within a one-mile radius of the Project area as specified in Public Resources Code (“PRC”): 210741, 5020.1(k), or 5024. Moreover, there was no indication of known TCRs within the Project site or within a one-mile radius of the Project Area. No impacts are anticipated through Project implementation.

Mitigation: No new mitigation measures are required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

19. UTILITIES AND SERVICE SYSTEMS. Would the project:

a. Require or result in the relocation or construction of new or expanded water, wastewater treatment or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

Discussion of Effects: The proposed Project is served by City of Ontario water system and has a 12-inch water line available for connection in Jurupa Street and adequate water supply for the Project. The proposed Project is served by the City of Ontario sewer system, which has an 8-inch sewer line available for connection off Jurupa Street, and which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. RP-1 is not at capacity and future development of this Project site will not cause RP-1 to exceed capacity. The Project will therefore not require the construction of new water or wastewater treatment facilities, or the expansion of existing facilities. No impacts are anticipated.

The proposed project is served by the City of Ontario by a 72-inch storm drain located in Jurupa Street. The Project is required to meet the requirements of the Ontario Engineering Department regarding storm drain facilities. No impacts are anticipated.

As discussed in the energy section above, the Project will have less than significant impacts with regard to electric power and natural gas. In addition, the Project will not have an impact on telecommunications facilities.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? In making this determination, the City shall consider whether the project is subject to the water supply assessment requirements of Water Code Section 10910, et seq. (SB 610), and the requirements of Government Code Section 664737 (SB 221).

Discussion of Effects: The Project site is served by the City of Ontario water system. There is currently sufficient water supply available to the City of Ontario to serve this Project as per the findings of TOP EIR. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Result in a determination by the wastewater treatment provider that serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

Discussion of Effects: The Project site is served by the City of Ontario sewer system, which has waste treated by the Inland Empire Utilities Agency at the RP-1 treatment plant. RP-1 is not at capacity and future development of this project site will not cause RP-1 to exceed capacity. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Generate solid waste in excess of state or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

Discussion of Effects: City of Ontario serves the Project site. Currently, the City of Ontario contracts with a waste disposal company that transports trash to a landfill with sufficient capacity to handle the City's solid waste disposal needs. No impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to analyses are necessary.

e. Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Discussion of Effects: This Project complies with federal, state, and local statutes and regulations regarding solid waste. Therefore, no impacts are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

20. WILDFIRE. If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:

a. Substantially impair an adopted emergency response plan or emergency evacuation plan?

Discussion of Effects: Wildfire impacts were not analyzed in the Certified EIR. A discussion of potential wildfire impacts is provided herein.

The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability, or drainage changes?

Discussion of Effects: The Project site is not located in or near a state responsibility area nor is it located in or near lands classified as very high fire hazard severity zones. Therefore, no impacts are anticipated.

Mitigation: No new mitigation measures required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

21. MANDATORY FINDINGS OF SIGNIFICANCE.

a. Does the project have the potential to substantially degrade the quality of the environment, substantially reduce the habitat or a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, substantially reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory?

Discussion of Effects: The proposed Project does not have the potential to reduce wildlife habitat and threaten a wildlife species; therefore, no environmental impacts resulting from the Project are anticipated.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

b. Does the project have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals?

Discussion of Effects: The Project does not have the potential to achieve short-term environmental goals to the disadvantage of long-term environmental goals.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

c. Does the project have impacts that are individually limited, but cumulatively considerable? ("Cumulatively considerable" means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current project, and the effects of probable future projects.)

Discussion of Effects: The Project does not have impacts that are cumulatively considerable.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

d. Does the project have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly?

Discussion of Effects: The Project does not have environmental effects that will cause substantial adverse effects on human beings, either directly or indirectly.

Mitigation: No additional mitigation required. The Project will not result in any new, increased or substantially different impacts, other than those previously considered and addressed in the Certified EIR. No changes or additions to the Certified EIR analyses are necessary.

EARLIER ANALYSES

(Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, one or more effects have been adequately analyzed in an earlier EIR or Negative Declaration. Section 15063(c)(3)(D)):

- 1) **Earlier Analyses Used.** Identify earlier analyses used and state where they are available for review.
 - a) The Ontario Plan Final EIR
 - b) The Ontario Plan
 - c) City of Ontario Official Zoning Map
 - d) City of Ontario Development Code
 - e) Ontario International Airport Land Use Compatibility Plan
 - f) Ontario International Airport Land Use Compatibility Plan Negative Declaration (SCH 2011011081)

All documents listed above are on file with the City of Ontario Planning Department, 303 East "B" Street, Ontario, California 91764, (909) 395-2036.

- 2) **Impacts Adequately Addressed.** Identify which effects from the above checklist were within the scope of, and adequately analyzed in, an earlier document pursuant to applicable legal standards.

MITIGATION MEASURES

(For effects that are "Less than Significant with Mitigation Incorporated," describe the mitigation measures, which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.)

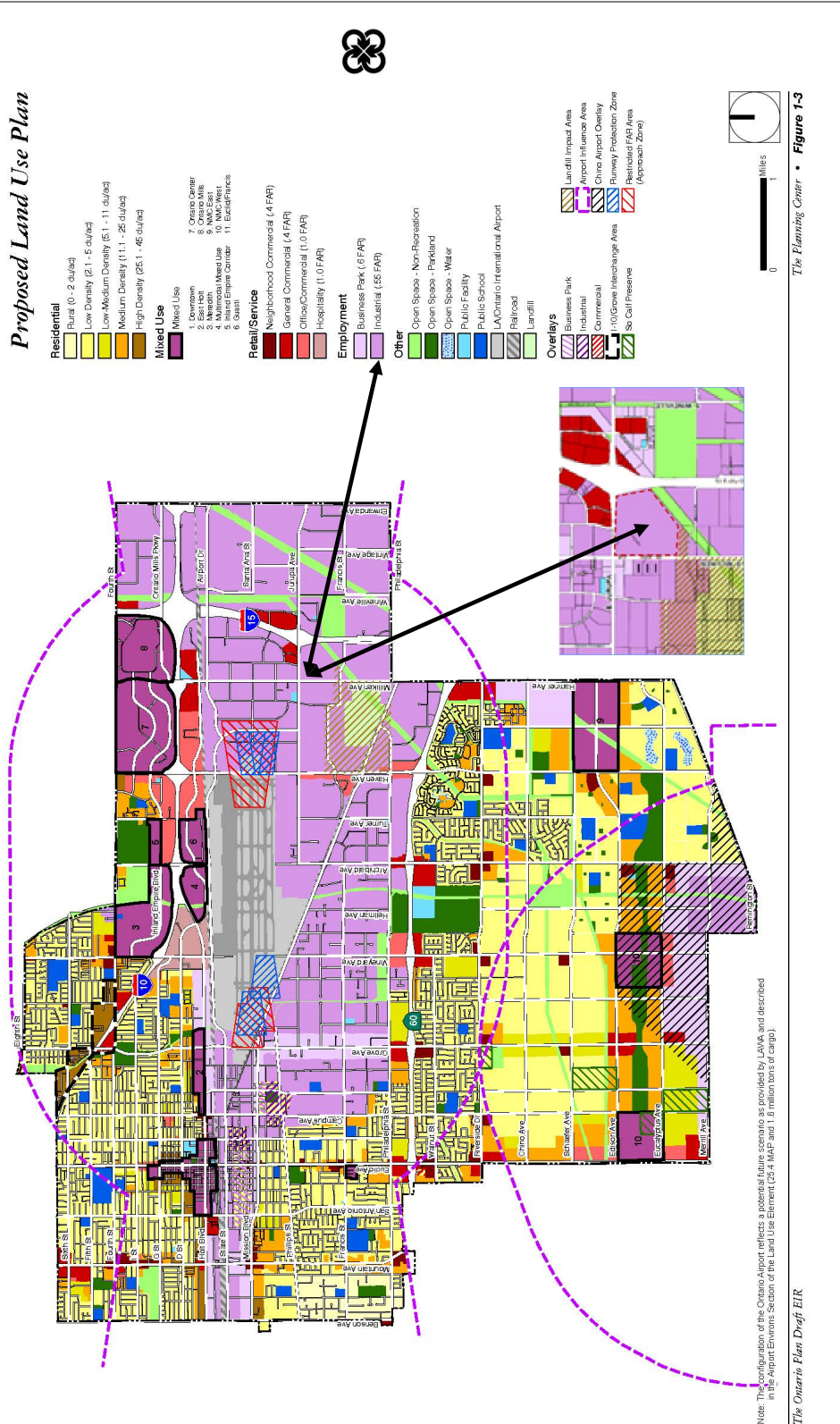
The Mitigation Measures contained in the Certified EIR adequately mitigate the impacts of the proposed Project. These mitigation measures are contained in the attached Mitigation Monitoring Program.

No additional mitigation beyond that previously imposed is required.

Exhibit A— Certified EIR Figure 1-3, Proposed Land Use Plan

1. Executive Summary

Proposed Land Use Plan



Mitigation Monitoring Program for The Ontario Plan Environmental Impact Report

1. Introduction

1.1 PURPOSE OF MITIGATION MONITORING PROGRAM

This Mitigation Monitoring Program has been developed to provide a vehicle by which to monitor mitigation measures and conditions of approval outlined in The Ontario Plan Environmental Impact Report (EIR), State Clearinghouse No. 2008101140. The Mitigation Monitoring Program has been prepared in conformance with Section 21081.6 of the Public Resources Code and City of Ontario Monitoring Requirements. Section 21081.6 states:

(a) When making the findings required by paragraph (1) of subdivision subsection (a) of Section 21081 or when adopting a mitigated negative declaration pursuant to paragraph (2) of subdivision (c) of Section 21080, the following requirements shall apply:

(1) The public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation. For those changes which have been required or incorporated into the project at the request of a responsible agency or a public agency having jurisdiction by law over natural resources affected by the project, that agency shall, if so requested by the lead agency or a responsible agency, prepare and submit a proposed reporting or monitoring program.

(2) The lead agency shall specify the location and custodian of the documents or other material which constitute the record of proceedings upon which its decision is based.

(b) A public agency shall provide that measures to mitigate or avoid significant effects on the environment are fully enforceable through permit conditions, agreements, or other measures. Conditions of project approval may be set forth in referenced documents which address required mitigation measures or, in the case of the adoption of a plan, policy, regulation, or other public project, by incorporating the mitigation measures into the plan, policy, regulation, or project design.

(c) Prior to the close of the public review period for a draft environmental impact report or mitigated negative declaration, a responsible agency, or a public agency having jurisdiction over natural resources affected by the project, shall either submit to the lead agency complete and detailed performance objectives for mitigation measures which would address the significant effects on the environment identified by the responsible agency or agency having jurisdiction over natural resources affected by the project, or refer the lead agency to appropriate, readily available guidelines or reference documents. Any mitigation measures submitted to a lead agency by a responsible agency or an agency having jurisdiction over natural resources affected by the project shall be limited to measures which mitigate impacts to resources which are subject to the statutory authority of, and definitions applicable to, that agency. Compliance or noncompliance by a responsible agency or agency having jurisdiction over natural resources affected by a project with that requirement shall not limit the authority of the responsible agency or agency having jurisdiction over natural resources affected by a project, or the authority of the lead agency, to approve, condition, or deny projects as provided by this division or any other provision of law.

1.2 PROJECT SUMMARY

The proposed project is the preparation of The Ontario Plan, which consists of a Vision, Governance Manual, Policy Plan, City Council Priorities, Implementation Plans, and Tracking and

Feedback. The Ontario Plan integrates components of city governance documents into a single guidance system that shapes the community 20 years or more into the future.

(a) The Ontario **Vision** describes the future community of Ontario. Its basic purpose is to improve the quality of life for the people of Ontario. It is the rationale and motivation for everything the City does.

(b) The **Governance Manual** describes the foundation for conducting the public's business on behalf of the present and future people of Ontario. It explains how The Ontario Plan is a tool for decision-making and communication.

(c) **City Council Priorities** define the short-term direction in City actions and initiatives. They are the primary means for exercising leadership in carrying out The Plan and realizing the Vision.

(d) The **Policy Plan** connects intent with action through the broad range of Goals and Policies that would guide the long-term growth and development required for the City to achieve its Vision. It also satisfies the California Government Code requirement for a general plan. Figure 3-6, *Proposed Land Use Plan*, shows the proposed General Plan land use designations that guide and regulate land use patterns, distributions, densities and intensities in the City of Ontario, including residential employment, retail, recreation, and public uses.

(e) **Implementation** consists of actions taken to carry out Plan policies. This includes initiatives by the City and decisions on public and private development projects.

(f) **Tracking and Feedback** allows the City to learn from experience and redirect efforts.

Pursuant to CEQA Guidelines Section 15064(d), the EIR considers the direct physical changes and reasonably foreseeable indirect physical changes in the environment that would be caused by The Ontario Plan. Consequently, the EIR focuses on impacts from changes to land use associated with buildout of the Proposed Land Use Plan, within the Policy Plan, and impacts from the resultant population and employment growth in the City. The Ontario Plan Proposed Land Use Plan for the ultimate development of the City is not linked to a timeline. However, for the purpose of this environmental analysis, buildout of the Proposed Land Use Plan is forecast for the year 2035.

1.3 PROJECT LOCATION

The City of Ontario is in the southwestern corner of San Bernardino County and is surrounded by the Cities of Chino and Montclair, and unincorporated areas of San Bernardino County to the west; the Cities of Upland and Rancho Cucamonga to the north; the City of Fontana and unincorporated land in San Bernardino County to the east; and unincorporated Riverside County land to the south. The City is in the central part of the Upper Santa Ana River Valley. This portion of the valley is bounded by the San Gabriel Mountains to the north; the Chino Hills, Puente Hills, and San Jose Hills to the west; the Santa Ana River to the south; and Lytle Creek Wash on the east.

The City comprises approximately 50 square miles (31,958 acres), which includes the 8,200-acre New Model Colony (NMC) in the southern portion of the City (formerly the City's Sphere of Influence). The northern urbanized portion of the City is known as the Original Model Colony (OMC). The City is generally bounded by Benson Avenue and Euclid Avenue on the west; Interstate 10 (I-10), 8th Street, and 4th Street on the north; Etiwanda Avenue and Hamner Avenue on the east; and Merrill Avenue and the San Bernardino County/Riverside County boundary on the south. Regional circulation to and through the City is provided by I-10 and State Route 60 (SR-60) east-west, and by I-15 and SR-83 (Euclid Avenue) north-south.

1.4 ENVIRONMENTAL IMPACTS

The environmental document for this project is a “program EIR” as defined by State CEQA Guidelines (Section 15161, California Code of Regulations, Title 14, Division 6, Chapter 3). As provided in Section 15168 of the State CEQA Guidelines, a Program EIR may be prepared on a series of actions that may be characterized as one large project that are related either 1) geographically; 2) as logical parts of a chain of contemplated events; 3) in connection with issuance of rules, regulations, plans, or other general criteria to govern the conduct of a continuing program; or 4) as individual activities carried out under the same authorizing statutory or regulatory authority and have generally similar environmental effects that can be mitigated in similar ways.

Although the legally required contents of a Program EIR are the same as those of a Project EIR, Program EIRs are typically more conceptual and may contain a more general discussion of impacts, alternatives, and mitigation measures than a Project EIR. Once a Program EIR has been prepared, subsequent activities within the program must be evaluated to determine whether an additional CEQA document needs to be prepared. However, if the Program EIR addresses the program’s effects as specifically and comprehensively as possible, many subsequent activities could be found to be within the Program EIR scope and additional environmental documents may not be required (Guidelines Section 15168[c]). When a Program EIR is relied on for a subsequent activity, the lead agency must incorporate feasible mitigation measures and alternatives developed in the Program EIR into the subsequent activities (Guidelines Section 15168[c][1]). If a later activity would have effects that were not examined in the Program EIR, a new Initial Study would need to be prepared leading to either an EIR or a Negative Declaration. Pursuant to Sections 15126.2 and 15126.4 of the State CEQA Guidelines, the EIR should identify any potentially significant adverse impacts and recommend mitigation that would reduce or eliminate these impacts to levels of insignificance.

1.4.1 Impacts Considered Less Than Significant

Ten environmental categories are identified as having less than significant impacts that do not require mitigation. These categories are:

- Aesthetics
- Biological Resources
- Geology/Soils
- Hazards & Hazardous Materials
- Hydrology/Water Quality
- Land Use & Planning
- Mineral Resources
- Population and Housing
- Public Services
- Recreation

1.4.2 Potentially Significant Adverse Impacts That Can Be Mitigated, Avoided, or Substantially Lessened

The following have been identified as potentially resulting in significant adverse impacts that can be mitigated, avoided, or substantially lessened:

- Cultural Resources: Mitigation Measures 5-2 through 5-4 would reduce archeological and prehistoric cultural resource impacts to less than significant.
- Noise: Mitigation Measure 12-3 would ensure that any new vibration-sensitive structures near the Union Pacific Railroad or Southern California Regional Rail Authority right-of-way would be constructed so that train-related vibration would not be perceptible and operational vibration impacts would be less than significant.
- Utilities and Service Systems: Mitigation Measures 17-1 through 17-4 would reduce impacts on water supply and demand from buildout of The Ontario Plan to less than significant.

1.4.3 Unavoidable Significant Adverse Impacts

There are six environmental categories considered to have impacts that would be significant and unavoidable and would not be lessened through mitigation.

Agricultural Resources

Buildout of The Ontario Plan would convert 3,269.3 acres of California Resource Agency designated Prime Farmland, Unique Farmland, and Farmland of Statewide Importance to residential, commercial, mixed-use, and industrial land uses. Consequently, impacts to Farmland would remain significant and unavoidable.

There are a number of Williamson Act contracts within the City that have yet to expire. Buildout of The Ontario Plan would most likely require the cancellation or nonrenewal of these contracts. The current use of these contracts would slow the rate of conversion from agricultural to nonagricultural land, but it would not impede the conversion. Since there are some Williamson Act contracts still active in the New Model Colony, implementation of the proposed land use plan for The Ontario Plan would conflict with these contracts and cause a significant impact. Consequently, impacts to Williamson Act contracts would remain significant and unavoidable.

Development of the City in accordance with The Ontario Plan would increase the amount of nonagricultural land uses. When nonagricultural land uses are placed near agricultural uses, the odors, noises, and other hazards related to agriculture conflict with the activities and the quality of life of the people living and working in the surrounding areas. Consequently, conversion of agricultural uses in the city may cause farms and agricultural land uses outside the City to be converted to nonagricultural uses because of the nuisances related to agriculture and impacts would remain significant and unavoidable.

Air Quality

The project would not be consistent with the Air Quality Management Plan (AQMP) because air pollutant emissions associated with buildout of the City of Ontario would cumulatively contribute to the nonattainment designations in the South Coast Air Basin (SoCAB). Furthermore, buildout of the Proposed Land Use Plan would exceed current estimates of population, employment, and vehicle miles traveled for Ontario and therefore these emissions are not included in the current regional emissions inventory for the SoCAB. As both criteria must be met in order for a project to be considered consistent with the AQMP, the project would be considered inconsistent with the AQMP. Consequently, this impact would remain significant and unavoidable.

Construction activities associated with buildout of The Ontario Plan would generate short-term emissions that exceed the South Coast Air Quality Management District's (SCAQMD) regional significance thresholds; cumulatively contribute to the SoCAB's nonattainment designations for ozone (O₃), coarse inhalable particulate matter (PM₁₀), and fine inhalable particulate matter (PM_{2.5}); and potentially elevate concentrations of air pollutants at sensitive receptors. Mitigation Measure 3-1 would reduce The Ontario Plan's short-term construction-related volatile organic compounds (VOC), carbon monoxide (CO), oxides of nitrogen (NO_x), PM₁₀, and PM_{2.5} emissions but they would not be reduced to levels below the SCAQMD's regional thresholds and they would not reduce these impacts to less than significant. Consequently, construction air pollutant emissions generated by buildout of The Ontario Plan would remain significant and unavoidable.

Buildout of The Ontario Plan would generate long-term emissions that would exceed SCAQMD'S regional significance thresholds and cumulatively contribute to the SoCAB nonattainment designations for O₃, PM₁₀, and PM_{2.5}. Mitigation Measure 3-2 would reduce long-term operational emissions of VOC, CO, NO_x, PM₁₀, and PM_{2.5} related to the buildout of The Ontario Plan but they would not reduce these emissions to levels below the SCAQMD's regional significance thresholds and impacts would not be less than significant. Consequently, operational impacts from buildout of The Ontario Plan would remain significant and unavoidable.

Approval of residential and other sensitive land uses within 500 feet of Interstate-10, Interstate-15, or State Route-60 would result in exposure of persons to substantial concentrations of diesel particulate matter. Mitigation Measure 3-3 would reduce impacts related to the exposure of sensitive receptors (residential and other sensitive land uses) to diesel particulate matter because of their placement near freeways within the City. However, it would not reduce this impact to be less than significant.

Conversion of agricultural land to nonagricultural uses would temporarily expose residents to objectionable odors and impacts would remain significant and unavoidable.

Cultural Resources

Although protective regulations are in place and preservation policies are included in The Ontario Plan, implementation of the Proposed Land Use Plan, especially within growth focus areas, has the potential to impact Tier III historic resources. Mitigation Measure 5-1 would require a historical evaluation for properties within historic resources in the Focus Areas under the City's ordinance. However, the ordinance does not provide a high level of protection for Tier III resources. As a result, demolition of historical resources categorized under the Ordinance as Tier III could potentially be impacted with implementation of the Proposed Land Use Plan. Consequently, Tier III historic resource impacts would remain significant and unavoidable.

Global Climate Change

Buildout of the City of Ontario would generate greenhouse gas emissions that would significantly contribute to global climate change impacts in California. Greenhouse gas (GHG) emissions generated in the City would significantly contribute to climate change impacts in California as a result of the growth in population and employment in the City and scale of development activity associated with buildout of the Proposed Land Use Plan. Mitigation Measures 6-1 through 6-6 would act to reduce the contributions of The Ontario Plan to global climate change but they would not reduce the impacts to less than significant.

Noise

Buildout of the Proposed Land Use Plan would result in an increase in traffic on local roadways in the City of Ontario, which would substantially increase noise levels. Consequently, impacts would remain significant and unavoidable.

Noise-sensitive uses could be exposed to elevated noise levels from transportation sources. Any siting of new sensitive land uses within a noise environment that exceeds the normally acceptable land use compatibility criterion would result in a potentially significant impact and would require a separate noise study through the development review process to determine the level of impacts and required mitigation. Mitigation Measure 12-1 would decrease the exposure of sensitive receptors to excessive noise levels within 65 dBA CNEL contours, whether near Los Angeles/Ontario International Airport (LAONT) or other noise-producing areas such as freeways and railroads, but it would not reduce these impacts to less than significant.

Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would expose sensitive uses to strong levels of groundborne vibration. Mitigation Measure 12-2 would reduce the impacts caused by construction-related vibrations on sensitive receptors but it would not reduce the impact to less than significant.

Impact 5.12-5. Significant. Construction activities associated with buildout of the individual land uses associated with the Proposed Land Use Plan would substantially elevate noise levels in the vicinity of sensitive land uses. Mitigation Measure 12-4 calls for the use of noise-reducing techniques during construction projects that would impact nearby sensitive receptors, such as the use of temporary sound walls and reduced unnecessary truck idling. However, these impacts would not be reduced to levels considered less than significant.

Noise-sensitive land uses within the 65 dBA CNEL contour of the Los Angeles/Ontario International Airport would be exposed to substantial levels of airport-related noise. Consequently, impacts would remain significant and unavoidable.

Transportation and Traffic

The increased development and population growth associated with the buildout of the Proposed Land Use Plan would cause deficient levels of service at area intersections without implementation of the recommended lane geometry improvements. In addition, buildout of the Proposed Land Use Plan would also cumulatively contribute to the cumulatively significant freeway level of service impact that is already projected to occur in the future. Mitigation Measure 16-1 would require the buildout of The Ontario Plan to be consistent with the traffic study prepared by Kimley-Horn and Associates. This traffic study indicates the appropriate lane geometry for area intersections. This would allow for intersections to have LOS values of E or above but it would not improve the cumulative freeway LOS standards to appropriate levels. The City has no jurisdiction over Caltrans projects, such as freeway improvements. Therefore, the impacts related to cumulative LOS deficiencies on freeways would not be reduced to levels considered less than significant.

2. Mitigation Monitoring Process

2.1 MITIGATION MONITORING PROGRAM ORGANIZATION

CEQA requires that a reporting or monitoring program be adopted for the conditions of project approval that are necessary to mitigate or avoid significant effects on the environment (Public Resources Code 21081.6). The mitigation monitoring and reporting program is designed to ensure compliance with adopted mitigation measures during project implementation. For each mitigation measure recommended in the Environmental Impact Report, specifications are made herein that identify the action required and the monitoring that must occur. In addition, a responsible agency is identified for verifying compliance with individual conditions of approval contained in the Mitigation Monitoring Program (MMP). In order to effectively track and document the status of mitigation measures, a mitigation matrix has been prepared and includes:

- Responsibility for implementation
- Timing
- Responsibility for monitoring
- Monitor

Mitigation measure timing of verification has been apportioned into several specific timing increments. Of these, the most common are:

- Prior to project approval
- Prior to issuance of grading permit(s)
- During construction

Information pertaining to compliance with mitigation measures or any necessary modifications or refinements will be documented in the comments portion of the matrix.

2.2 MITIGATION MONITORING PROCEDURES

The City of Ontario Planning Department is the designated lead agency for the Mitigation Monitoring and Reporting Program. The City of Ontario includes the Mitigation Measures within the Special Conditions of Approval. The City is responsible for review of all monitoring reports, enforcement actions, and document disposition. The Planning Department shall designate a Project Mitigation Monitor for the proposed project.

2.2.1 In-Field Monitoring

The Responsible Monitoring Party shall exercise caution and professional practices at all times when monitoring construction. Protective wear (hard hats, glasses, etc.) shall be worn at all times in construction areas. Injuries shall be reported immediately to the Project Mitigation Monitor.

2.2.2 Coordination with Contractors

The construction manager/superintendent is responsible for coordination of contractors and for contractor completion of required measures in accordance with the provisions of this program.

2.2.3 Recognized Experts

The use of recognized experts as a component of the monitoring team is required to ensure compliance with scientific and engineering mitigation measures. While the recognized experts assess

compliance with required mitigation measures, consultation with the City of Ontario planning staff shall take place in the event of a dispute.

2.2.4 Enforcement

Agencies may enforce conditions of approval through their existing police power, using stop-work orders, fines, infraction citations, loss of entitlements, refusal to issue building permits or certificates of use and occupancy or, in some cases, notice of violation for tax purposes. Criminal misdemeanor sanctions could be available where the agency has adopted an ordinance requiring compliance with the monitoring program, similar to the provision in many zoning ordinances that affirm the enforcement power to bring suit against violators of the ordinances.

3. Mitigation Monitoring Requirements

3.1 CATEGORIZED MITIGATION MEASURES/MATRIX

Project-specific mitigation measures have been categorized in matrix format, as shown in Table 3-1. The matrix identifies the environmental factor, specific mitigation measures, schedule, and responsible monitor. The mitigation matrix will serve as the basis for scheduling the implementation of, and compliance with, all mitigation measures.

3.2 IN-FIELD MONITORING

Project monitors and technical subconsultants shall exercise caution and professional practices at all times when monitoring implementation of mitigation measures. Protective wear (e.g., hard hat, glasses) shall be worn at all times in construction areas. Injuries shall be immediately reported to the mitigation monitoring committee.

3.3 DATABASE MANAGEMENT

All mitigation monitoring reports, letters, and memos shall be prepared using Microsoft Word software on IBM-compatible PCs and processed according to the City's Environmental Compliance Program.

3.4 COORDINATION WITH CONTRACTORS

The construction manager is responsible for coordination of contractors and for contractor completion of required mitigation measures.

3.5 LONG-TERM MONITORING

Long-term monitoring related to several mitigation measures will be required, including fire safety inspections. Post-construction fire inspections are conducted on a routine basis by the Ontario Fire Department.

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
5.3 AIR QUALITY				
<p>3-1 The City of Ontario Building Department shall require that all new construction projects incorporate feasible mitigation measures to reduce air quality emissions. Potential measures shall be incorporated as conditions of approval for a project and may include:</p> <ul style="list-style-type: none"> • Requiring fugitive dust control measures that exceed South Coast Air Quality Management District's Rule 403, such as: <ul style="list-style-type: none"> ○ Requiring use of nontoxic soil stabilizers to reduce wind erosion. ○ Applying water every four hours to active soil-disturbing activities. ○ Tarping and/or maintaining a minimum of 24 inches of freeboard on trucks hauling dirt, sand, soil, or other loose materials. • Using construction equipment rated by the United States Environmental Protection Agency as having Tier 3 or higher exhaust emission limits. • Ensuring construction equipment is properly serviced and maintained to the manufacturer's standards. • Limiting nonessential idling of construction equipment to no more than five consecutive minutes. • Using Super-Compliant VOC paints for coating of architectural surfaces whenever possible. A list of Super-Compliant architectural coating manufactures can be found on the South Coast Air Quality Management District's website at: http://www.aqmd.gov/prdas/brochures/Super-Compliant_AIM.pdf . 	<p>City of Ontario Building Department in coordination with the landowner/project applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building and Department and Developer/Contractor</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
3-2 The City of Ontario shall evaluate new development proposals within the City and require all developments to include access or linkages to alternative modes of transportation, such as transit stops, bike paths, and/or pedestrian paths (e.g., sidewalks).	City of Ontario Planning/Engineering Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
3-3 The City of Ontario shall evaluate new development proposals within the City for potential incompatibilities with regard to the California Air Resources Board's <i>Air Quality and Land Use Handbook: A Community Health Perspective</i> (April 2005). New development that is inconsistent with the recommended buffer distances shall only be approved if feasible mitigation measures, such as high efficiency Minimum Efficiency Reporting Value filters have been incorporated into the project design to protect future sensitive receptors from harmful concentrations of air pollutants as a result of proximity to existing air pollution sources.	City of Ontario Planning Department in coordination with the landowner/project applicant	Prior to individual project approvals	City of Ontario Planning Department	
5.5 CULTURAL RESOURCES				
5-1 Historic or potentially historic resources in the City shall be evaluated for historic significance through the City's tier system prior to the issuance of plan or development approvals.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5-2 In areas of documented or inferred archaeological and/or paleontological resource presence, City staff shall require applicants for development permits to provide studies to document the presence/absence of such resources. On properties where resources are identified, such studies shall provide a detailed mitigation plan, including a monitoring program and recovery and/or in situ preservation plan, based on the recommendations of a qualified cultural preservation expert. The mitigation plan shall include the following requirements:	City of Ontario Planning Department in coordination with the Landowner/Project Applicant	Prior to individual project approvals	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Archaeologists and/or paleontologist shall be retained for the project and will be on call during grading and other significant ground-disturbing activities. • Should any cultural resources be discovered, no further grading shall occur in the area of the discovery until the Planning Director or designee is satisfied that adequate provisions are in place to protect these resources. • Unanticipated discoveries shall be evaluated for significance by a San Bernardino County Certified Professional Archaeologist/Paleontologist. If significance criteria are met, then the project shall be required to perform data recovery, professional identification, radiocarbon dates, and other special studies; submit materials to a museum for permanent curation; and provide a comprehensive final report including catalog with museum numbers. 				
<p>5-3 Upon receipt of an application for a Specific Plan or a project that requires a General Plan amendment subject to CEQA and is within the City's jurisdiction, the City's representative shall consult with the relevant tribe(s)' representative(s) to determine if the proposed project is within a culturally sensitive area to the tribe. If sufficient evidence is provided to reasonably ascertain that the site is within a [tribal] culturally sensitive area, then a cultural resources assessment prepared by an archaeologist shall be required. The findings of the cultural resources assessment shall be incorporated into the CEQA documentation. A copy of the report shall be forwarded to the tribe(s). If mitigation is recommended in the CEQA document, the procedure described in Mitigation Measure 5-4 shall be followed.</p>	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
<p>5-4 Prior to the issuance of grading permits for a Specific Plan or project that requires a General Plan amendment for which the CEQA document defines cultural resource mitigation for potential tribal resources, the project applicant</p>	City of Ontario Planning Department in coordination with the	Prior to issuance of grading permit(s)	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
shall contact the designated tribe(s) to notify them of the grading, excavation, and monitoring program. The applicant shall coordinate with the City of Ontario and the tribal representative(s) to develop mitigation measures that address the designation, responsibilities, and participation of tribal monitors during grading, excavation, and ground-disturbing activities; scheduling; terms of compensation; and treatment and final disposition of any cultural resources, sacred sites, and human remains discovered on the site. The City of Ontario shall be the final arbiter of the conditions for projects within the City's jurisdiction.	Landowner/Project Applicant			
5.6 Global Climate Change				
<p>6-1 The City of Ontario shall prepare a Climate Action Plan within 18 months after adopting The Ontario Plan. The goal of the Climate Action Plan shall be to reduce GHG emissions from all activities within the City boundaries to support the State's efforts under AB 32 and to mitigate the impact of climate change on the City, State, and world. Once completed, the City shall update The Ontario Plan and associated policies, as necessary, to be consistent with the Climate Action Plan and prepare a subsequent or supplemental Environmental Impact Report, if new significant impacts are identified. The Climate Action Plan shall include the following:</p> <ul style="list-style-type: none"> • Emission Inventories: The City shall establish GHG emissions inventories including emissions from all sectors within the City, using methods approved by, or consistent with guidance from, the CARB; the City shall update inventories every 3 years or as determined by state standards to incorporate improved methods, better data, and more accurate tools and methods, and to assess progress. If the City is not on-schedule to achieve the GHG reduction targets, additional measured shall be implemented, as identified in the CAP. 	City of Ontario	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department/ Municipal Utilities Agency (MUA)	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ The City shall establish a baseline inventory of GHG emissions including municipal emissions, and emissions from all business sectors and the community. ○ The City shall define a “business as usual” scenario of municipal, economic, and community activities, and prepare a projected inventory for 2020 based on that scenario. ● Emission Targets: The City will develop Plans to reduce or encourage reductions in GHG emissions from all sectors within the City: <ul style="list-style-type: none"> ○ A Municipal Climate Action Plan which shall include measures to reduce GHG emissions from municipal activities by at least 30 percent by 2020 compared to the "business as usual" municipal emissions (including any reductions required by the California Air Resource Board under AB 32. ○ A Business Climate Action Plan in collaboration with the business community, which shall include measures to reduce GHG emissions from business activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" business emissions. ○ A Community Climate Action Plan in collaboration with the stakeholders from the community at large, which shall include measures reduce GHG emissions from community activities, and which shall seek to reduce emissions by at least 30 percent by 2020 compared to "business as usual" community emissions. 				
<p>6-2 The Climate Action Plan shall include specific measures to achieve the GHG emissions reduction targets identified in Mitigation Measure 6-1. The Climate Action Plan shall quantify the approximate greenhouse gas emissions reductions of each measure and measures shall be</p>	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>enforceable. Measures listed below, along with others, shall be considered during the development of the Climate Action Plan (CAP):</p> <ul style="list-style-type: none"> • Require all new or renovated municipal buildings to seek Silver or higher Leadership in Energy and Environmental Design (LEED) standard, or compliance with similar green building rating criteria. • Require all municipal fleet purchases to be fuel efficient vehicles for their intended use based on the fuel type, design, size, and cost efficiency. • Require that new development projects in Ontario that require demolition prepare a demolition plan to reduce waste by recycling and/or salvaging a nonhazardous construction and demolition debris. • Require that new developments design buildings to be energy efficient by siting buildings to take advantage of shade, prevailing winds, landscaping, and sun screening to reduce energy required for cooling. • Require that cool roofs for non-residential development and cool pavement to be incorporated into the site/building design for new development where appropriate. • Evaluate the feasibility of implementing a Public Transit Fee to support Omnitrans in developing additional transit service in the City. • Require diesel emission reduction strategies to eliminate and/or reduce idling at truck stops, warehouses, and distribution facilities throughout the City. • Install energy efficient lighting and lighting control systems in all municipal buildings. • Require all new traffic lights installed be energy efficient traffic signals. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Require the use of reclaimed water for landscape irrigation in all new development and on public property where such connections are within the service boundaries of the City's reclaimed water system. • Require all new landscaping irrigation systems installed within the City to be automated, high-efficient irrigation systems to reduce water use and require use of bubbler irrigation; low-angle, low-flow spray heads; or moisture sensors. • Conduct energy efficiency audits of existing municipal buildings by checking, repairing, and readjusting heating, ventilation, and air conditioning systems, lighting, water heating equipment, insulation, and weatherization. • Ensure that its local Climate Action, Land Use, Housing, and Transportation Plans are aligned with, support, and enhance any regional plans that have been developed consistent with state guidance to achieve reductions in GHG emissions. • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping. • Work with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking. • Provide safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Support and promote the use of low-and zero-emission vehicles, by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient green building practices where not prohibited by Airport Land Use Compatibility Plan (ALUCP)/Federal Aviation Administration (FAA). • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Providing information, marketing, training, and technical assistance about green building practices. ○ Adopting a Green Building ordinance with guidelines for green building practices in residential and commercial development. ○ Adopt energy efficiency performance standards for buildings designed to achieve a greater reduction in energy and water use than currently required by state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. • Encourage the performance of energy audits for residential and commercial buildings prior to completion of sale, and that audit results and information about opportunities for energy efficiency improvements be presented to the buyer. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish policies and programs that facilitate the siting of new renewable energy generation. • Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. • Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.).</p> <ul style="list-style-type: none"> ○ Installing irrigation control systems which maximize water use efficiency and minimize off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. • Ensure that staff receives appropriate training and support to implement objectives and policies to reduce GHG emissions, including: <ul style="list-style-type: none"> ○ Providing energy efficiency training to design, engineering, building operations, and maintenance staff. ○ Providing information on energy use and management, including data from the tracking and management system, to managers and others making decisions that influence energy use. ○ Providing energy design review services to departments undertaking new construction or renovation projects, to facilitate compliance with LEED standards. • Maximize efficiency at drinking water treatment, pumping, and distribution facilities, including development of off-peak demand schedules for heavy commercial and industrial users. • Establish a replacement policy and schedule to replace fleet vehicles and equipment with the most fuel-efficient vehicles practical, including gasoline hybrid and alternative fuel or electric models. • Require the installation of outdoor electrical outlets on buildings to support the use, where practical, of 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>electric lawn and garden equipment, and other tools that would otherwise be run with small gas engines or portable generators.</p> <ul style="list-style-type: none"> • Implement measures to reduce employee vehicle trips and to mitigate emissions impacts from municipal travel. • Conduct a comprehensive inventory and analysis of the urban forest, and coordinate tree maintenance responsibilities with all responsible departments, consistent with best management practices. • Evaluate existing landscaping and options to convert reflective and impervious surfaces to landscaping and will install or replace vegetation with drought-tolerant, low- maintenance native species or edible landscaping that can also provide shade and reduce heat-island effects. • Implement enhanced programs to divert solid waste from landfill operations, by: <ul style="list-style-type: none"> ○ Establishing a diversion target which meets or exceeds AB 939 requirements. ○ Promoting and expanding recycling programs, purchasing policies, and employee education to reduce the amount of waste produced. • Reduce per capita water consumption consistent with state law by 2020. • Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with state law. • Establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and checklists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low- flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Organize workshops on waste reduction activities for the home or business, such as backyard composting, or office paper recycling, and shall schedule recycling drop-off events and neighborhood chipping/mulching days. • Organize workshops on steps to increase energy efficiency in the home or business, such as weatherizing the home or building envelope, installing smart lighting systems, and how to conduct a self-audit for energy use and efficiency. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>6-3 The City of Ontario will amend the Municipal Code within 18 months after adopting The Ontario Plan, with provisions implementing the following GHG emission reduction concepts:</p> <ul style="list-style-type: none"> • Increase densities in urban core areas to support public transit, by, among other means: <ul style="list-style-type: none"> ○ Removing barriers to the development of accessory dwelling units in existing residential neighborhoods. • Reduce required road width standards wherever feasible to calm traffic and encourage alternative modes of transportation. • Add bicycle facilities to city streets and public spaces, where feasible. • Promote infill, mixed-use, and higher density development, and provide incentives to support the creation of affordable housing in mixed use zones. • Plan for and create incentives for mixed-use development. • Identify sites suitable for mixed-use development and establish appropriate site- specific standards to accommodate mixed uses which could include: <ul style="list-style-type: none"> ○ Increasing allowable building height or allow height limit bonuses, in appropriate areas and where safe to do so. ○ Allowing flexibility in applying development standards (such as FAR2 and lot coverage) based on the location, type, and size of the units, and the design of the development. ○ Allowing reduced and shared parking based on the use mix, and availability of and proximity to public transit stops. 	City of Ontario Planning Department	Within 18 months of adopting The Ontario Plan	City of Ontario Planning Department	

**Table 3-1
 Mitigation Monitoring Requirements**

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Allowing for tandem parking, shared parking and off-site parking leases. • Enable prototype mixed-use structures for use in neighborhood center zones that can be adapted to new uses over time with minimal internal remodeling. • Identify and facilitate the inclusion of complementary land uses not already present in local zoning districts, such as supermarkets, parks and recreational fields, schools in neighborhoods, and residential uses in business districts, to reduce the vehicle miles traveled and promote bicycling and walking to these uses. • Revise zoning ordinance(s) to allow local-serving businesses, such as childcare centers, restaurants, banks, family medical offices, drug stores, and other similar services near employment centers to minimize midday vehicle use. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Implement a Housing Overlay Zone for residential properties at transit centers and along transit corridors. This may include average minimum residential densities of 25 units per acre within one quarter miles of transit centers; average minimum densities of 15 units per acre within one quarter mile of transit corridors; and minimum FAR of 0.5:1 for non- residential uses within a quarter mile of transit centers or corridors. • Identify transit centers appropriate for mixed-use development, and promote transit-oriented, mixed-use development within these targeted areas, by: <ul style="list-style-type: none"> ○ Providing maximum parking standards and flexible building height limitations. ○ Providing density bonus programs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing guidelines for private and public spaces for transit-oriented and mixed-use development. ○ Discouraging auto-oriented development. • Ensure new development is designed to make public transit a viable choice for residents, including: <ul style="list-style-type: none"> ○ Locating medium to high density development near activity centers that can be served efficiently by public transit and alternative transportation modes. ○ Locating medium to high density development near streets served by public transit whenever feasible. ○ Linking neighborhoods to bus stops by continuous sidewalks or pedestrian paths. • Develop form-based community design standards to be applied to development projects and land use plans, for areas designated mixed-use. • Create and preserve distinct, identifiable neighborhoods whose characteristics support pedestrian travel, especially within, but not limited to, mixed-use and transit-oriented development areas, by: <ul style="list-style-type: none"> ○ Designing or maintaining neighborhoods where the neighborhood amenities can be reached in approximately five minutes of walking. ○ Encouraging pedestrian-only streets and/or plazas within developments, and destinations that may be reached conveniently by public transportation, walking, or bicycling. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Allowing flexible parking strategies in neighborhood activity centers to foster a pedestrian-oriented streetscape. ○ Providing continuous sidewalks with shade trees and landscape strips to separate pedestrians from traffic. ○ Encouraging neighborhood parks and recreational centers near concentrations of residential areas (preferably within one quarter mile) and include pedestrian walkways and bicycle paths that encourage non- motorized travel. ● Ensure pedestrian access to activities and services, especially within, but not limited to, mixed-use and transit- oriented development areas, by: <ul style="list-style-type: none"> ○ Ensuring new development that provides pedestrian connections in as many locations as possible to adjacent development, arterial streets, and thoroughfares. ○ Ensuring a balanced mix of housing, workplaces, shopping, recreational opportunities, and institutional uses, including mixed-use structures. ○ Locating schools in neighborhoods, within safe and easy walking distances of residences served. ○ Encouraging new development in which primary entrances are pedestrian entrances, with automobile entrances and parking located to the rear. ○ Supporting development where automobile access to buildings does not impede pedestrian access, by consolidating driveways between buildings or developing alley access. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Utilizing street parking as a buffer between sidewalk pedestrian traffic and the automobile portion of the roadway. ○ Prioritizing the physical development of pedestrian connectors for existing areas that do not meet established connectivity standards. • Mitigate climate change by decreasing heat gain from pavement and other hard surfaces associated with infrastructure. • Reduce heat gain from pavement and other similar hardscaping, by: <ul style="list-style-type: none"> ○ Including low-water landscaping in place of hardscaping around transportation infrastructure and in parking areas. ○ Establishing standards that provide for pervious pavement options. ○ Removing obstacles to natural, drought tolerant landscaping and low-water landscaping. • Coordinate with appropriate agencies to create an interconnected transportation system that allows a shift in travel from private passenger vehicles to alternative modes, including public transit, ride sharing, car-sharing, bicycling and walking, including, but not limited to: <ul style="list-style-type: none"> ○ Providing safe and convenient access for pedestrians and bicyclists to, across, and along major transit priority streets. • Upgrade and maintain the following transit system infrastructure to enhance public use, including: <ul style="list-style-type: none"> ○ Ensuring transit stops and bus lanes are safe, convenient, clean and efficient. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Ensuring transit stops have clearly marked street-level designation, and are accessible. ○ Ensuring transit stops are safe, sheltered, benches are clean, and lighting is adequate. ○ Working with transit providers to place transit stations along transit corridors within mixed-use or transit- oriented development areas at intervals appropriate for the mode of transit. ● Facilitate employment opportunities that minimize the need for private vehicle trips, by: <ul style="list-style-type: none"> ○ Amending zoning ordinances and the Development Code to include live/work sites and satellite work centers in appropriate locations. ○ Encouraging telecommuting options with new and existing employers, through project review and incentives, as appropriate. ● Establish standards for new development and redevelopment projects to support bicycle use, including: <ul style="list-style-type: none"> ○ Amending the Development Code to include standards for pedestrian and bicyclist accommodations, including: <ul style="list-style-type: none"> ➤ Providing access for pedestrians and bicyclist to public transportation through construction of dedicated paths, where feasible. ○ Requiring new development and redevelopment projects to include bicycle facilities, as appropriate with the new land use, including: <ul style="list-style-type: none"> ➤ Where feasible, promote the construction of weatherproof bicycle facilities and at a minimum, provide bicycle racks or covered, secure parking near the building entrances. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Establish a network of multi-use trails to facilitate direct off- street bicycle and pedestrian travel, and will provide bike racks along these trails at secure, lighted locations. • Establish policies and programs to reduce onsite parking demand and promote ride-sharing and public transit at large events. • Require new commercial and retail developments to provide prioritized parking for electric vehicles and vehicles using alternative fuels. • Support and promote the use of low-and zero-emission vehicles (NEV), by: <ul style="list-style-type: none"> ○ Encouraging the necessary infrastructure to facilitate the use of zero- emission vehicles and clean alternative fuels, such as electric vehicle charging facilities and conveniently located alternative fueling stations. ○ Encouraging new construction to include vehicle access to properly wired outdoor receptacles to accommodate ZEV and/or plug in electric hybrids (PHEV). ○ Encouraging transportation fleet standards to achieve the lowest emissions possible, using a mix of alternate fuels, PZEV or better fleet mixes. ○ Establishing incentives, as appropriate, to taxicab owners to use alternative fuel or gas-electric hybrid vehicles. • Establish green building requirements and standards for new development and redevelopment projects, and work to provide incentives for green building practices and remove barriers that impede their use. • Allow increased height limits and/or flexibility in other standards for projects that incorporate energy efficient 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>green building practices where not prohibited by ALUCP/FAA.</p> <ul style="list-style-type: none"> • Identify and remove regulatory or procedural barriers to implementing green building practices within its jurisdiction, such as updating codes, guidelines, and zoning, and ensure that all plan review and building inspection staff are trained in green building materials, practices, and techniques. • Support the use of green building practices by: <ul style="list-style-type: none"> ○ Establishing guidelines for green building practices in residential and commercial development. ○ Providing incentives, which may include reduction in development fees, administrative fees, and/or expedited permit processing for projects that use green building practices. • Adopt energy efficiency performance standards for buildings that achieve a greater reduction in energy and water use than otherwise required by current state law, including: <ul style="list-style-type: none"> ○ Standards for the installation of "cool roofs". ○ Standards for improved overall efficiency of lighting systems. ○ Requirements for the use of Energy Star appliances and fixtures in discretionary new development. ○ Requirements for new residential lots and/or structures to be arranged and oriented to maximize effective use of passive solar energy. • Require that affordable housing development incorporate energy efficient design and features to the maximum extent feasible. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> • Identify possible sites for production of renewable energy (such as solar, wind, small hydro, and biogas). • Identify and remove or otherwise address barriers to renewable energy production, including: <ul style="list-style-type: none"> ○ Reviewing and revising building and development codes, design guidelines, and zoning ordinances to remove renewable energy production barriers. ○ Working with related agencies, such as fire, water, health and others that may have policies or requirements that adversely impact the development or use of renewable energy technologies. ○ Developing protocols for safe storage of renewable and alternative energy products with the potential to leak, ignite or explode, such as biodiesel, hydrogen, and/or compressed air. • Allow renewable energy projects in areas zoned for open space, where consistent with the Land Use element, and other uses and values. • Promote and encourage renewable energy generation, and co-generation projects where feasible and appropriate. • Require that, where feasible, all new buildings be constructed to allow for easy, cost-effective installation of solar energy systems in the future, using such “solar-ready” features as: <ul style="list-style-type: none"> ○ Optimal roof orientation (between 20 to 55 degrees from the horizontal), with sufficient south-sloped roof surface, where such buildings architecture and construction are designed for sloped roofs. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Clear access without obstructions (chimneys, heating and plumbing vents, etc.) on the south sloped roof. ○ Roof framing that will support the addition of solar panels. ○ Installation of electrical conduit to accept solar electric system wiring. ○ Installation of plumbing to support a solar hot water system and provision of space for a solar hot water storage tank. ● Require that any building constructed in whole or in part with City funds incorporate passive solar design features, such as daylighting and passive solar heating, where feasible. ● Prepare and implement a comprehensive plan to improve energy efficiency of municipal facilities, including: <ul style="list-style-type: none"> ○ Conducting energy audits. ○ Retrofitting municipal facilities for energy efficiency where feasible and when remodeling or replacing components, including increased insulation, installing green or reflective roofs and low-emissive window glass. ○ Implementing an energy tracking and management system for its municipal facilities. ○ Installing energy-efficient exit signs, street signs, and traffic lighting, subject to life/safety considerations. ○ Installing energy-efficient lighting retrofits and occupancy sensors, and institute a "lights out at night" policy, subject to life/safety considerations. 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Retrofitting heating and cooling systems to optimize efficiency (e.g., replace chillers, boilers, fans, pumps, belts, etc.). ○ Installing Energy Star® appliances and energy-efficient vending machines. ○ Improving water use efficiency, including a schedule to replace or retrofit system components with high- efficiency units (i.e., ultra-low-flow toilets, fixtures, etc.). ○ Installing irrigation control systems maximizing water use efficiency and minimizing off- peak use. ○ Adopting an accelerated replacement schedule for energy inefficient systems and components. ● Require that any newly constructed, purchased, or leased municipal space meet minimum standards, such as: <ul style="list-style-type: none"> ○ The Energy Star® New Homes Program established by U.S. EPA. ○ The incorporation of passive solar design features in new buildings, including daylighting and passive solar heating. ● Reduce per capita water consumption consistent with state law by 2020. ● Establish a water conservation plan that may include such policies and actions as: <ul style="list-style-type: none"> ○ Maintaining and refining the City's tiered rate structure for water use. ○ Establishing restrictions on time of use for landscape watering, or other demand management strategies. 				

**Table 3-1
 Mitigation Monitoring Requirements**

Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing performance standards for irrigation equipment and water fixtures, consistent with State Law. • The City will establish programs and policies to increase the use of recycled water, including: <ul style="list-style-type: none"> ○ Promoting the use of recycled water for agricultural, industrial, and irrigation purposes, including grey water systems for residential irrigation. • Ensure that building standards and permit approval processes promote and support water conservation, by: <ul style="list-style-type: none"> ○ Establishing building design guidelines and criteria to promote water-efficient building design, including minimizing the amount of non-roof impervious surfaces around the building(s). ○ Establishing menus and check-lists for developers and contractors to ensure water-efficient infrastructure and technology are used in new construction, including low- flow toilets and shower heads, moisture-sensing irrigation, and other such advances. • Install water-efficient landscapes and irrigation, including: <ul style="list-style-type: none"> ○ Requiring planting drought-tolerant and native species, and covering exposed dirt with moisture-retaining mulch or other materials such as decomposed granite. ○ Requiring the installation of water-efficient irrigation systems and devices, including advanced technology such as moisture-sensing irrigation controls. • Promote the planting of shade trees and establish shade tree guidelines and specifications, including: 				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<ul style="list-style-type: none"> ○ Establishing guidelines for tree planting based on the land use (residential, commercial, parking lots, etc.). ○ Establishing guidelines for tree types based on species size, branching patterns, whether deciduous or evergreen, whether roots are invasive, etc. ○ Establishing tree guidelines for placement, including distance from structures, density of planting, and orientation relative to structures and the sun. ● Develop an Urban Forestry Program to consolidate policies and ordinances regarding tree planting, maintenance, and removal, including: <ul style="list-style-type: none"> ○ Establishing guidelines for tree planting, including criteria for selecting deciduous or evergreen trees low-VOC- producing trees, and emphasizing the use of drought- tolerant native trees and vegetation. 				
6-4 Measures listed in Mitigation Measure 6-2 and 6-3 shall be considered by the City while reviewing all new development, as appropriate, between the time of adoption of The Ontario Plan and adoption of the Climate Action Plan (CAP).	City of Ontario Planning Department	Prior to adoption of the Climate Action Plan	City of Ontario Planning Department	
6-5 Pursuant to a goal of overall consistency with the Sustainable Communities Strategies, the City of Ontario shall evaluate new development for consistency with the development pattern set forth in the Sustainable Communities Strategies plan, upon adoption of the plan by the Southern California Association of Governments.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
6-6 The City of Ontario shall participate in the County of San Bernardino's Green Valley Initiative.	City of Ontario Planning Department	Prior to individual project approvals	City of Ontario Planning Department	
5.12 NOISE				

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>12-1 Prior to the issuance of building permits for any project that involves a noise-sensitive use within the 65 dBA CNEL contour along major roadways, freeways, railroads, or the Los Angeles/Ontario International Airport, the project property owner/developers shall retain an acoustical engineer to conduct an acoustic analysis and identify, where appropriate, site design features (e.g., setbacks, berms, or sound walls) and/or required building acoustical improvements (e.g., sound transmission class rated windows, doors, and attic baffling), to ensure compliance with the City's Noise Compatibility Criteria and the California State Building Code and California Noise Insulation Standards (Title 24 and 21 of the California Code of Regulations).</p>	<p>City of Ontario Planning/Building Department in coordination with the Landowner/Project Applicant</p>	<p>Prior to individual project approvals</p>	<p>City of Ontario Planning/Building Department</p>	
<p>12-2 Individual projects that involve vibration-intensive construction activities, such as pile drivers, jack hammers, and vibratory rollers, occurring near sensitive receptors shall be evaluated for potential vibration impacts. If construction-related vibration is determined to be perceptible at vibration-sensitive uses (i.e., exceed the Federal Transit Administration vibration-annoyance criteria of 78 VdB during the daytime), additional requirements, such as use of less vibration intensive equipment or construction techniques, shall be implemented during construction (e.g., drilled piles to eliminate use of vibration-intensive pile driver).</p>	<p>City of Ontario Planning/Building/MUA Department in coordination with the Landowner/Project Applicant's construction contractor</p>	<p>During construction</p>	<p>City of Ontario Building/MUA Department</p>	
<p>12-3 Prior to the issuance of building permits for any project that involves a vibration-sensitive use directly adjacent to the Union Pacific Railroad or Southern California Regional Rail Authority main lines shall retain an acoustical engineer to evaluate potential for trains to create perceptible levels of vibration indoors. If vibration-related impacts are found, mitigation measures, such as use of concrete, iron, or steel, or masonry materials to ensure that levels of vibration amplification are within acceptable limits to building occupants, shall be implemented. Pursuant to the Federal Transit Administration vibration-annoyance criteria, these acceptable limits are 78 VdB during the daytime and 72 VdB</p>	<p>City of Ontario Planning/Building Department with collaboration with the Landowner/Project Applicant</p>	<p>Prior to individual project approvals</p>	<p>City of Ontario Building Department</p>	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
during the nighttime for residential uses, 84 VdB for office uses, and 90 VdB for workshops.				
12-4 Construction activities associated with new development that occurs near sensitive receptors shall be evaluated for potential noise impacts. Mitigation measures such as installation of temporary sound barriers for adjacent construction activities that occur adjacent to occupied noise-sensitive structures, equipping construction equipment with mufflers, and reducing non-essential idling of construction equipment to no more than five minutes shall be incorporated into the construction operations to reduce construction-related noise to the extent feasible.	City of Ontario Building/Planning/MUA Department in coordination with the Landowner/Project Applicant's construction contractor	During construction	City of Ontario Building/Planning/MUA Department	
5.16 TRANSPORTATION AND CIRCULATION				
16-1 The Mobility Element of the Ontario Plan shall be consistent with the traffic study prepared by Kimley-Horn and Associates in 2009. Table 5.16-6 in Section 5.16, <i>Transportation and Traffic</i> , shows the recommended lane geometry for the Proposed Land Use Plan.	City of Ontario Engineering/Planning Department	Ongoing	City of Ontario Engineering/Planning Department	
5.17 UTILITIES AND SERVICE SYSTEMS				
17-1 The City shall include a policy in the Policy Plan that requires water conservation measures for development projects to improve water use efficiency and reduce overall water demand. Reduce potable water demand, through conservation measures, including but not limited to: <ul style="list-style-type: none"> • Work cooperatively with all developers to incorporate conservation measures into project designs (such as those recommended by the California Urban Water Conservation Council). • Continue to develop and implement drought contingency plans to assist citizens and businesses 	City of Ontario Planning/MUA Department	Ongoing	City of Ontario Planning/MUA/Engineering Department	

Table 3-1 Mitigation Monitoring Requirements				
Mitigation Measure	Responsibility for Implementation	Timing	Responsibility for Monitoring	Monitor (Signature Required) (Date of Compliance)
<p>reduce water use during water shortages and emergencies.</p> <ul style="list-style-type: none"> Revise the City Code to include a Water-Efficient Landscape Ordinance to encourage or, as appropriate, require the use of water-efficient landscaping consistent with AB 1881. 				
<p>17-2 The City shall include a policy in the Policy Plan that maximizes the use of recycled water as an irrigation (nonpotable) source for landscaping, parks, and other irrigation opportunities in all areas of the City and requires use of recycled water in dual-system office and industrial uses in selected urban areas of the City, where available and feasible.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	
<p>17-3 The City shall include a policy in the Policy Plan that the City participate through the Chino Basin Water Master and the Inland Empire Utilities Agency in regional efforts to develop finding additional sources of water for groundwater recharge, such as capture of stormwater runoff, recycled water, or other sources to ensure that the Chino Basin stays in long-term hydraulic balance and sustainability and that adequate additional local water sources would be available to increase the flexibility of the City's water supply.</p>	<p>City of Ontario Planning Department in coordination with City of Ontario MUA/Engineering Department</p>	<p>Ongoing</p>	<p>City of Ontario Planning/MUA/Engineering Department</p>	



CITY OF ONTARIO

MEMORANDUM

TO: Chairman and Members of the Planning Commission

FROM: Cathy Wahlstrom, Planning Director 

DATE: April 28, 2020

SUBJECT: File No. PSPA19-007— An Amendment to the Parkside Specific Plan (File No. PSPA19-007) to: [1] Reconfigure residential Planning Areas 1 through 4, and 17 through 19; [2] Reconfigure the Great Park Planning Area 22 (east of the Cucamonga Creek Channel); [3] Revise internal circulation to improve access into the neighborhood commercial Planning Area 21; [4] Update and revise Residential Design Guidelines (Sections 7.1 through 7.6) to introduce new housing types and architectural styles; and [5] Update and revise Landscape Standards (Section 7.7). (APN's): 0218-231-06, 0218-231-08, 0218-231-09, 0218-231-10, 0218-231-11, 0218-231-12, 0218-231-13, 0218-231-14, 0218-231-15, 0218-231-16, 0218-231-17, 0218-231-18, 0218-231-19, 0218-231-20, 0218-231-21, 0218-231-22, 0218-231-28, 0218-231-30, 0218-231-31, 0218-231-39, 0218-221-09, and 0218-221-10); **submitted by SC Ontario Development Company, LLC.**

Staff is requesting that the public hearing for the above-described application be continued to the next regular meeting scheduled on May 26, 2020.



CITY OF ONTARIO

MEMORANDUM

TO: Chairman and Members of the Planning Commission

FROM: Cathy Wahlstrom, Planning Director *CW*

DATE: April 28, 2020

SUBJECT: MONTHLY PLANNING DEPARTMENT ACTIVITY REPORT; MONTH OF MARCH 2020

Attached, you will find the Planning Department Monthly Activity Report for the month of March 2020. The report describes all new applications received by the Planning Department and actions taken on applications during the month. Please contact me if you have any questions regarding this information.

The attached reports, along with reports from past months, may also be viewed on the City's web site. New applications may be viewed at <http://www.ontarioca.gov/planning/reports/monthly-activity-reports-applications>, and actions taken on applications may be viewed at <http://www.ontarioca.gov/planning/reports/monthly-activity-reports-actions>.

City of Ontario Planning Department
Monthly Activity Report—Actions
Month of March 2020

DEVELOPMENT ADVISORY BOARD MEETING
March 2, 2020

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT PLAN REVIEW FOR FILE NO. PDEV19-053:

A Development Plan to construct a 940-square foot addition to an existing 82,347 square foot automobile dealership (Mercedes Benz) on 8.17 acres of land located at 3787 East Guasti Road, within the Auto land use designation of the Ontario Gateway Specific Plan. The environmental impacts of this project were previously reviewed in conjunction with File No. PSP05-005, for which an Environmental Impact Report (SCH#2006091039) was certified by City Council on June 19, 2007. This project introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 210-212-55) **submitted by Jones Ontario Acquisition, LLC.**

Action: Approved, subject to conditions

ZONING ADMINISTRATOR MEETING
March 2, 2020

Meeting Cancelled

CITY COUNCIL/HOUSING AUTHORITY MEETING
March 3, 2020

ENVIRONMENTAL ASSESSMENT AND DEVELOPMENT CODE AMENDMENT REVIEW FOR FILE

NO. PDCA20-001: A request to amend Ontario Development Code Section 4.02.010.D.2.f, Billboard Relocation Agreements, Interagency Relocation Exception, to revise the locational criteria and the number of billboards to be eliminated within the City. Staff has determined that the application is exempt from the requirements the California Environmental Quality Act (CEQA) pursuant to Section 15061 (b) (3) (General Rule). The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT) and was evaluated and found to be consistent with the policies and criteria of the ONT Airport Land Use Compatibility Plan (ALUCP). **City Initiated.** The Planning Commission recommended approval of this item on January 28, 2020, with a vote of 7 to 0.

Action: Introduced and waived further reading of an ordinance approving the Development Code Amendment.

ENVIRONMENTAL ASSESSMENT AND GENERAL PLAN AMENDMENT REVIEW FOR FILE NO.

PGPA20-001: A General Plan Amendment to modify the Future Buildout Table (Exhibit LU-03) to

City of Ontario Planning Department
Monthly Activity Report—Actions
Month of March 2020

be consistent with the land use designation change for the approved Amendment to the Meredith International Centre Specific Plan (File No. PSPA19-002) that established a Mixed-Use Overlay district on 22.39 acres of land within a portion of Planning Area 2 (Urban Commercial) land use district. The proposed modification to the Future Buildout Table (Exhibit LU-03) will update the Buildout Table to reflect the addition of the 925 multi-family units. The environmental impacts of this project were previously reviewed in conjunction with an Addendum, to Meredith International Centre Specific Plan Amendment Environmental Impact Report (SCH# 2014051020), approved by City Council on December 17, 2019. This Application introduces no new significant environmental impacts. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APNs: 0110-311-52, 0110-311-53, 0110-311-54, and 0110-311-55) **submitted by City of Ontario**. The Planning Commission recommended approval of this item on January 28, 2020, with a vote of 7 to 0.

Action: Adopt a resolution approving the General Plan Amendment.

DEVELOPMENT ADVISORY BOARD MEETING
March 16, 2020

ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP EXTENSION REVIEW FOR FILE NO. PMTT16-013 (TM 20050): A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT 20050), subdividing 3.47 acres of land, located at 1910 South Euclid Avenue, within the MDR-18 (Medium Density Residential - 11.1 to 18.0 DUs/acre) and EA (Euclid Avenue) Overlay zoning districts. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15304 (Class 4, Minor Alterations to Land) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT), and was evaluated and found to be consistent with the policies and criteria of the ONT Airport Land Use Compatibility Plan; (APNs: 1050-381-04, 1050-381-05, 1050-381-06, 1050-381-07, 1050-381-08 and 1050-381-09) **submitted by 1902 Euclid Property LLC. Planning Commission action is required.**

Action: Recommended Planning Commission approval, subject to conditions.

ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP EXTENSION REVIEW FOR FILE NO. PMTT17-006 (TT 19832): A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT 19832), subdividing a 2.7 acre site located at the northeast corner of Euclid Avenue and Riverside Drive, within the CN (Neighborhood Commercial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence

City of Ontario Planning Department
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Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1051-614-08) **submitted by Creative Design Associates. Planning Commission action is required.**

Action: Recommended Planning Commission approval, subject to conditions.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND CONDITIONAL USE PERMIT

REVIEW FOR FILE NOS. PDEV19-020 AND PCUP19-018: A Development Plan (File No. PDEV19-020) and Conditional Use Permit (File No. PDEV19-018) to construct a 65-foot tall stealth wireless telecommunication facility (monopine) on 1.9 acres of land generally located on the south side of Riverside Drive, approximately 180 feet west of Sultana Avenue, at 7247 East Riverside Drive, within the SP(AG) (Specific Plan/Agriculture Overlay) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the State CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1052-071-05) **submitted by AT&T. Planning Commission action is required.**

Action: Recommended Planning Commission approval, subject to conditions.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN REVIEW AND VARIANCE FOR FILE NOS.

PDEV19-056 AND PVAR19-007: A Development Plan (File No. PDEV19-056) to construct one industrial building totaling 71,800 square feet on 3 acres of land in conjunction with a Variance (File No. PVAR19-007) to reduce the south property line building setback from 25 feet to 5 feet, located on the west side of Milliken Avenue, approximately 300 feet north of Greystone Drive, within the Light Industrial land use district of the Bridgestone/Firestone Industrial Park Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15305 (Class 5, Minor Alterations in Land Use) and Section 15332 (Class 32, Infill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1083-351-09) **submitted by David L. Ball. Planning Commission action is required.**

Action: Recommended Planning Commission approval, subject to conditions.

ZONING ADMINISTRATOR MEETING
March 16, 2020

ENVIRONMENTAL ASSESSMENT AND CONDITIONAL USE PERMIT REVIEW FOR FILE NO.

PCUP18-039: A Conditional Use Permit to establish a 491-square foot addition to an existing place

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of worship (Prayer and Praise Ministries Church of God in Christ) on 0.34-acre of land located at 130 West Phillips Street, within the LDR-5 (Low Density Residential - 2.1 to 5.0 DU/Acre) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15301 (Class 1, Existing Facilities) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1049-552-10) submitted by Dorothy Porter. This project was continued from February 19, 2020.

Action: Continued to an unspecified meeting date. Public notification will be provided prior to the new hearing date.

CITY COUNCIL/HOUSING AUTHORITY MEETING
March 17, 2020

HOUSING ELEMENT ANNUAL PROGRESS REPORT REVIEW FOR FILE NO. PADV20-001: Housing Element Annual Progress Report for Calendar Year 2019. The Housing Element Annual Report is Categorically Exempt from California Environmental Quality Act (CEQA), as amended in accordance with Section 15306 (Information Collection).

Action: Adopted a resolution authorizing staff to transmit the 2019 Housing Element Annual Progress Report to the California Department of Housing and Community Development and the Governor's Office of Planning and Research.

PLANNING/HISTORIC PRESERVATION COMMISSION MEETING March 24, 2020

Meeting Cancelled

SPECIAL PLANNING/HISTORIC PRESERVATION COMMISSION MEETING
March 26, 2020

ENVIRONMENTAL ASSESSMENT AND TENTATIVE TRACT MAP EXTENSION REVIEW FOR FILE NO. PMTT16-013 (TM 20050): A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT 20050), subdividing 3.47 acres of land, located at 1910 South Euclid Avenue, within the MDR-18 (Medium Density Residential - 11.1 to 18.0 DUs/acre) and EA (Euclid Avenue) Overlay zoning districts. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15304 (Class 4, Minor Alterations to Land) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport (ONT), and was evaluated and found to be consistent with the policies and criteria of the ONT Airport Land Use

City of Ontario Planning Department
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Compatibility Plan; (APNs: 1050-381-04, 1050-381-05, 1050-381-06, 1050-381-07, 1050-381-08 and 1050-381-09) **submitted by 1902 Euclid Property LLC.**

Action: Approved one-year time extension.

ENVIRONMENTAL ASSESSMENT, TENTATIVE PARCEL MAP EXTENSION REVIEW FOR FILE NO. PMTT17-006 (TT 19832): A one-year Time Extension for a previously approved Tentative Tract Map for condominium purposes (TT 19832), subdividing a 2.7 acre site located at the northeast corner of Euclid Avenue and Riverside Drive, within the CN (Neighborhood Commercial) zoning district. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-fill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan (ALUCP); (APN: 1051-614-08) **submitted by Creative Design Associates.**

Action: Approved one-year time extension.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN, AND CONDITIONAL USE PERMIT REVIEW FOR FILE NOS. PDEV19-020 AND PCUP19-018: A Development Plan (File No. PDEV19-020) and Conditional Use Permit (File No. PCUP19-018) to construct a 65-foot tall stealth wireless telecommunication facility (monopine) on 1.9 acres of land generally located on the south side of Riverside Drive, approximately 180 feet west of Sultana Avenue, at 7247 East Riverside Drive, within the SP(AG) (Specific Plan/Agriculture Overlay) zoning district. Staff has determined that the project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15332 (Class 32, In-Fill Development Projects) of the State CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1052-071-05) **submitted by AT&T.**

Action: Approved, subject to conditions.

ENVIRONMENTAL ASSESSMENT, DEVELOPMENT PLAN REVIEW AND VARIANCE FOR FILE NOS. PDEV19-056 AND PVAR19-007: A Development Plan (File No. PDEV19-056) to construct one industrial building totaling 71,800 square feet on 3 acres of land in conjunction with a Variance (File No. PVAR19-007) to reduce the south property line building setback from 25 feet to 5 feet, located on the west side of Milliken Avenue, approximately 300 feet north of Greystone Drive, within the Light Industrial land use district of the Bridgestone/Firestone Industrial Park Specific Plan. The project is categorically exempt from the requirements of the California Environmental Quality Act (CEQA) pursuant to Section 15305 (Class 5, Minor Alterations in Land Use) and Section 15332 (Class 32, Infill Development Projects) of the CEQA Guidelines. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and

City of Ontario Planning Department
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Month of March 2020

found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APN: 1083-351-09) **submitted by David L. Ball.**

Action: Approved, subject to conditions.

ENVIRONMENTAL ASSESSMENT AND SPECIFIC PLAN AMENDMENT REVIEW FOR FILE NO.

PSPA19-004: An Amendment to the 95.35-acre Toyota Ontario Business Park Specific Plan, revising the current land use district covering Planning Area 1, from Office/Research and Development (“Office/R&D”) to Industrial Mixed Use, allowing for warehouse, distribution, and manufacturing land uses on the site in conjunction with the currently allowed Office/R&D land uses. Additionally, the Amendment will update the Specific Plan’s landscape palette to conform to current California friendly landscape practices. The Specific Plan area is generally located south of Jurupa Street, east of Milliken Avenue, north of Francis Street, and west of the I-15 freeway. The environmental impacts of this project were analyzed in an Addendum to The Ontario Plan Environmental Impact Report (State Clearinghouse No. 2008101140), which was certified by the City Council on January 27, 2010, in conjunction with File No. PGPA06-001. This Application introduces no new significant environmental impacts and all previously adopted mitigation measures are a condition of project approval. The proposed project is located within the Airport Influence Area of Ontario International Airport and was evaluated and found to be consistent with the policies and criteria of the Ontario International Airport Land Use Compatibility Plan; (APNs: 0238-121-75) **submitted by MIG. City Council action is required.**

Action: Continued to the 4/28/2020 Planning Commission meeting.

SPECIAL CITY COUNCIL/HOUSING AUTHORITY MEETING
March 31, 2020

No Planning Department items on the agenda

Monthly Activity Report—New Applications

Month of March 2020

PCUP20-006:

Submitted by Stater Bros Development

A Conditional Use Permit to establish alcoholic beverage sales, including beer, wine, and distilled spirits, for off-premises consumption (Type 21 ABC license) in conjunction with a proposed 45,204-square foot grocery store on 10.06 acres of land located at 3460 East Ontario Ranch Road, within the Retail land use district of the Avenue Specific Plan (APN: 0218-412-02). Related File: PDEV19-017. **Zoning Administrator action is required.**

PCUP20-007:

Submitted by Eureka Restaurant Group, LLC

A Conditional Use Permit to establish live entertainment and alcoholic beverage sales, including beer, wine and distilled spirits, for on-premises consumption (Type 47 ABC license) in conjunction with an a proposed 3,603-square foot restaurant, with an additional 1,093 square feet of patio dining, on 4.34 acres of land located at 900 North Via Piemonte, within the Special Use land use district of the Piemonte Overlay area of the Ontario Center Specific Plan (APN: 210-204-18). **Zoning Administrator action is required.**

PDEV20-005:

Submitted by Inland Harbor, LLC

A Development Plan to construct a 257,920-square foot industrial building on 11.24 acres of land located at 835 West State Street, within the IL (Light Industrial) zoning district (APNs: 1011-161-05 and 1011-161-04). **Planning Commission action is required.**

PDEV20-006:

Submitted by Lennar Homes of California, Inc.

A Development Plan to construct 226 traditional single-family dwellings on 53.79 acres of land generally located at the northwest corner of Haven and Bellegrave Avenues, within Planning Area 28 of the Subarea 29 Specific Plan (APNs: 0218-321-17 and 0218-321-30). Related Files: PMTT14-025 and PMTT14-024. **Planning Commission action is required.**

PDEV20-007:

Submitted by Brookfield Properties Development

A Development Plan to construct 162 multiple-family dwellings on 6.63 acres of land generally located at the northeast corner of Haven Avenue and Ontario Ranch Road, within the Regional Commercial land use district of the Rich Haven Specific Plan (APNs: 0218-393-06, 0218-393-07, 0218-393-10, 0218-393-22, 0218-393-36, 0218-393-38, and 0218-393-39). Related Files: PSPA20-002 and PMTT20-003. **Planning Commission action is required.**

PDEV20-008:

Submitted by Herdman Architecture and Design

A Development Plan to construct a 237,398-square foot industrial building on 10.64 acres of land located at the northeast corner Haven Avenue and Airport Drive, within the Commercial/Food/Hotel land use district of the California Commerce Center Specific Plan (APN: 0211-222-66). Related File: PSPA20-003. **Planning Commission action is required.**

Monthly Activity Report—New Applications

Month of March 2020

PDIF20-007: **Submitted by Lennar Homes of California, Inc.**

A Development Impact Fee Credit Agreement with Lennar Homes of California, Inc., associated with the development of Tract Map Nos. 17932, 17933, 17935, 17936, and 18878, generally located at the northeast corner of Bellegrave Avenue and Mill Creek Avenue, within the Esperanza Specific Plan. **City Council action is required.**

PDIF20-008: **Submitted by Lennar Homes of California, Inc.**

A Development Impact Fee Credit Agreement with Lennar Homes of California, Inc., associated with the development of Tract Map No. 20012, located at the northwest corner of Ontario Ranch Road and Turner Avenue, within The Avenue Specific Plan. **City Council action is required.**

PDIF20-009: **Submitted by In-N-Out Burgers, A California Corporation**

A Development Impact Fee Credit and Reimbursement Agreement with In-N-Out Burgers, A California Corporation, associated with a Development Plan (File No. PDEV19-034) to construct a new 4,071-square foot drive-thru restaurant on 1.57 acres of land generally located at the northwest corner of G Street and Vineyard Avenue, within the CCS (Convention Center Support Commercial) zoning district (APNs: 0110-241-50 and 0110-241-54). **City Council action is required.**

PHP-20-002: **Submitted by Jose Vladimir Felix**

A Local Historic Landmark designation for a single-family residence constructed in the Craftsman architectural style, located at 535 East D Street (APN: 1048-393-18). **City Council action is required.**

PHP-20-003: **Submitted by City of Ontario**

A Certificate of Appropriateness for the adaptive reuse of the first floor (2,198 square feet) and grounds of the historic landmark Fallis House (previously converted to an office occupancy), for occupancy with a new mobility hub for bicycle rental, repair, and storage, on 0.41-acre of land located at 122 South Vine Avenue, within the MU-1 (Downtown Mixed Use) zoning district. A portion of the upper story of the building will be used as storage and the remainder will be closed to the public (APN: 1049-021-19). **Historic Preservation Commission action is required.**

PMTT20-003: **Submitted by BrookCal Ontario, LLC**

A Tentative Tract Map for Common Interest Subdivision purposes to subdivide 6.63 acres of land into 8 numbered lots and 17 lettered common lots, located at the northeast corner of Haven Avenue and Ontario Rancho Road, within the Regional Commercial land use district of the Rich-Haven Specific Plan (APNs: 0218-393-06, 0218-393-07, 0218-393-10, 0218-393-22, 0218-393-36, 0218-393-38, and 0218-393-39). **Planning Commission action is required.**

Monthly Activity Report—New Applications

Month of March 2020

PSGN20-025:

Submitted by Yara Harris

A Sign Plan for the installation of a wall sign for SPECTATORS SPORTS BAR located at 750 North Archibald Avenue, Suite G, within the Garden Commercial II land use district of the Ontario Festival Specific Plan (APN: 0110-431-10). **Staff action is required.**

PSGN20-026:

Submitted by 9 Star Construction, Inc.

A Sign Plan for the installation of a new commercial center identification monument sign for 5 tenants, approximately 7 FT in height with an area of 50 SF, to be integrated into the Village Wall design located at 1337 North Mountain Avenue, within the Main Street land use district of the Mountain Village Specific Plan (APN: 1008-431-26). Related Files: PSGP20-001 and PSPA20-001. **Staff action is required.**

PSGN20-027:

Submitted by Centerline Sign Company

A Sign Plan for the installation of two new illuminated wall signs, the reface of a panel in an existing monument sign, and the reface of a panel in an existing freeway pylon sign for PLANET FITNESS, located at 1670 East Fourth Street, within the CN (Neighborhood Commercial) zoning district (APN: 0110-181-15). **Staff action is required.**

PSGN20-028:

Submitted by Mall Signs & Service

A Sign Plan for the installation of three illuminated wall signs on the east, north, and west building elevations, and an internally illuminated monument sign per an approved Temporary Encroachment Easement for CANDLEWOOD SUITES, located at 1818 East Holt Boulevard, within the CCS (Convention Center Support Commercial) zoning district (APN: 0110-101-03). **Staff action is required.**

PSGN20-029:

Submitted by Empire Sign

A Sign Plan for the installation of a wall sign to add to existing wall sign on the exterior of The Children's Place, for GYMBOREE, located at 4543 East Mills Circle, Space 83, within the Ontario Mills Specific Plan (APN: 0238-104-37). **Staff action is required.**

PSGN20-030:

Submitted by Coast Sign

A Sign Plan for the installation of a new illuminated wall sign for BBVA BANK located at 901 North Via Piemonte, within the Office land use district of the Piemonte Overlay of the Ontario Center Specific Plan (APN: 0210-204-17). **Staff action is required.**

PSGN20-031:

Submitted by Signs of Success

A Sign Plan for the installation of one illuminated wall sign for CITY BEST INSURANCE TAXES, located at 1630 East Fourth Street, Suite E, within the CC (Community Commercial) zoning district (APN: 0110-181-13). **Staff action is required.**

Monthly Activity Report—New Applications

Month of March 2020

PSGN20-032:

Submitted by Metro Signs

A Sign Plan for the installation of three signs (new wall-mounted, reface existing freeway pylon, and reface existing monument sign) for AMERICAN FREIGHT APPLIANCE-FURNITURE-MATTRESS located at 2401 South Vineyard Avenue, within the CC (Community Commercial) zoning district (APN: 0113-285-08). **Staff action is required.**

PSGN20-033:

Submitted by Expedition Sign, Inc.

A Sign Plan for the installation of a wall-mounted sign for CHECK INTO CASH located at 727 North Euclid Avenue, within the MU-1 (Downtown Mixed Use) zoning district (APN: 1048-271-18). **Staff action is required.**

PSGN20-034:

Submitted by Trulite Signs

A Sign Plan for the installation of a wall-mounted illuminated sign for METRO BY T-MOBILE located at 2252 South Euclid Avenue, Suite D, within the CC (Community Commercial) zoning district (APN: 1051-051-72). **Staff action is required.**

PSPA20-002:

Submitted by BrookCal Ontario, LLC

An Amendment to the Rich-Haven Specific Plan, changing the land use designation on 6.63 acres of land from Regional Commercial to Stand Alone Residential Overlay, generally located at the northeast corner of Haven Avenue and Ontario Rancho Road, within the Regional Commercial land use district of the Rich Haven Specific Plan (APNs: 0218-393-06, 0218-393-07, 0218-393-10, 0218-393-22, 0218-393-36, 0218-393-38, and 0218-393-39). Related File: PMTT20-003. **City Council action is required.**

PSPA20-003:

Submitted by Herdman Architecture and Design

An Amendment to the California Commerce Center Specific Plan, changing the land use designation on 10.64 acres of land from Commercial/Food/Hotel to Light Industrial, generally located at the northeast corner of Haven Avenue and Airport Drive, within the Commercial/Food/Hotel land use district of the California Commerce Center Specific Plan (APN: 0211-222-66). Related File: PDEV20-008. **City Council action is required.**

PTUP20-017:

Submitted by MW Appliances

A Temporary Use Permit submitted by MW Appliances for an outdoor event involving the sales of appliances within the parking lot of the Ontario Mills, located at 1 Mills Circle. Event to be held on 3/27/2020 through 3/29/2020. **Note: This permit was cancelled by the City in response to COVID-19 Pandemic. Staff action is required.**

Monthly Activity Report—New Applications

Month of March 2020

PTUP20-018: **Submitted by St George Parish School**

A Temporary Use Permit for a one-day 100-year anniversary dinner event within the church hall of the St George Parish School, located at 505 North Palm Avenue. **Note:** This permit was cancelled by the City in response to COVID-19 Pandemic. **Staff action is required.**

PTUP20-019: **Submitted by Mix Champagne Lounge**

A Temporary Use Permit for a one-day non-profit celebrity fundraising event in conjunction with the Mix Champagne Lounge located at 4481 East Ontario Mills Parkway, within the Commercial/Office land use district of the Ontario Mills Specific Plan. Event to be held on 3/29/2020, 1:30PM to 9:00PM. **Note:** This permit was cancelled by the City in response to COVID-19 Pandemic. **Staff action is required.**

PTUP20-020: **Submitted by Church of God of Prophecy**

A Temporary Use Permit for a one-day car wash fund raising event located at 1130 South Campus Avenue. Event to be held on 4/11/2020, 7:00AM to 3:00PM. **Note:** This permit was cancelled by the City in response to COVID-19 Pandemic. **Staff action is required.**

PTUP20-021: **Submitted by Homestead Steaks, LLC**

A Temporary Use Permit for a temporary outdoor sales event within the Ontario Mills Mall parking lot for Homestead Steaks, LLC, located at 1 East Mills Circle. Event to be held on 03/26/2020 through 04/06/2020. **Note:** This permit was cancelled by the City in response to COVID-19 Pandemic. **Staff action is required.**

PVER20-012: **Submitted by Ethan Verite**

A Zoning Verification for property located at 112 South Vine Avenue, within the MU-1 (Downtown Mixed Use) zoning district (APN:1049-021-18). **Staff action is required.**

PVER20-013: **Submitted by Zoning Info**

A Zoning Verification for property located at 2550 South Archibald Avenue, within the CC (Community Commercial) zoning district (APN: 1083-011-13). **Staff action is required.**

PVER20-014: **Submitted by Glenn Weissman**

A Zoning Verification for property located at 1421 North Amador Avenue, within the LDR-5 (Low Density Residential – 2.1 to 5.0 DUs/Acre) zoning district (APN:0108-441-02). **Staff action is required.**

PVER20-015: **Submitted by NDDS Zoning**

A Zoning Verification for property located at 4237 East Airport Drive, within the Light Industrial land use district of the California Commerce Center Specific Plan (APN: 0211-222-06). **Staff action is required.**

Monthly Activity Report—New Applications

Month of March 2020

PVER20-016:

Submitted by AEI Consultants

A Zoning Verification for properties located at 1720 East Acacia Street and 1431 South Baker Avenue, within the IG (General Industrial) zoning district (APN: 0113-415-73 and 0113-415-72).

Staff action is required.

PVER20-017:

Submitted by AEI Consultants

A Zoning Verification for properties located at 5100, 5200, 5300, and 5400 Shea Center Drive, within the Industrial/Commercial/Office land use district of the Shea Business Center Specific Plan (APNs: 0238-052-46, 0238-052-41, 0238-052-16, 0238-052-36, 0238-052-37, 0238-052-38, 0238-052-45, 0238-052-43, 0238-052-44). **Staff action is required.**

PVER20-018:

Submitted by AEI Consultants

A Zoning Verification for property located at 5101 East Airport Drive, within the IH (Heavy Industrial) zoning district (APN: 0238-052-48). **Staff action is required.**