

**CHAPTER 2.0
PLAN SUMMARY**

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This Specific Plan establishes standards and procedures for development on a 5.4 acre site in southeastern Ontario. To accomplish this, the plan contains the following chapters: Land Use and Development Criteria (3.0), Design Standards and Guidelines (4.0), and Infrastructure/Utilities/Public Services (5.0).

Within Chapter 3.0, land uses are categorized into permitted, conditional, and prohibited categories. Standards regulating site configuration and physical development are also established within this section.

Chapter 4.0, Design Standards and Guidelines, establishes the criteria to be utilized in reviewing applications for their architectural and landscape character. Also included within Chapter 4.0 are the design criteria for signing and lighting, which contribute to the aesthetic coherence of the project.

Infrastructure/Utilities/Public Services (Chapter 5.0) describes the existing and proposed infrastructure, utilities, and public services necessary for development of the project.

Chapter 6.0 (Environmental Review) briefly describes the environmental context and related issues associated with the site and proposed project.

With City approval, this document will enable the implementation of the goals, objectives, and policies of the City of Ontario's General Plan. All items not discussed within this Specific Plan are subject to the provisions of the City of Ontario's Municipal Code.

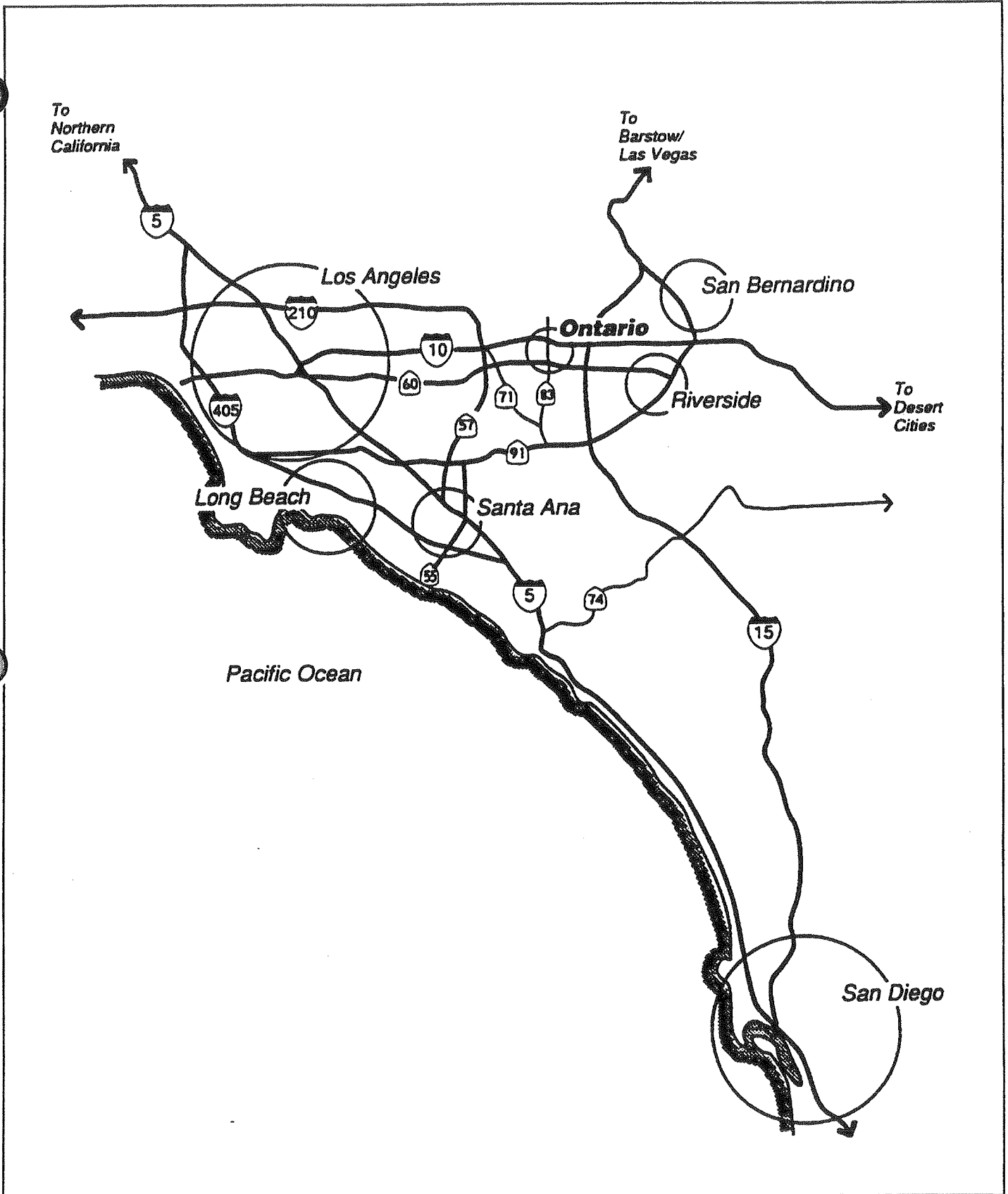
2.1 PROJECT LOCATION

The project is located within the City of Ontario, California, which is situated approximately 50 miles east of Los Angeles and 80 miles north of San Diego (Figure 1). The site is in the southeast portion of Ontario. Relative to the airport, the site is south of the easternmost portion of the airport. This eastern portion of the airport is where the new passenger terminal is nearing completion, in addition to an extended section of runway for takeoffs.

The site enjoys direct access to the I-10 Freeway (north), SR-60 Freeway (south) via Haven Avenue, and I-15 Freeway to the east via Jurupa Street (Figure 2). In addition, the site is located at the southwestern corner of the Haven-Jurupa intersection, which is anticipated to be a highly visible node for the City and surrounding area (Figure 3).

2.2 SITE DESCRIPTION

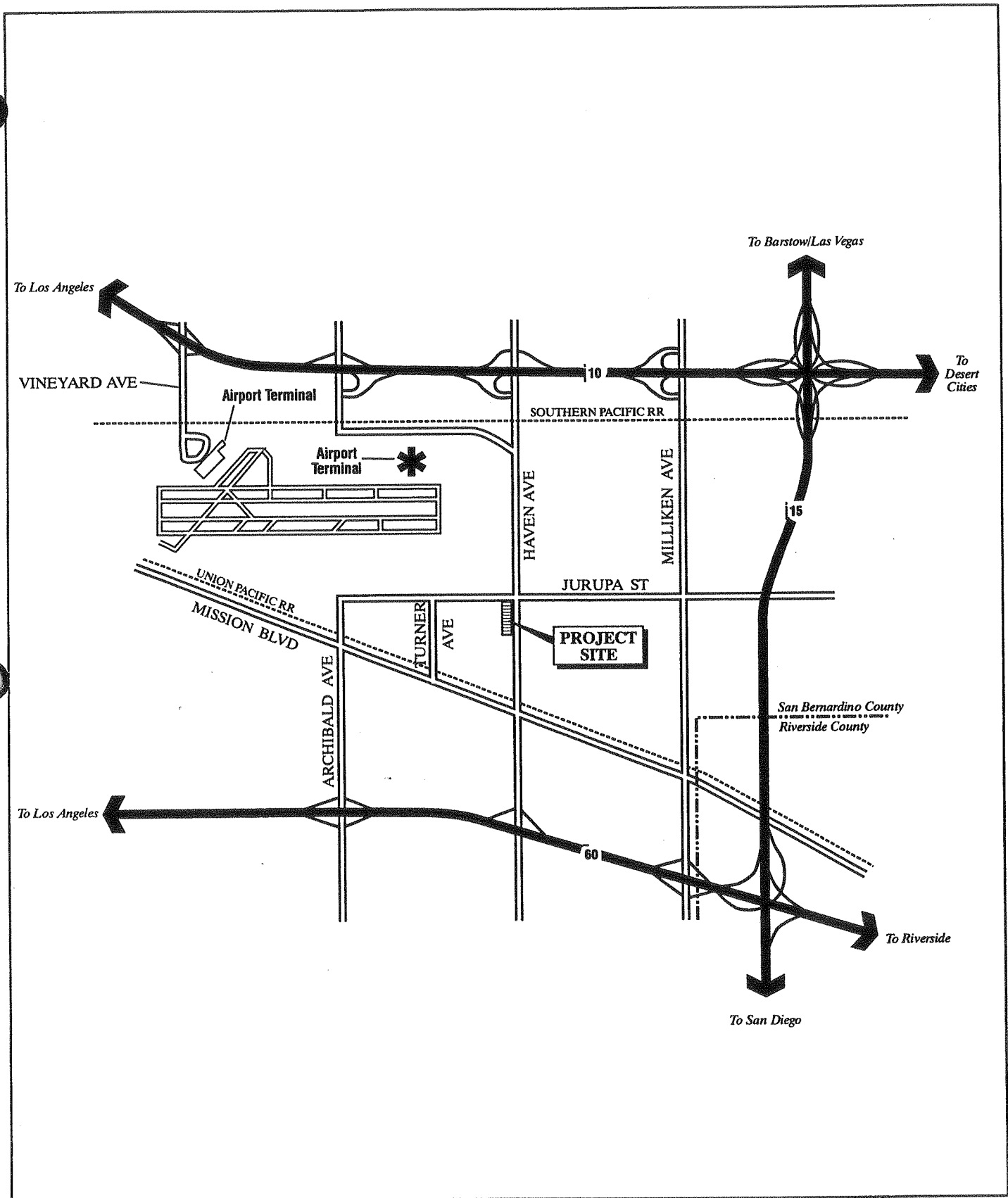
The project is situated on a 5.4 acre site, one-quarter mile south of the ONT. Originally, the site was 7.5 acres; however, dedications of land to the City were required for the ultimate public right-of-ways for Haven Avenue and Jurupa Street. The resulting property is relatively long and narrow, with dimensions of approximately 266 feet by



6/23/98(CDO830)

Figure 1

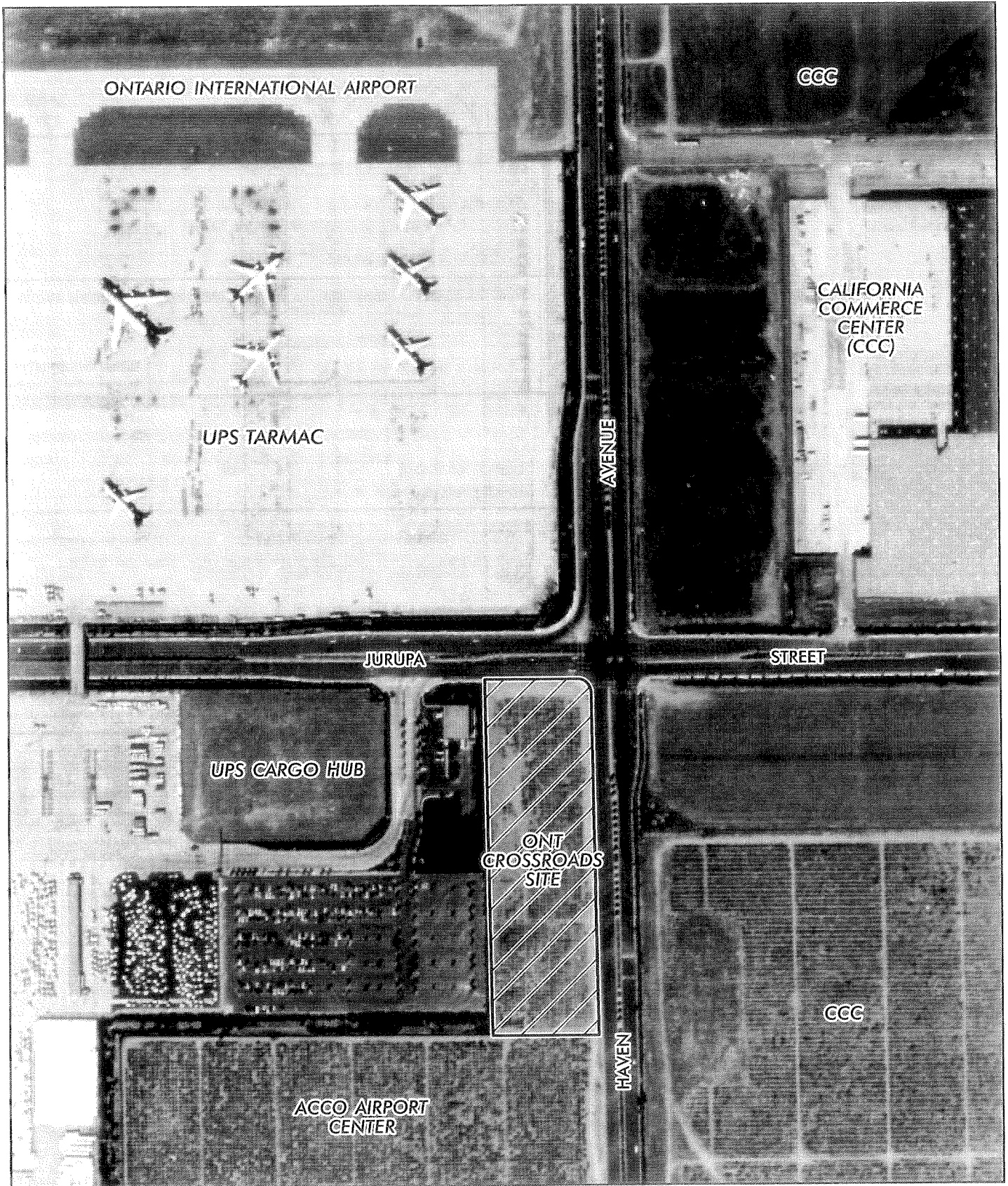




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Figure 2





11/18/98(CD0830)

Figure 3



LSA

929 feet (Figure 4). The site slopes gently south, at approximately 1.5 percent to 2 percent.

The intersection of Jurupa and Haven is an integral part of the southern gateway to the Ontario International Airport. Haven Avenue proceeds southerly, connecting with SR-60. Jurupa Street has an interchange with I-15, from which the street continues due west to the site. These two routes provide direct access to the ONT's terminal.

The City has designated this property as Planned Commercial to encourage development in accordance with the area's commercial capability.

The City General Plan defines the Planned Commercial designation as retail, services, and office commercial uses developed under specific plans. Mixed use projects, which could include light industrial and/or residential uses are encouraged in this category in order to promote jobs/housing balance. The maximum permitted Planned Commercial FAR shall be 1.00.

Surrounding land uses to the site are as follows:

- *North:* The site is bordered to the north by Jurupa Street. On the north side of Jurupa Street is the United Parcel Service (UPS) aircraft tarmac.
- *East:* Haven Avenue borders the eastern edge of the site. On the opposite side of Haven is the California Commercial Center (CCC) Specific Plan area. This development area is a large industrial park (approximately 1,500 acres) with ancillary uses. The area within CCC that is adjacent to the southeast intersection of Jurupa and Haven is zoned for commercial uses such as food, retail, and hotel. For a detailed listing of permitting uses for this adjacent area, reference can be made to the CCC Specific Plan document on file at the City of Ontario Planning Department.
- *South:* South of the site is the Accomazzo property. The Acco Airport Center Specific Plan provides regulations and guidelines for industrial and business park uses for the Accomazzo property. The Acco Specific Plan designates the property that is directly adjacent (to the south) to the JURUPA HAVEN AIRPORT CENTRE site as Business Park. This area provides for "high-end" commercial uses - corporate offices, support commercial, restaurants, and lodging. Within the Business Park designation, a wide range of commercial and service oriented uses are permitted, either by right or conditionally.
- *West:* The UPS facility is directly to the west. This portion of the UPS facility is a customer drop-off building for packages, and a continuous use employee parking area. The parking area accommodates approximately 400 vehicles.

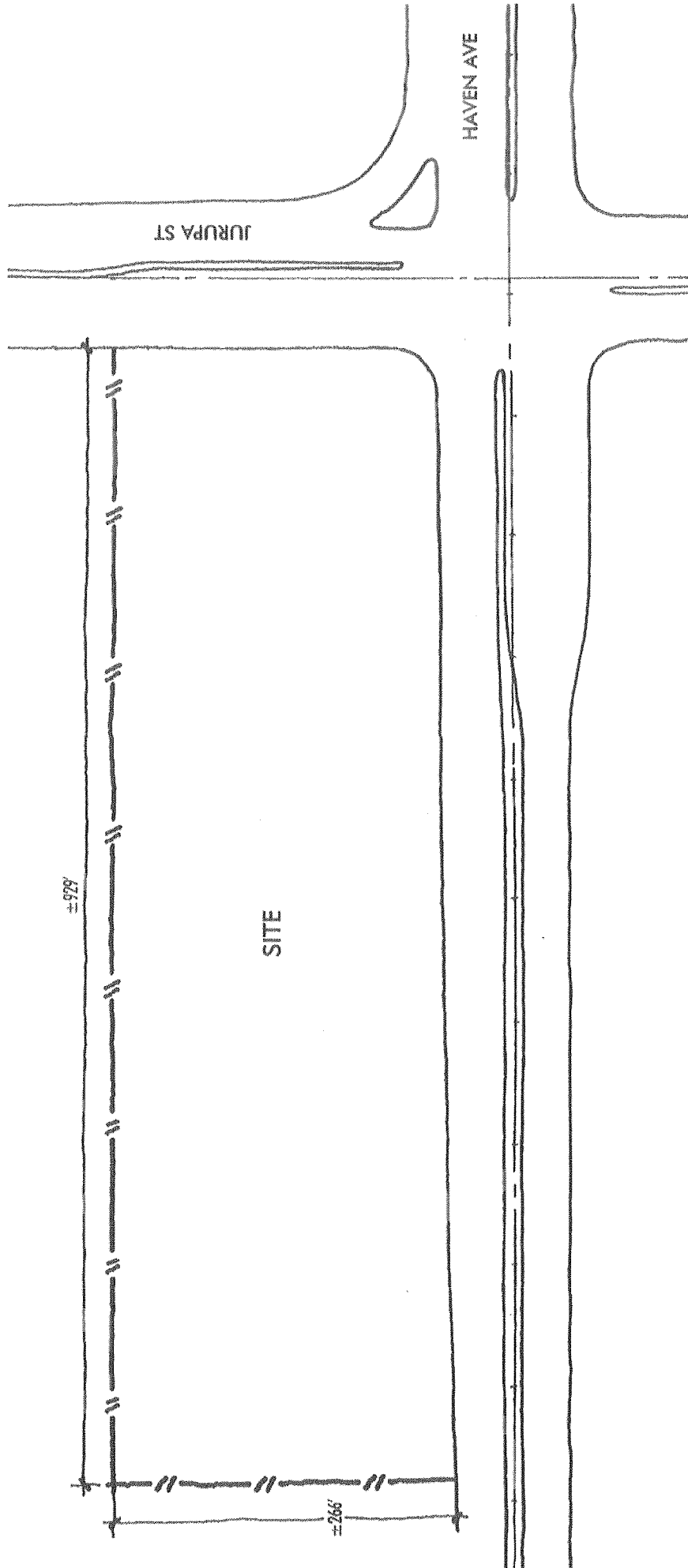


Figure 4

2.3 *DEVELOPMENT CONCEPT/VISUAL IMAGE*

The development concept of the proposed project capitalizes on the strategic location of the intersection of Jurupa and Haven. This area is one of the most visible locations in the City due to its proximity and relationship to the airport.

The land use objective of this plan is to permit projects that will commercially support the growth of the airport and the surrounding area, and be capable of evolving in scale complementary to the changing environment. Therefore, land uses that support the airport and the surrounding land uses both in the present and future have been chosen as permitted and conditional uses within the Land Use and Development Criteria Chapter (3.0) of this Specific Plan. In addition, it is the purpose of this site to blend with the uses and scale of the business park area of the Acco Specific Plan, located directly to the south.

The structural and design concepts for the built environment not only would be appropriate for the area, but may also increase the visibility of the property. The design elements are discussed in further detail in Chapter 4.0, Design Standards and Guidelines. Design standards will be established for this intersection through the implementation of the Specific Plan.