

## Section 1 - Introduction/Overview

### 1.1 Vision of The Avenue

The Avenue will be a truly walkable community, featuring pedestrian amenities that encourage walking and biking to schools, parks and basic commercial needs—all within five minutes, for the majority of residents. The Avenue is named after the central La Avenida Drive, a tree-lined street with one-way traffic on each side, and a continuous 13' parkway and 8' sidewalk on both sides without curb cuts or driveways to create an uninterrupted path. The Avenue will provide a connection from Archibald Avenue to the Recreation Area, Active Recreation Park, Schools, and adjacent neighborhood and pocket parks. With a diversity of housing types and styles, The Avenue will have something for everyone—providing a place for everyone to live, learn, play and shop.

### 1.2 Project Location

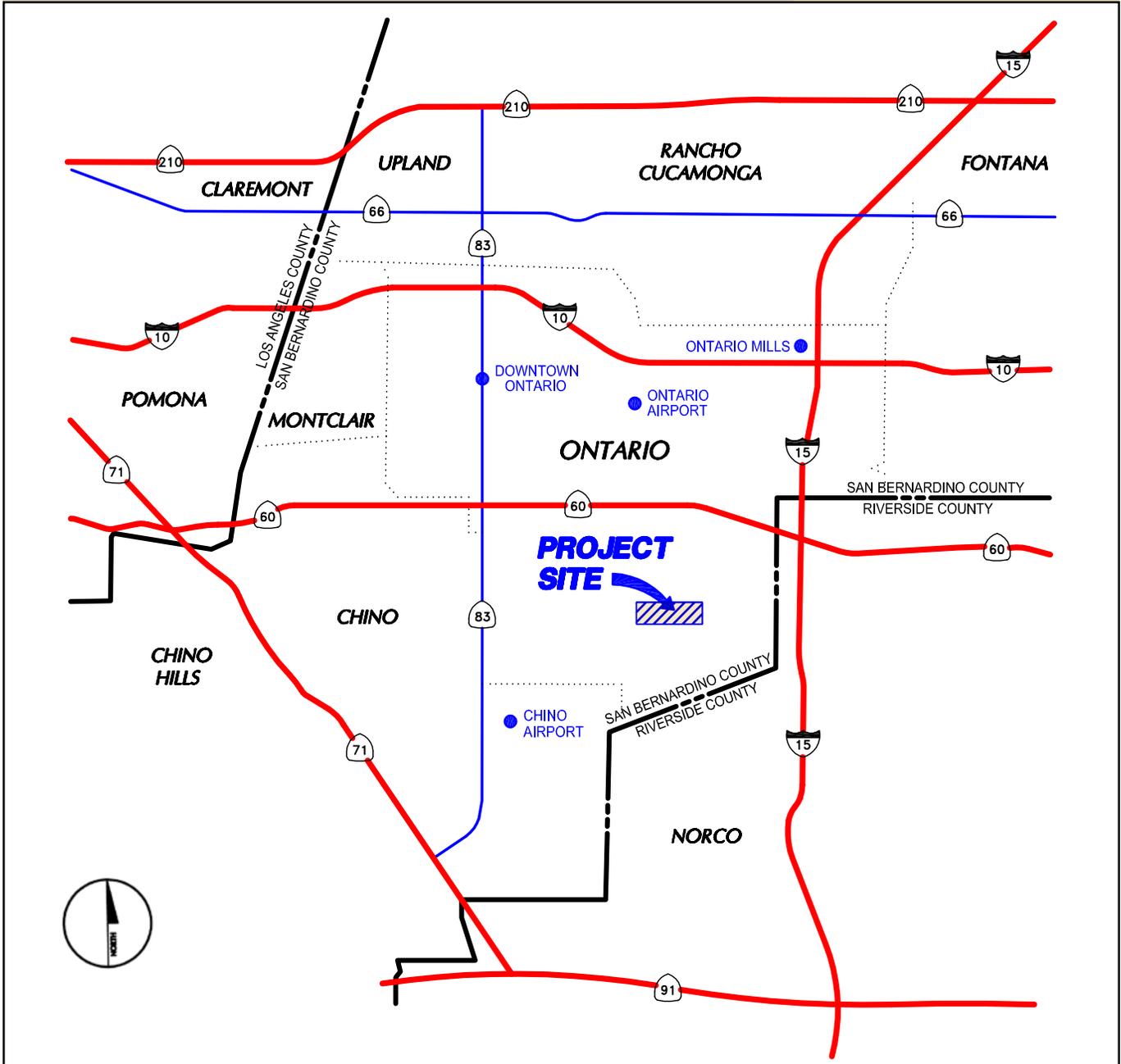
The Avenue Specific Plan is comprised of approximately 568.1 gross acres. The project site is bounded by Schaefer Avenue to the north, Ontario Ranch Road to the south, Haven Avenue to the east, Carpenter Avenue to the west as illustrated in Exhibit 1, “Regional Map” and Exhibit 2, “Project Location”.



The Avenue will offer a variety of residential housing types and neighborhoods within walking distance to parks, schools, and recreation facilities. The Land Use Plan, as illustrated in Exhibit 8, “Land Use Plan” is consistent with the goals and policies of the New Model Colony General Plan Amendment, combining walkable residential neighborhoods served by recreational spaces that offer opportunities for social interaction among residents. A Planned retail and commercial area is located within La Avenida Drive at Ontario Ranch Road and Haven Avenues to serve the commercial needs of the residents of The Avenue as well as surrounding neighborhoods.

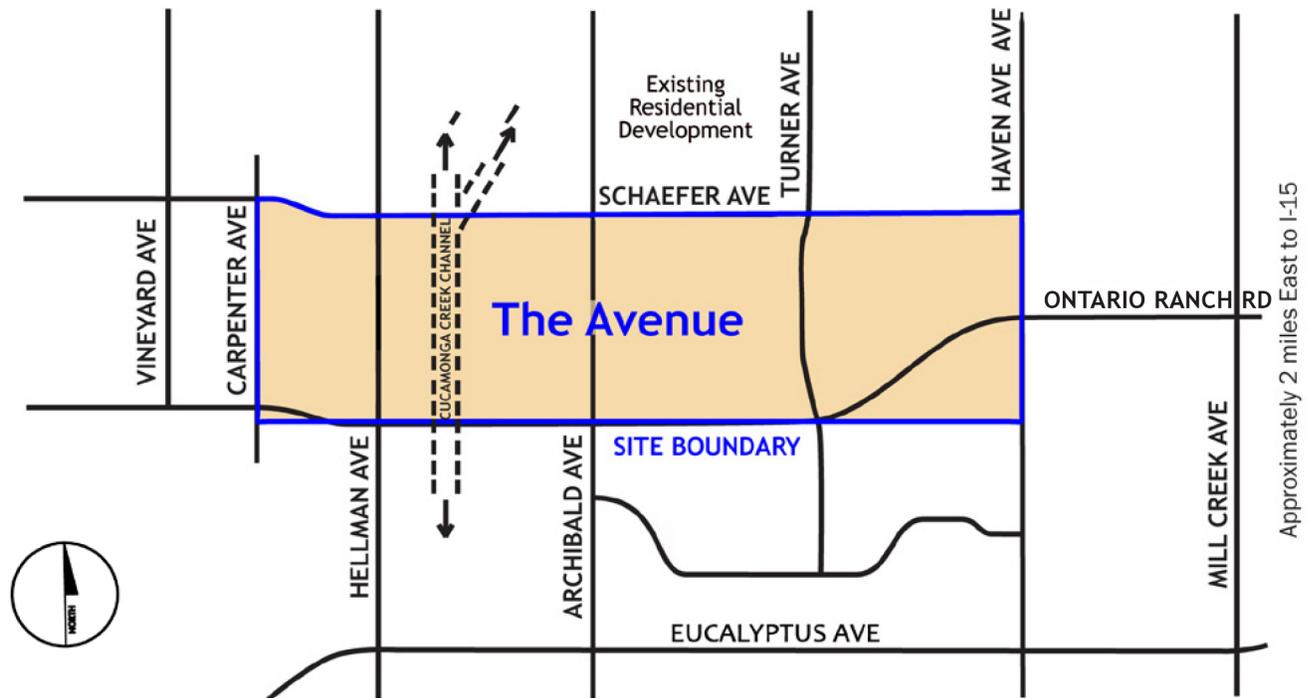
The regional context and local setting of The Avenue (the Project Site) are illustrated in Exhibit 1, "Regional Map", and Exhibit 2, "Project Location Map".

Exhibit 1 - Regional Map



# The Avenue

Exhibit 2 - Project Location Map



### 1.3 Project History and Background

After an extensive public participation program, the Ontario City Council adopted the Sphere of Influence General Plan Amendment for the 8,069 acre area known as the “The New Model Colony” on January 7, 1998. On November 30, 1999, the area was annexed into the City of Ontario. The New Model Colony General Plan provides the framework to create a high quality environment where residents will live, work and play. The area was divided into 30 subareas, each requiring the preparation of a specific plan reflecting the unique goals and qualities of each area. In this way, each subarea could be part of the overall New Model Colony, but retain a sense of individuality. The Avenue site has historically been used for agricultural operations, primarily dairy use. Those areas not in active agricultural production are occupied by rural residential housing, farm buildings, and other ancillary agricultural facilities. The natural vegetation and soil conditions that once occurred throughout La Avenida Drive have been significantly altered through agricultural uses, leaving little or no native vegetation.

On January 26th, 2010, the City of Ontario adopted The Ontario Plan (TOP) which serves as the City’s new business plan and includes a long term Vision and a principle based Policy Plan (General Plan). The city’s Policy Plan, which acts as the City’s General Plan, designates (Policy Plan Exhibit LU-1 -Land Use Plan) the project site for development of Low Density Residential (2.1 -5.0 dwelling units per acre), Low Medium Residential (5.1 - 11 .0 dwelling units per acre), Medium Density Residential (11.1 - 25.0 dwelling units per acre), Neighborhood Commercial, Public Schools, and Open Space- Parkland.

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SPECIFIC PLAN

The New Model Colony

ONTARIO, CALIFORNIA

The Avenue Specific Plan was amended on February 2, 2010, June 17, 2104, and March 21, 2017 to modify the Land Use Plan. The city of Ontario General Plan (The Ontario Plan – TOP) and The Avenue Specific Plan was amended concurrently and reflects the final language to convert a 10 acre portion of the 20 acre school site located west of Turner Road; identified as PA 6B to Low Medium Density Residential (5.1 – 11.0 dwelling units per acre).

## 1.4 Specific Plan Purpose

The Avenue Specific Plan serves to implement the City's Policy Plan for the project site and provides zoning regulations for development of the project site by establishing permitted land use, development standards, infrastructure requirements, and implementation requirements for development. A comprehensive set of design guidelines and development regulations are included to guide and regulate site planning, landscape and architectural character within the community ensuring that excellence in community design is achieved during project development. The Avenue Specific Plan establishes the procedures and requirements to approve new development within the project site.



## 1.5 Specific Plan Objectives

The following objectives are established for The Avenue Specific Plan:

### Residential Areas

1. Development of a variety of housing types into the land use plan addressing a wide variety of lifestyles and economic segments
2. Provide for both single family attached and detached housing in low density and low medium density residential districts
3. Plan for seamless transitions between housing product types in order to create cohesive neighborhoods that include a range of types and styles
4. Plan residential neighborhoods around a series of parks and open space areas, promoting outdoor activity and interaction among neighbors
5. Provide for connectivity between residential neighborhoods, recreational areas, and adjacent commercial land uses, as well as to the schools, by means of pedestrian and bicycle trail linkages along the Avenue and trails incorporated into both the Southern California Edison Easement and the Cucamonga Creek Channel

The **Avenue**

6. Create a strong functional relationship between homes and schools
7. Create an hierarchy of parks, providing for active and passive recreation
8. Create residential neighborhoods with diverse architectural styles and traditional design elements reflecting some of the characteristics of older established Ontario neighborhoods

### Commercial Area

1. Develop retail and commercial uses to meet the needs of the residential community and larger surrounding market area, as well as implement Policy Plan Policies
2. Provide trails and sidewalks to connect the residential community with the retail and commercial area
3. Consider the development of plazas and other public space amenities within the retail and commercial area providing space for social interaction
4. Orient retail and commercial buildings to the street, wherever possible, to create an urban edge and sense of arrival



## 1.6 Development Approval Components

The components of the development approval process for The Avenue are discussed below.

### General Plan

The Ontario Plan establishes the direction and vision for the City of Ontario providing a single guidance system that will shape the Ontario community for the future. The Plan provides for policies to accommodate change over a 30 year period commencing in 2010, the beginning of the planning period. The Ontario Plan consists of a six part Component Framework: 1) Vision, 2) Governance Manual, 3) Policy Plan, 4) City Council Priorities, 5) Implementation, and 6) Tracking and Feedback.

### Specific Plan

The Policy Plan requires the approval of a Specific Plans for development of the project site to ensure that sufficient land area is included to achieve unified districts and neighborhoods. The City of Ontario has zoned the project site as AG-Specific Plan. The zoning designation of AG-Specific Plan requires that a Specific Plan be approved to guide development of the project site and to implement the goals and policies of the Policy Plan. The Specific Plan provides the zoning regulations to govern development of the project

site. The requirements of the Specific Plan shall take precedence over the City of Ontario Development Code. In instances where the Specific Plan is silent, the City of Ontario Development Code shall prevail.

### **Subdivision Maps**

Tentative tract maps will be approved by the City of Ontario for the residential portion of The Avenue indicating the approximate location of lot lines, streets, and proposed grading. Following approval by the City of the tentative tract maps, final maps will be prepared. The final maps become legal documents that are recorded and define legal parcels and lots that can be sold for development. Parcel maps may be approved by the City of Ontario for development proposed within the commercial and business park/light industrial portions of The Avenue.

### **Development Plan Review**

Following the approval of The Avenue Specific Plan, all development proposals for individual neighborhoods or product areas within the Specific Plan will be subject to the Development Plan Review process pursuant to Article 8 of the City's Development Code.

### **Development Agreement(s) Required**

Unless done in a coordinated manner and with adequate fiscal planning, development projects within the New Model Colony are likely to present a challenge in their implementation because of the lack of existing public facilities including streets, sewerage, transportation, drinking water, school, and utility facilities. California law has established a mechanism for ensuring the adequate provision of such facilities, while at the same time providing assurances to applicants that, upon approval of the project, the applicants can proceed with their projects. Approval of this Specific Plan without development agreements may result in a waste of resources, escalate the cost of housing to the consumer, and discourage investment in and commitment to comprehensive planning, as envisioned by the City, which seeks to make maximum efficient utilization of resources at the least economic cost to the public.

Therefore, a single statutory development agreement, or multiple agreements, authorized pursuant to California Government Code sections 65864 et seq., shall be required as part of the approval of this Specific Plan. For the above-mentioned reasons, the development agreement(s) for this Specific Plan shall include, among other things, methods for financing acquisition and construction of infrastructure, acquisition and development of adequate levels of parkland, and schools, as well as the provision of adequate housing opportunities for various segments of the community consistent with the regional housing needs assessment. Such development agreements shall have been fully approved before the issuance of building permits for plans within this project.

## **1.7 Specific Plan Authority and Requirements**

State of California Government Code, Title 7, Division 1, Chapter 3, Article 8, Section 65450-57 grants authority to cities to adopt Specific Plans for purposes of implementing the goals and policies of their

General Plans. The Government Code specifies that Specific Plans will be adopted by ordinance, and that the Specific Plan is required to be consistent with the General Plan. The City of Ontario will be adopting this Specific Plan, as with other New Model Colony Specific Plans, by ordinance, thereby establishing the zoning regulations for the development of The Avenue. The standards set by The Avenue Specific Plan shall take precedence over the City of Ontario Development Code, and in instances where The Avenue Specific Plan is silent, the City of Ontario Development Code shall prevail.

California Government Code Section 65451 sets forth the minimum requirements and review procedures for specific plans as follows:

A Specific Plan shall include a text and a diagram or diagrams, which specify all of the following in detail:

1. The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
2. The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
3. Standards and criteria by which improvements will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable, and a program of implementation measures including regulations, programs, public works projects and the financing measures necessary to carry out paragraphs 1 and 2 above. The Specific Plan shall include a statement of the relationship of the Specific Plan to the General Plan.



Typical outdoor eating area - School

## 1.8 Relationship to General Plan, Airport Land Use Compatibility and Zoning

The City's Policy Plan, which acts as the City's General Plan, designates the project site for development as follows:

- Low Density Residential  
(2.1 -5.0 dwelling units per acre)
- Low Medium Residential  
(5.1 - 11 .0 dwelling units per acre)
- Medium Density Residential  
(11.1 - 25.0 dwelling units per acre)
- Neighborhood Commercial
- Public Schools
- Open Space- Parkland.
- Open Space - Non-Recreation

The Policy Plan (General Plan) establishes a development capacity of 4,010 dwelling units and a maximum FAR of 0.40 for Neighborhood Commercial for a total of up to 172,240 square feet of commercial uses for the Specific Plan Area. The Policy Plan residential capacity is further broken down as follows:

| Gross Acres  | TOP Land Use Designation       | Maximum Permitted Dwelling Units |
|--------------|--------------------------------|----------------------------------|
| 391.72       | Low Density Residential        | 1,763                            |
| 29.9         | Low Medium Density Residential | 254                              |
| 94.97        | Medium Density Residential     | 2,078                            |
| Total 516.59 |                                | 4,095*                           |

*\*Densities do not reflect maximum allowable densities by the Policy Plan and are based on TOP EIR densities (average) of 4.5 du/ac for Low Density, 8.5 du/ac for Low Medium Density and 22 du/ac for Medium Density.*

California Government Code (Title 7, Division 1, Chapter 3, Article 8, Section 65450-65457) permits the adoption and administration of specific plans as an implementation tool for elements contained in the local general plan. Specific plans must demonstrate consistency in regulations, guidelines, and programs with the goals and policies set forth in the general plan. The Avenue Specific Plan has been prepared in conformance with the goals and policies of the City of Ontario Policy Plan (General Plan). The policy analysis is located in the Appendix of this document, describes the manner in which the Avenue Specific Plan complies with the Policy Plan policies applicable to the project.

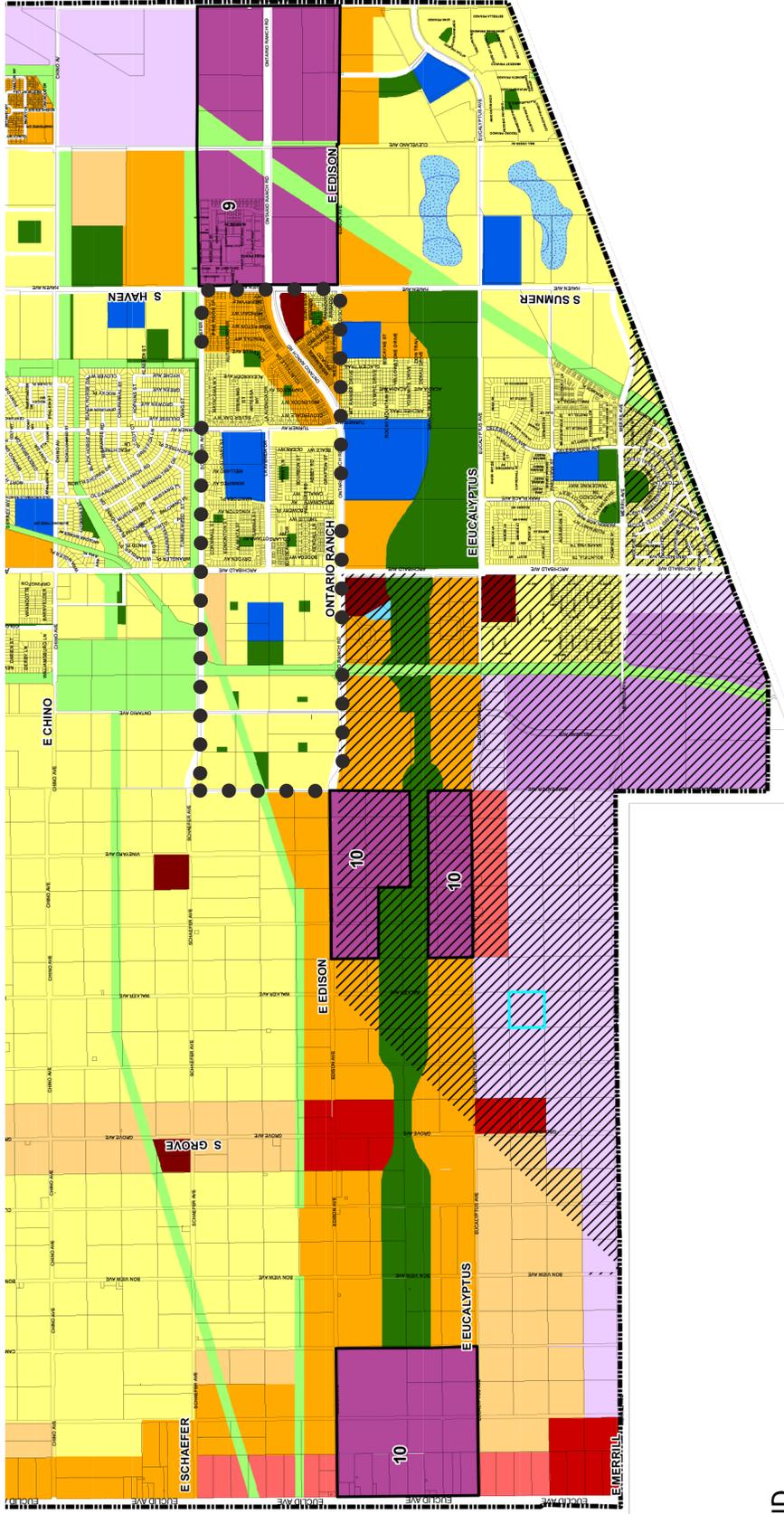
The Avenue Specific Plan is located within the Airport Influence Area (AIA) of Chino Airport and Ontario International Airport and is required to be consistent with both Airport Land Use Compatibility Plans.

## 1.9 CEQA Compliance

An Environmental Impact Report (EIR) was prepared by the City of Ontario for The Avenue Specific Plan in accordance with the California Environmental Quality Act (CEQA), to address impacts associated with the Specific Plan. The EIR was prepared as a basis for the environmental review for all subsequent discretionary and ministerial actions. On December 19, 2006, the City Council certified the EIR for The Avenue Specific Plan.



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SPECIFIC PLAN



**LEGEND**

- Neighborhood Commercial
- General Commercial
- Office Commercial
- Hospitality
- Open Space-Non-Recreation
- Open Space-Parkland
- Open Space-Water
- Public Facility
- Public School
- Railroad
- Landfill
- Business Park
- Industrial
- Mixed Use
- Rural Density Residential
- Low Density Residential
- Low-Medium Density Residential
- Medium Density Residential
- High Density Residential
- The Avenue
- Boundary

Note: Exhibit reflects the adjustments to the boundaries of Subareas 12, 17, 18 and 23.

Policy Plan Land Use Map

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